

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 110564							1	44

2 MONROE CO. LN.-GOODWIN (CABLE MEDIAN BARRIER) (S)

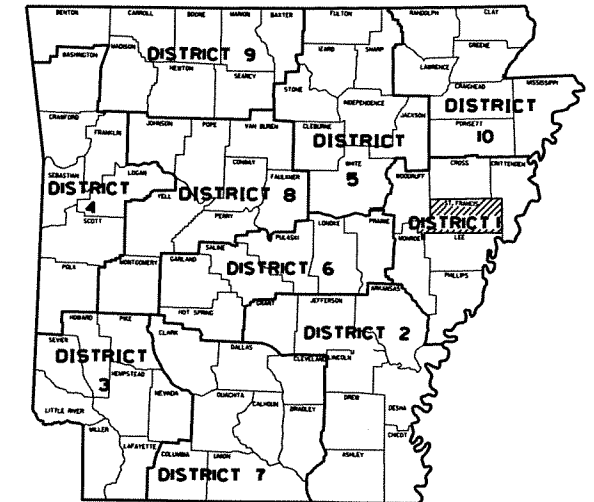
"A FULLY-CONTROLLED ACCESS FACILITY"
 ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT
 CONSTRUCTION PLANS FOR STATE HIGHWAY

MONROE CO. LN.-GOODWIN
 (CABLE MEDIAN BARRIER) (S)

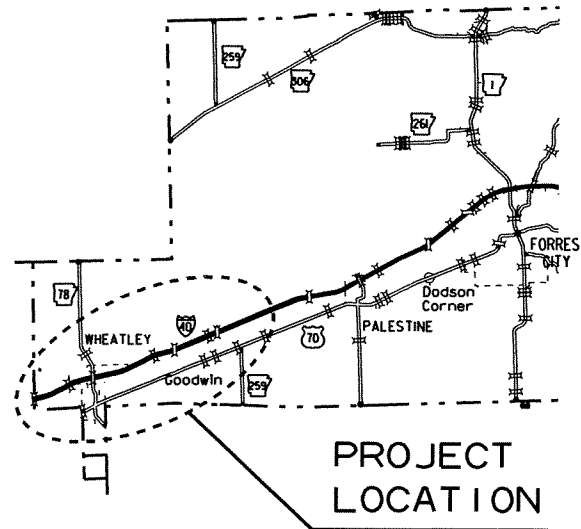
ST. FRANCIS COUNTY
 ROUTE 40 SECTION 51

JOB 110564

FED. AID PROJ. HS IP-0076(79)



ARK. HWY. DIST. NO. 1



VICINITY MAP

EQUATIONS

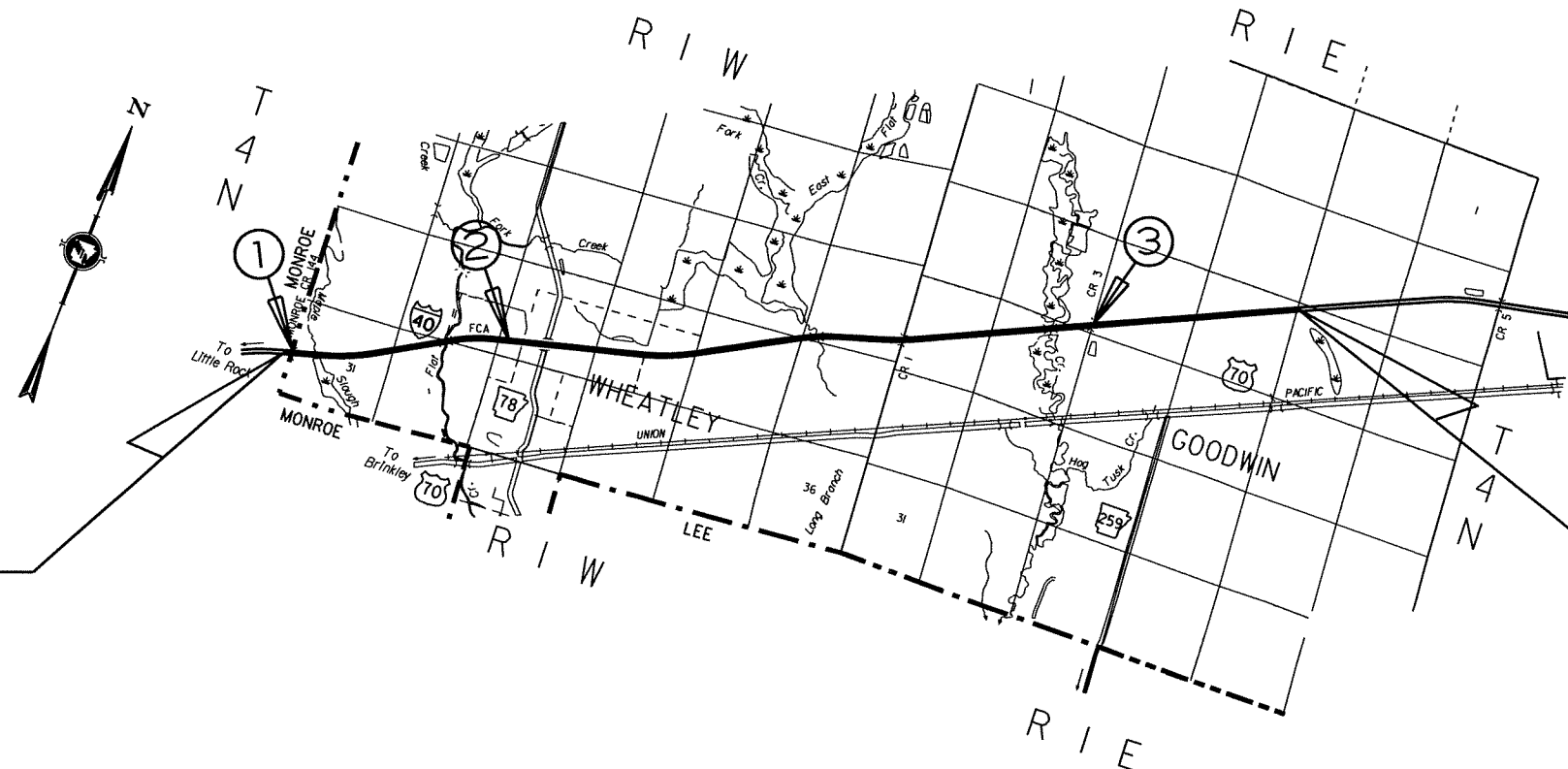
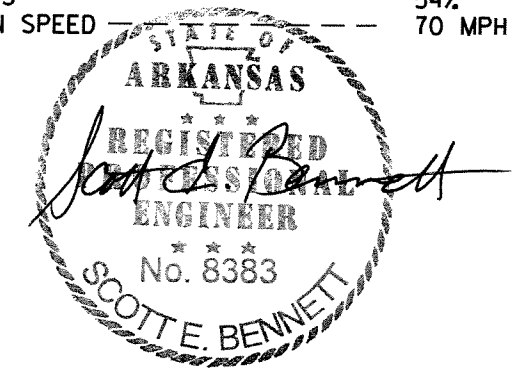
- ① EQUATION
 STA. 2896+79.1 BK. =
 STA. 2899+85.0 AHD.
- ② EQUATION
 STA. 3019+15.1 BK. =
 STA. 3019+07.5 AHD.
- ③ EQUATION
 STA. 3339+36.8 BK. =
 STA. 3340+36.8 AHD.

EXCEPTIONS

- STA. 2983+75 - STA. 2985+10 [135']
- STA. 3316+00 - STA. 3317+40 [140']
- STA. 3392+75 - STA. 3393+25 [50']
- TOTAL LENGTH OF EXCEPTIONS = 325'

• DESIGN TRAFFIC DATA •

DESIGN YEAR	-----	2032
2012 ADT	-----	31,500
2032 ADT	-----	41,000
2032 DHV	-----	4510
DIRECTIONAL DISTRIBUTION	-----	60%
TRUCKS	-----	54%
DESIGN SPEED	-----	70 MPH



STA. 2892+95.00
 BEGIN JOB 110564
 LOG MILE 218.30

STA. 3419+00.00
 END JOB 110564
 LOG MILE 228.20

BEGINNING OF PROJECT	MID-POINT OF PROJECT	END OF PROJECT
LAT. • N 34°55'03.65"	LAT. • N 34°56'21.45"	LAT. • N 34°57'46.07"
LONG. • W 91°09'13.65"	LONG. • W 91°04'06.84"	LONG. • W 90°59'14.96"

GROSS LENGTH OF PROJECT	52206.70	FEET	OR	9.888	MILES
NET " " ROADWAY	5188.70	" "	" "	9.826	" "
NET " " BRIDGES	0.00	" "	" "	0.000	" "
NET " " PROJECT	5188.70	" "	" "	9.826	" "

P.E. #0564
 NON-PART.

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				6	ARK.			
						110564	2	44

2 INDEX, GOV. SPECIFICATIONS, & GEN. NOTES



6/6/12

INDEX OF SHEETS

SHEET NO.	TITLE	DRWG. NO.	DATE
1	TITLE SHEET		
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES		
3	TYPICAL SECTIONS OF IMPROVEMENT		
4-5	SPECIAL DETAILS		
6-15	TEMPORARY EROSION CONTROL DETAILS		
16-18	MAINTENANCE OF TRAFFIC		
19-21	QUANTITY SHEETS		
22	SUMMARY OF QUANTITIES AND REVISIONS		
23-32	PLAN SHEETS		
33	CONCRETE DITCH PAVING	CDP-1	11-17-10
34	GUARD RAIL DETAILS	GR-8	7-14-10
35	GUARD RAIL DETAILS	GR-8A	7-14-10
36	GUARD RAIL DETAILS	GR-9	4-17-08
37	GUARD RAIL DETAILS	GR-9A	4-17-08
38	GUARD RAIL DETAILS	GR-10	7-14-10
39	GUARD RAIL DETAILS	GR-10A	7-14-10
40	GUARD RAIL DETAILS	GRT-1	7-14-10
41	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-1	12-15-11
42	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-2	3-11-10
43	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-3	10-15-09
44	TEMPORARY EROSION CONTROL DEVICES	TEC-1	12-15-11

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2003, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	FHWA-1273 REVISIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-2	MANUAL FOR ASSESSING SAFETY HARDWARE (MASH)
102-1	BIDDING REQUIREMENTS AND CONDITIONS
103-1	DETERMINATION OF DBE PARTICIPATION
105-1	CONSTRUCTION CONTROL MARKINGS
105-2	EQUIPMENT AND MATERIAL STORAGE ON BRIDGE STRUCTURES
105-3	CONTROL OF WORK
107-1	WORKER VISIBILITY
108-1	LIQUIDATED DAMAGES
303-1	AGGREGATE BASE COURSE
404-1	PRODUCTION VERIFICATION OF ASPHALT CONCRETE HOT MIX
409-1	MINERAL AGGREGATES
410-3	DENSITY TESTING FOR ACHM LEVELING COURSES AND BOND BREAKERS
600-1	WATER FOR VEGETATION
603-1	MAINTENANCE OF TRAFFIC
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-2	INSPECTION OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 110564	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB 110564	CONCRETE DITCH PAVING
JOB 110564	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 110564	INTERNET BIDDING
JOB 110564	MAINTENANCE OF TRAFFIC
JOB 110564	PARTNERING REQUIREMENTS
JOB 110564	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIERS
JOB 110564	SEQUENCE OF CONSTRUCTION
JOB 110564	SITE USE (A + C METHOD)
JOB 110564	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 110564	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 110564	UTILITY ADJUSTMENTS
JOB 110564	VALUE ENGINEERING
JOB 110564	WARM MIX ASPHALT
JOB 110564	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS
JOB 110564	WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS
JOB 110564	WRSF TRAINING WORKSHOP

GENERAL NOTES

- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210 - UNCLASSIFIED EXCAVATION
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

06/06/112

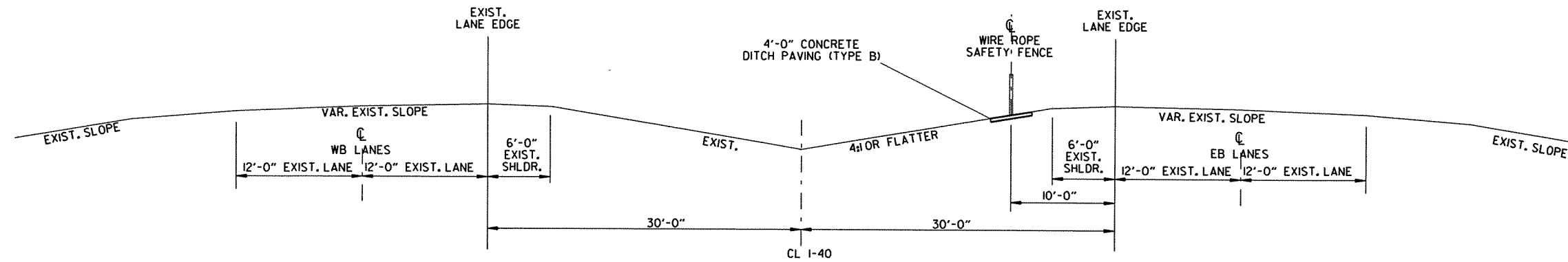
R110564.DGN/INDEX

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		3	44

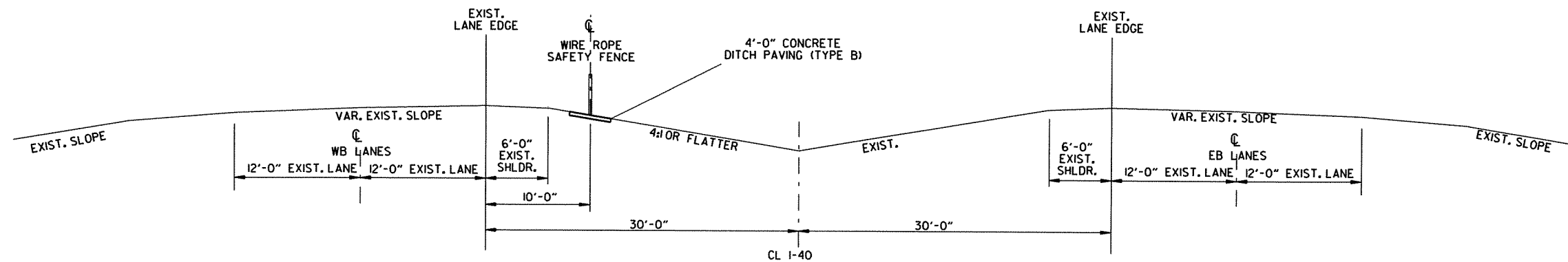
② TYPICAL SECTIONS OF IMPROVEMENT



5/24/12



TYPICAL SECTION OF IMPROVEMENT
WIRE ROPE SAFETY FENCE ON EASTBOUND LANES FORESLOPE

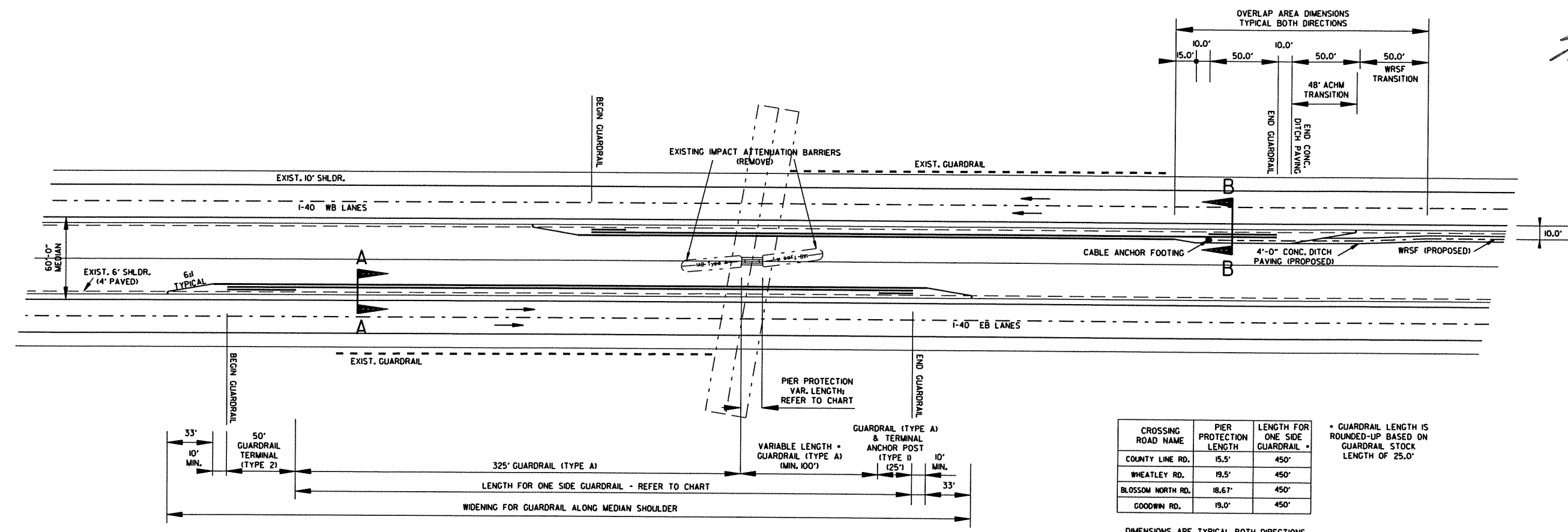
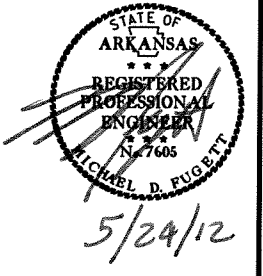


TYPICAL SECTION OF IMPROVEMENT
WIRE ROPE SAFETY FENCE ON WESTBOUND LANES FORESLOPE

TYPICAL SECTIONS OF IMPROVEMENT

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO.	110564
							SHEET NO.	4
							TOTAL SHEETS	44

2 SPECIAL DETAILS



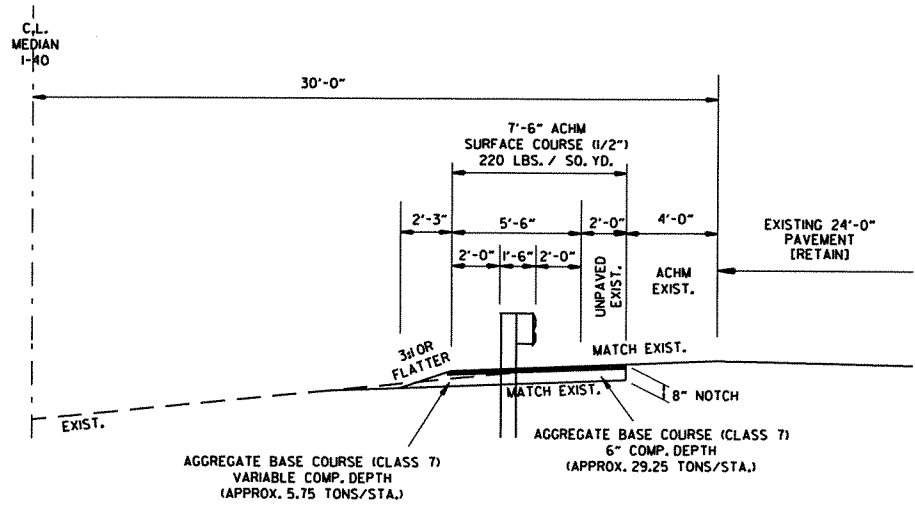
CROSSING ROAD NAME	PIER PROTECTION LENGTH	LENGTH FOR ONE SIDE GUARDRAIL *
COUNTY LINE RD.	15.5'	450'
WHEATLEY RD.	19.5'	450'
BLOSSOM NORTH RD.	18.67'	450'
GOODWIN RD.	19.0'	450'

* GUARDRAIL LENGTH IS ROUNDED-UP BASED ON GUARDRAIL STOCK LENGTH OF 25.0'

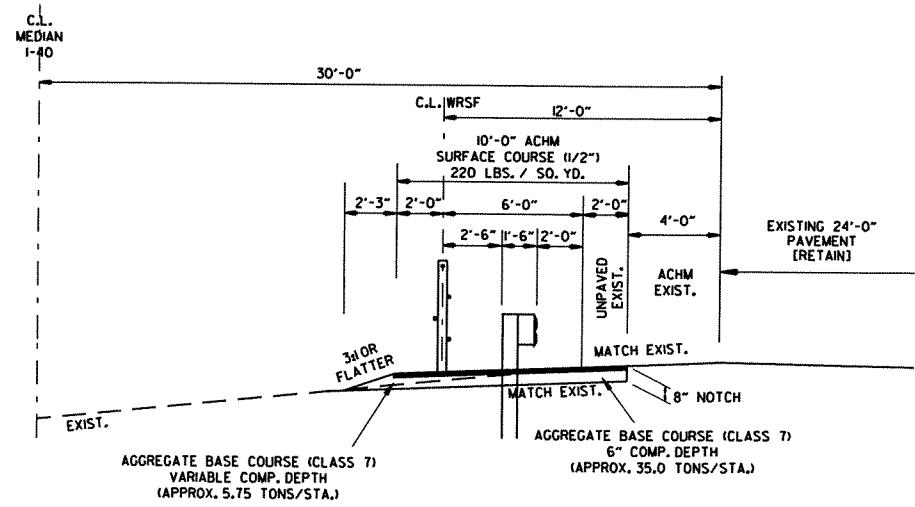
DIMENSIONS ARE TYPICAL BOTH DIRECTIONS

DETAIL AT OVERPASSES

NOTE: REFER TO PLAN SHEETS FOR PLACEMENT OF WIRE ROPE SAFETY FENCE ON EASTBOUND OR WESTBOUND FORESLOPES.



SECTION A-A

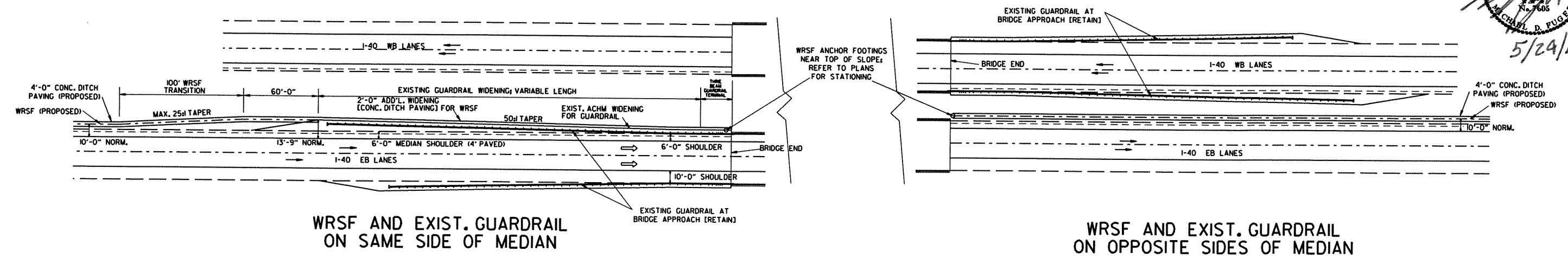


SECTION B-B

SPECIAL DETAILS

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		5	44

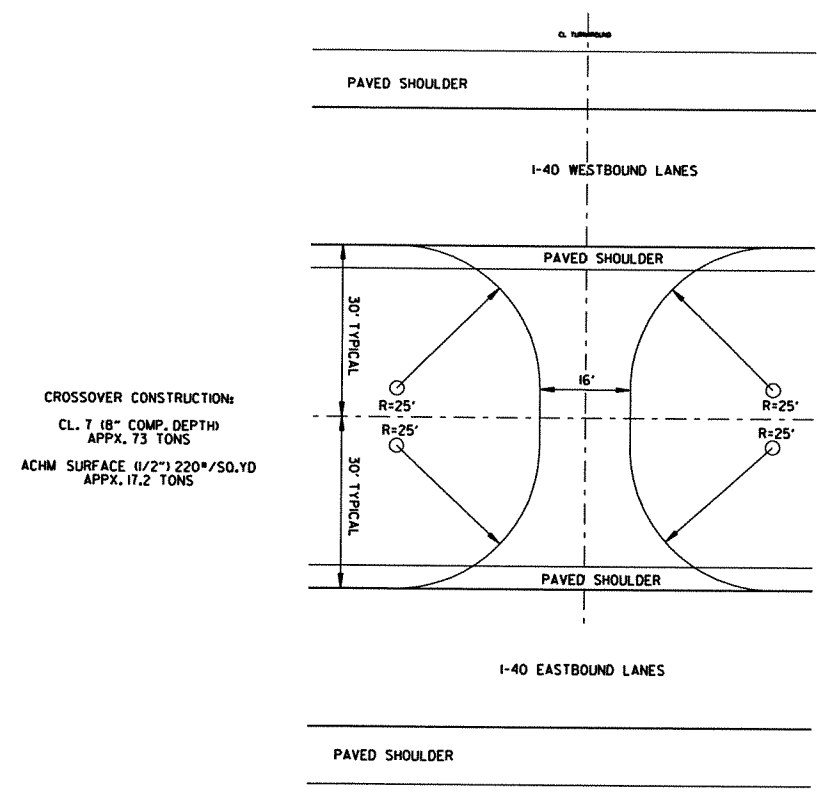
2 SPECIAL DETAILS



WRSF AND EXIST. GUARDRAIL ON SAME SIDE OF MEDIAN

WRSF AND EXIST. GUARDRAIL ON OPPOSITE SIDES OF MEDIAN

DETAIL OF WIRE ROPE SAFETY FENCE AT EXISTING BRIDGE ENDS



DETAIL OF MEDIAN CROSSOVER CONSTRUCTED AT STA. 3209+00

SPECIAL DETAILS

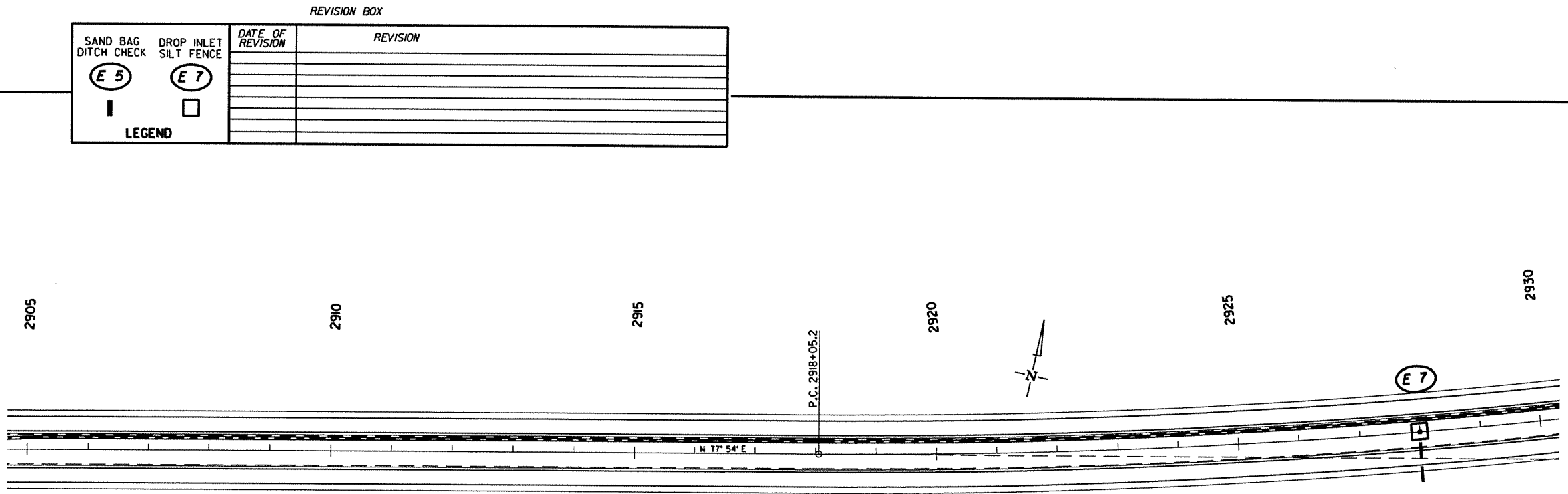
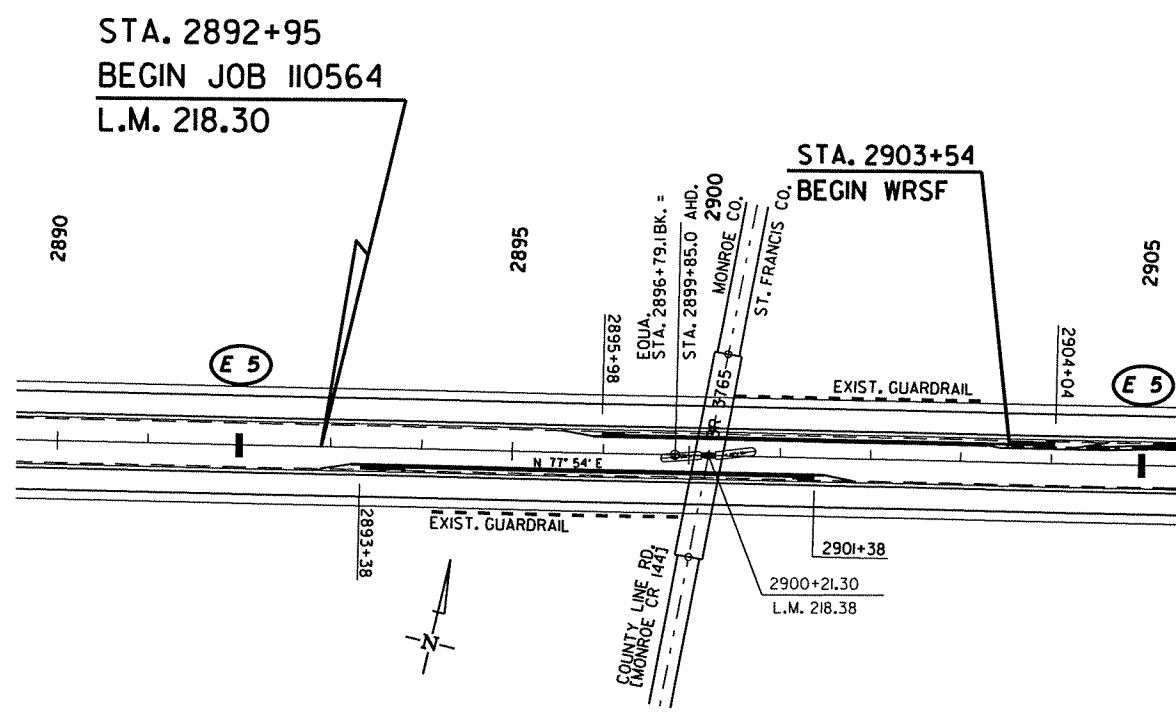
r110564.dgn/SPEC DET 04/23/2012

