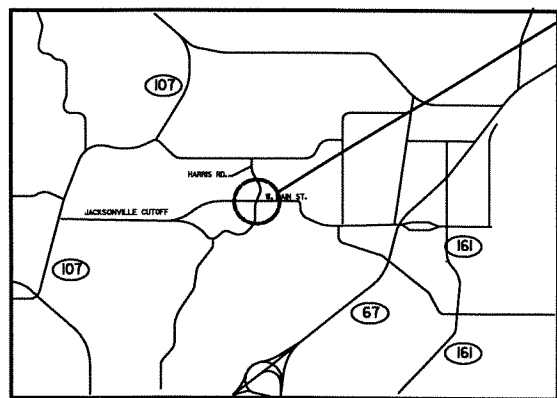


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				6	ARK.			
				JOB NO.		061267	1	56
W. MAIN ST./HARRIS RD./JACKSONVILLE CUTOFF INTERS. IMPVTS. (JACKSONVILLE)(S)								

PROJECT LOCATION

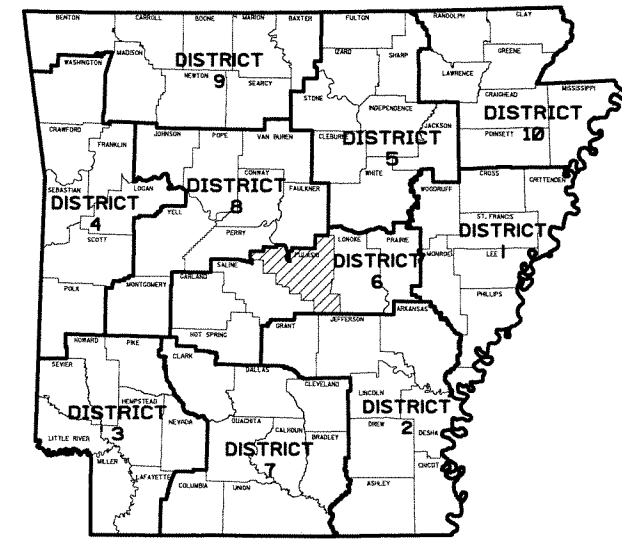


VICINITY MAP

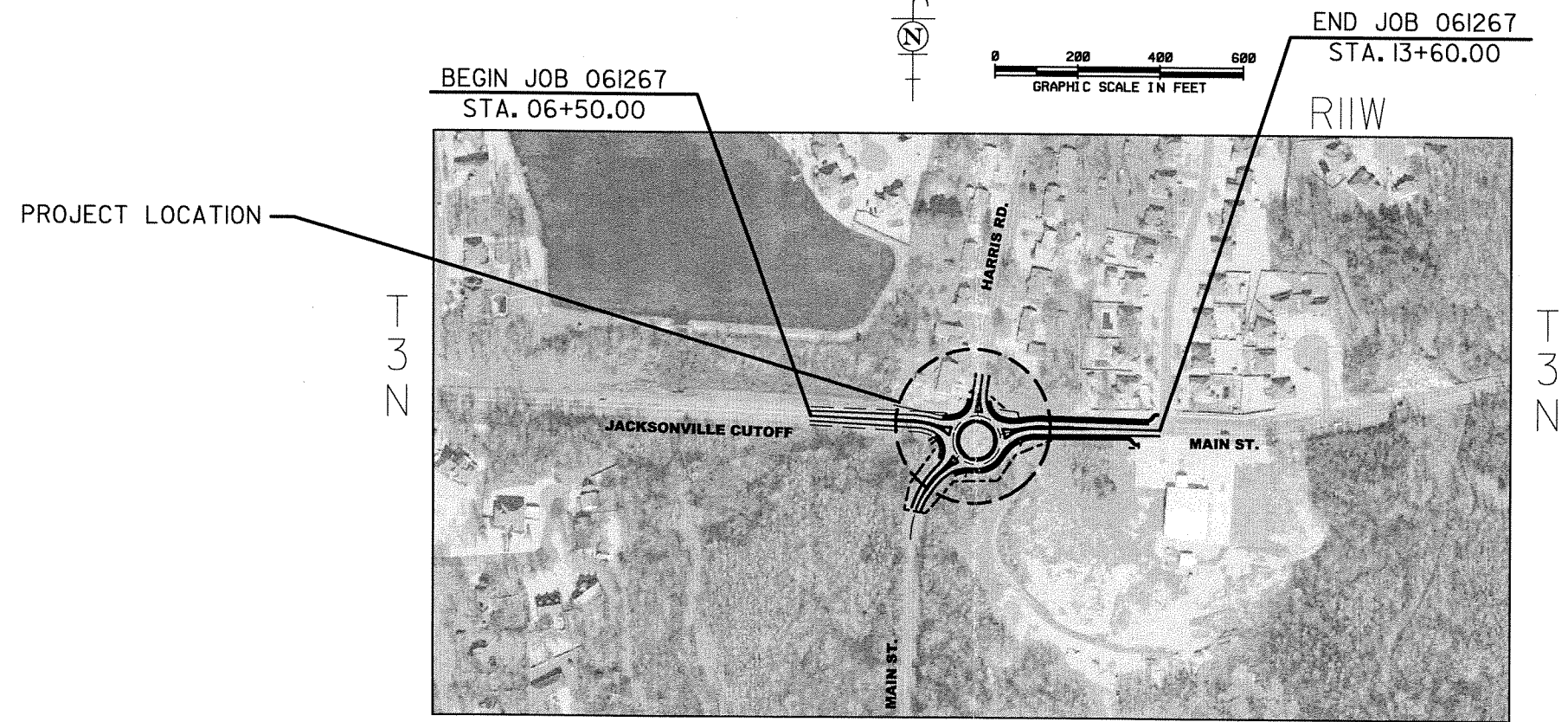
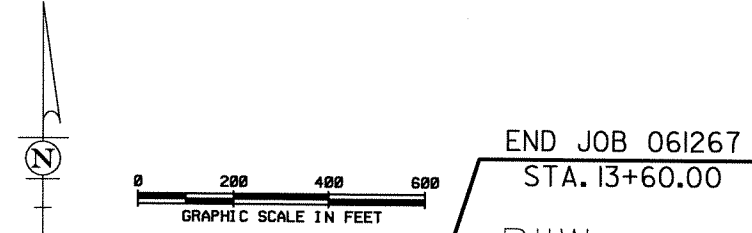
CITY OF JACKSONVILLE  
CONSTRUCTION PLANS FOR CITY STREET

**W. MAIN ST./HARRIS RD./  
JACKSONVILLE CUTOFF  
INTERIS. IMPVTS. (JACKSONVILLE)(S)**

PULASKI COUNTY  
FEDERAL AID PROJ. NO. STPU-9222(9)  
**JOB 061267**



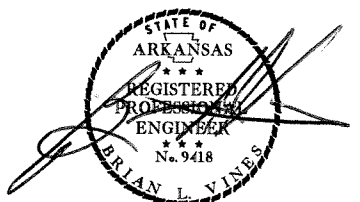
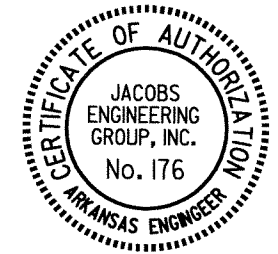
ARK.HWY.DIST.NO.6



DESIGN TRAFFIC DATA

DESIGN YEAR	-----	2032
2012 ADT	-----	9500
2032 ADT	-----	12800
2032 DHV	-----	1408
DIRECTIONAL DISTRIBUTION	-----	0.60
TRUCKS	-----	3%
DESIGN SPEED	-----	25 MPH

NOTE: TRAFFIC VOLUME FROM HIGHEST VOLUME LEG



7.23.13

MID POINT OF PROJECT  
LATITUDE 34° 52' 16" N  
LONGITUDE 92° 09' 23" W

THIS PROJECT IS SET ASIDE FOR SMALL BUSINESS

P.E. JOB NO. 061297  
F.A.P. NO. Q230-9222-009

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08-20-2013				6	ARK.			
						JOB NO. 061267	2	56