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				6	ARK.			
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2 TEMPORARY EROSION CONTROL DETAILS



5/24/12

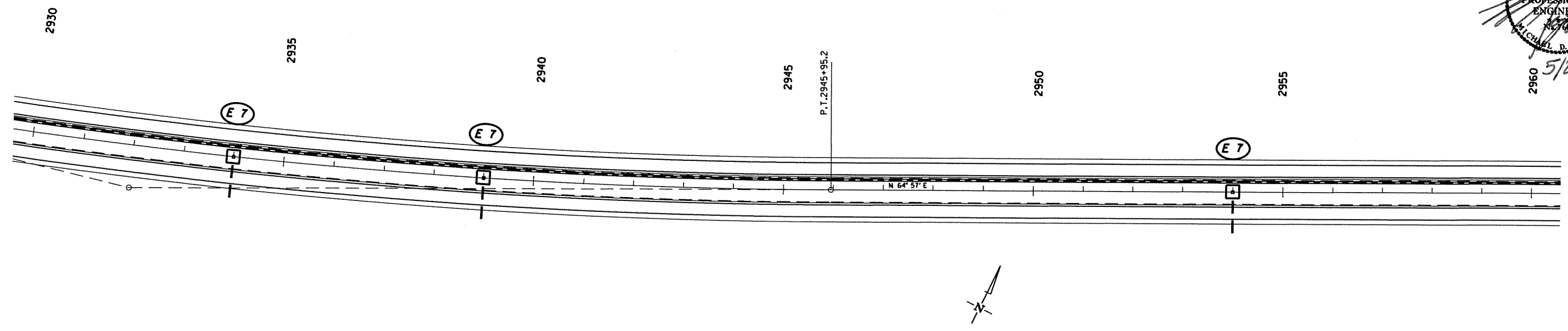
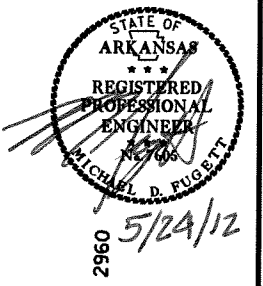


LEGEND		REVISION BOX	
SAND BAG DITCH CHECK	DROP INLET SILT FENCE	DATE OF REVISION	REVISION
(E 5)	(E 7)		
	□		

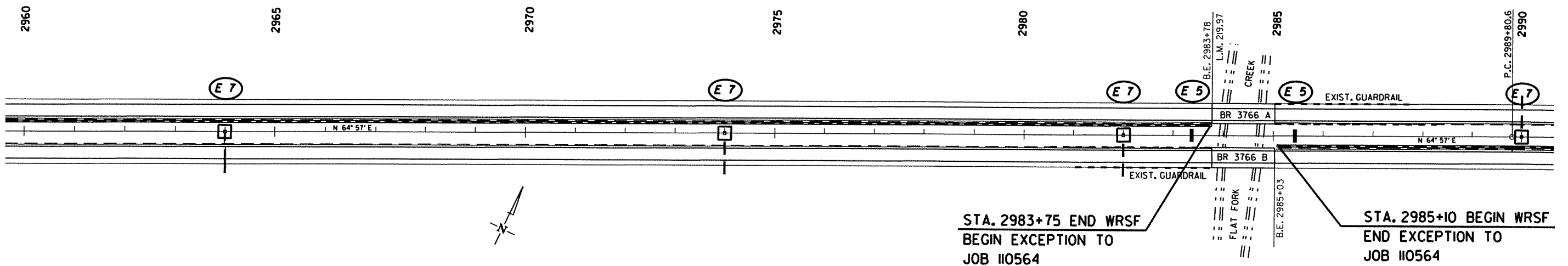
TEMPORARY EROSION CONTROL DETAILS
STA. 2890+00 - STA. 2930+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		7	44
				JOB NO.		110564		

② TEMPORARY EROSION CONTROL DETAILS



REVISION BOX		DATE OF REVISION	REVISION
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DROP INLET SILT FENCE	(E 7)		
LEGEND			



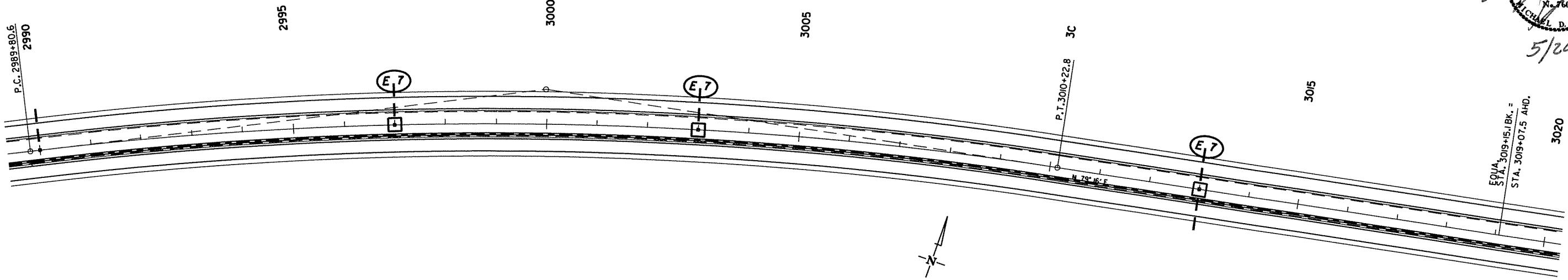
TEMPORARY EROSION CONTROL DETAILS
STA. 2930+00 - STA. 2990+00

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						JOB NO. 110564	8	44

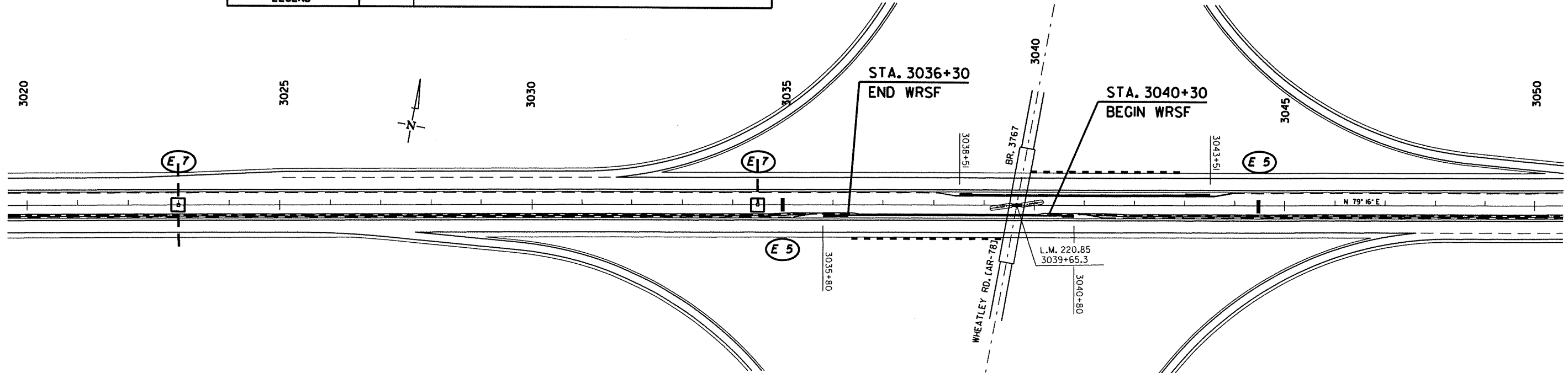
② TEMPORARY EROSION CONTROL DETAILS



5/29/12



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LEGEND			



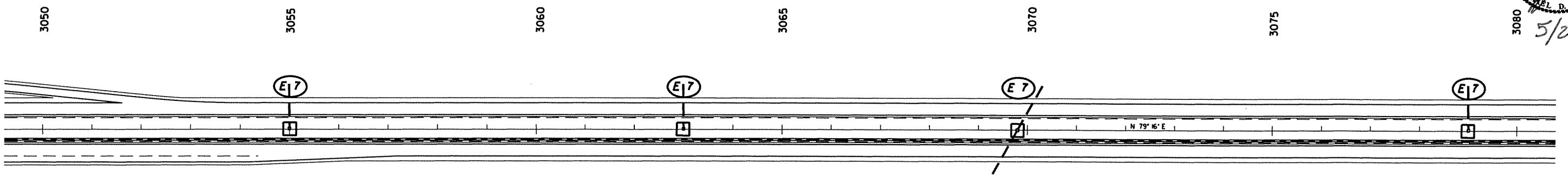
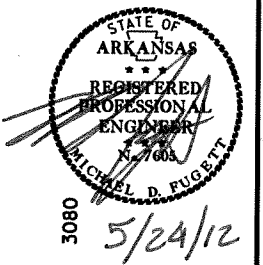
TEMPORARY EROSION CONTROL DETAILS
STA. 2990+00 - STA. 3050+00

05/08/2012

r110564.dgn/EROSION

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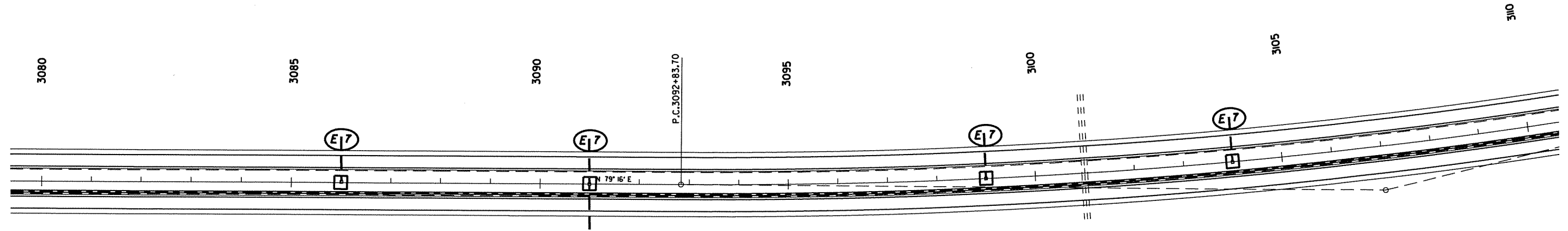
② TEMPORARY EROSION CONTROL DETAILS



REVISION BOX

DATE OF REVISION	REVISION

SAND BAG DITCH CHECK	DROP INLET SILT FENCE
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LEGEND	



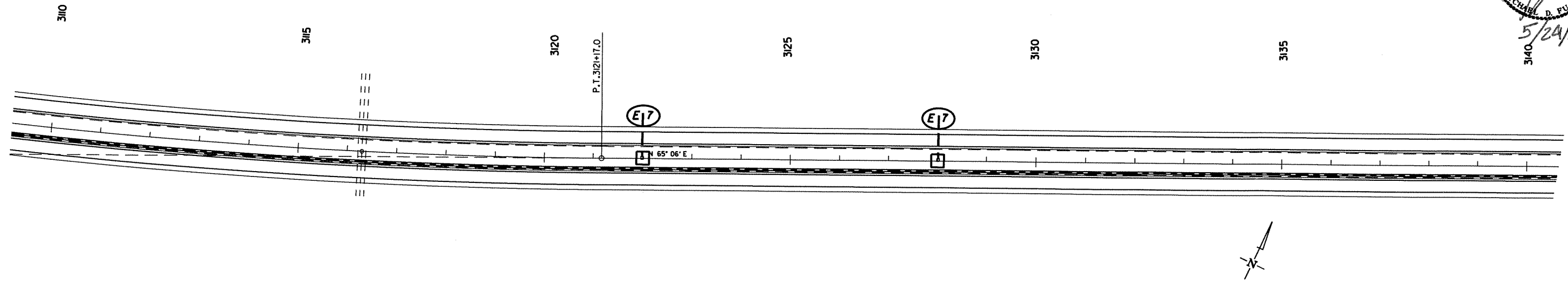
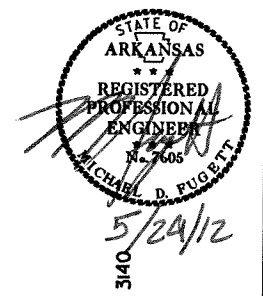
TEMPORARY EROSION CONTROL DETAILS
STA. 3050+00 - STA. 3110+00

05/08/2012

r110564.dgn/EROSION

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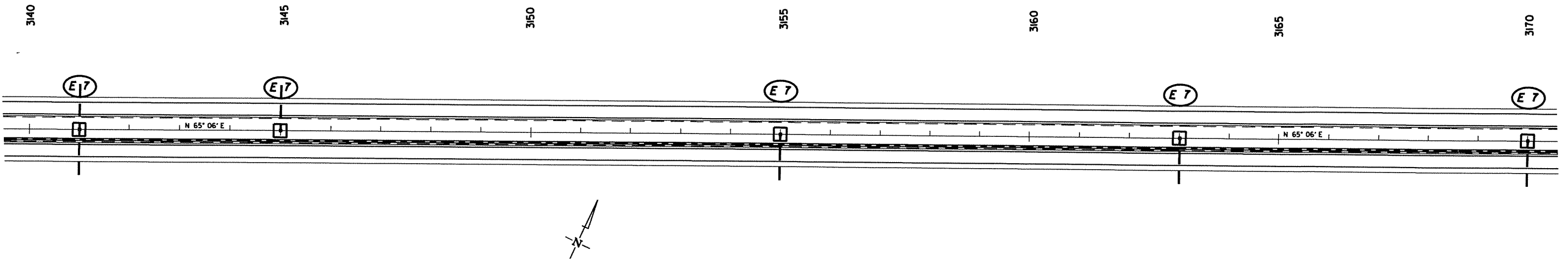
2 TEMPORARY EROSION CONTROL DETAILS



REVISION BOX

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(E 5)	(E 7)		
I	□		

LEGEND



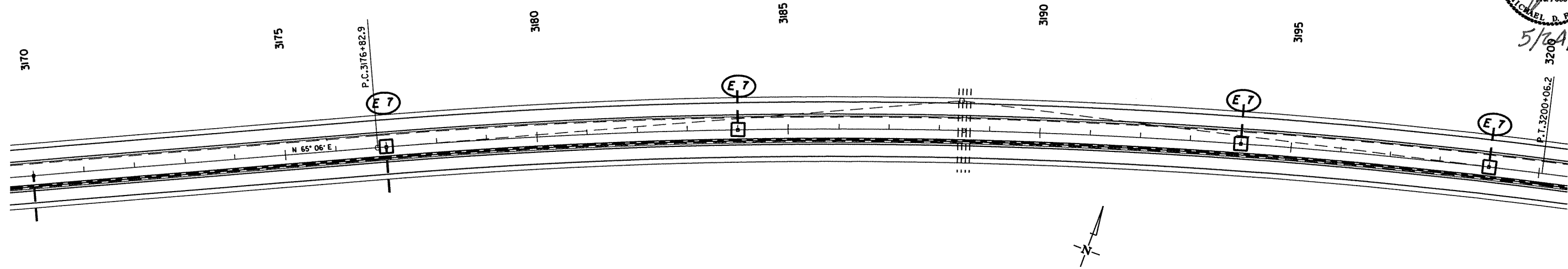
TEMPORARY EROSION CONTROL DETAILS
STA. 3110+00 - STA. 3170+00

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2 TEMPORARY EROSION CONTROL DETAILS

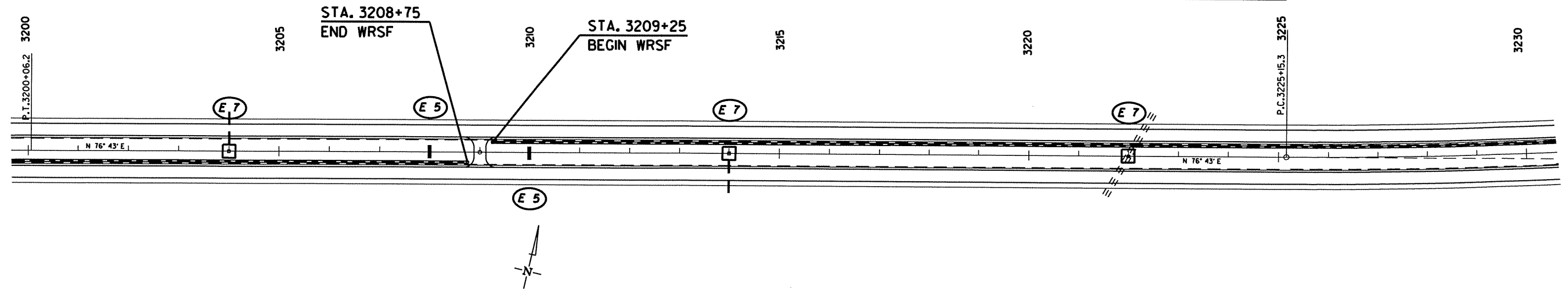


5/20/12



REVISION BOX

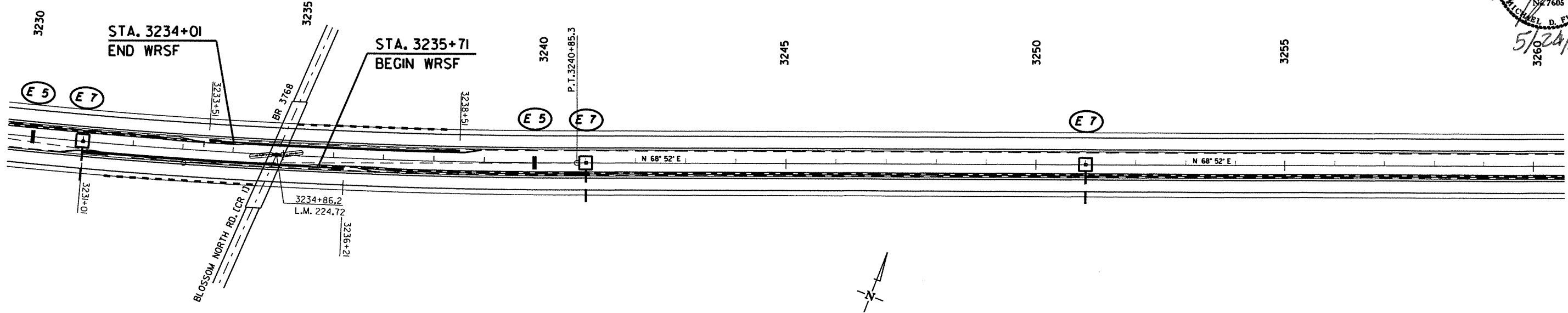
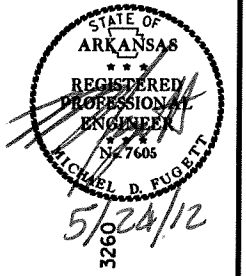
SAND BAG DITCH CHECK	DROP INLET SILT FENCE	DATE OF REVISION	REVISION
(E 5)	(E 7)		
I	□		
LEGEND			



TEMPORARY EROSION CONTROL DETAILS
STA. 3170+00 - STA. 3230+00

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				JOB NO.		110564	12	44


② TEMPORARY EROSION CONTROL DETAILS




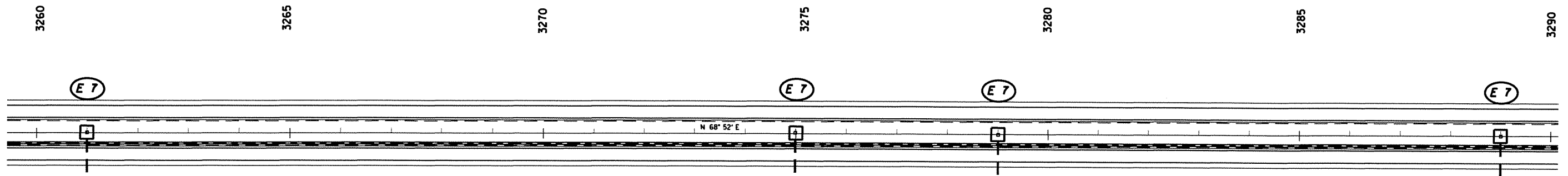
REVISION BOX

DATE OF REVISION	REVISION

LEGEND

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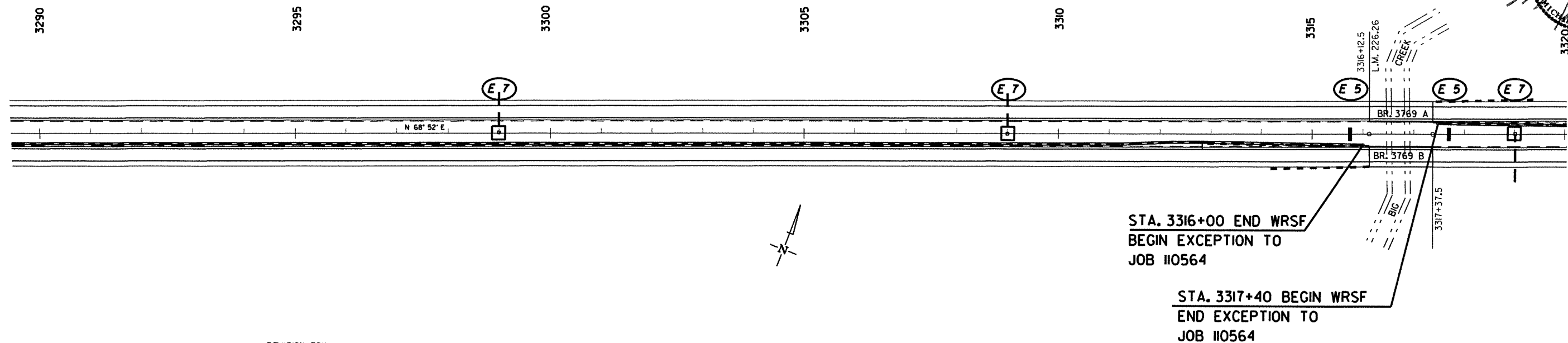
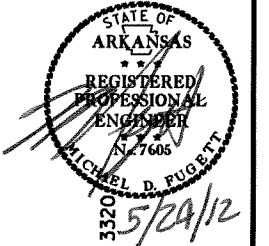
DROP INLET SILT FENCE:  (E 7)



TEMPORARY EROSION CONTROL DETAILS
STA. 3230+00 - STA. 3290+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. AID DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
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2 TEMPORARY EROSION CONTROL DETAILS



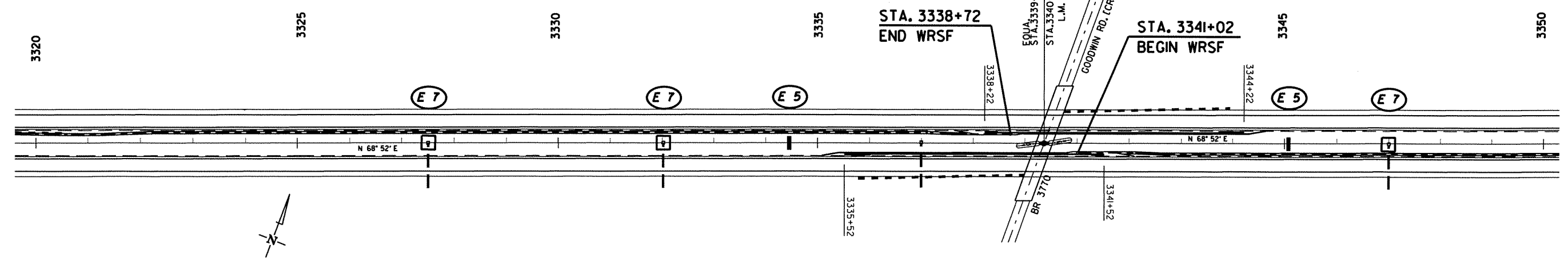
STA. 3316+00 END WRSF
 BEGIN EXCEPTION TO
 JOB 110564

STA. 3317+40 BEGIN WRSF
 END EXCEPTION TO
 JOB 110564

REVISION BOX

DATE OF REVISION	REVISION

SAND BAG DITCH CHECK (E 5) 	DROP INLET SILT FENCE (E 7) □
LEGEND	



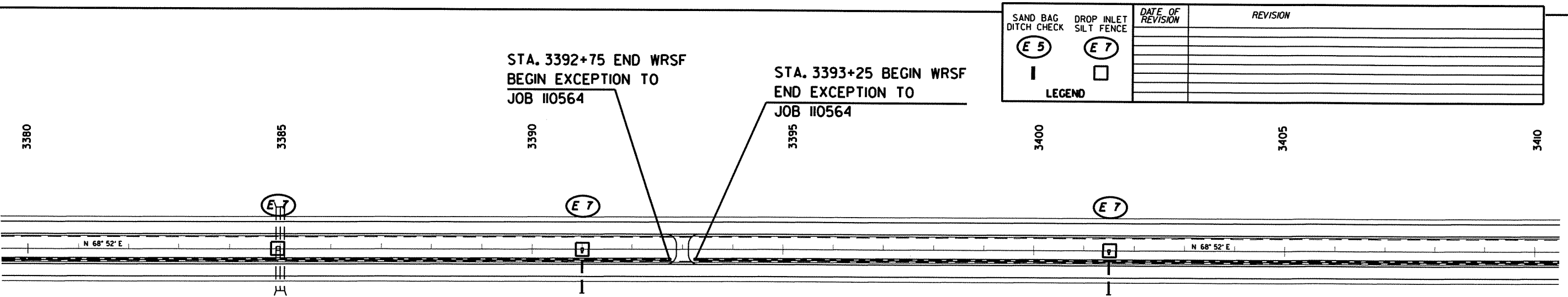
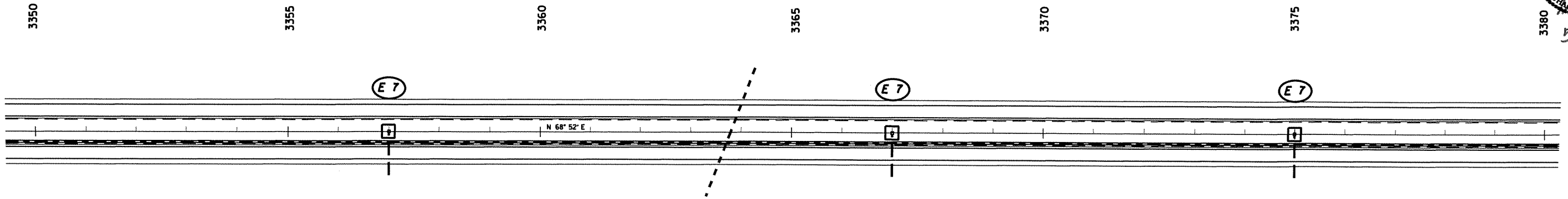
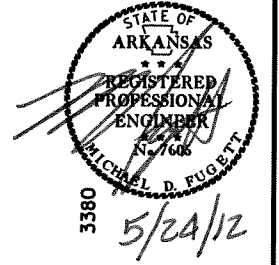
STA. 3338+72
 END WRSF

STA. 3341+02
 BEGIN WRSF

TEMPORARY EROSION CONTROL DETAILS
 STA. 3290+00 - STA. 3350+00

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				JOB NO.		110564	14	44

② TEMPORARY EROSION CONTROL DETAILS

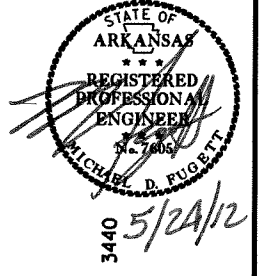


SAND BAG DITCH CHECK (E 5) I	DROP INLET SILT FENCE (E 7) □	DATE OF REVISION	REVISION
LEGEND			

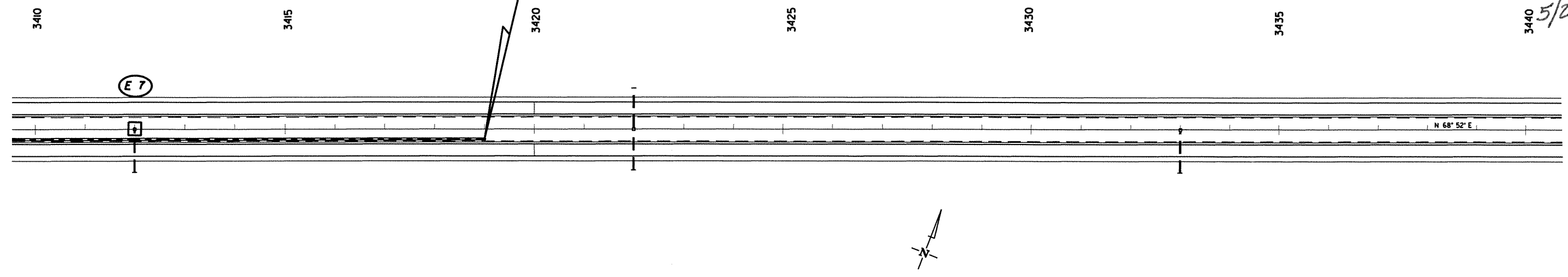
TEMPORARY EROSION CONTROL DETAILS
STA. 3350+00 - STA. 3410+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		15	44
				JOB NO.		110564		

2 TEMPORARY EROSION CONTROL DETAILS



STA. 3419+00
END JOB 110564
L.M. 228.20



REVISION BOX

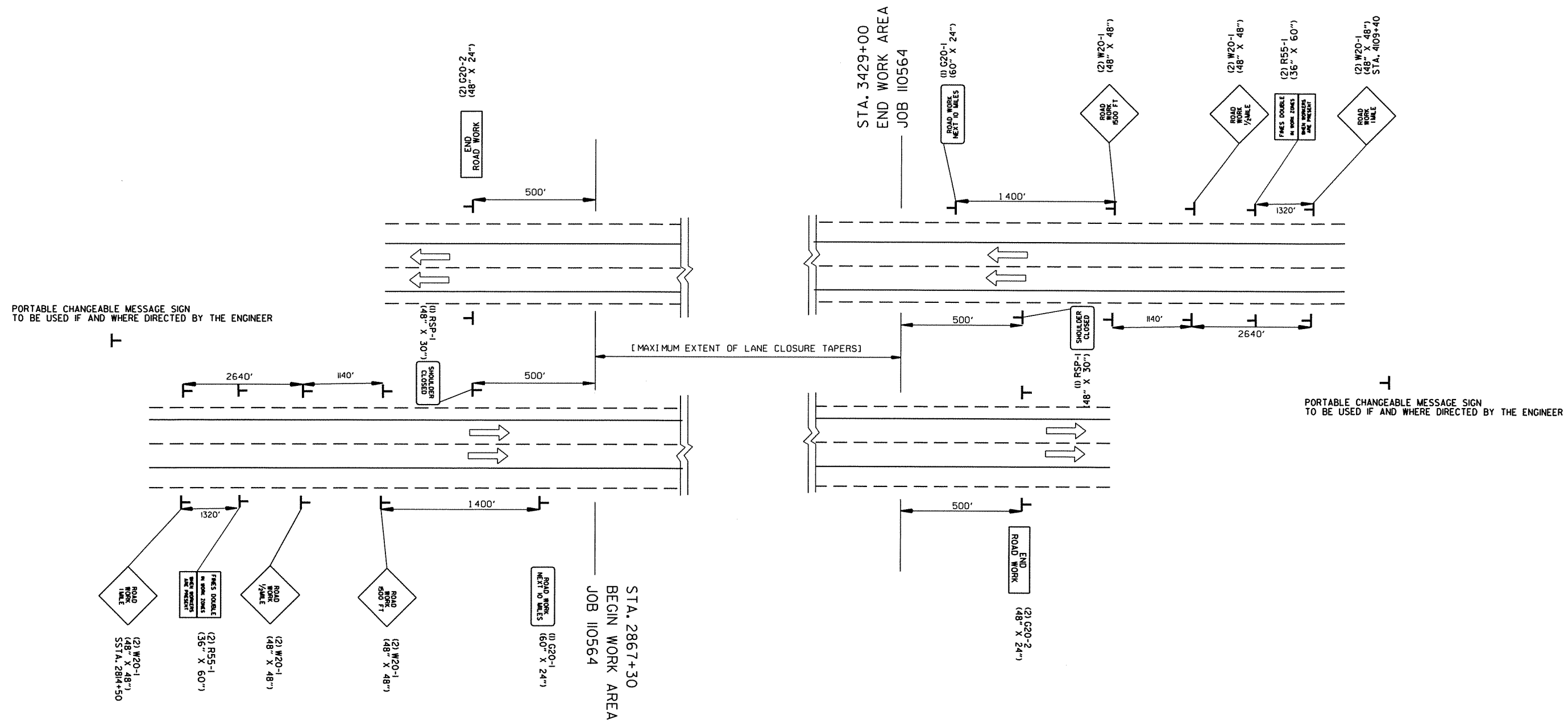
SAND BAG DITCH CHECK	DROP INLET SILT FENCE	DATE OF REVISION	REVISION
(E 5)	(E 7)		
I	□		
LEGEND			

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		16	44

② MAINTENANCE OF TRAFFIC



NOTE :
W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS
TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS
AS WORKING AREA SHIFTS.



NOTE :
W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS
TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS
AS WORKING AREA SHIFTS.

ADVANCE SIGNS AT BEGINNING AND END OF JOB
ALL STAGES

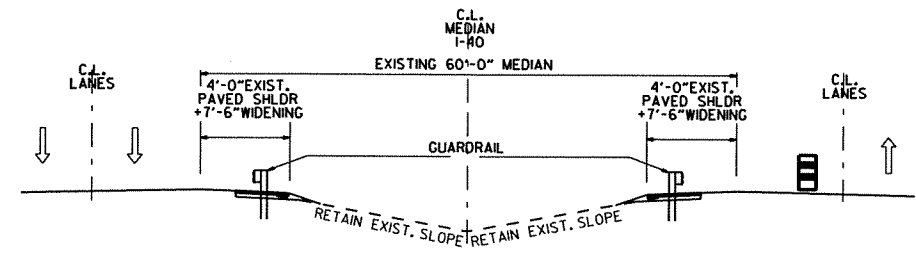
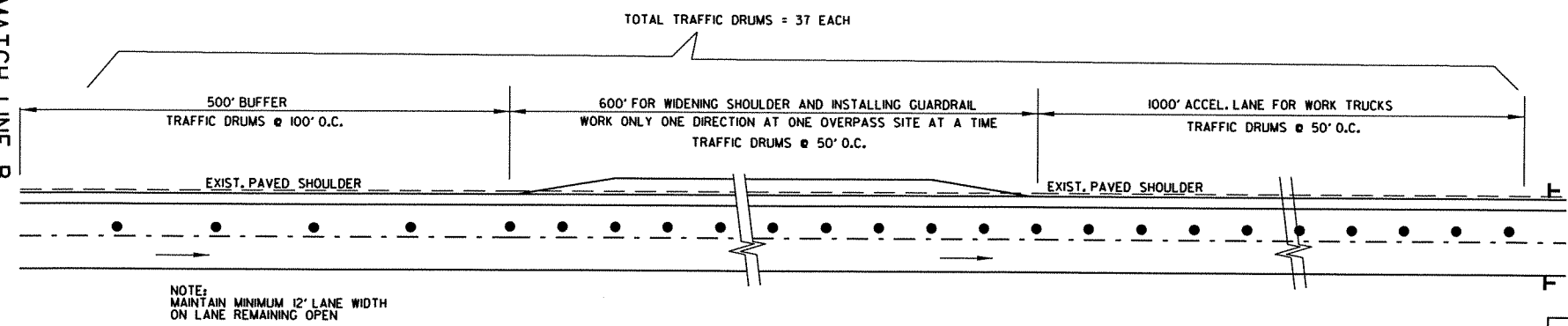
MAINTENANCE OF TRAFFIC
ADVANCE SIGNS AT JOB ENDS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		18	44

② MAINTENANCE OF TRAFFIC



MATCH LINE B



65	TRUCKS	70	SPEED LIMIT
		(2) R2-1	(48" X 60")
		(2) R2-2	(48" X 48")

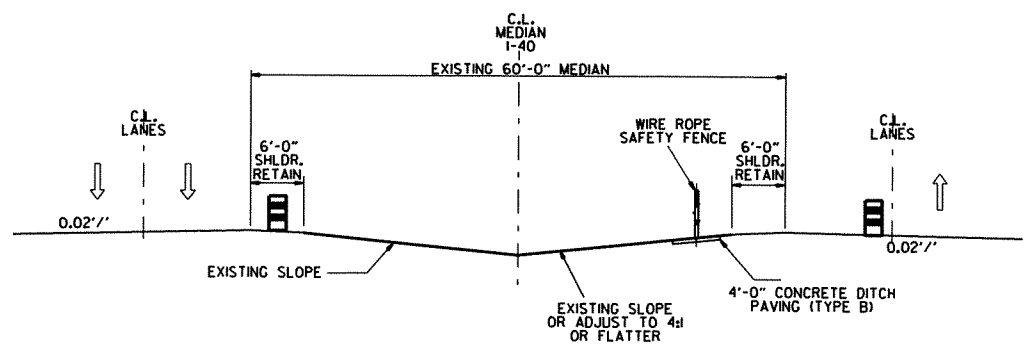
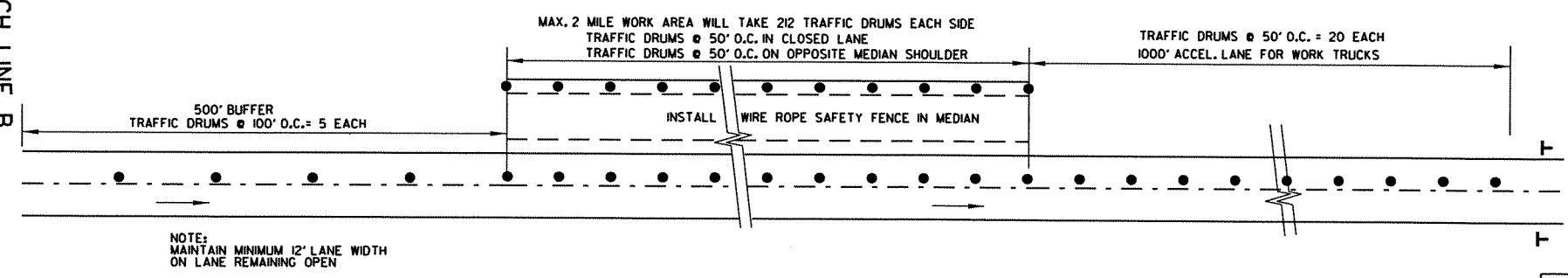


(4) W20-1 (48" X 48") ON ENTRANCE RAMPS

MOVABLE WORK ZONE FOR GUARDRAIL INSTALLATION

NOTE: REFER TO SP-MAINTENANCE OF TRAFFIC FOR LANE CLOSURE LIMITATIONS AND RESTRICTIONS. QUANTITY OF TRAFFIC DRUMS PROVIDED IN THE CONTRACT IS THE MAXIMUM NUMBER REQUIRED FOR ONE LANE CLOSURE.

MATCH LINE B



65	TRUCKS	70	SPEED LIMIT
		(2) R2-1	(48" X 60")
		(2) R2-2	(48" X 48")

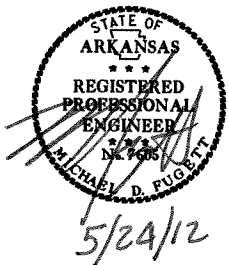
MOVABLE WORK ZONE FOR WRSF INSTALLATION

MAINTENANCE OF TRAFFIC WORK AREAS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		19	44
				JOB NO.		110564		

ADVANCE WARNING SIGNS AND DEVICES

② QUANTITIES



SIGN NUMBER	DESCRIPTION	SIGN SIZE	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS EACH	ADVANCE WARNING ARROW PANEL DAY	PORTABLE CHANGEABLE MESSAGE SIGN WEEK
				NO.	SQ. FT.			
W20-1	ROAD WORK 1 MILE	48"x48"	4	4	64.0			
W20-1	ROAD WORK 1/2 MILE	48"x48"	4	4	64.0			
W20-1	ROAD WORK 1500 FT.	48"x48"	4	4	64.0			
W20-1	ROAD WORK AHEAD	48"x48"	6	6	96.0			
G20-2	END ROAD WORK	48"x24"	4	4	32.0			
G20-1	ROAD WORK NEXT xx MILES	60"x24"	2	2	20.0			
SPECIAL 1	MERGE NOW + ARROW	48"x48"	1	1	16.0			
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1500 FEET	48"x48"	2	2	32.0			
W4-2R	RIGHT LANE CLOSING GRAPHIC	48"x48"	2	2	32.0			
W1-6	LARGE ARROW	30"x60"	3	3	36.0			
R4-1	DO NOT PASS	24"x30"	4	4	20.0			
R55-1	FINES DOUBLE IN WORK ZONES	36"x60"	4	4	60.0			
R2-5A	REDUCED SPEED AHEAD	48"x60"	2	2	40.0			
R2-1	SPEED LIMIT 60 MPH	48"x60"	2	2	40.0			
R2-1	SPEED LIMIT 70 MPH	48"x60"	2	2	40.0			
R2-2	TRUCKS SPEED LIMIT 65 MPH	48"x60"	2	2	40.0			
RSP-1	SHOULDER CLOSED	48"x30"	2	2	20.0			
	TRAFFIC DRUMS		494			494		
	ADVANCE WARNING ARROW PANEL		1				165	
	PORTABLE CHANGEABLE MESSAGE SIGN		3					69
TOTALS					780.0	494	165	69

REMOVAL AND DISPOSAL ITEMS

STATION	DESCRIPTION	IMPACT ATTENUATION BARRIER
		EACH
2900+21	ON CENTERLINE OF MEDIAN	2
3039+65	ON CENTERLINE OF MEDIAN	2
3234+86	ON CENTERLINE OF MEDIAN	2
3339+37	ON CENTERLINE OF MEDIAN	2
TOTAL		8

THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2003 EDITION.

SOIL LOG

STATION	LOCATION	LATITUDE			LONGITUDE			DEPTH FEET	AASHTO CLASSIFICATION	LIQUID LIMIT	PLASTICITY INDEX	REMARKS
		DEG	MIN	SEC	DEG	MIN	SEC					
2902+00	CL	34	55	4.8	91	9	6.5	0-5	A-4 (5)	27	8	BROWN
2953+00	CL	34	55	16.0	91	8	21.1	0-5	A-6 (14)	35	16	BROWN
3006+00	CL	34	55	28.8	91	7	42.4	0-5	A-7-6 (27)	44	27	BROWN
3058+00	CL	34	55	44.5	91	6	40.6	0-5	A-6 (5)	26	12	BROWN
3110+00	CL	34	55	56.0	91	5	26.5	0-5	A-6 (7)	26	12	BROWN
3162+00	CL	34	56	9.3	91	4	38.7	0-5	A-6 (13)	34	15	RED-BR
3215+00	CL	34	56	37.1	91	3	7.1	0-5	A-6 (16)	37	19	BR/GR
3266+00	CL	34	56	51.9	91	2	11.3	0-5	A-6 (16)	37	17	BROWN
3320+00	CL	34	57	10.8	91	1	9.5	0-5	A-4 (1)	21	6	BR/GR
3370+00	CL	34	57	27.5	91	0.0	15.1	0-5	A-6 (9)	31	11	BROWN

SOIL CHARACTERISTICS TABULATED ABOVE ARE REPRESENTATIVE AT THE LOCATION OF THE SAMPLE, AND FROM SURFACE INDICATIONS ARE TYPICAL FOR THE LIMITS SHOWN. THESE DATA ARE SHOWN FOR INFORMATION ONLY. THE STATE WILL NOT BE RESPONSIBLE FOR VARIATIONS IN THE SOIL CHARACTERISTICS AND/OR EXTENT OF SAME DIFFERING FROM THE ABOVE TABULATIONS.

TEMPORARY EROSION CONTROL

LOCATION	SAND BAG DITCH CHECKS	DROP INLET SILT FENCE	*SEDIMENT REMOVAL & DISPOSAL
	(E-5) BAG	(E-7) LIN.FT.	CU. YD.
ENTIRE JOB			
	240	864	50
TOTALS			
	240	864	50

BASIS OF ESTIMATE:
SAND BAG DITCH CHECKS.....20 BAGS / LOCATION
DROP INLET SILT FENCE18 LIN.FT./LOCATION

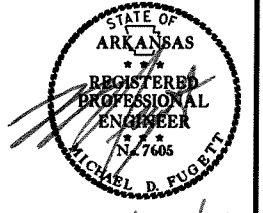
NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

*QUANTITIES ARE ESTIMATED.
REFER TO SECTION 104.03 OF THE STANDARD SPECIFICATIONS.

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. NO. PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO. 110564		20		44

2 QUANTITIES



5/24/12

BASE AND SURFACING

STATION	STATION	LOCATION	LENGTH FEET	AGGREGATE BASE COURSE (CLASS 7)		ACHM SURFACE COURSE (1/2") (PG 64-22)			
				TON / STATION	TON	AVG. WID. FEET	SQ. YD.	POUND / SQ. YD.	TON
2892+95	2893+28	RT SHLDR TRANSITION MONROE CO. LINE OVERPASS	33.0	24.25	8.0	4.8	17.6	220.0	1.9
2893+28	2896+79	RT SHLDR WIDENING MONROE CO. LINE OVERPASS	351.0	35.00	122.9	7.5	292.5	220.0	32.2
2899+85	2901+48	RT SHLDR WIDENING MONROE CO. LINE OVERPASS	163.0	35.00	57.1	7.5	135.8	220.0	14.9
2901+48	2901+81	RT SHLDR TRANSITION MONROE CO. LINE OVERPASS	33.0	24.25	8.0	4.8	17.6	220.0	1.9
2895+55	2895+88	LT SHLDR TRANSITION MONROE CO. LINE OVERPASS	33.0	24.00	7.9	4.8	17.6	220.0	1.9
2895+88	2896+79	LT SHLDR WIDENING MONROE CO. LINE OVERPASS	91.0	35.00	31.9	7.5	75.8	220.0	8.3
2899+85	2903+29	LT SHLDR WIDENING MONROE CO. LINE OVERPASS	344.0	35.00	120.4	7.5	286.7	220.0	31.5
2903+29	2903+44	LT SHLDR TRANSITION MONROE CO. LINE OVERPASS	15.0	38.00	5.7	8.8	14.7	220.0	1.6
2903+44	2904+14	LT SHLDR EXTRA-WIDENING MONROE CO. LINE OVERPASS	70.0	41.00	28.7	10.0	77.8	220.0	8.6
2904+14	2904+62	LT SHLDR TRANSITION MONROE CO. LINE OVERPASS	48.0	24.50	11.8	6.0	32.0	220.0	3.5
3035+22	3035+70	RT SHLDR TRANSITION WHEATLEY RD. OVERPASS	48.0	24.50	11.8	6.0	32.0	220.0	3.5
3035+70	3036+40	RT SHLDR EXTRA-WIDENING WHEATLEY RD. OVERPASS	70.0	41.00	28.7	10.0	77.8	220.0	8.6
3036+40	3036+55	RT SHLDR TRANSITION WHEATLEY RD. OVERPASS	15.0	38.00	5.7	8.8	14.7	220.0	1.6
3036+55	3040+05	RT SHLDR WIDENING WHEATLEY RD. OVERPASS	350.0	35.00	122.5	7.5	291.7	220.0	32.1
3040+05	3040+20	RT SHLDR TRANSITION WHEATLEY RD. OVERPASS	15.0	38.00	5.7	8.8	14.7	220.0	1.6
3040+20	3040+90	RT SHLDR EXTRA-WIDENING WHEATLEY RD. OVERPASS	70.0	41.00	28.7	10.0	77.8	220.0	8.6
3040+90	3041+38	RT SHLDR TRANSITION WHEATLEY RD. OVERPASS	48.0	24.50	11.8	6.0	32.0	220.0	3.5
3038+08	3038+41	LT SHLDR TRANSITION WHEATLEY RD. OVERPASS	33.0	21.90	7.2	4.8	17.6	220.0	1.9
3038+41	3043+61	LT SHLDR WIDENING WHEATLEY RD. OVERPASS	520.0	30.30	157.6	7.5	433.3	220.0	47.7
3043+61	3043+94	LT SHLDR TRANSITION WHEATLEY RD. OVERPASS	33.0	21.90	7.2	4.8	17.6	220.0	1.9
	3209+00	CONSTRUCT MEDIAN CROSSOVER		73.00					17.2
3230+58	3230+91	RT SHLDR TRANSITION AT BLOSSOM NORTH RD.	33.0	24.25	8.0	4.8	17.6	220.0	1.9
3230+91	3235+46	RT SHLDR WIDENING AT BLOSSOM NORTH RD.	455.0	35.00	159.3	7.5	379.2	220.0	41.7
3235+46	3235+61	RT SHLDR TRANSITION AT BLOSSOM NORTH RD.	15.0	38.00	5.7	8.8	14.7	220.0	1.6
3235+61	3236+31	RT SHLDR EXTRA WIDENING AT BLOSSOM NORTH RD.	70.0	41.00	28.7	10.0	77.8	220.0	8.6
3236+31	3236+79	RT SHLDR TRANSITION AT BLOSSOM NORTH RD.	48.0	24.50	11.8	6.0	32.0	220.0	3.5
3232+93	3233+41	LT SHLDR TRANSITION AT BLOSSOM NORTH RD.	48.0	24.50	11.8	6.0	32.0	220.0	3.5
3233+41	3234+11	LT SHLDR EXTRA WIDENING AT BLOSSOM NORTH RD.	70.0	41.00	28.7	10.0	77.8	220.0	8.6
3234+11	3234+26	LT SHLDR TRANSITION AT BLOSSOM NORTH RD.	15.0	38.00	5.7	8.8	14.7	220.0	1.6
3234+26	3238+61	LT SHLDR WIDENING AT BLOSSOM NORTH RD.	435.0	35.00	152.3	7.5	362.5	220.0	39.9
3238+61	3238+94	LT SHLDR TRANSITION AT BLOSSOM NORTH RD.	33.0	24.25	8.0	4.8	17.6	220.0	1.9
3335+09	3335+42	RT SHLDR TRANSITION GOODWIN RD. OVERPASS	33.0	24.25	8.0	4.8	17.6	220.0	1.9
3335+42	3340+77	RT SHLDR WIDENING GOODWIN RD. OVERPASS	535.0	35.00	187.3	7.5	445.8	220.0	49.0
3340+37	3340+92	RT SHLDR TRANSITION GOODWIN RD. OVERPASS	55.0	38.00	20.9	8.8	53.8	220.0	5.9
3340+92	3341+62	RT SHLDR EXTRA WIDENING GOODWIN RD. OVERPASS	70.0	41.00	28.7	10.0	77.8	220.0	8.6
3341+62	3342+10	RT SHLDR TRANSITION GOODWIN RD. OVERPASS	48.0	24.50	11.8	6.0	32.0	220.0	3.5
3337+64	3338+12	LT SHLDR TRANSITION GOODWIN RD. OVERPASS	48.0	24.50	11.8	6.0	32.0	220.0	3.5
3338+12	3338+82	LT SHLDR EXTRA WIDENING GOODWIN RD. OVERPASS	70.0	41.00	28.7	10.0	77.8	220.0	8.6
3338+82	3338+97	LT SHLDR TRANSITION GOODWIN RD. OVERPASS	15.0	38.00	5.7	8.8	14.7	220.0	1.6
3338+97	3339+37	LT SHLDR WIDENING GOODWIN RD. OVERPASS	40.0	35.00	14.0	7.5	33.3	220.0	3.7
3340+37	3344+32	LT SHLDR WIDENING GOODWIN RD. OVERPASS	395.0	35.00	138.3	7.5	329.2	220.0	36.2
3344+32	3344+65	LT SHLDR TRANSITION GOODWIN RD. OVERPASS	33.0	24.25	8.0	4.8	17.6	220.0	1.9
	3393+00	CONSTRUCT MEDIAN CROSSOVER		73.00					17.2
TOTALS					1672.5		4134.8		488.9

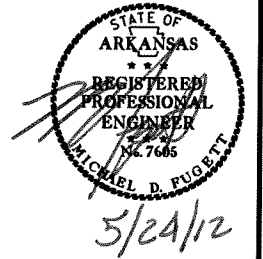
BASIS OF ESTIMATE:
 ACHM SURFACE COURSE (1/2").....94.7% MIN. AGGR.....5.3% ASPHALT BINDER
 MAXIMUM NUMBER OF GYRATIONS = 115

QUANTITIES

05/24/2012 R110564.DGN/QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		21	44

② QUANTITIES



GUARDRAIL

STATION	STATION	LOCATION	GUARDRAIL (TYPE A)	TERMINAL ANCHOR POST (TYPE 1)	GUARDRAIL TERMINAL (TYPE 2)
			LIN. FT.	EACH	
2893+39	2901+38	RT. SHLDR AT MONROE CO. LINE OVERPASS	450	1	1
2895+98	2904+04	LT. SHLDR AT MONROE CO. LINE OVERPASS	450	1	1
3035+80	3040+80	RT. SHLDR AT WHEATLEY RD. OVERPASS	450	1	1
3038+51	3043+51	LT. SHLDR AT WHEATLEY RD. OVERPASS	450	1	1
3231+01	3236+21	RT. SHLDR AT BLOSSOM NORTH RD. OVERPASS	450	1	1
3233+51	3238+51	LT. SHLDR AT BLOSSOM NORTH RD. OVERPASS	450	1	1
3335+52	3341+52	RT. SHLDR AT GOODWIN RD. OVERPASS	450	1	1
3338+22	3344+22	LT. SHLDR AT GOODWIN RD. OVERPASS	450	1	1
TOTALS			3600	8	8

EARTHWORK

STATION	STATION	LOCATION	UNCLASSIFIED EXCAVATION	COMPACTED EMBANKMENT
			CU. YD.	CU. YD.
2892+95	2901+81	RT MEDIAN SHOULDER AT MONROE CO. LINE OVERPASS	110	
2895+55	2904+62	LT MEDIAN SHOULDER AT MONROE CO. LINE OVERPASS	114	
3035+22	3041+38	RT MEDIAN SHOULDER AT WHEATLEY RD. OVERPASS	116	
3038+08	3043+94	LT MEDIAN SHOULDER AT WHEATLEY RD. OVERPASS	110	
	3209+00	CONSTRUCT MEDIAN CROSSOVER		95
3230+58	3236+79	RT MEDIAN SHOULDER AT BLOSSOM NORTH RD.	117	
3232+93	3238+94	LT MEDIAN SHOULDER AT BLOSSOM NORTH RD.	114	
3335+09	3342+10	RT MEDIAN SHOULDER AT GOODWIN RD. OVERPASS	114	
3337+64	3344+65	LT MEDIAN SHOULDER AT GOODWIN RD. OVERPASS	114	
	3393+00	CONSTRUCT MEDIAN CROSSOVER		95
TOTALS			909	190

NOTE: EARTHWORK QUANTITIES SHOWN ABOVE SHALL BE PAID FOR AS PLAN QUANTITY.

CONCRETE DITCH PAVING

STATION	STATION	LOCATION	LENGTH	WIDTH	CONC. DITCH PAVING (TYPE B)	SOLID SODDING	*WATER
			FEET	FEET	SQ. YD.		M. GAL.
2904+14.00	2983+75.00	LT OF CL I-40	7961.00	4	3538	3538	44.6
2985+10.00	3035+70.00	RT OF CL I-40	5060.00	4	2249	2249	28.3
3040+90.00	3208+75.00	RT OF CL I-40	16785.00	4	7460	7460	94.0
3209+25.00	3233+41.00	LT OF CL I-40	2416.00	4	1074	1074	13.5
3236+31.00	3316+00.00	RT OF CL I-40	7969.00	4	3542	3542	44.6
3317+40.00	3338+12.00	LT OF CL I-40	2072.00	4	921	921	11.6
3341+62.00	3392+75.00	RT OF CL I-40	5113.00	4	2272	2272	28.6
3393+25.00	3419+00.00	RT OF CL I-40	2575.00	4	1144	1144	14.4
TOTALS					22200	22200	279.6

BASIS OF ESTIMATE:
WATER.....12.6 GAL. / SQ. YD. OF SOLID SODDING.