2 INDEX OF SHEETS, GOVERNING SPECIFICATIONS & GENERAL NOTES

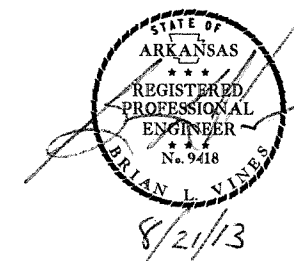
**INDEX OF SHEETS**

SHEET NO.	TITLE	BRIDGE NO.	DRWG. NO.	DATE
1	TITLE SHEET			
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS AND GENERAL NOTES			
3 - 7	TYPICAL SECTIONS OF IMPROVEMENT			
8 - 9	SPECIAL DETAILS			
10 - 13	TEMPORARY EROSION CONTROL DETAILS			
14 - 22	MAINTENANCE OF TRAFFIC			
23 - 24	PERMANENT PAVEMENT MARKING DETAILS			
25 - 26	QUANTITY SHEETS			
27	SUMMARY OF QUANTITIES AND REVISIONS			
28	SURVEY CONTROL DETAILS			
29 - 32	PLAN AND PROFILE SHEETS			
33	CURBING DETAILS			
34	DETAILS OF DRIVEWAYS & ISLANDS		CG-1	11-29-07
35	FLARED END SECTION		DR-1	11-29-07
36	FLARED END SECTION		FES-1	10-18-96
37	DETAILS OF DROP INLETS (TYPE C)		FES-2	10-18-96
38	DETAILS OF DROP INLET (TYPE MO)		FPC-9E	8-22-02
39	MAILBOX DETAILS		FPC-9M	8-22-02
40	CONCRETE PIPE CULVERT FILL HEIGHTS & BEDDING		MB-1	11-18-04
41	PAVEMENT MARKING DETAILS		PCC-1	12-15-11
42	STANDARD HIGHWAY SIGNS & SUPPORTS ASSEMBLIES		PM-1	11-17-10
43	U-CHANNEL POST ASSEMBLIES		SHS-1	4-17-08
44	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		SHS-2	10-09-03
45	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-1	12-15-11
46	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-2	3-11-10
47	TEMPORARY EROSION CONTROL DEVICES		TC-3	10-15-09
48	TEMPORARY EROSION CONTROL DEVICES		TEC-1	12-15-11
49	WHEELCHAIR RAMPS NEW CONSTRUCTION AND ALTERATIONS		TEC-3	11-03-94
50 - 56	CROSS SECTIONS		WR-1	11-10-05

**GOVERNING SPECIFICATIONS**

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2003, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-2	MANUAL FOR ASSESSING SAFETY HARDWARE (MASH)
102-1	BIDDING REQUIREMENTS AND CONDITIONS
105-1	CONSTRUCTION CONTROL MARKINGS
105-2	EQUIPMENT AND MATERIAL STORAGE ON BRIDGE STRUCTURES
105-3	CONTROL OF WORK
107-1	WORKER VISIBILITY
108-1	LIQUIDATED DAMAGES
110-1	PROTECTION OF WATER QUALITY AND WETLANDS
303-1	AGGREGATE BASE COURSE
404-1	PRODUCTION VERIFICATION OF ASPHALT CONCRETE HOT MIX
404-2	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
409-1	MINERAL AGGREGATES
410-3	DENSITY TESTING FOR ACHM LEVELING COURSES AND BOND BREAKERS
411-1	ASPHALT CONCRETE COLD PLANT MIX
600-1	WATER FOR VEGETATION
603-1	MAINTENANCE OF TRAFFIC
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-2	INSPECTION OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
606-2	PIPE CULVERTS
718-2	REFLECTORIZED PAINT PAVEMENT MARKINGS
719-2	THERMOPLASTIC PAVEMENT MARKING MATERIAL
723-1	GENERAL REQUIREMENTS FOR SIGNS
JOB 061267	ARCHEOLOGICAL MONITORING
JOB 061267	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB 061267	BROADBAND INTERNET SERVICE FOR FIELD OFFICE
JOB 061267	DOCUMENTATION OF PAYMENTS MADE TO DISADVANTAGED BUSINESS ENTERPRISES
JOB 061267	INTERNET BIDDING
JOB 061267	SMALL BUSINESS SET-ASIDE
JOB 061267	SOIL STABILIZATION
JOB 061267	STORM WATER POLLUTION PREVENTION PLAN
JOB 061267	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 061267	THERMOPLASTIC PAVEMENT MARKING (YIELD LINE)
JOB 061267	UTILITY ADJUSTMENTS
JOB 061267	WARM MIX ASPHALT