WIRE ROPE SAFETY FENCE

STATION	STATION	LOCATION	WIRE ROPE SAFETY FENCE	WRSF ANCHOR*	WRSF MAINTENANCE MATERIALS
			LIN. FT.	EACH	LUMP SUM
2903+54.00	2983+75.00	LT OF CL I-40	8021.00	2	
2985+10.00	3036+30.00	RT OF CL I-40	5120.00	1	
3040+30.00	3208+75.00	RT OF CL I-40	16845.00	2	
3209+25.00	3234+01.00	LT OF CL I-40	2476.00	2	
3235+71.00	3316+00.00	RT OF CL I-40	8029.00	2	
3317+40.00	3338+72.00	LT OF CL I-40	2132.00	2	
3341+02.00	3392+75.00	RT OF CL I-40	5173.00	2	
3393+25.00	3419+00.00	RT OF CL I-40	2575.00	2	
ENTIRE PROJECT					1.00
TOTALS			50371	15	1.00

* THIS ITEM SHOWN FOR INFORMATION ONLY

05/18/2012 RI10564.DGN/QUANTITIES

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		22	44

2 SUMMARY OF QUANTITIES AND REVISIONS



6/6/12

SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
210	UNCLASSIFIED EXCAVATION	909	CU.YD.
210	COMPACTED EMBANKMENT	190	CU.YD.
SS & 303	AGGREGATE BASE COURSE (CLASS 7)	1673	TON
SP,SS & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")	463	TON
SP,SS & 407	ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (1/2")	26	TON
601	MOBILIZATION	1.00	LUMP SUM
SP,SS & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	780	SQ.FT.
SS & 604	TRAFFIC DRUMS	494	EACH
SS & 604	ADVANCE WARNING ARROW PANEL	165	DAY
SP,SS & 604	PORTABLE CHANGEABLE MESSAGE SIGN	69	WEEK
SP& 605	CONCRETE DITCH PAVING (TYPE B)	22200	SQ.YD.
617	GUARDRAIL (TYPE A)	3600	LIN.FT.
SS & 617	TERMINAL ANCHOR POSTS (TYPE 1)	8	EACH
SS & 617	GUARDRAIL TERMINAL (TYPE 2)	8	EACH
SS & 620	WATER	279.6	M.GAL.
621	SAND BAG DITCH CHECKS	240	BAG
621	DROP INLET SILT FENCE	864	LIN.FT.
621	SEDIMENT REMOVAL AND DISPOSAL	50	CU. YD.
624	SOLID SODDING	22200	SQ. YD.
635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
SP	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIER	8	EACH
SP	WIRE ROPE SAFETY FENCE	50371	LIN.FT.
SP	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS	1.00	LUMP SUM

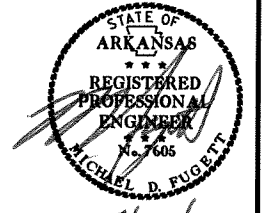
REVISIONS

DATE	REVISION	SHEET NUMBER

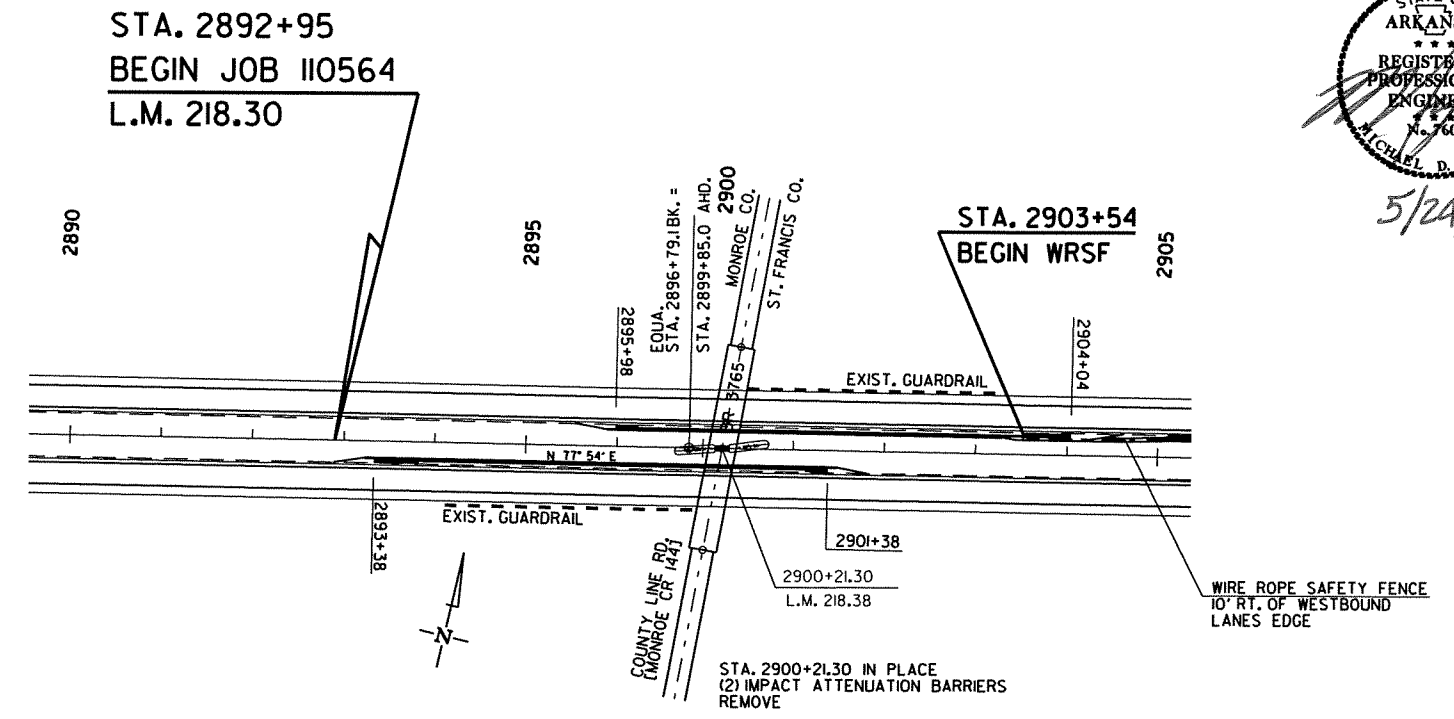
SUMMARY OF QUANTITIES AND REVISIONS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		110564	23	44

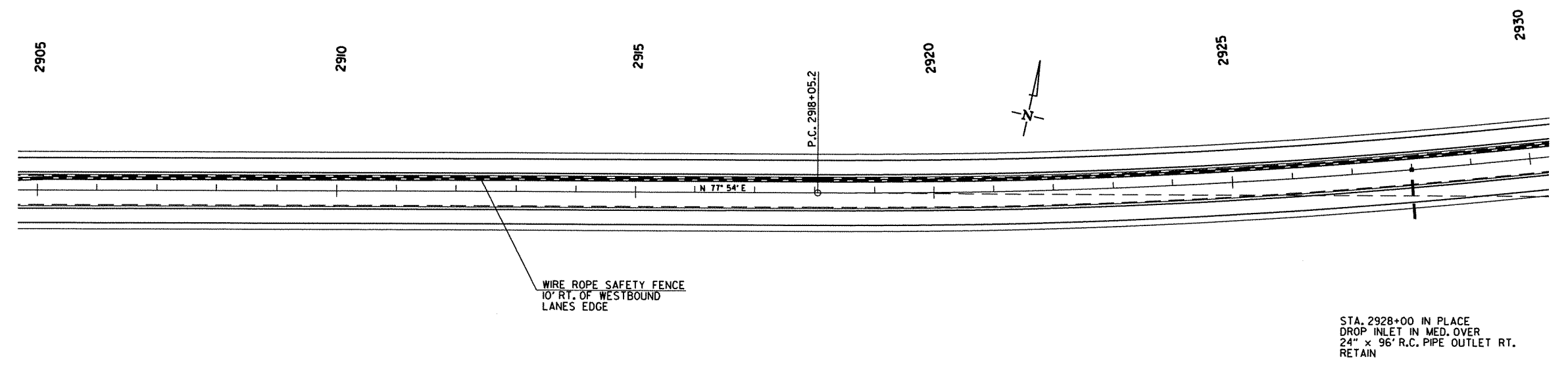
② PLAN SHEETS



5/24/12



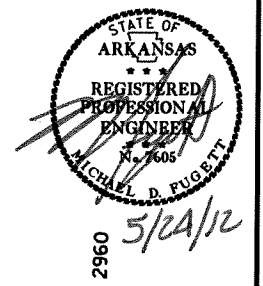
GUARDRAIL INSTALLATION	GUARDRAIL (TYPE A)	ANCHOR POST (TYPE D)	GUARDRAIL TERMINAL (TYPE 2)
STA. 2893+38 - STA. 2901+38	RT. 450 LIN.FT.	1 EACH	1 EACH
STA. 2895+98 - STA. 2904+04	LT. 450 LIN.FT.	1 EACH	1 EACH



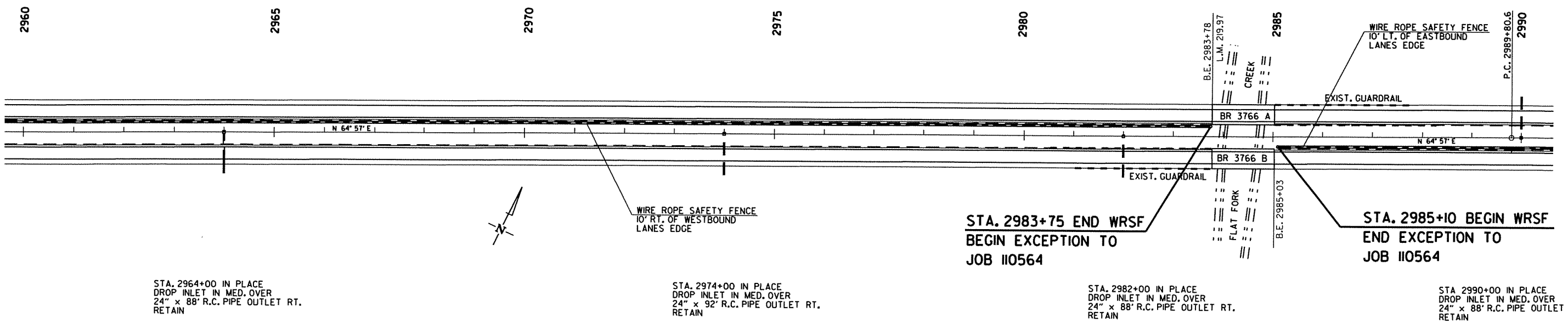
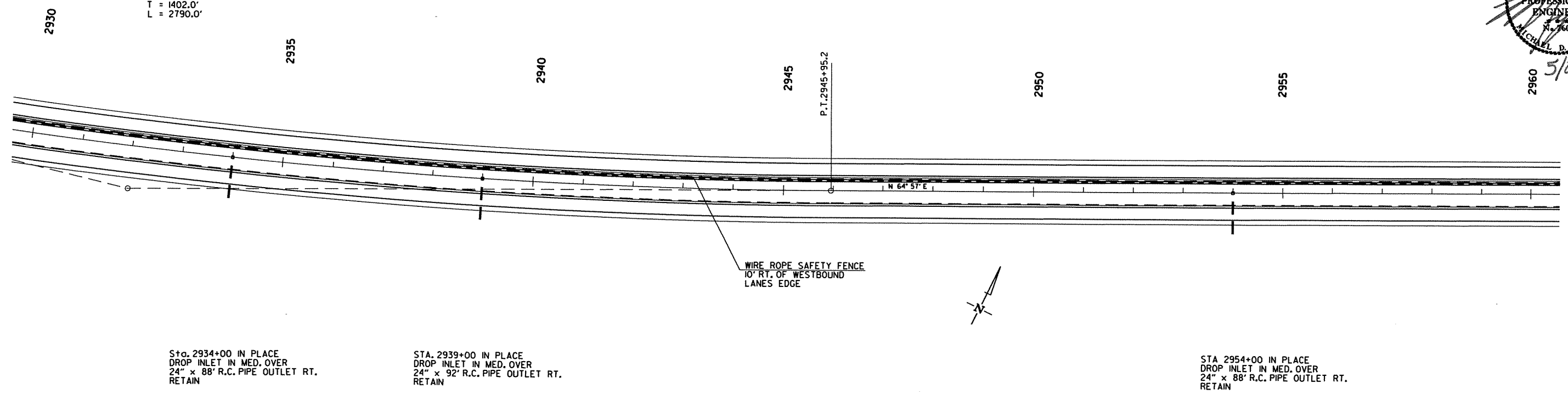
PLAN SHEETS STA. 2890+00 - STA. 2930+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 110564							24	44

2 PLAN SHEETS



P.C. MEDIAN
 = 2932+07.2
 P.I. = 13'57" LT.
 D.T. = 0'30"
 L = 1402.0'
 L = 2790.0'



PLAN SHEETS STA. 2930+00 - STA. 2990+00

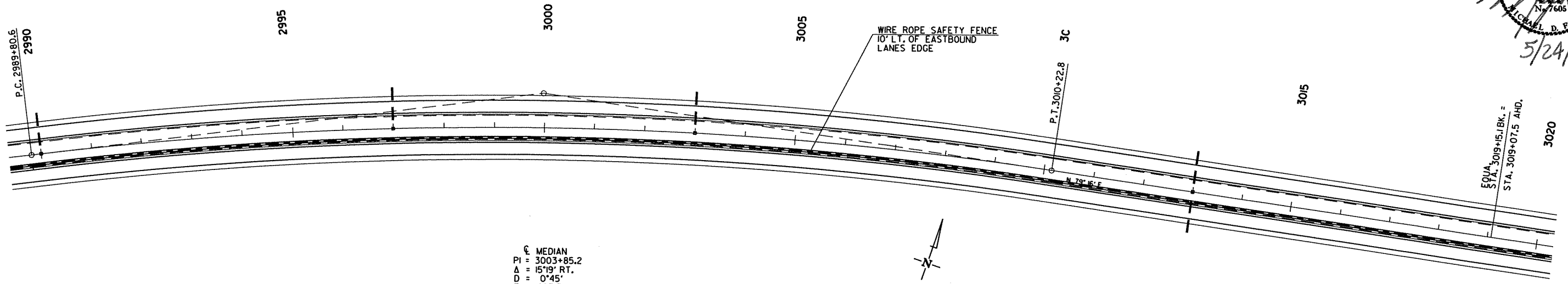
05/22/2012
 r110564.dgn/PLAN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 110564							25	44

② PLAN SHEETS



5/24/12



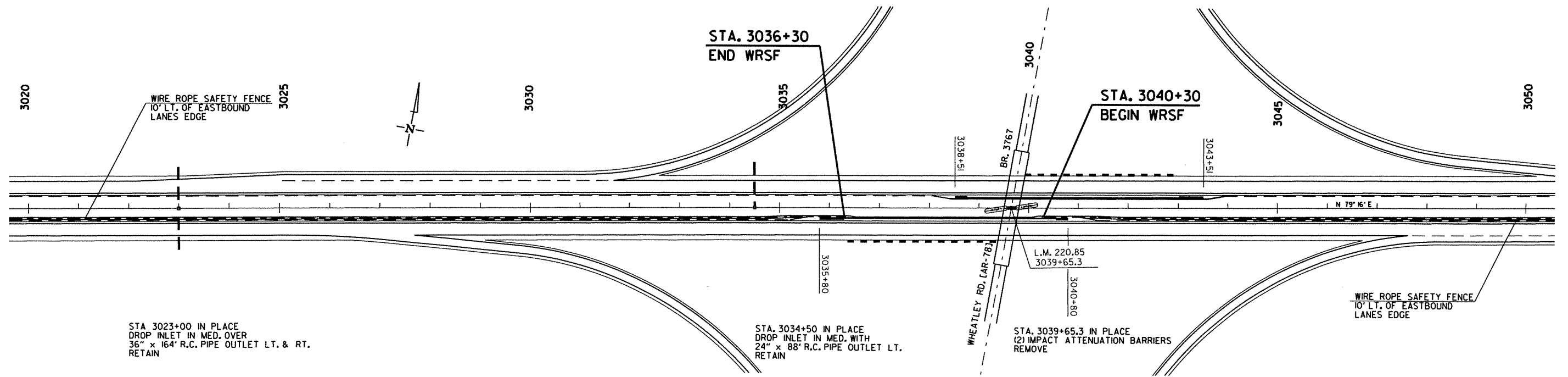
☉ MEDIAN
 PI = 3003+85.2
 Δ = 15°19' RT.
 D = 0°45'
 T = 1027.3'
 L = 2042.2'

STA. 2997+00 IN PLACE
 DROP INLET IN MED. OVER
 24" x 88" R.C. PIPE OUTLET LT.
 RETAIN

STA. 3003+00 IN PLACE
 DROP INLET IN MED. OVER
 24" x 88" R.C. PIPE OUTLET LT.
 RETAIN

Sta 3013+00 IN PLACE
 (NO DROP INLET IN MED. INDICATED)
 24" x 72" R.C. PIPE CULV'T UNDER LT. LANES
 24" x 68" R.C. PIPE CULV'T UNDER RT. LANES
 RETAIN

GUARDRAIL INSTALLATION	GUARDRAIL (TYPE A)	TERMINAL ANCHOR POST (TYPE D)	GUARDRAIL TERMINAL (TYPE 2)
STA. 3035+80 - STA. 3040+80 RT. 450 LIN.FT.	1 EACH	1 EACH	1 EACH
STA. 3038+51 - STA. 3043+51 LT. 450 LIN.FT.	1 EACH	1 EACH	1 EACH



STA 3023+00 IN PLACE
 DROP INLET IN MED. OVER
 36" x 164" R.C. PIPE OUTLET LT. & RT.
 RETAIN

STA. 3034+50 IN PLACE
 DROP INLET IN MED. WITH
 24" x 88" R.C. PIPE OUTLET LT.
 RETAIN

STA. 3039+65.3 IN PLACE
 (2) IMPACT ATTENUATION BARRIERS
 REMOVE

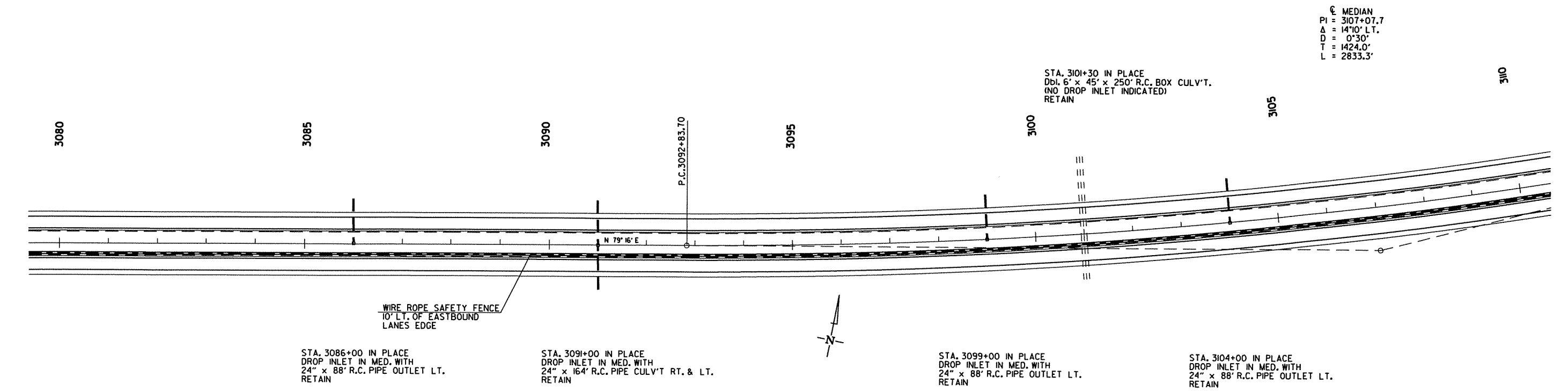
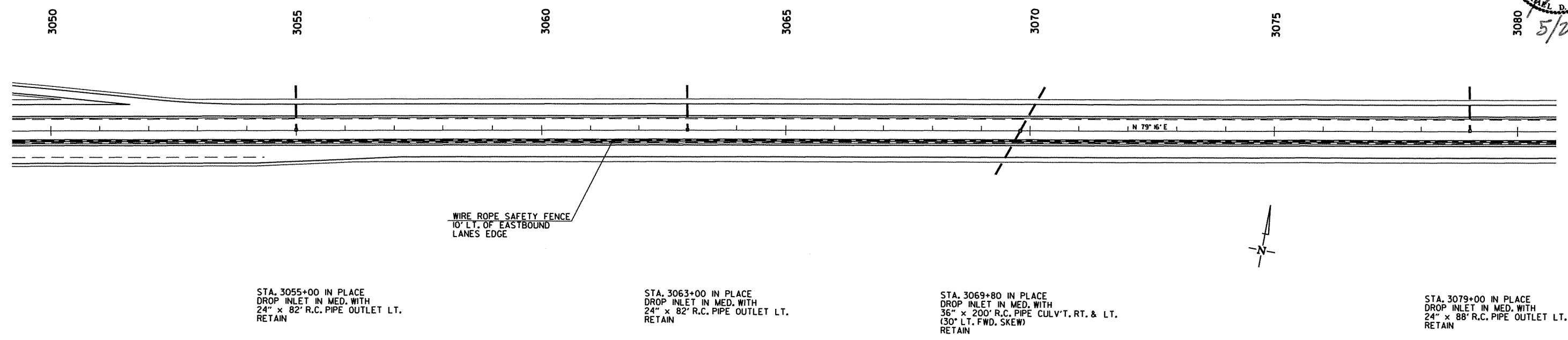
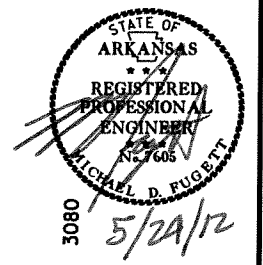
WIRE ROPE SAFETY FENCE
 10' LT. OF EASTBOUND
 LANES EDGE

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PLAN SHEETS STA. 2990+00 - STA. 3050+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		26	44

② PLAN SHEETS



PLAN SHEETS STA. 3050+00 - STA. 3110+00

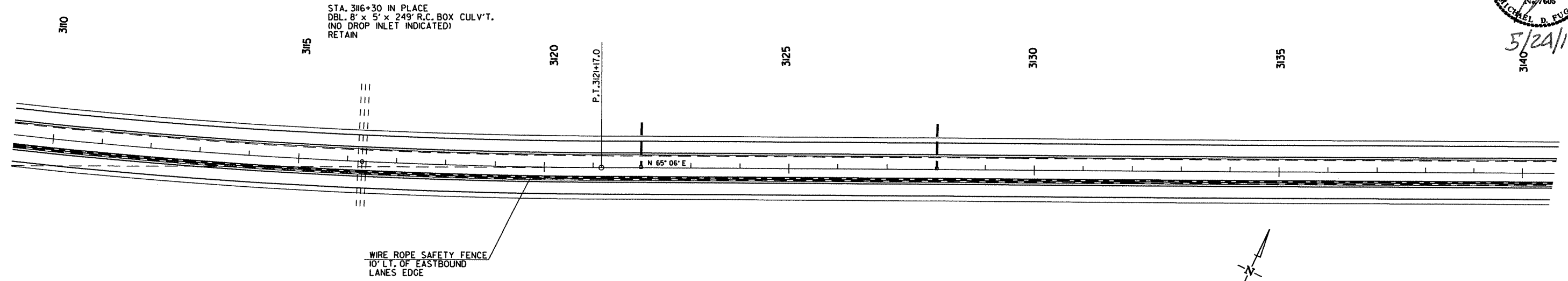
r110564.dgn/PLAN 05/22/2012

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		27	44
				JOB NO.		110564		

2 PLAN SHEETS



5/24/12
3140



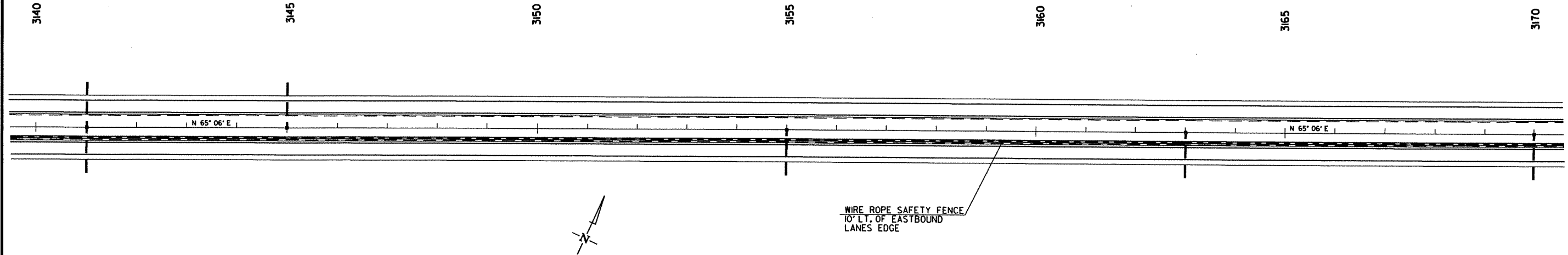
STA. 3116+30 IN PLACE
DBL. 8' x 5' x 249' R.C. BOX CULV'T.
(NO DROP INLET INDICATED)
RETAIN

P.I. 3121+17.0

WIRE ROPE SAFETY FENCE
10' LT. OF EASTBOUND
LANES EDGE

STA. 3122+00 IN PLACE
DROP INLET IN MED. WITH
24" x 82' R.C. PIPE OUTLET LT.
RETAIN

STA. 3128+00 IN PLACE
DROP INLET IN MED. WITH
24" x 82' R.C. PIPE OUTLET LT.
RETAIN



STA. 3141+00 IN PLACE
DROP INLET IN MED. OVER
24" x 160' R.C. PIPE CULV'T. RT. & LT.
RETAIN

Sta 3145+00 IN PLACE
DROP INLET IN MED. WITH
24" x 88' R.C. PIPE OUTLET LT.
RETAIN

STA. 3155+00 IN PLACE
DROP INLET IN MED. WITH
24" x 88' R.C. PIPE OUTLET RT.
RETAIN

STA. 3163+00 IN PLACE
DROP INLET IN MED. WITH
24" x 88' R.C. PIPE OUTLET RT.
RETAIN

STA. 3170+00 IN PLACE
DROP INLET IN MED. WITH
24" x 88' R.C. PIPE OUTLET RT.
RETAIN

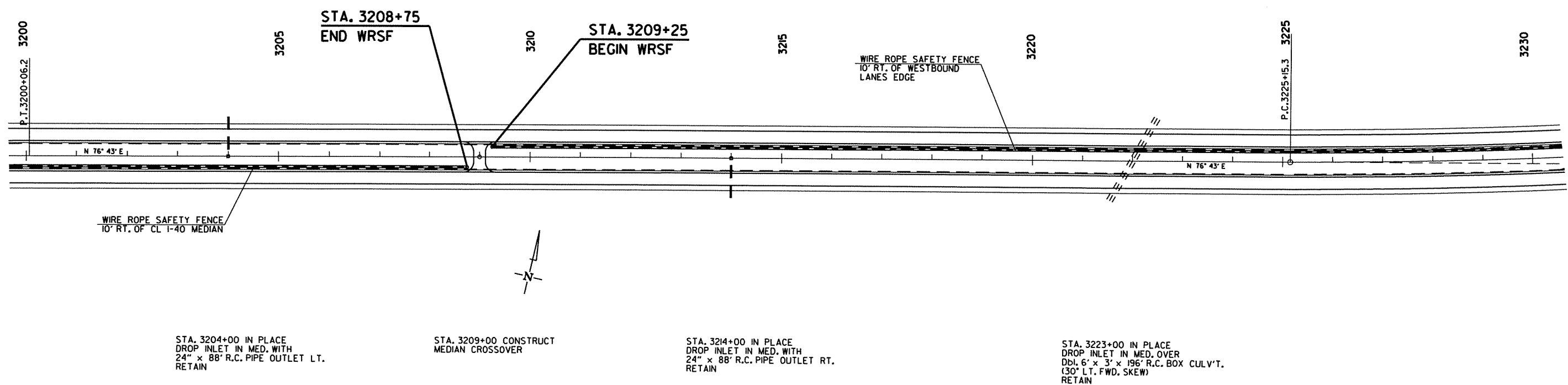
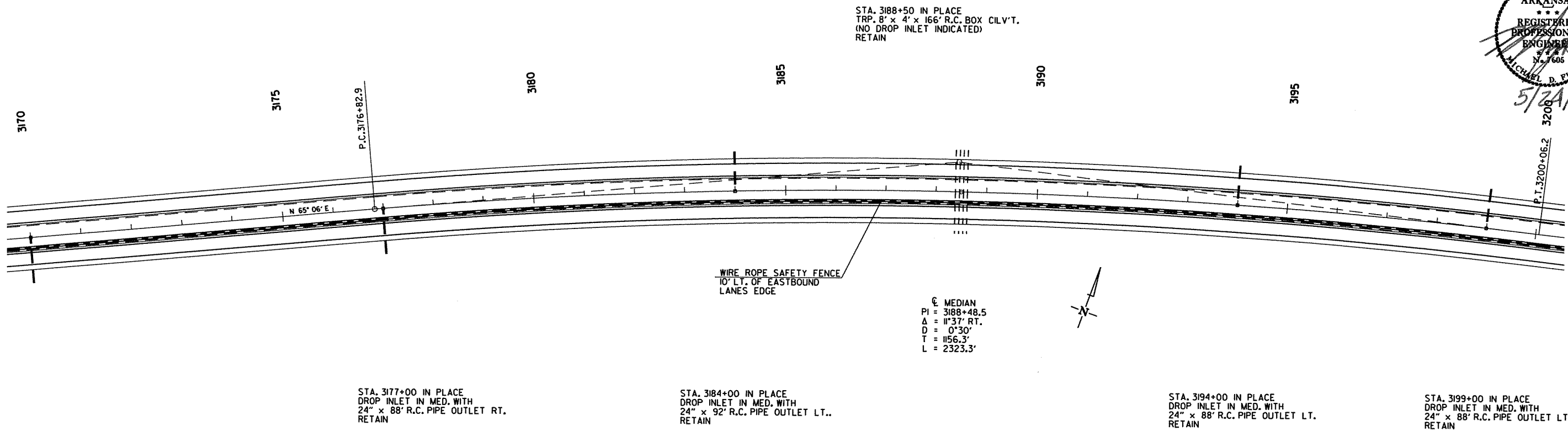
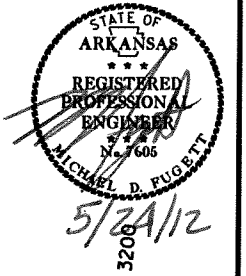
WIRE ROPE SAFETY FENCE
10' LT. OF EASTBOUND
LANES EDGE

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PLAN SHEETS STA. 3110+00 - STA. 3170+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							110564	28	44

2 PLAN SHEETS

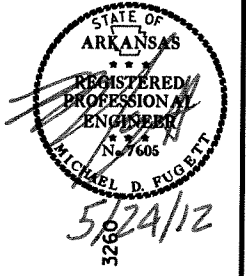


PLAN SHEETS STA. 3170+00 - STA. 3230+00

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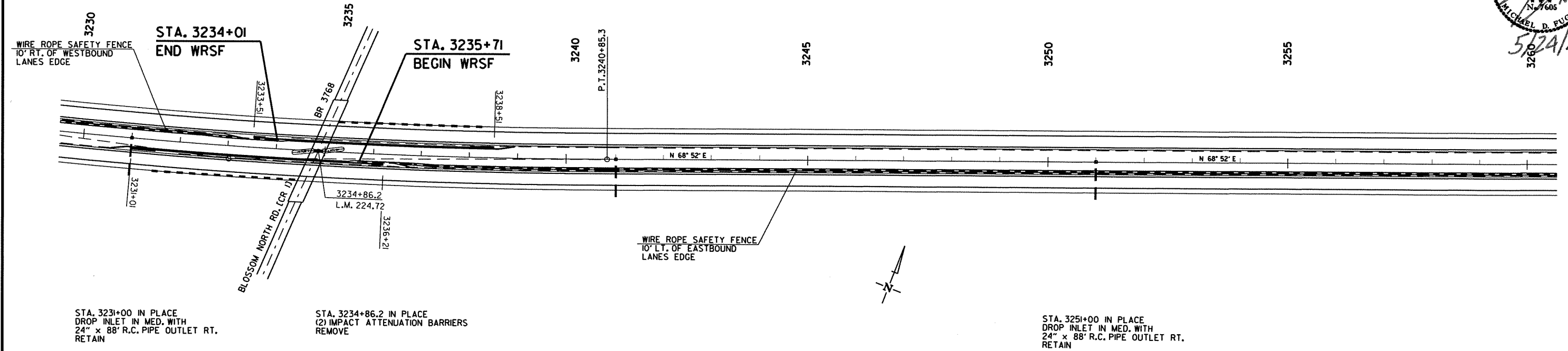
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		29	44

2 PLAN SHEETS



@ MEDIAN
 PI = 3233+01.5
 A = 75° LT.
 D = 0°30'
 T = 786.2'
 L = 1570.0'

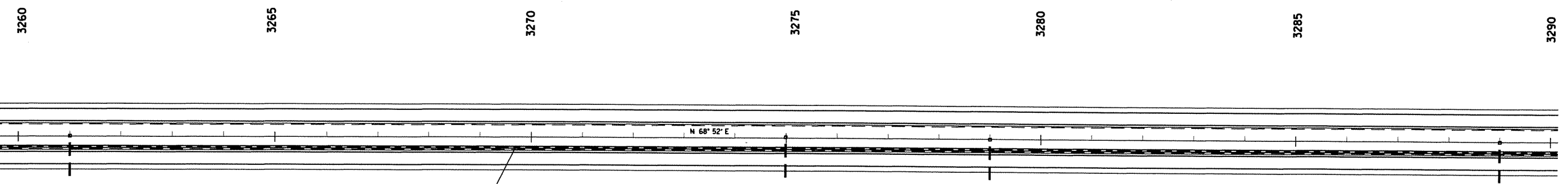
GUARDRAIL INSTALLATION
 STA. 3231+01 - STA. 3236+21 RT. 450 LIN. FT. IEACH IEACH
 STA. 3233+51 - STA. 3238+51 LT. 450 LIN. FT. IEACH IEACH
 GUARDRAIL (TYPE A) GUARDRAIL (TYPE 2)
 TERMINAL ANCHOR POST (TYPE 1) TERMINAL ANCHOR POST (TYPE 2)



STA. 3231+00 IN PLACE
 DROP INLET IN MED. WITH
 24" x 88" R.C. PIPE OUTLET RT.
 RETAIN

STA. 3234+86.2 IN PLACE
 (2) IMPACT ATTENUATION BARRIERS
 REMOVE

STA. 3251+00 IN PLACE
 DROP INLET IN MED. WITH
 24" x 88" R.C. PIPE OUTLET RT.
 RETAIN



STA. 3261+00 IN PLACE
 DROP INLET IN MED. WITH
 24" x 88" R.C. PIPE OUTLET RT.
 RETAIN

STA. 3275+00 IN PLACE
 DROP INLET IN MED. WITH
 24" x 88" R.C. PIPE OUTLET RT.
 RETAIN

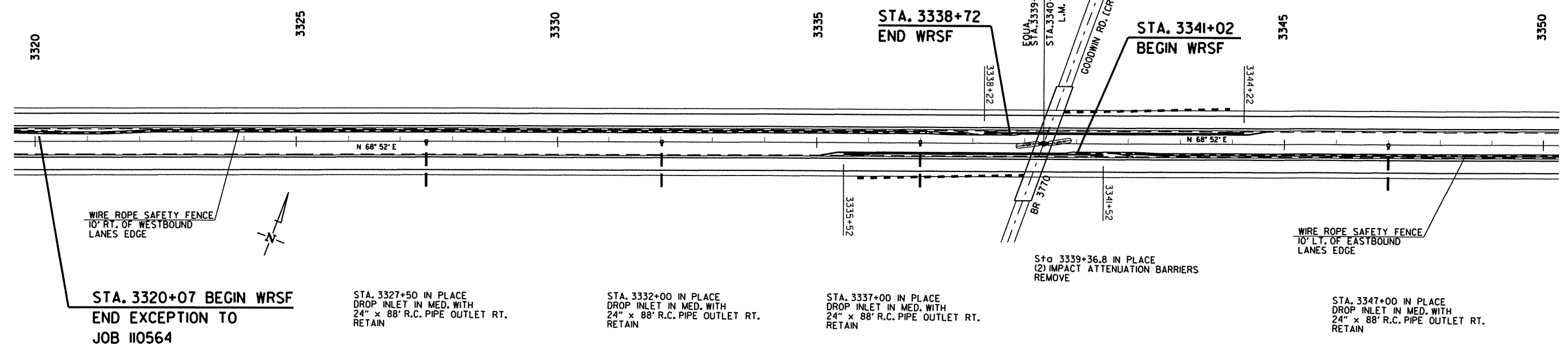
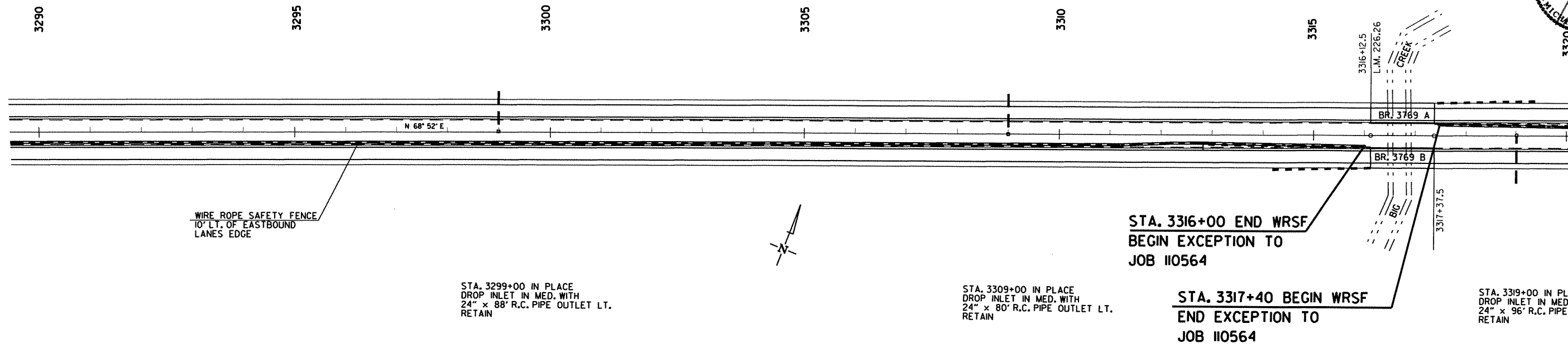
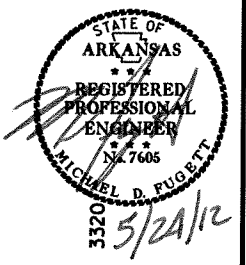
STA. 3279+00 IN PLACE
 DROP INLET IN MED. WITH
 24" x 88" R.C. PIPE OUTLET RT.
 RETAIN

STA. 3289+00 IN PLACE
 DROP INLET IN MED. WITH
 24" x 88" R.C. PIPE OUTLET RT.
 RETAIN

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		30	44

2 PLAN SHEETS



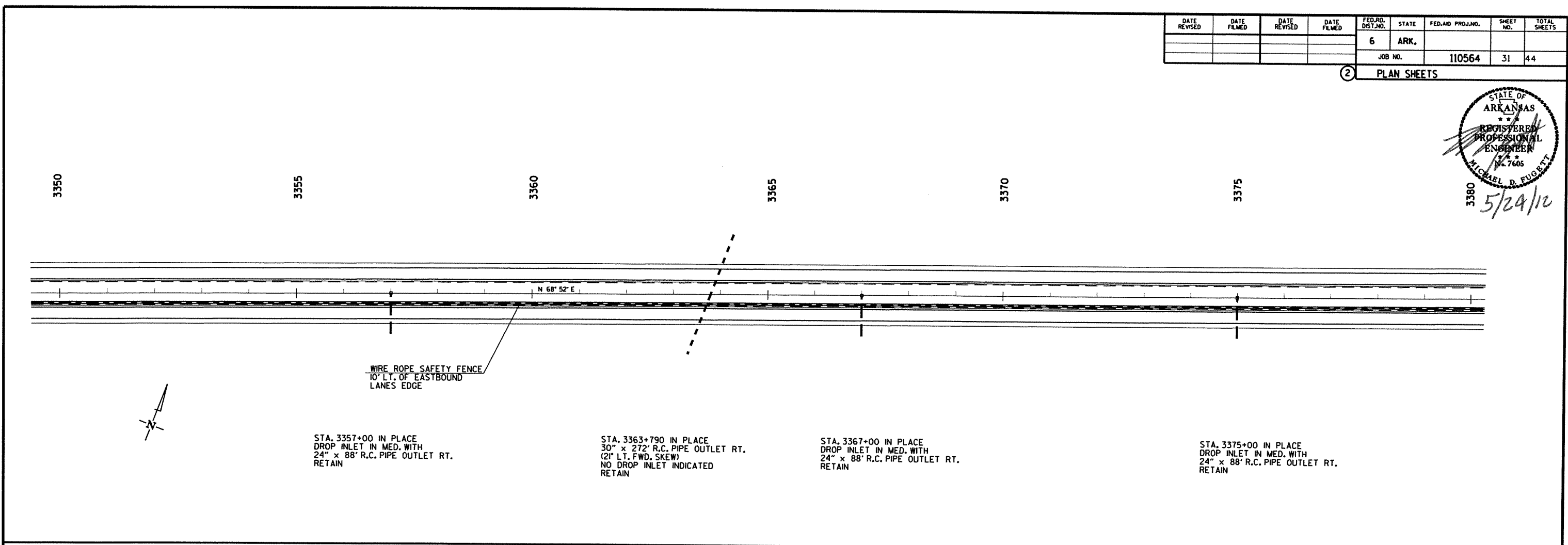
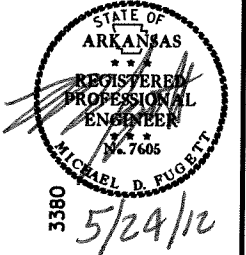
GUARDRAIL INSTALLATION	GUARDRAIL (TYPE A)	TERMINAL ANCHOR POST (TYPE 1)	GUARDRAIL TERMINAL (TYPE 2)
STA. 3231+01 - STA. 3236+21 RT. 450 LIN.FT.		1 EACH	1 EACH
STA. 3233+51 - STA. 3238+51 LT. 450 LIN.FT.		1 EACH	1 EACH

PLAN SHEETS STA. 3290+00 - STA. 3350+00

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110564		31	44

② PLAN SHEETS

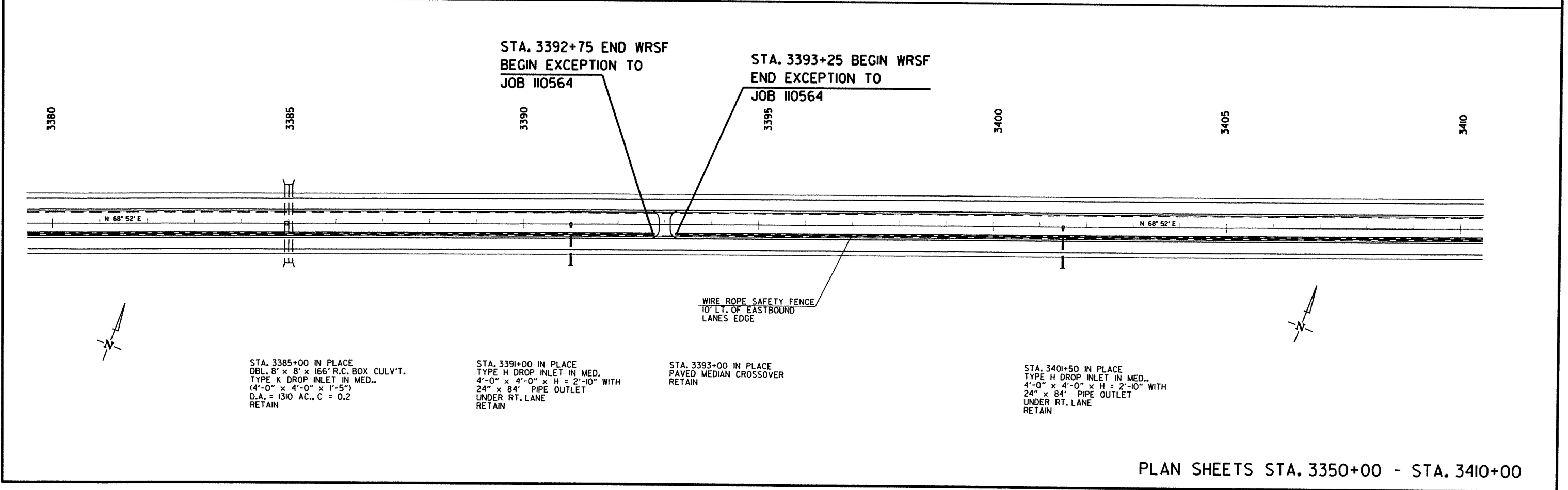


STA. 3357+00 IN PLACE
DROP INLET IN MED. WITH
24" x 88" R.C. PIPE OUTLET RT.
RETAIN

STA. 3363+790 IN PLACE
30" x 272" R.C. PIPE OUTLET RT.
(21" LT. FWD. SKEW)
NO DROP INLET INDICATED
RETAIN

STA. 3367+00 IN PLACE
DROP INLET IN MED. WITH
24" x 88" R.C. PIPE OUTLET RT.
RETAIN

STA. 3375+00 IN PLACE
DROP INLET IN MED. WITH
24" x 88" R.C. PIPE OUTLET RT.
RETAIN



STA. 3392+75 END WRSF
BEGIN EXCEPTION TO
JOB 110564

STA. 3393+25 BEGIN WRSF
END EXCEPTION TO
JOB 110564

STA. 3385+00 IN PLACE
DBL. 8' x 8' x 166" R.C. BOX CULV'T.
TYPE K DROP INLET IN MED..
(4'-0" x 4'-0" x 1'-5")
D.A. = 1310 AC., C = 0.2
RETAIN

STA. 3391+00 IN PLACE
TYPE H DROP INLET IN MED.
4'-0" x 4'-0" x H = 2'-10" WITH
24" x 84" PIPE OUTLET
UNDER RT. LANE
RETAIN

STA. 3393+00 IN PLACE
PAVED MEDIAN CROSSOVER
RETAIN

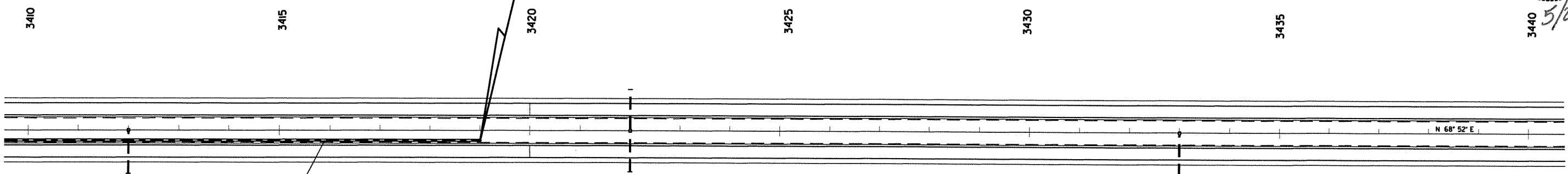
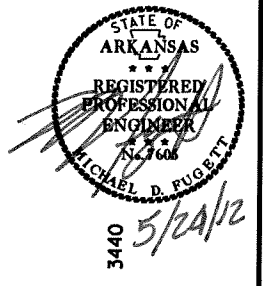
STA. 3401+50 IN PLACE
TYPE H DROP INLET IN MED..
4'-0" x 4'-0" x H = 2'-10" WITH
24" x 84" PIPE OUTLET
UNDER RT. LANE
RETAIN

PLAN SHEETS STA. 3350+00 - STA. 3410+00

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							110564	32	44

② PLAN SHEETS

STA. 3419+00
END JOB 110564
L.M. 228.20



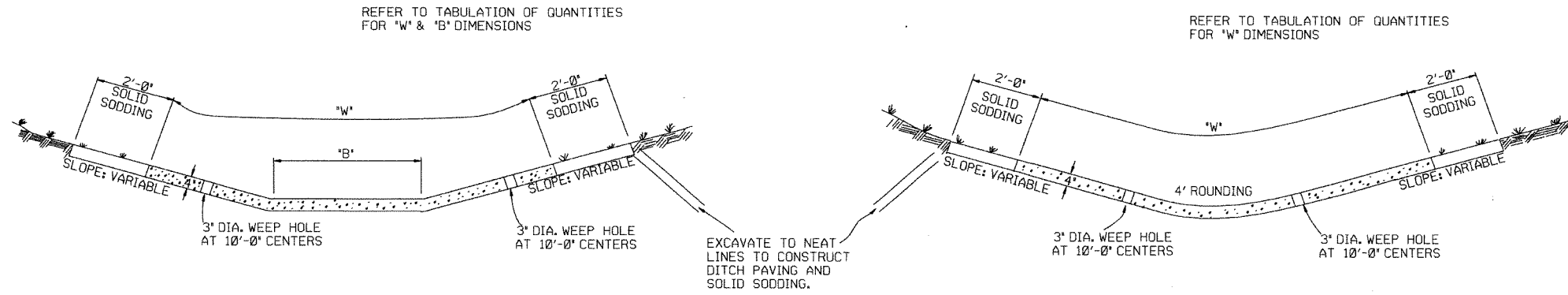
WIRE ROPE SAFETY FENCE
10' LT. OF EASTBOUND
LANES EDGE

STA. 3412+00 IN PLACE
TYPE H DROP INLET IN MED.,
4'-0" x 4'-0" x H = 2'-10" WITH
24" x 84" PIPE OUTLET
UNDER RT. LANE
RETAIN

STA. 3422+00 IN PLACE
TYPE H DROP INLET IN MED.,
4'-0" x 4'-8" x H = 3'-10" WITH
30" x 164" PIPE CULV'T
RETAIN

STA. 3433+00 IN PLACE
TYPE H DROP INLET IN MED.,
4'-0" x 4'-0" x H = 2'-10" WITH
24" x 84" PIPE OUTLET
UNDER RT. LANE
RETAIN

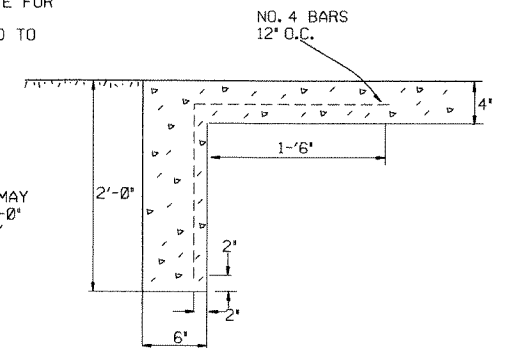
r110564.dgn/PLAN 05/22/2012



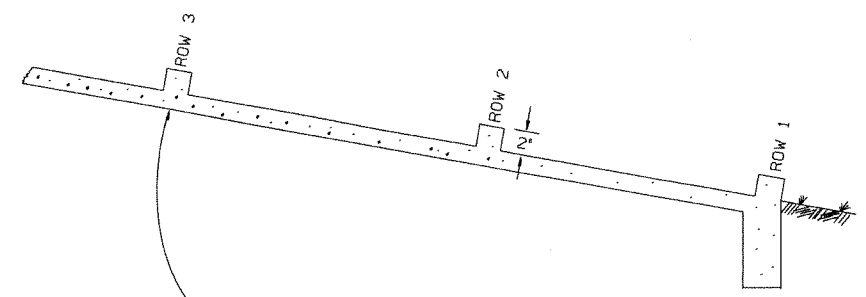
TYPE A

TYPE B

THE STEEL AND ADDITIONAL CONCRETE FOR THE WALLS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR 'CONCRETE DITCH PAVING.'

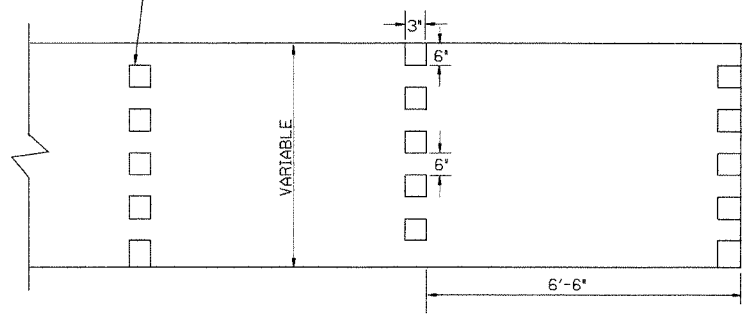


TOE WALL DETAIL FOR CONCRETE DITCH PAVING



NUMBER OF ELEMENTS PER ROW VARIES WITH WIDTH OF PAVING SPECIFIED

ENERGY DISSIPATORS TO BE USED FOR THE ENTIRE LENGTH OF DITCH WHEN SLOPE OF DITCH PAVING EXCEEDS 7%. THE DISSIPATORS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR CONCRETE DITCH PAVING.



ENERGY DISSIPATORS (NO SCALE)

GENERAL NOTES:

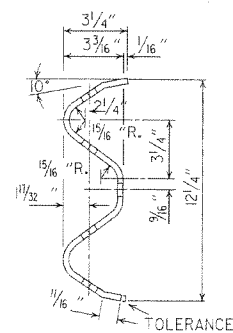
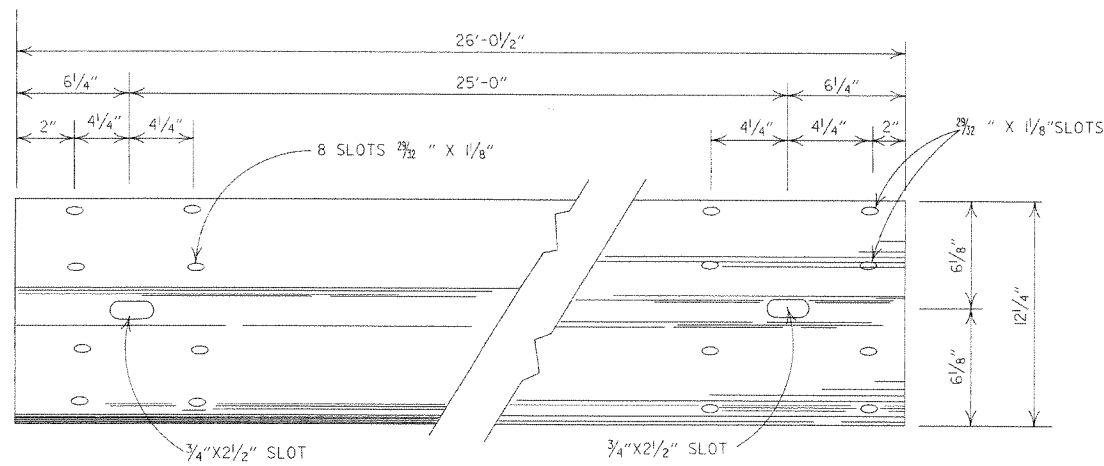
- THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.
- TOE WALLS TO BE CONSTRUCTED FULL WIDTH AT EACH END OF DITCH PAVING, AND POURED MONOLITHICALLY.
- SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.
- 1' WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.

DATE	REVISION	DATE FILM'D
11-17-10	ADDED GENERAL NOTE	
6-2-94	ADDED GENERAL NOTE ABOUT SOLID SODDING	
11-30-88	ELIMINATED MIN. ROWS OF ELEMENTS	11-30-89
7-15-88	REVISED DISSIPATOR NOTE	653-7-15-88
4-3-87	REVISED ENERGY DISSIPATOR	671-4-3-87
1-9-87	MODIFIED NOTE ON ENERGY DISS.	532-1-9-87
11-3-86	ADDED NOTE TO ENERGY DISS.	599-12-1-86
11-1-84	ENERGY DISSIPATOR DETAILS	508-11-1-84
11-1-84	ADDED EXCAVATION DETAILS	
10-2-72	TYPED A & B	
10-2-72	REVISED AND REDRAWN	508-10-2-72

ARKANSAS STATE HIGHWAY COMMISSION

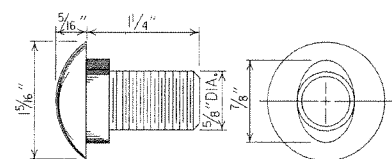
CONCRETE DITCH PAVING

STANDARD DRAWING CDP-1

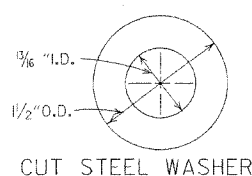


DETAILS OF W-BEAM GUARD RAIL

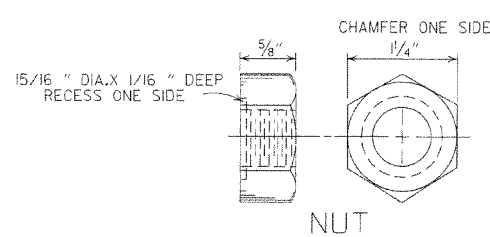
RAIL SECTION OF CLOSELY SIMILAR DIMENSIONS AND COMPARABLE STRENGTH MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.



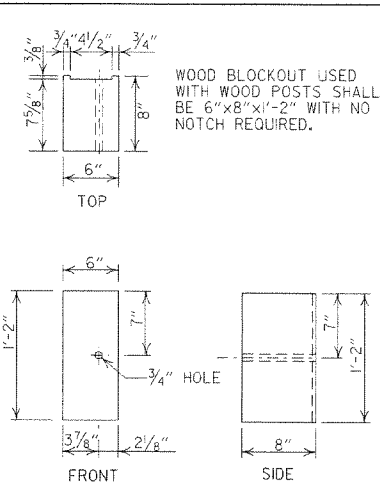
SPLICE BOLT
POST BOLT - SAME EXCEPT LENGTH



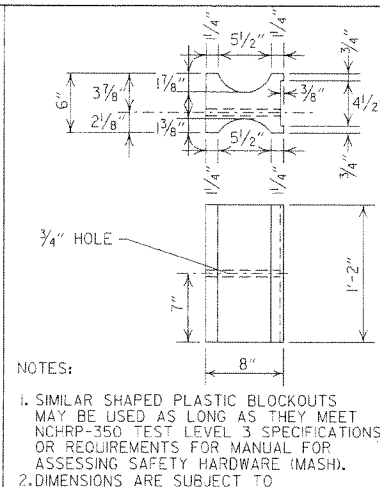
CUT STEEL WASHER



NUT

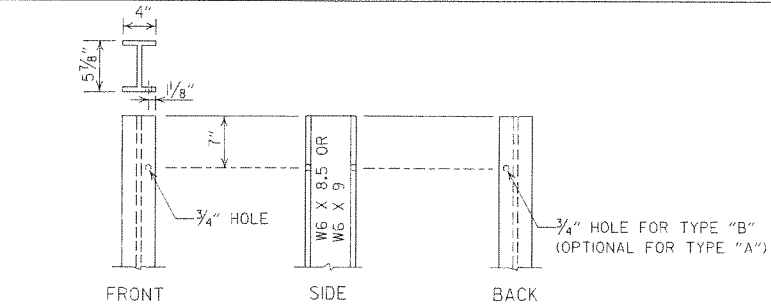


WOOD BLOCKOUT (W-BEAM)

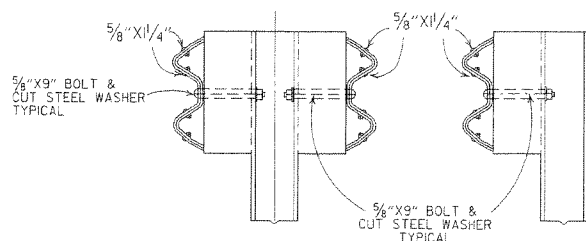


PLASTIC BLOCKOUT (W-BEAM)

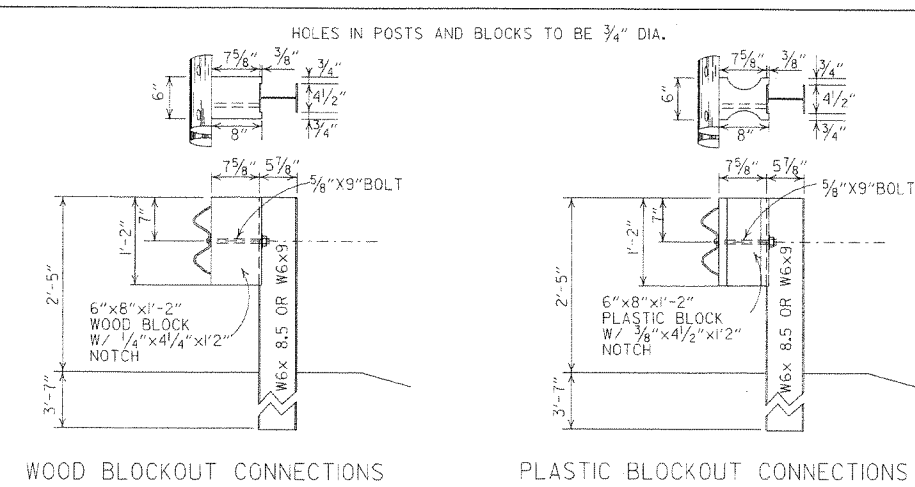
NOTES:
1. SIMILAR SHAPED PLASTIC BLOCKOUTS MAY BE USED AS LONG AS THEY MEET NCHRP-350 TEST LEVEL 3 SPECIFICATIONS OR REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
2. DIMENSIONS ARE SUBJECT TO MANUFACTURERS TOLERANCES.