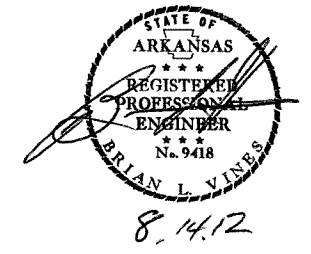


**GENERAL NOTES**

- GRADE LINE DENOTES FINISHED GRADE WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U. S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- THIS PROJECT IS COVERED UNDER A NATIONWIDE 14 SECTION 404 PERMIT. REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS, EDITION OF 2003, FOR PERMIT REQUIREMENTS.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210 - UNCLASSIFIED EXCAVATION.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		061267	3	56

② TYPICAL SECTIONS OF IMPROVEMENT



NOTES:

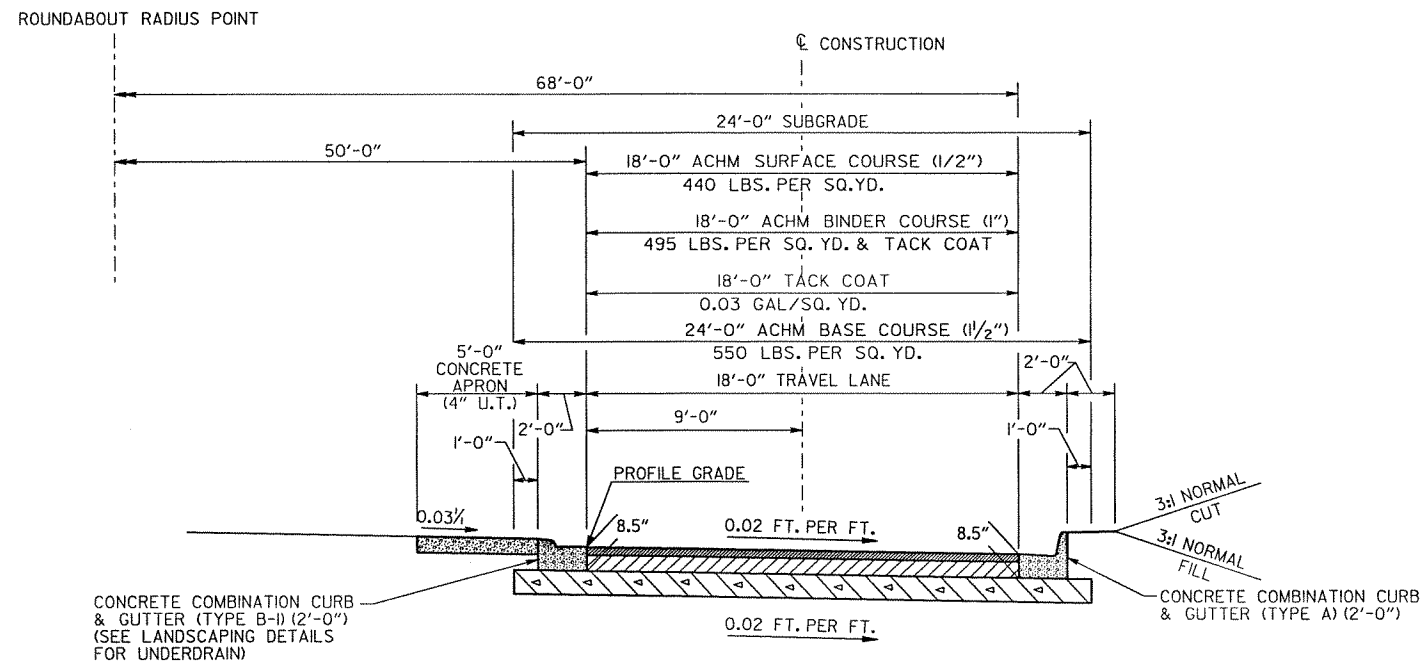
REFER TO CROSS SECTIONS FOR DEVIATION FROM THE NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.