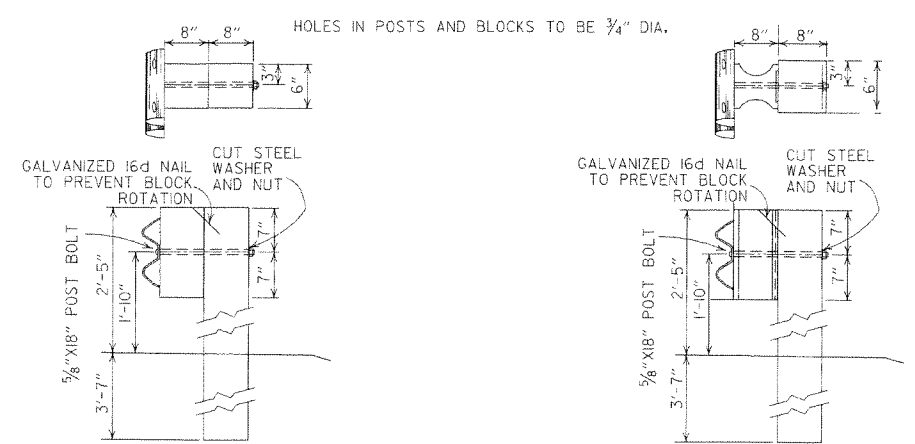
STEEL POST



TYPE "B" TYPE "A"
DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



WOOD BLOCKOUT CONNECTIONS PLASTIC BLOCKOUT CONNECTIONS
DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



WOOD BLOCKOUT CONNECTIONS PLASTIC BLOCKOUT CONNECTIONS
DETAILS OF WOOD LINE POST CONNECTIONS (W-BEAM)

-GENERAL NOTES-

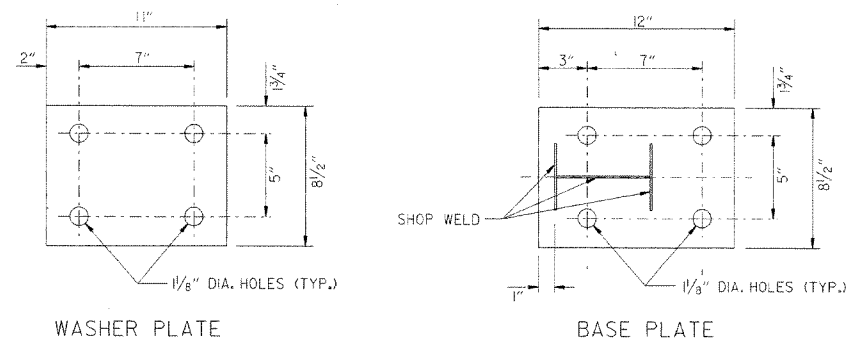
ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.
WHERE W-BEAM GUARD RAIL CONTINUES, THE INTERMEDIATE SECTIONS SHALL HAVE A POST SPACING OF 6'-3" UNLESS OTHERWISE NOTED.
W-BEAM GUARD RAIL REPRESENTING INTERMEDIATE SECTIONS WILL BE MEASURED ALONG THE ROADWAY FACE FROM CENTERLINE OF POST TO CENTERLINE OF POST.
USE W-BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. FOR EXTENSIONS OR MODIFICATION OF EXISTING GUARD RAIL, W-BEAM GUARD RAIL COMPONENTS OF THE SAME TYPE AS THOSE EXISTING SHALL BE USED.
ANY BACKFILLING UNDER OR AROUND POST SHALL BE DAMP SAND THOROUGHLY TAMPED IN PLACE.
WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1 1350 f SOUTHERN PINE.
CONTRACTOR SHALL HAVE THE OPTION OF USING WOOD BLOCKOUTS FOR W-BEAM GUARD RAIL OR PLASTIC BLOCKOUTS, AS LONG AS BLOCKOUT USED MEETS NCHRP-350 TEST LEVEL 3 SPECIFICATIONS OR REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) FOR W-BEAM GUARD RAIL.

7-4-10	RAISED HEIGHT OF GUARD RAIL 1"	
10-15-09	ADDED REFERENCE TO MASH	
4-10-03	REVISED GENERAL NOTES	
8-22-02	REVISED DIMENSION ON WOOD & PLASTIC BLOCKOUT CONNECTIONS & ON STEEL POST	
11-16-01	REVISED WOOD BLOCKOUT & DETAILS OF WOOD LINE POST CONNECTIONS	
3-30-00	REMOVED GUARD RAIL AT BRIDGE ENDS	
1-12-00	ADDED PLASTIC BLOCKOUT	
8-12-98	REV. BLOCKOUTS TO WOOD, DELETED CONC. POST & REV. GENERAL NOTE, DELETED DET. OF GUARD RAIL REPLACE. BEHIND CURB & DET. OF POST PLACE. IN SOLID ROCK. & ADDED DETAILS OF STEEL LINE POST CONN. REMOVED BACK-UP PLATE, REVISED HOLES IN STEEL POLES	
4-3-97	REMOVED "LAP IN DIRECTION OF TRAFFIC" NOTE & PLACED ARROWS ON WASHERS	
10-18-96	REVISED WOOD POST NOTE	
6-2-94	ADDED ALT. STEEL POST SIZE	
8-5-93	REVISED STEEL POST SIZE	8-5-93
10-1-92	REDRAWN & REVISED	10-1-92
8-15-91	REVISED WASHER NOTE	8-5-91
8-2-90	REV. GEN. NOTE & DEPTH OF ANC. POST IN ROCK	8-2-90
7-15-88	REVISED SECTION 3 & GENERAL NOTES	
3-4-88	REV. ANCHOR POST, ELEV. NOTES & POST IN ROCK	780-3-4-88
10-30-87	REVISED WOOD LINE POST DETAIL	546-10-30-87
10-9-87	REDRAWN & REVISED	802-10-9-87
DATE	REVISION	DATE FILM

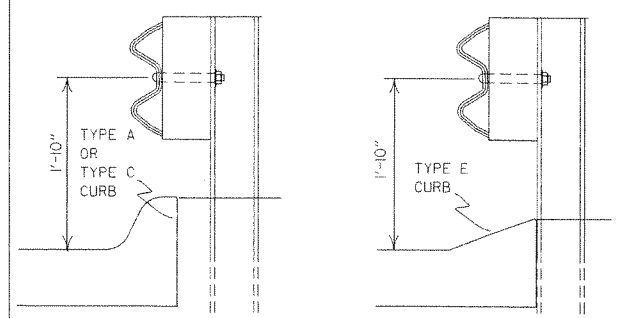
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

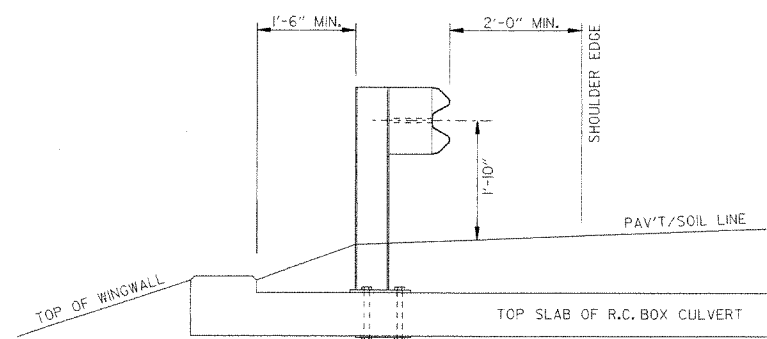
STANDARD DRAWING GR-8



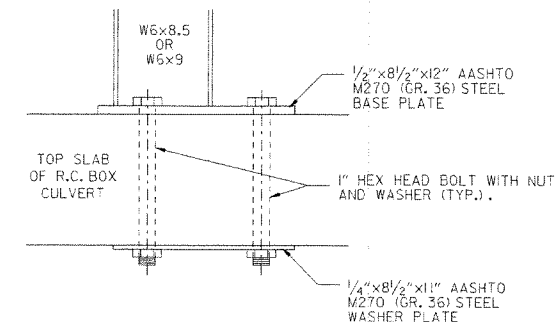
Note: Bolts, nuts, washers and plates shall be galvanized in accordance with Section 807 of the Standard Specifications.



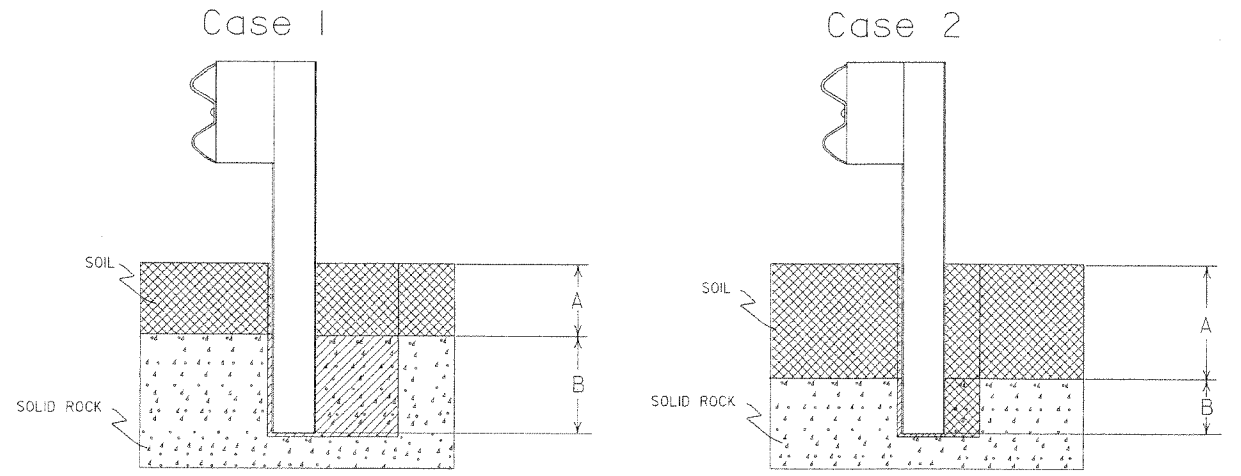
FOR DESIGN SPEEDS OF 50 MPH OR LESS ALL CURB FACES, AS SHOWN ON STD. DRWG. CG-1, MAY BE USED. FOR DESIGN SPEEDS OF 55 MPH OR MORE TYPE "E" CURB FACE SHALL BE USED.



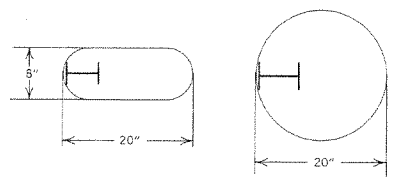
SECTION A-A



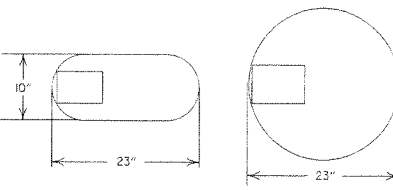
DETAIL OF CONNECTION



Plan View Steel Posts
 Either hole configuration acceptable



Plan View Wood Posts
 Either hole configuration acceptable



Notes: For overlying soil depths (A) ranging from 0 to 18", the depth of required drilling (B) is equal to 24".

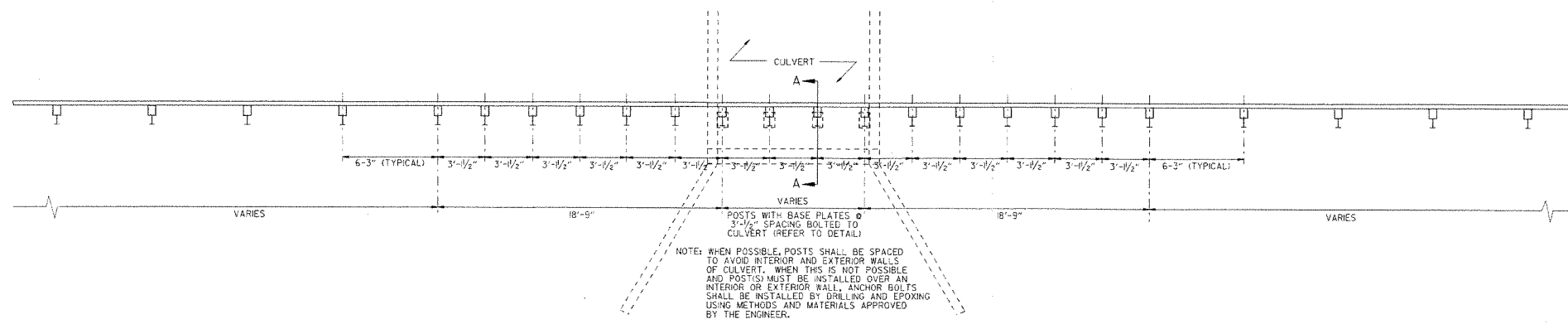
Zone A: Backfill according to Section 617.03(a).

Zone B: Backfill hole in 6" lifts with material meeting the requirements of Section 802.02(c) - Alternate gradation. Compact to 95% maximum dry density per ASTM D-698.

Notes: For overlying soil depths (A) ranging from 18" to 44", the depth of required drilling (B) is equal to either 12" or 44" minus the depth of soil whichever is less.

Zone A & B: Backfill according to Section 617.03(a).

DETAIL OF POST PLACEMENT IN SOLID ROCK (W-BEAM)



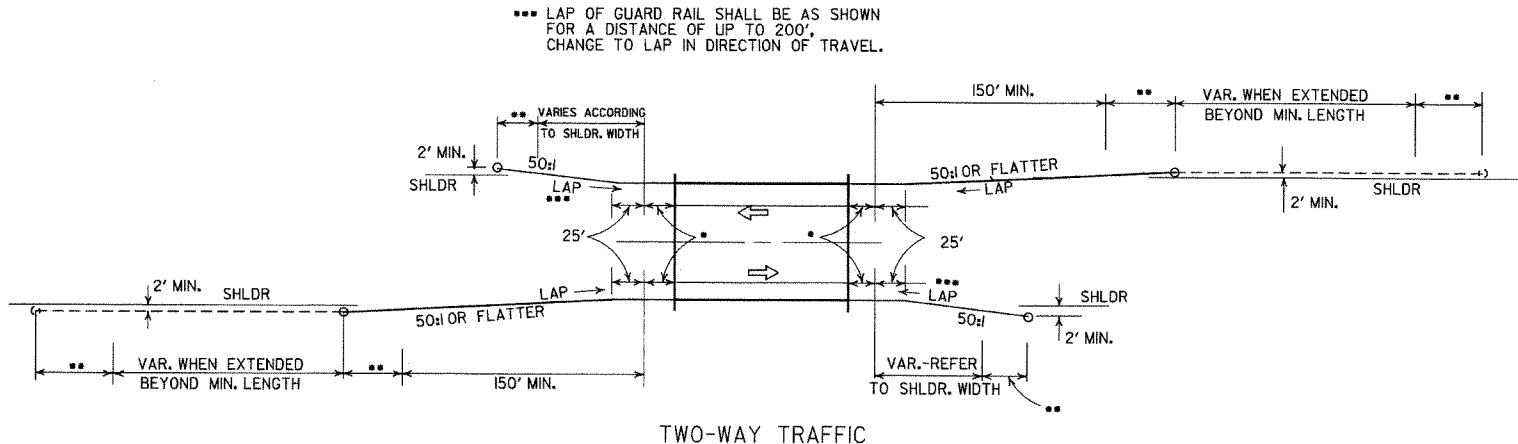
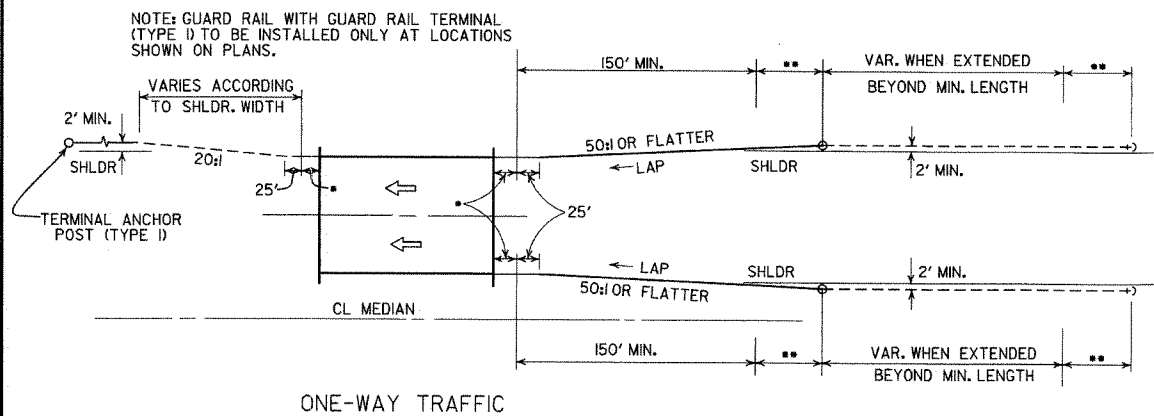
PLAN LAYOUT OF TYPE A GUARD RAIL AT LOW-FILL CULVERTS
 NOTE: THIS DETAIL IS TO BE USED ONLY WHEN THE COVER OVER THE CULVERT DOES NOT PERMIT FULL EMBEDMENT OF GUARD RAIL POSTS AS SHOWN ON STD. DRWG. GR-8.

7-14-10	RAISED HEIGHT OF GUARD RAIL 1"	
4-12-07	REVISED DETAIL OF GUARD RAIL PLACEMENT BEHIND CURB	
11-10-05	ADDED GUARD RAIL PLACEMENT BEHIND CURB; REVISED DETAIL OF CONNECTION	
11-18-04	REVISED POST PLACEMENT IN ROCK & CULVERT CONNECTION DETAILS. ADDED DETAIL FOR GUARD RAIL PLACEMENT AT LOW-FILL CULVERTS	
3-30-00	REMOVED CONCRETE INSERT ANCHOR	
8-12-98	CHANGED STEEL SPACER BLOCK TO WOOD BLOCKOUT, ADD DET. OF GUARD RAIL CONNECTION TO R.C. BOX CULVERT. DELETED DET. OF STEEL LINE POST CONN. & ADDED DET. OF GUARD RAIL PLACE. BEHIND CURB & DET. OF POST PLACE. IN SOLID ROCK	
4-3-96	PLACED ARROWS AT CUT STEEL WASHERS	4-3-96
10-16-96	REV. ASTM REF. TO AASHTO	
11-22-95	ADDED OPTIONAL HOLES	
6-2-94	REVISED ALTERNATE POST SIZE	
8-5-93	REVISED STEEL POST SIZE	
10-1-92	REDRAWN & REVISED	10-1-92
8-2-90	DEL. WASHER ON ANCHOR ASSEMBLY	8-2-90
7-15-88	CONFORMED TO 1988 SPECS	
3-4-88	REVISED ANCHOR NOTE	
10-30-87	REVISED ANCHOR ASSEMBLY	712-10-30-87
10-30-87	REVISED PLACEMENT BEHIND CURB	547-10-30-87
10-9-87	REDRAWN & REVISED	803-10-9-87
DATE	REVISION	DATE/FIRM

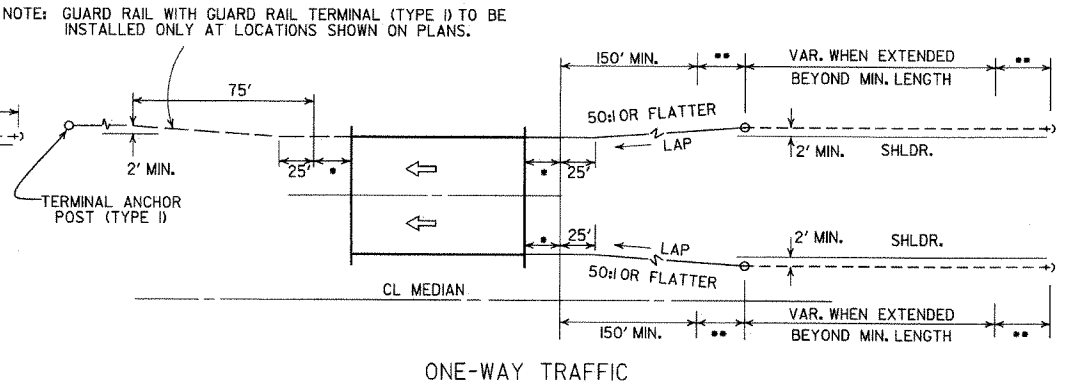
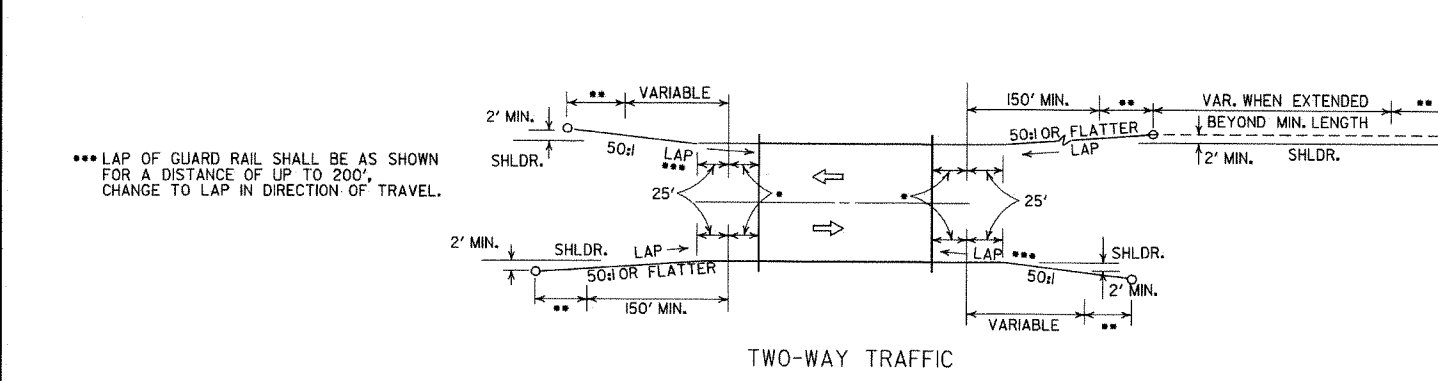
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

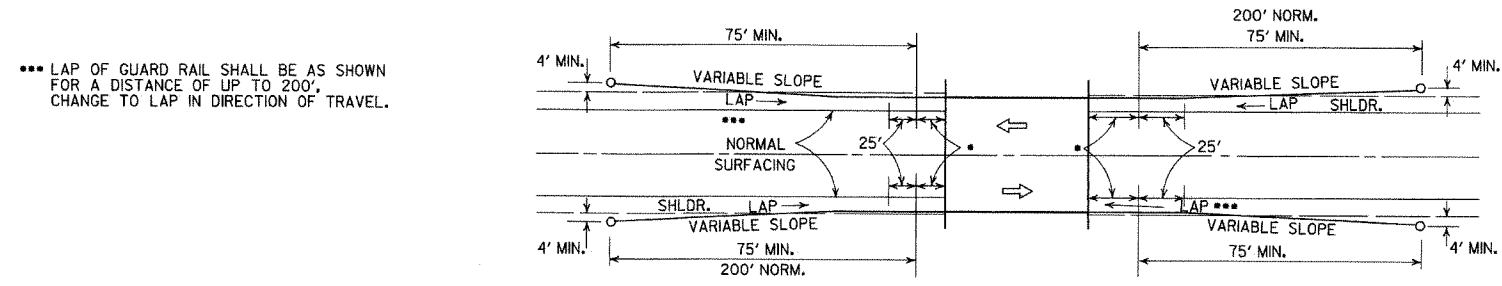
STANDARD DRAWING GR-8A



METHODS OF INSTALLATION OF GUARD RAIL AT LESS THAN FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)



METHOD OF INSTALLATION OF GUARD RAIL AT FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)

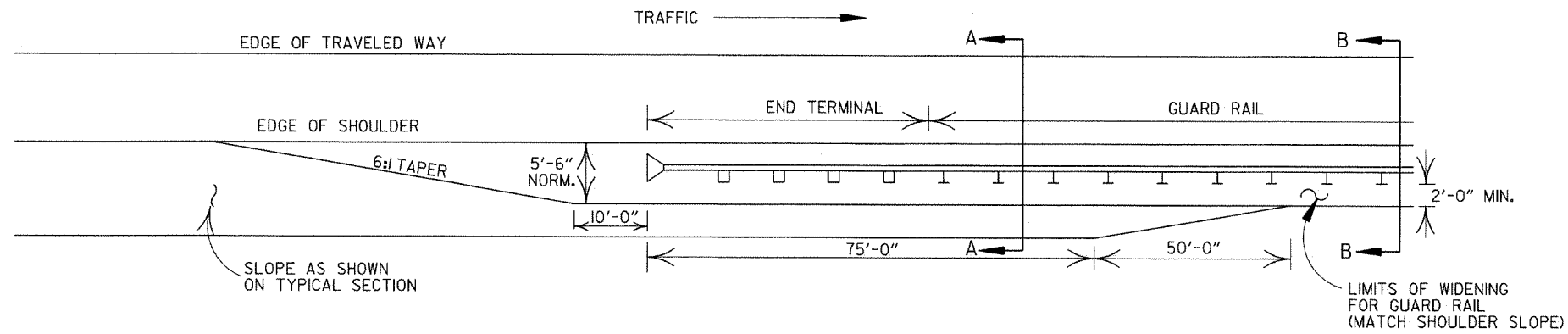


LEGEND

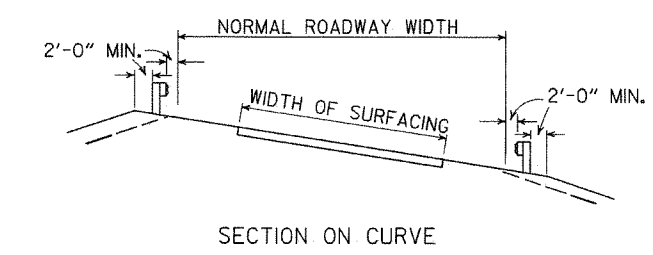
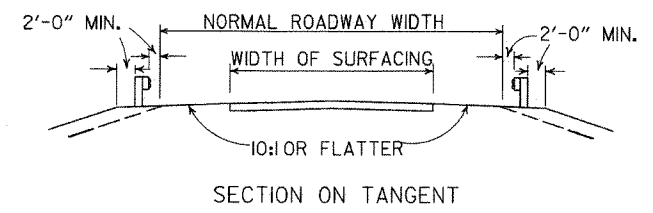
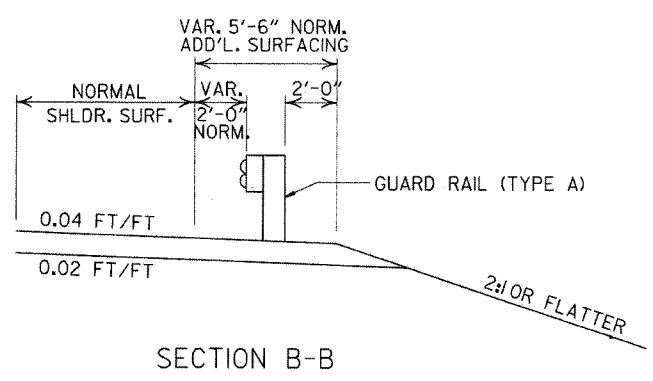
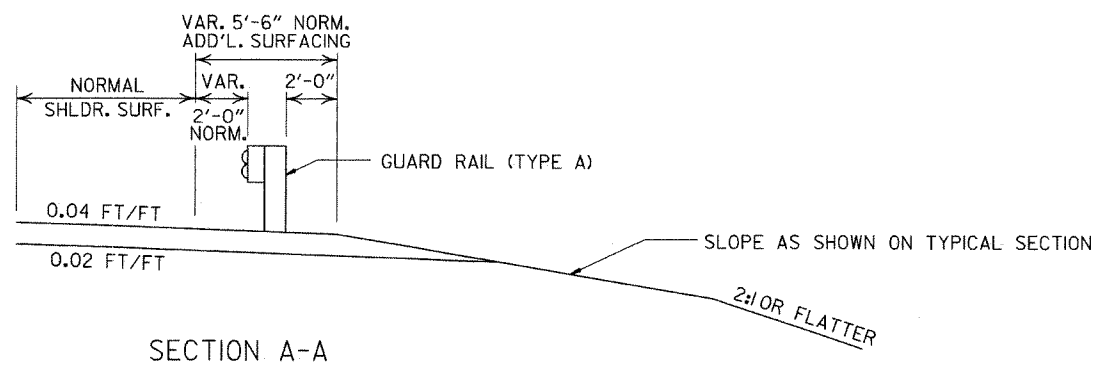
- THRIE BEAM GUARD RAIL TERMINAL
- GUARD RAIL TERMINAL (TYPE 2)

METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERMINAL (TYPE 1) (FULL SHOULDER WIDTH OR LESS BRIDGES)

ARKANSAS STATE HIGHWAY COMMISSION		
GUARD RAIL DETAILS		
STANDARD DRAWING GR-9		
4-17-08	REVISED LAYOUTS	
11-10-05	REMOVED GUARD RAIL NOTES AND DETAILS	
11-16-01	DELETED NOTE-METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERM. (TY. 1)	
1-12-00	ADDED CONSTRUCTION NOTE	1-12-00
6-26-97	REVISED LAYOUT	
10-1-92	REDRAWN & REVISED	10-1-92
10-9-87	ADDED NOTE	
10-9-87	REDRAWN & REVISED	
DATE	REVISION	DATE FILM

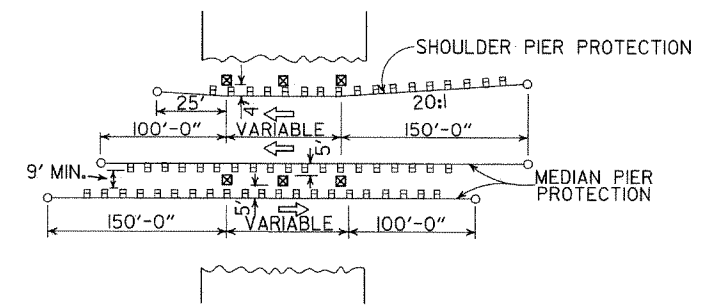


NOTE: NORMAL SECTION TO BE WIDENED APPROX. 5'-6" EACH SIDE TO SUPPORT GUARD RAIL.



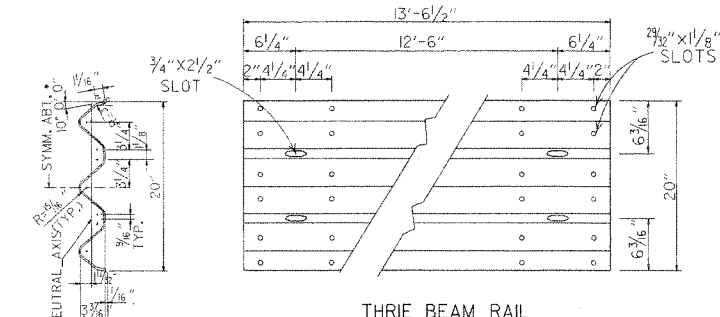
DETAILS OF WIDENING FOR GUARD RAIL

DETAILS SHOWING POSITION OF GUARD RAIL ON HIGHWAY

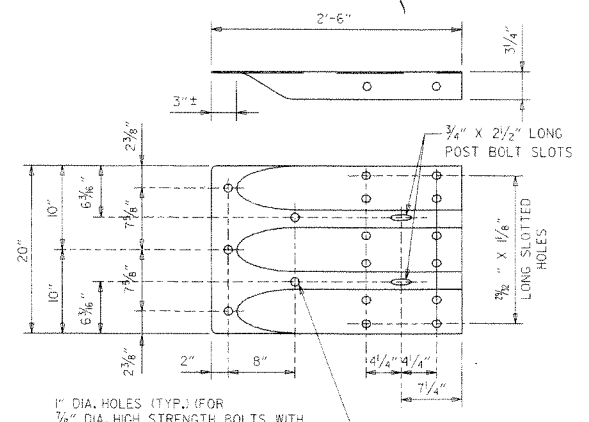


METHOD OF INSTALLATION OF GUARD RAIL AT FIXED OBSTACLE

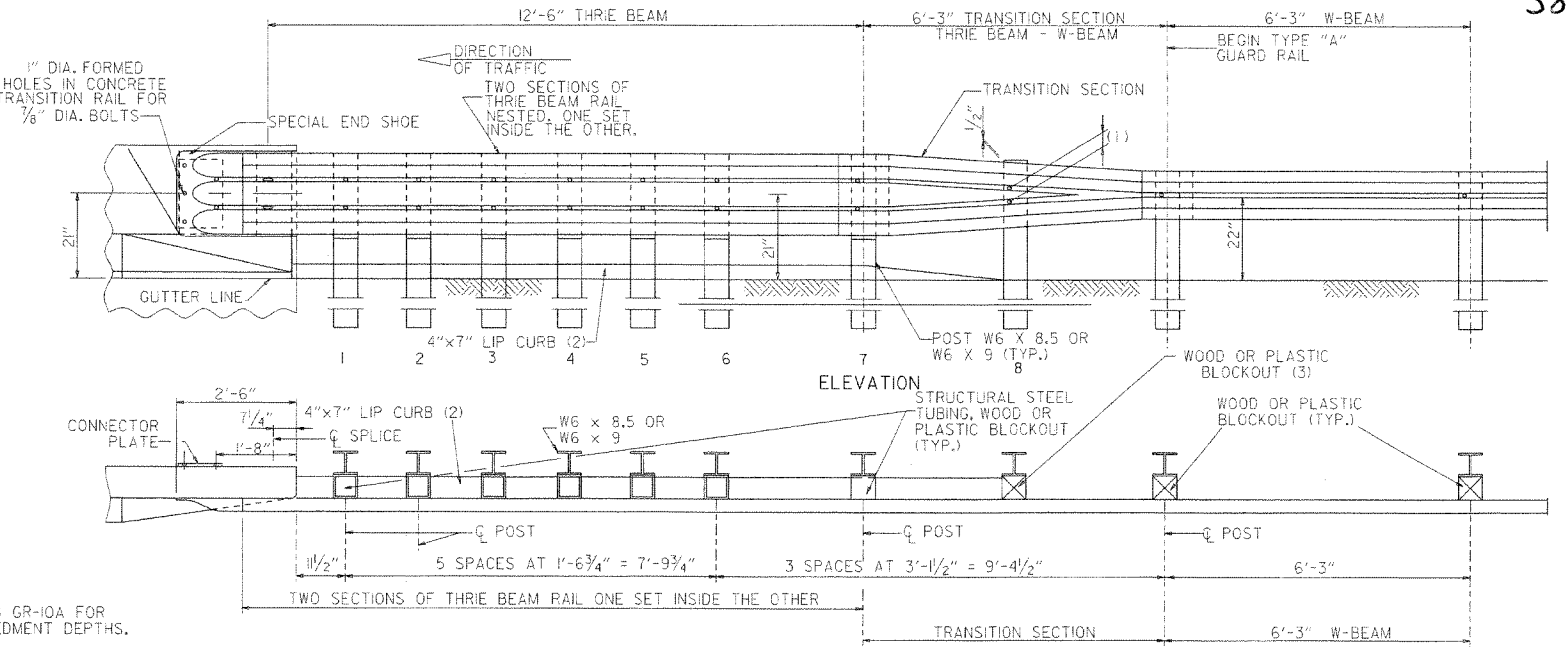
				ARKANSAS STATE HIGHWAY COMMISSION	
				GUARD RAIL DETAILS	
				STANDARD DRAWING GR-9A	
4-17-08	MINOR REVISION				
11-10-05	DRAWN				
DATE	REVISION			DATE	FILM



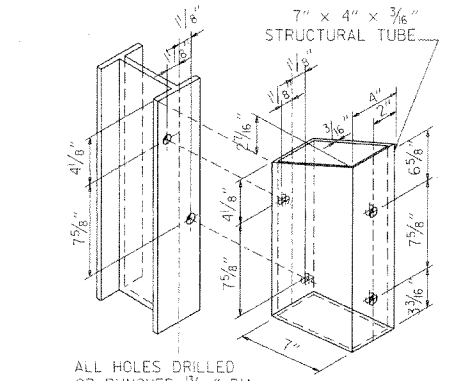
SECTION THRU THRIE BEAM RAIL



SPECIAL END SHOE



ELEVATION

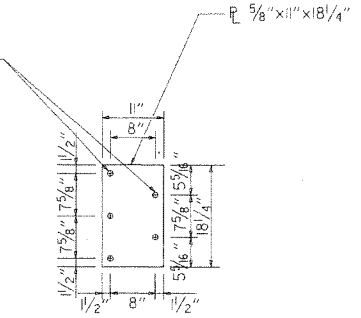


STRUCTURAL STEEL TUBING BLOCKOUT DETAIL

ATTACH BLOCKOUT TO POST USING 3/8" DIA. HEX HEAD BOLTS WITH 1/2" O.D. CUT STEEL WASHERS AND NUT.

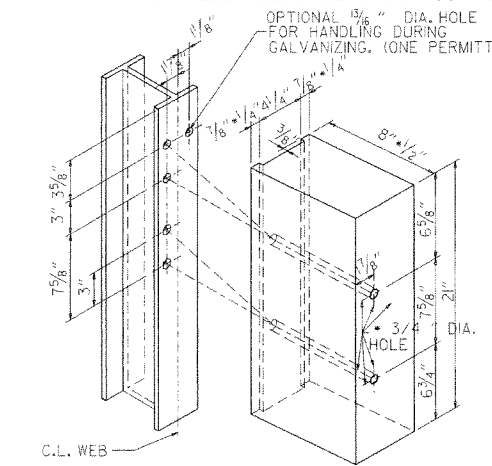
1" DIA. HOLES (TYP.) FOR 7/8" DIA. HIGH-STRENGTH BOLTS

NOTE: SEE STANDARD DRAWING GR-10A FOR GUARD RAIL POST EMBEDMENT DEPTHS.



CONNECTOR PLATE

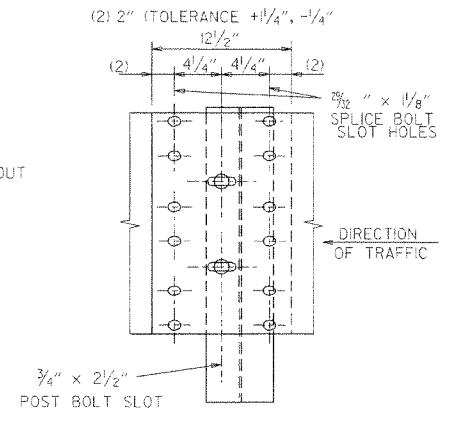
CONNECTOR PLATE SHALL BE AASHTO M270, GR. 36 AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING SHALL CONFORM TO SUBSECTION 807.19 OF THE STANDARD SPECIFICATIONS. CONNECTOR PLATE TO BE BOLTED TO SPECIAL END SHOE USING 3/8" DIA. HIGH STRENGTH BOLTS, WITH THE HEADS PLACED ON THE TRAFFIC FACE. WASHERS SHALL BE USED UNDER THE HEAD AND NUT. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED AND SHALL CONFORM TO SUBSECTION 807.06.



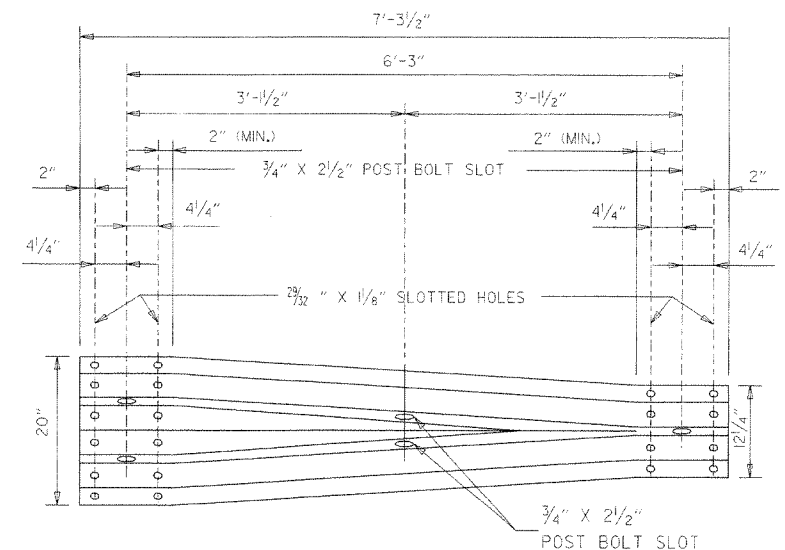
ALL HOLES 1/8" DIAMETER EXCEPT AS NOTED

HOLE PUNCHING DETAIL FOR STEEL POST & WOOD OR PLASTIC BLOCKOUTS

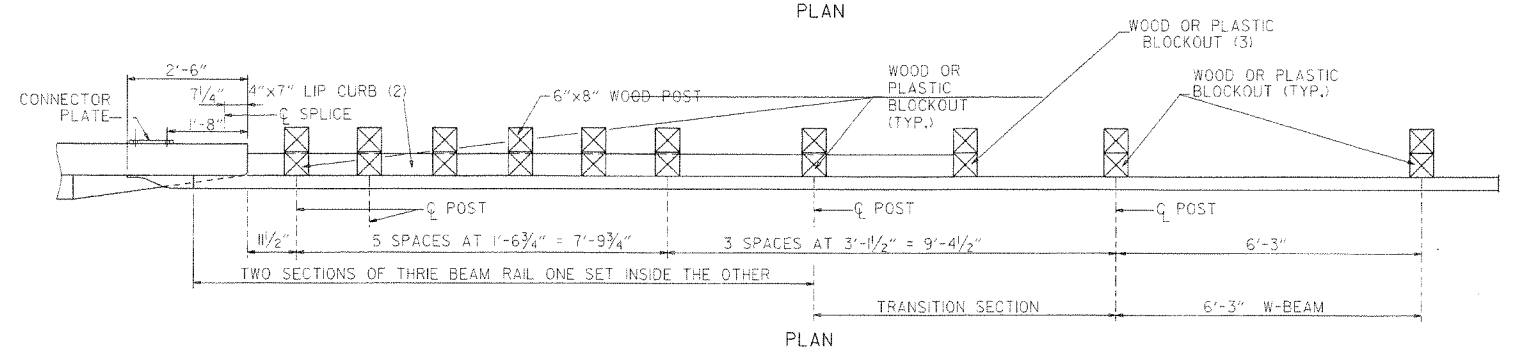
NOTE: BLOCKS SHALL BE THE SAME TYPE THROUGHOUT THE PROJECT LIMITS.



THRIE BEAM RAIL SPLICE AT POST



TRANSITION SECTION



PLAN

PLAN

- (1) VERIFY BOLT SPACING FROM RAIL TRANSITION PRODUCER.
- (2) REFER TO APPROACH GUTTER DETAILS.
- (3) LENGTH OF BLOCKOUT ON POST B TO BE MODIFIED TO FIT RAIL WIDTH.

THRIE BEAM GUARD RAIL CONNECTION AT BRIDGE ENDS

GENERAL NOTES:

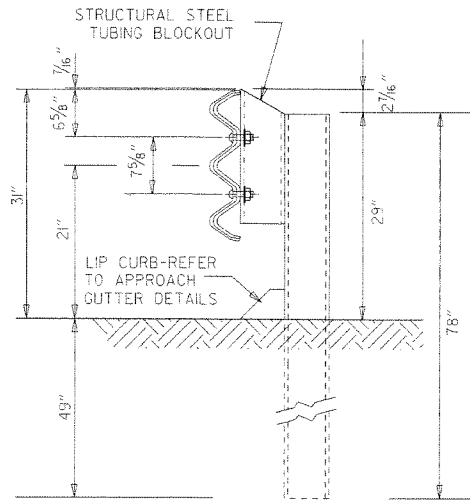
THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE I. RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION. ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT. ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-9 & GR-11. WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (1400 F) OR NO. 1 350 F SOUTHERN PINE. REFER TO STD. DRWG. GR-10A FOR POST DETAILS. USE THRIE BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W-BEAM POSTS FOR ENTIRE JOB.

DATE	REVISION	DATE FILM
7-14-10	RAISED HEIGHT OF W-BEAM 1"	
11-29-07	ADDED PLASTIC BLOCKOUTS	
11-10-05	ADDED NOTE FOR ATTACHING STEEL BLOCKOUT	
11-18-04	REVISED GENERAL NOTES	
10-9-03	REVISED GENERAL NOTES	
4-10-03	REVISED GENERAL NOTES	
8-22-02	REVISED NOTE (2)	
6-29-00	MOVED DIMENSION LINES	
5-18-00	ADDED NOTE	
3-30-00	DRAWN & ISSUED	

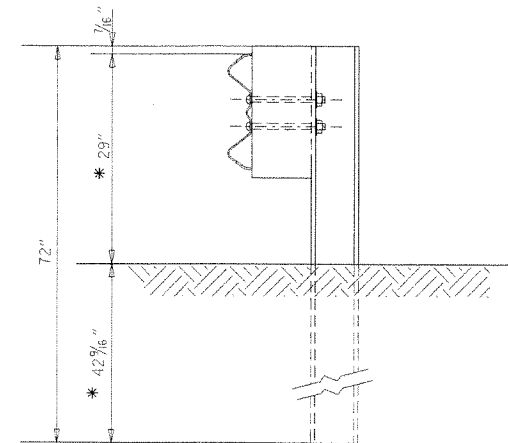
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

STANDARD DRAWING GR-10

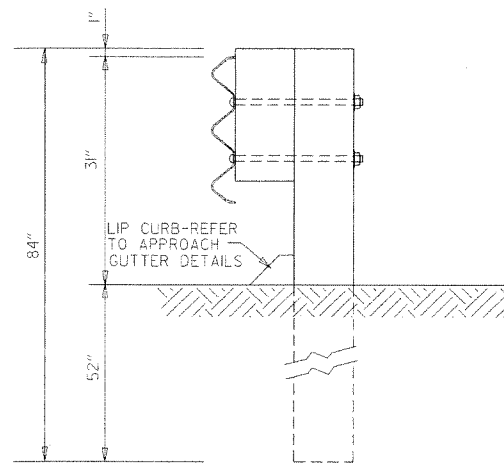


THRIE BEAM RAIL WITH STEEL TUBING BLOCKOUT AND STEEL POST
POSTS 1-7

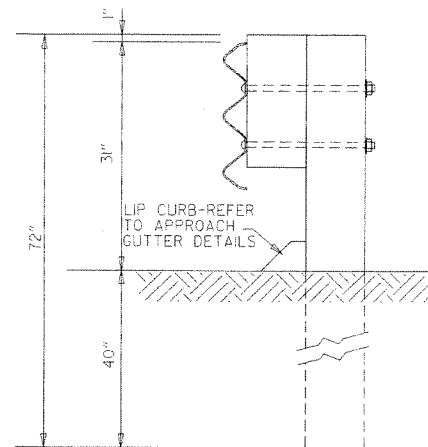


W-BEAM TO THRIE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT AND STEEL POST
POST 8

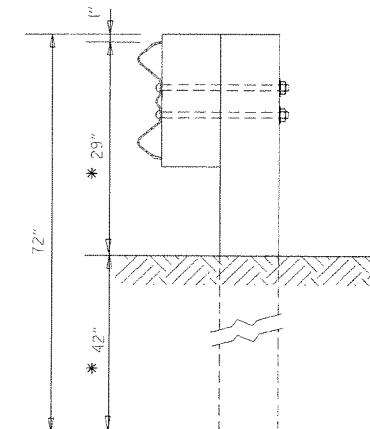
* NOTE:
THESE DIMENSIONS WILL NEED TO BE ADJUSTED IN THE FIELD TO MAKE THE TRANSITION FROM 21" MID POINT OF THRIE BEAM TO 22" MID POINT OF W-BEAM.



THRIE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUTS & WOOD POSTS
POSTS 1-6



THRIE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 7



W-BEAM TO THRIE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 8

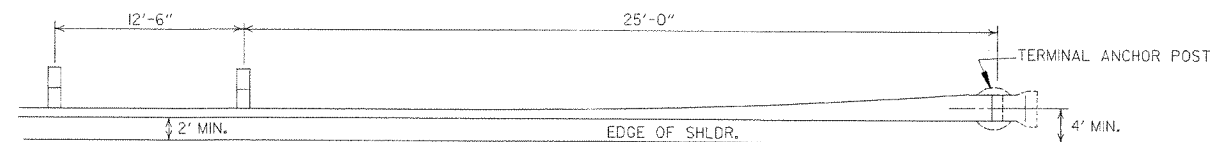
GENERAL NOTES:
RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.
WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (400 F) OR NO. 1 350 F SOUTHERN PINE.

ARKANSAS STATE HIGHWAY COMMISSION

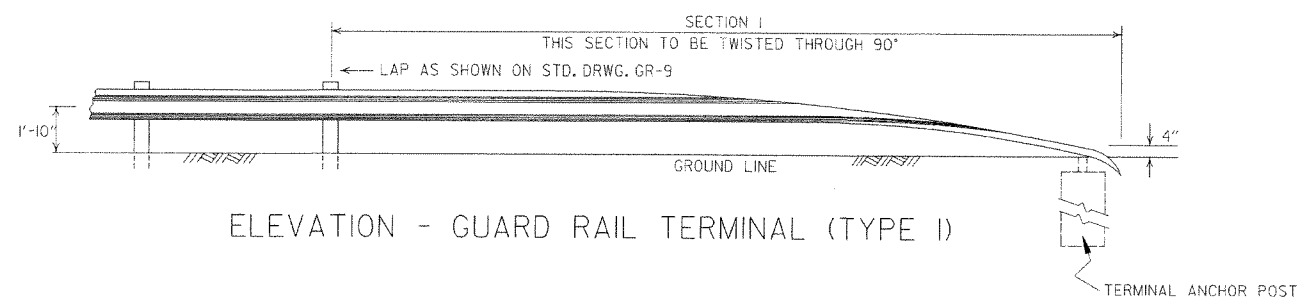
GUARD RAIL DETAILS

STANDARD DRAWING GR-10A

DATE	REVISION	DATE FILM
7-14-10	REVISED POST 8 DIMENSIONS	
11-29-07	ADDED PLASTIC BLOCKOUTS	
8-22-02	REVISED LIP CURB NOTE	
3-30-00	DRAWN & ISSUED	

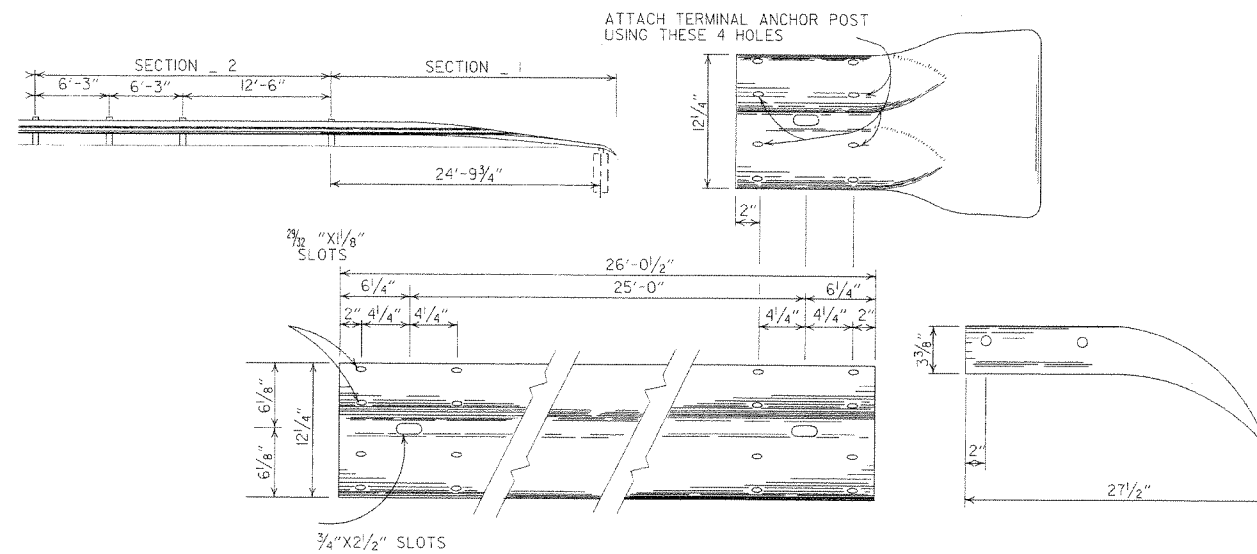


PLAN - GUARD RAIL TERMINAL (TYPE I)



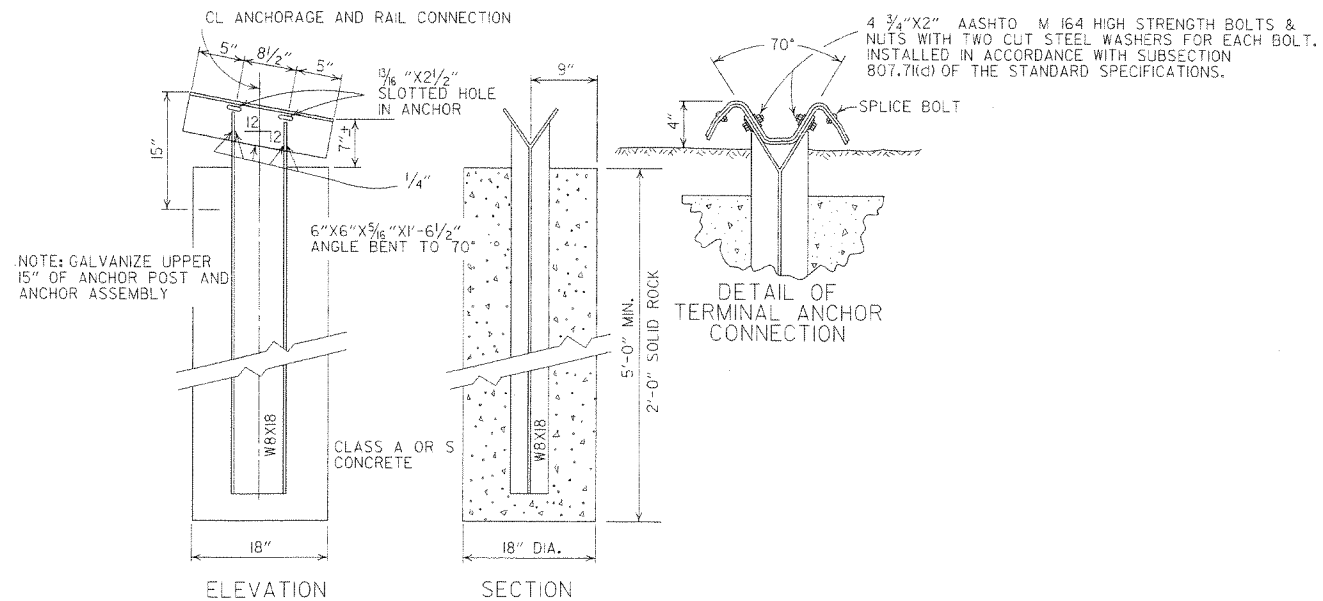
ELEVATION - GUARD RAIL TERMINAL (TYPE I)

NOTE:
SECTIONS 1 AND 2 OF GUARD RAIL TERMINAL SHALL BE PAID FOR AT THE PRICE BID PER LINEAR FOOT OF THE TYPE OF GUARD RAIL SPECIFIED.



SECTION 1

TERMINAL SECTION



ELEVATION

SECTION

DETAIL OF TERMINAL ANCHOR POST (TYPE I)

NOTE: RAIL MEMBERS MAY BE BOLTED TO ANGLE AT TERMINAL ANCHOR AND THE TWO ASSEMBLIES POSITIONED TO PROPER ALIGNMENT PRIOR TO PLACING CONCRETE AROUND 8 W/ 17 POST IF CONTRACTOR SO DESIRES.

			ARKANSAS STATE HIGHWAY COMMISSION
			GUARD RAIL DETAILS
			STANDARD DRAWING GRT-1
7-14-10	RAISED HEIGHT OF GUARD RAIL 1"		
6-26-97	REVISED LAP NOTE		
10-18-96	REVISED ASTM REF. TO AASHTO		
11-3-94	DIMENSION TERMINAL DETAIL		
11-11-92	ADDED NOTE FOR PAYMENT	11-11-92	
10-1-92	DRAWN & ISSUED	10-1-92	
DATE	REVISION	DATE FILM	

ADVANCE DISTANCES
(XXXX)

500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD


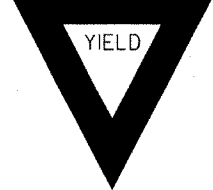
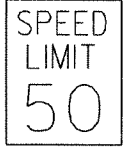
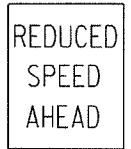





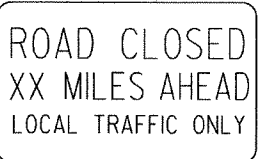
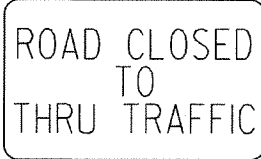
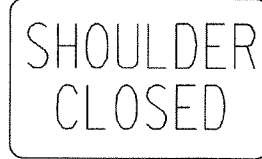
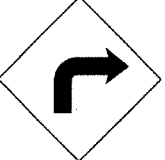





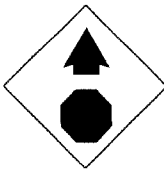
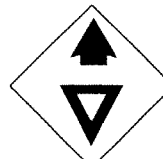
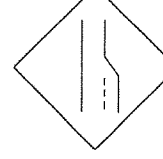



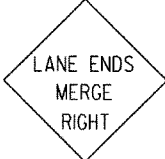


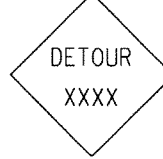






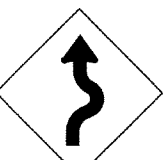



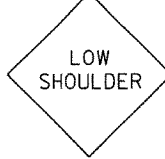
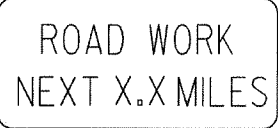
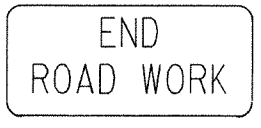
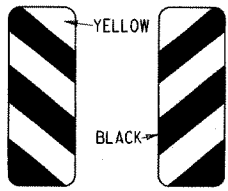


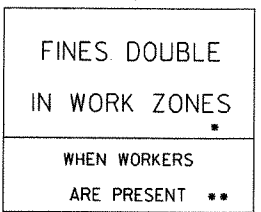
GENERAL NOTES:

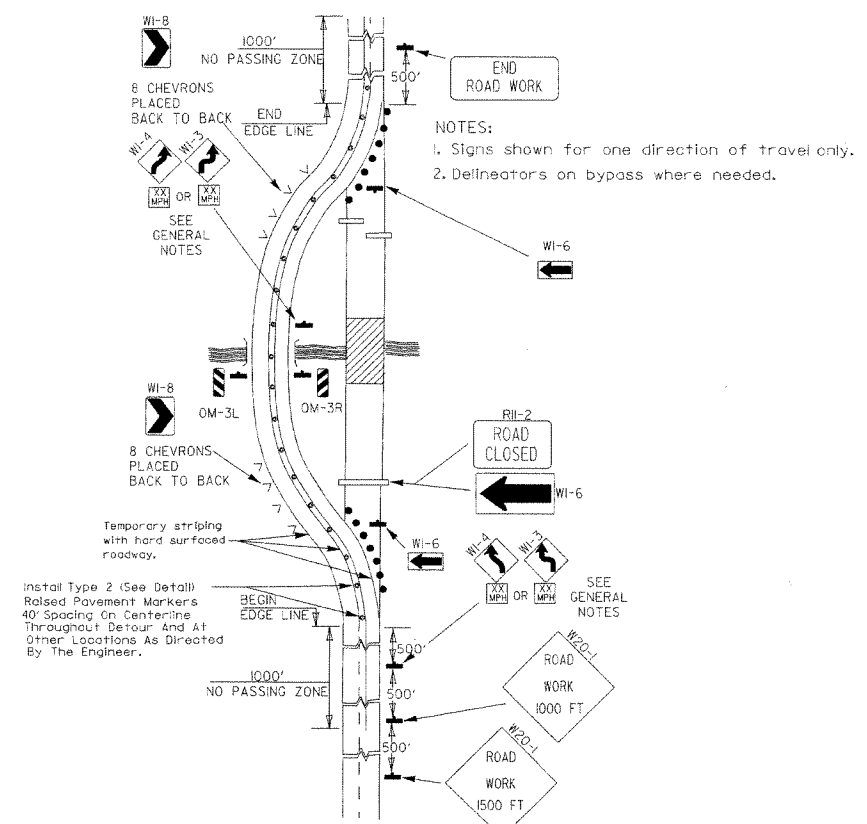
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. EXCEPT A MINIMUM OF 6" SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

* NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

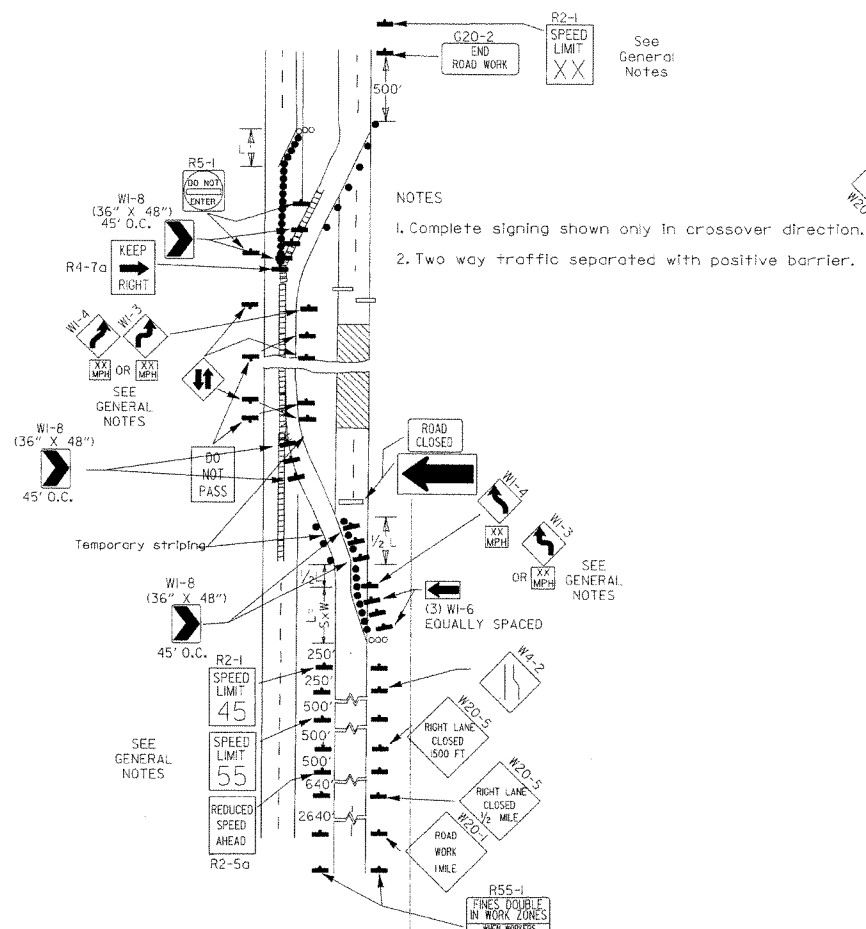
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
1-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-1

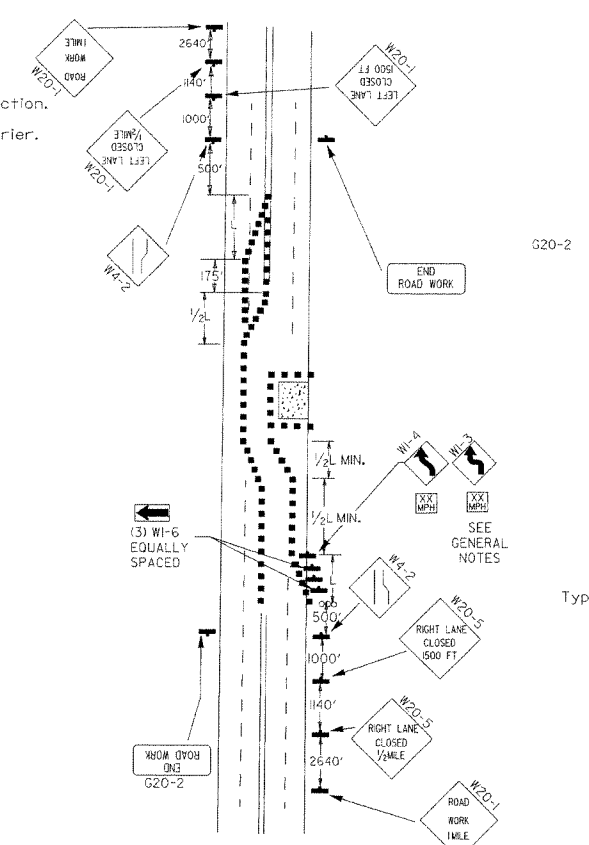
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>
<p>W20-3</p>  <p>STD. 48"x48"</p>	<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>
<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>	<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>
<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>* USE 6" C LETTERS ** USE 4" D LETTERS</p>				



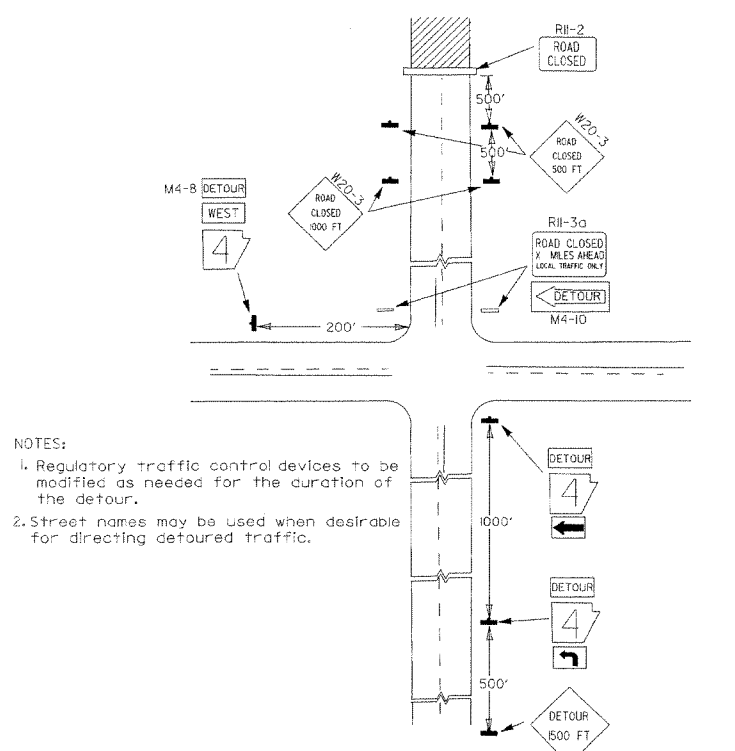
(A) Typical application of traffic control devices on a 2-lane highway where the entire roadway is closed and a bypass detour is provided.



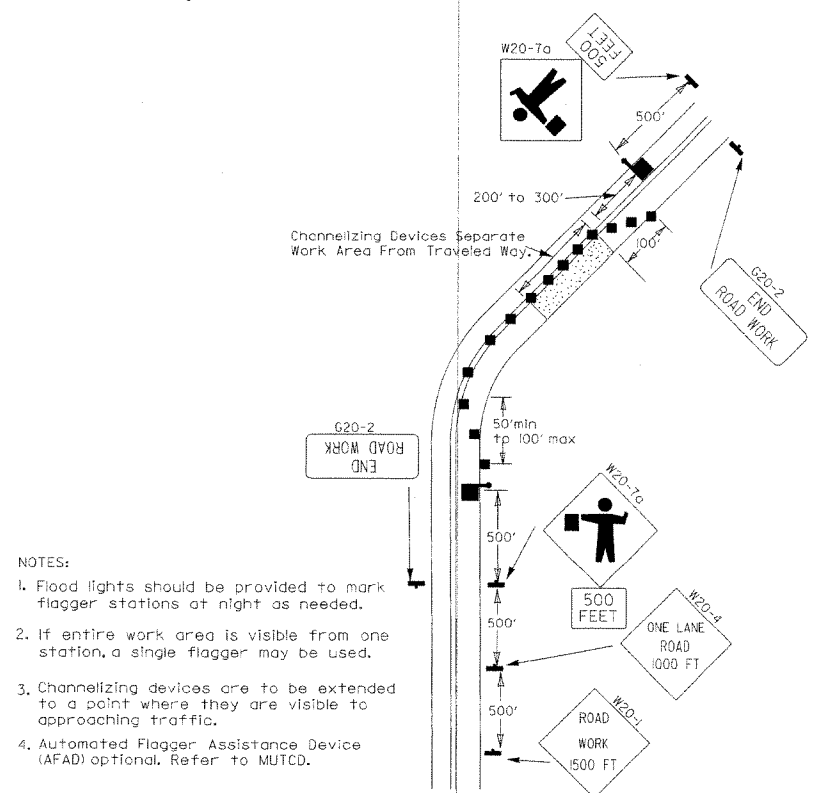
(B) Typical application - 4-lane divided roadway where one roadway is closed.



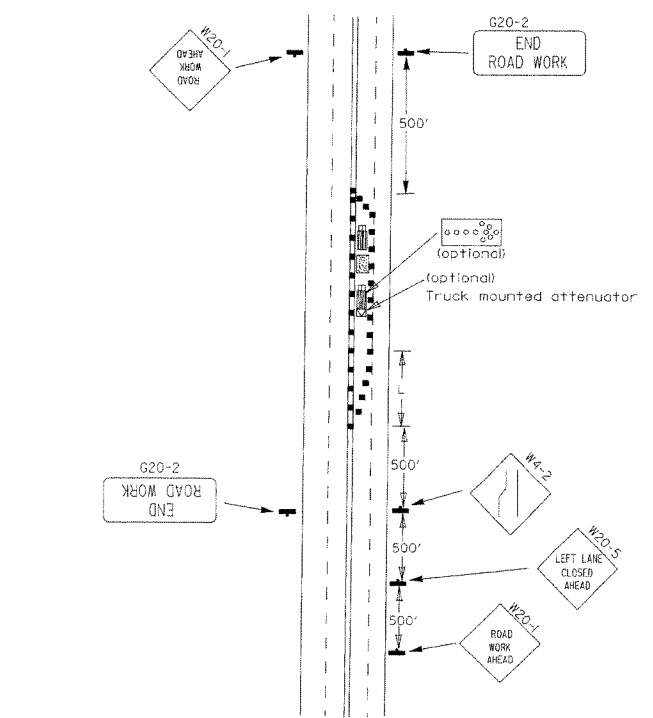
(C) Typical application - 4-lane undivided roadway where half of the roadway is closed.



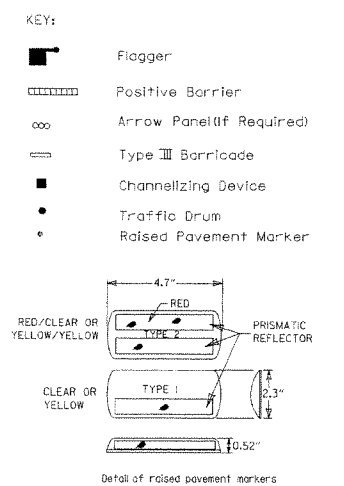
(D) Typical application - roadway closed beyond detour point.



(E) Typical application of traffic control devices on 2-lane highway where one lane is closed and flagging is provided.



(F) Typical application - 4-lane undivided roadway with inside lane closed.



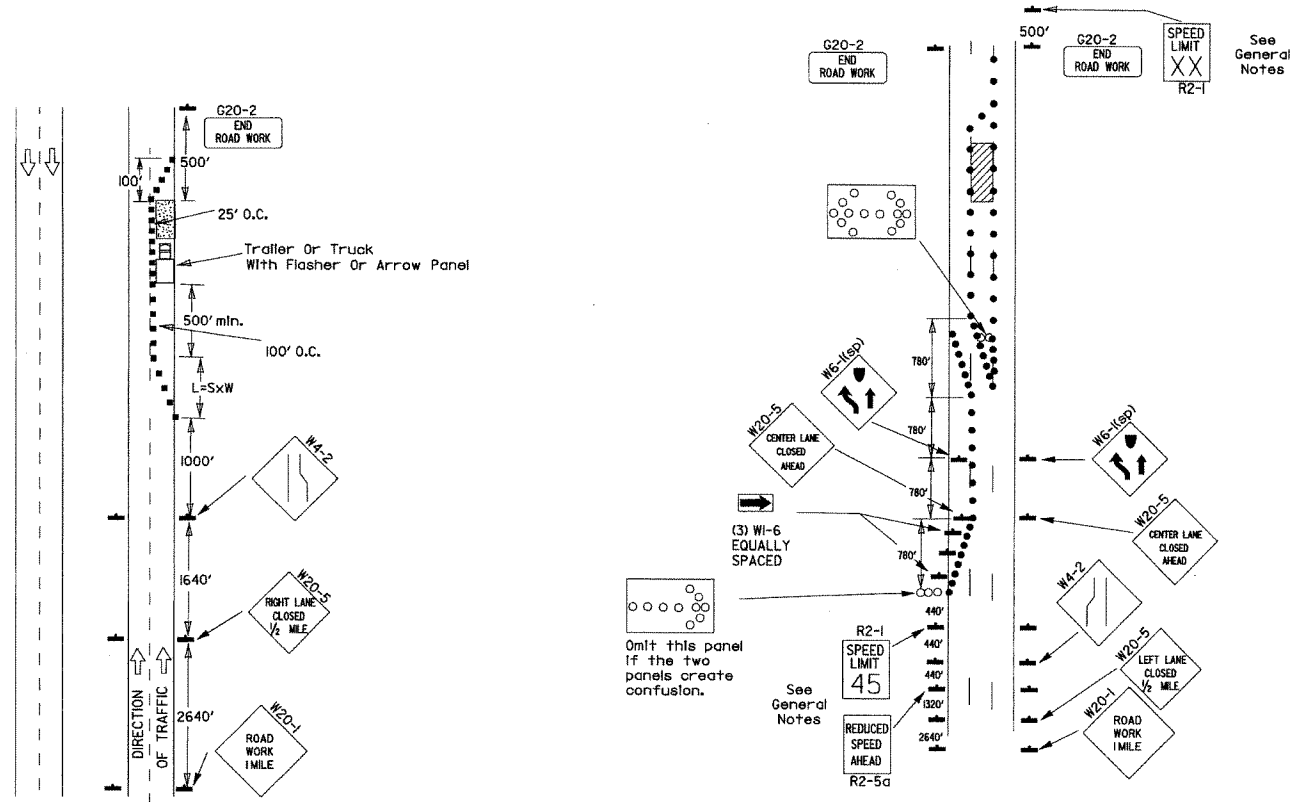
Typical advance warning sign placement

Taper formulae:
 L = SxW for speeds of 45mph or more.
 L = $\frac{WS^2}{60}$ for speeds of 40mph or less.
 Where:
 L = Minimum length of taper.
 S = Numerical value of posted speed limit prior to work or 85th percentile speed.
 W = Width of offset.

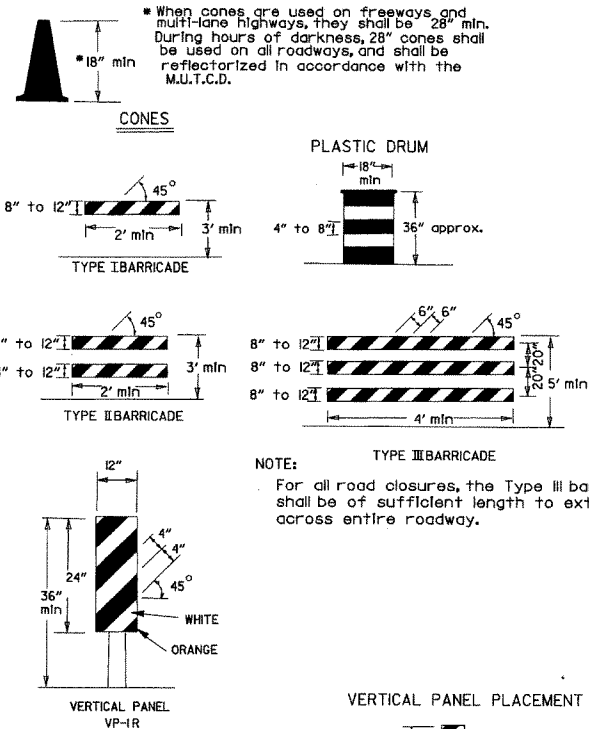
- GENERAL NOTES:**
- Advisory speed posted on W1-3 or W1-4 curve warning signs to be determined at site. Use W1-4 when speed is greater than 30mph and W1-3 when 30mph or less.
 - When the existing speed limit is 45mph, the R2-145 shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-145 shall be installed to match original speed limit.
 - When the existing speed limit is 65mph and the plans require a speed limit of 45mph, the R2-145 shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-155 shall be installed to match original speed limit.
 - The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit, or as directed by the Engineer.
 - Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
 - Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
 - Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

DATE	REVISION	FILMED
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED @ BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

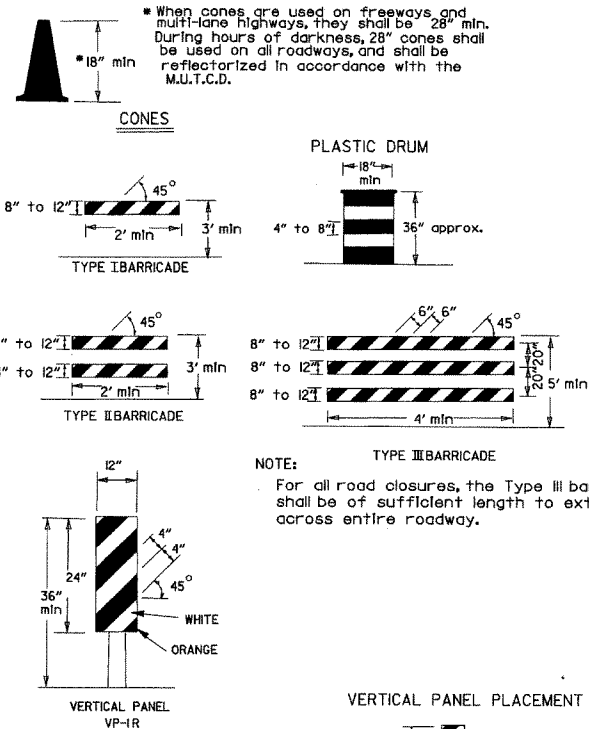
Channellizing devices



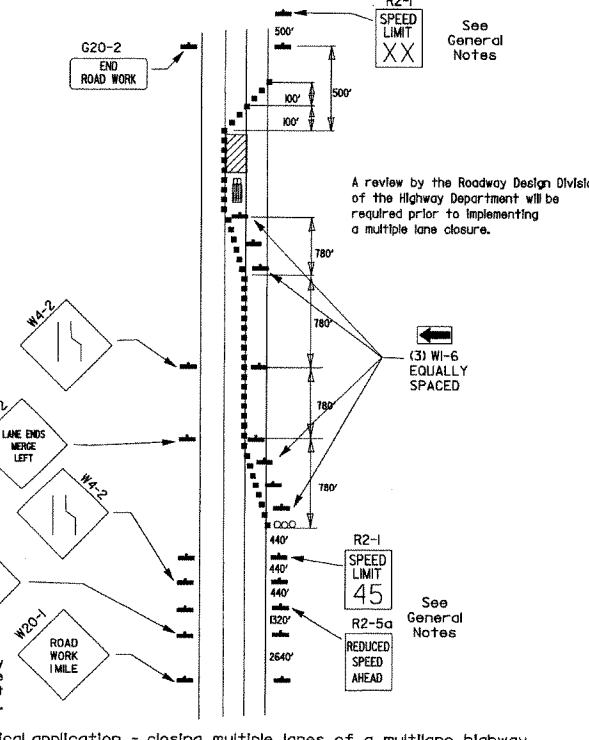
(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.



(B) Typical application - 3-lane oneway roadway where center lane is closed.



(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.



(D) Typical application - closing multiple lanes of a multilane highway.

- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

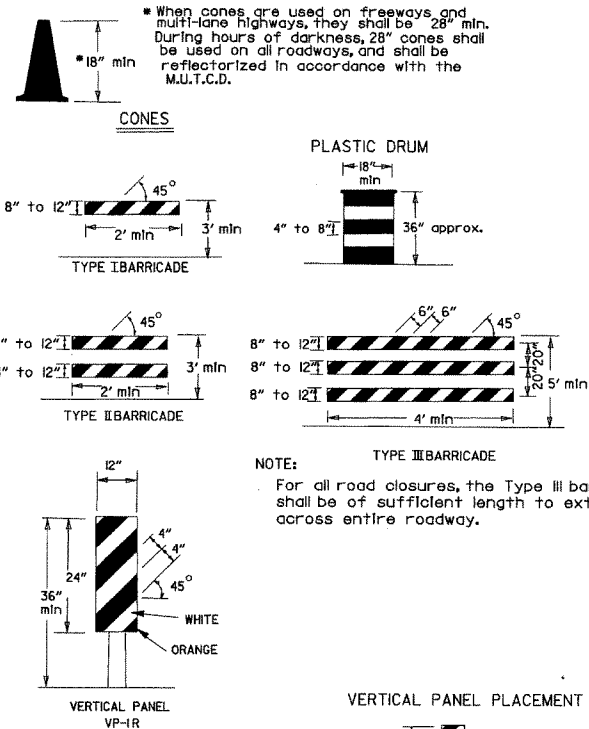
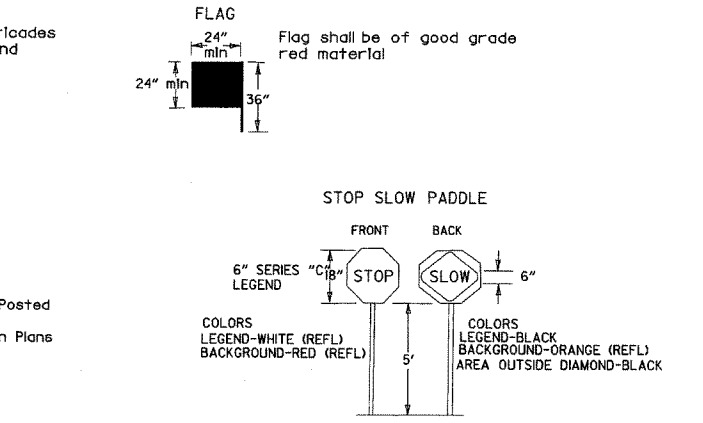
GENERAL NOTES:

1. A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
2. When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
4. The maximum spacing between channellizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
5. Warning lights and/or flags may be mounted to signs or channellizing devices at night as needed.
6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
7. The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1 (1/2 MILE) signs are not required in advance of lane closures that begin inside the project limits.
8. Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
9. All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
10. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

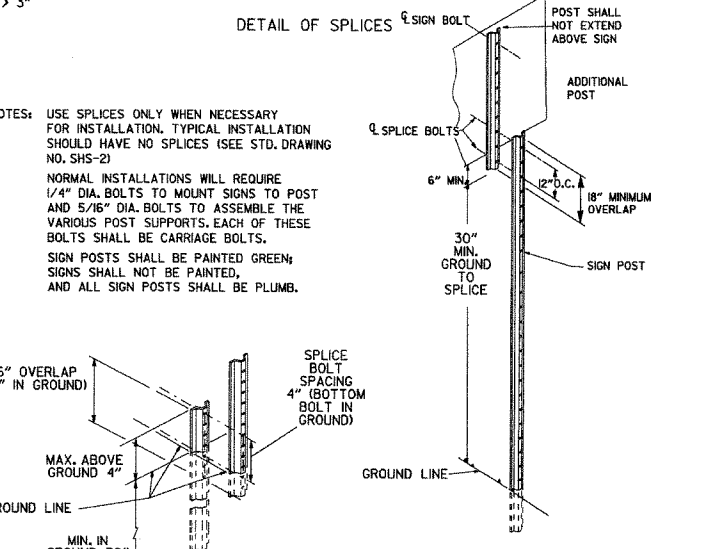
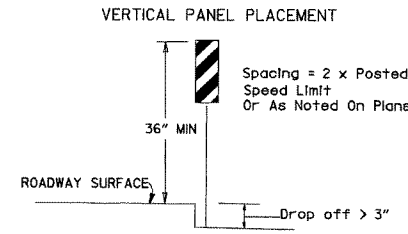
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-1 and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

* When shown on the plans concrete barrier will be used.
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



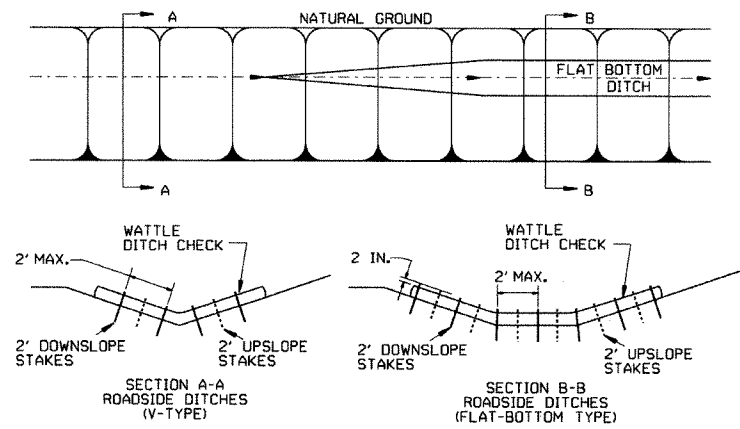
NOTE: For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



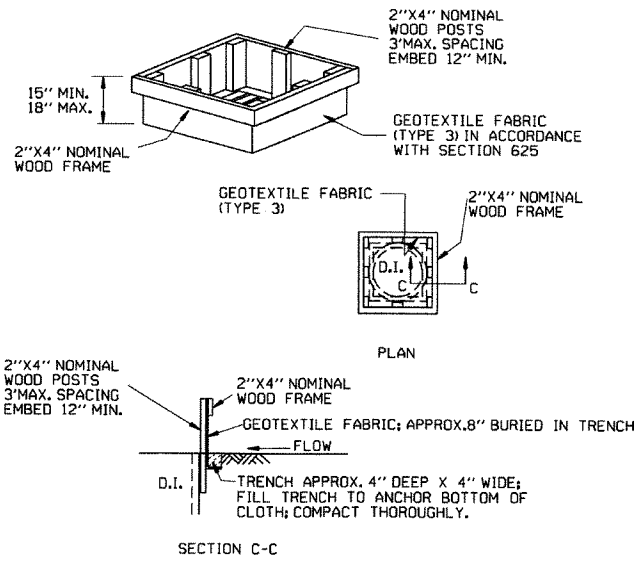
NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2) NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.

DATE	REVISION	FILED
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

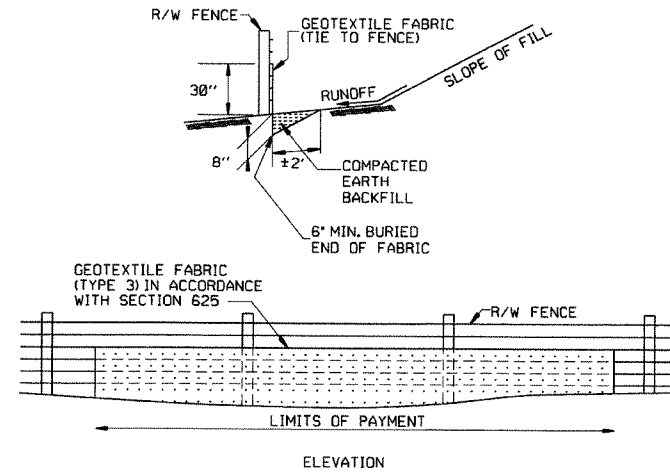
GENERAL NOTES
 INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.



WATTLE DITCH CHECK (E-1)

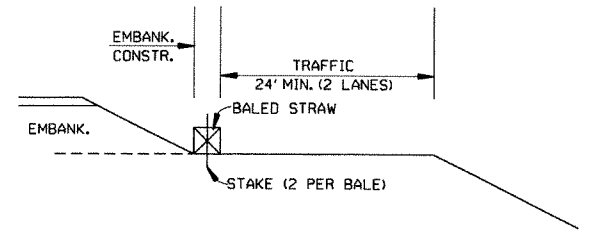


DROP INLET SILT FENCE (E-7)

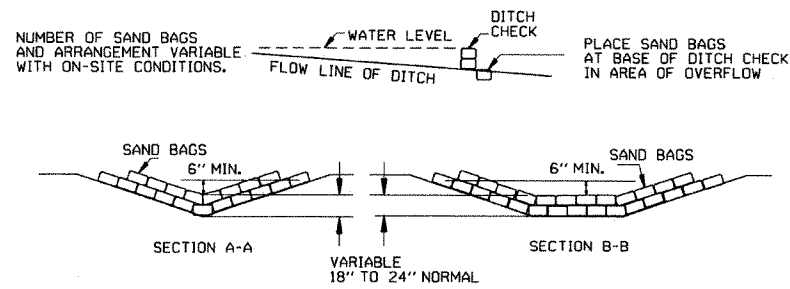


SILT FENCE ON R/W FENCE (E-4)

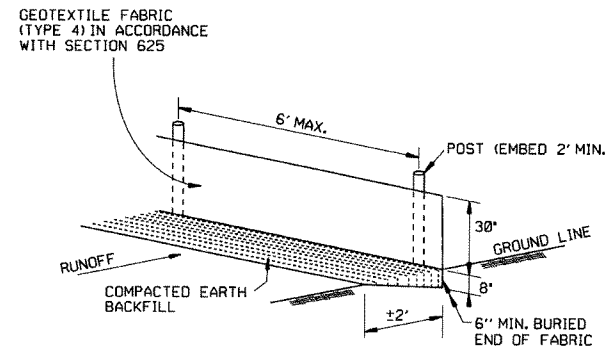
GENERAL NOTES
 GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST, OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



BALED STRAW FILTER BARRIER (E-2)

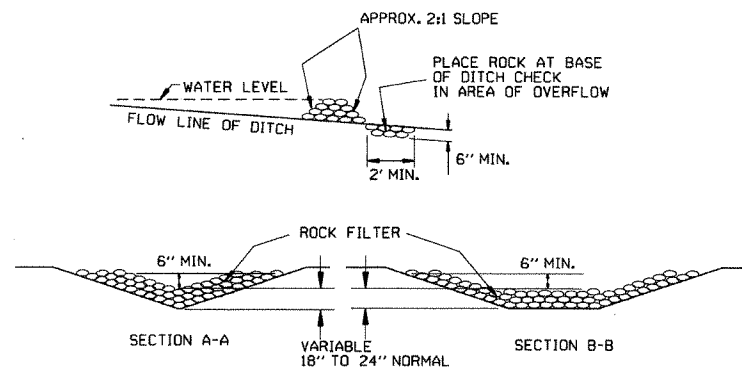


SAND BAG DITCH CHECK (E-5)



SILT FENCE (E-11)

GENERAL NOTES
 GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



ROCK DITCH CHECK (E-6)

12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
11-18-98	ADDED NOTES	
7-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
7-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
7-15-94	REV. E-4 & E-11 MIN. 13\"/>	
6-2-94	REVISED E-1,4,7 & 11; DELETED E-2 & 3	6-2-94
4-1-93	REDRAWN	
10-1-92	REDRAWN	
8-2-76	ISSUED R.D.M.	298-7-28-76
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

TEMPORARY EROSION CONTROL DEVICES

STANDARD DRAWING TEC-1