• IN CUT SLOPE SIDEWALK 0.02 '//' TOWARD CURB. IN FILL SLOPE SIDEWALK 0.02 '//' AWAY FROM CURB. CONSTRUCT SIDEWALK TO A UNIFORM THICKNESS OF 4".

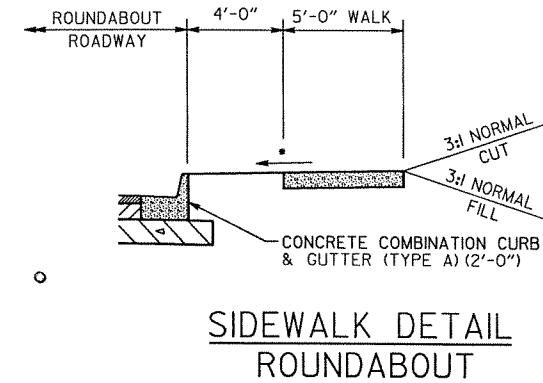
FOR CROSS SLOPES IN TRANSITION AREAS, SEE PLAN & PROFILE SHEETS AND CROSS SECTIONS.

PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHOD(S) USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.

THE FINAL 2" OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT LANE LINES.

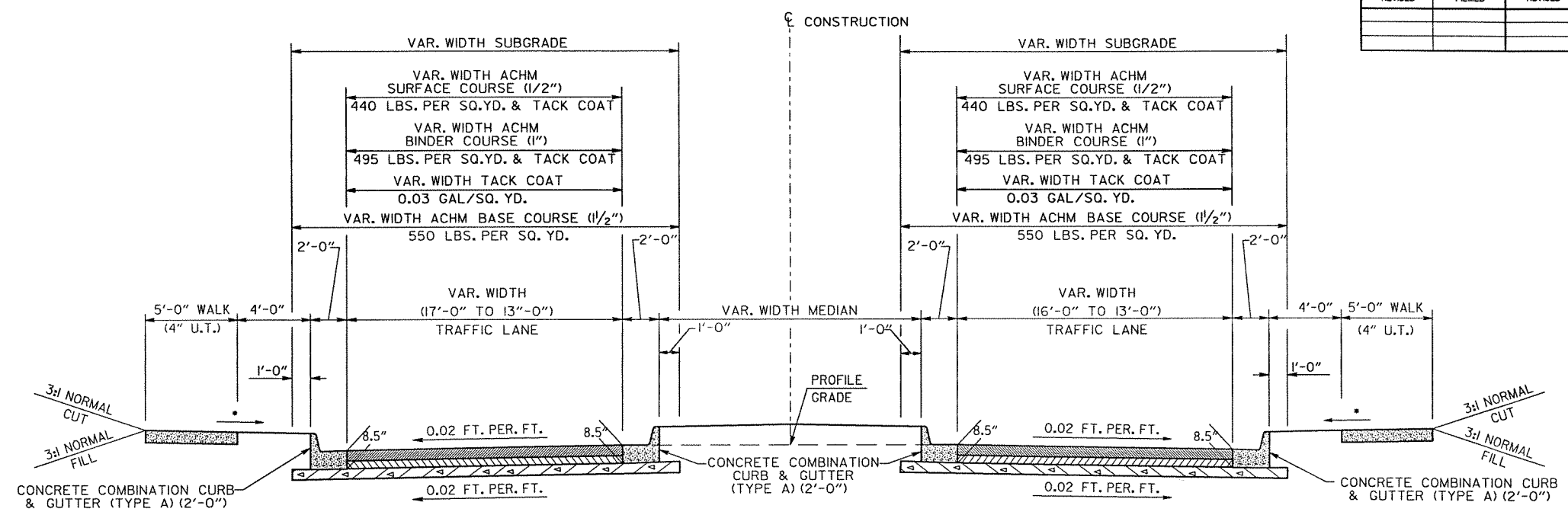


ROUNDABOUT SECTION  
STA. 10+00.00 TO STA. 13+70.71



SIDEWALK DETAIL  
ROUNDABOUT

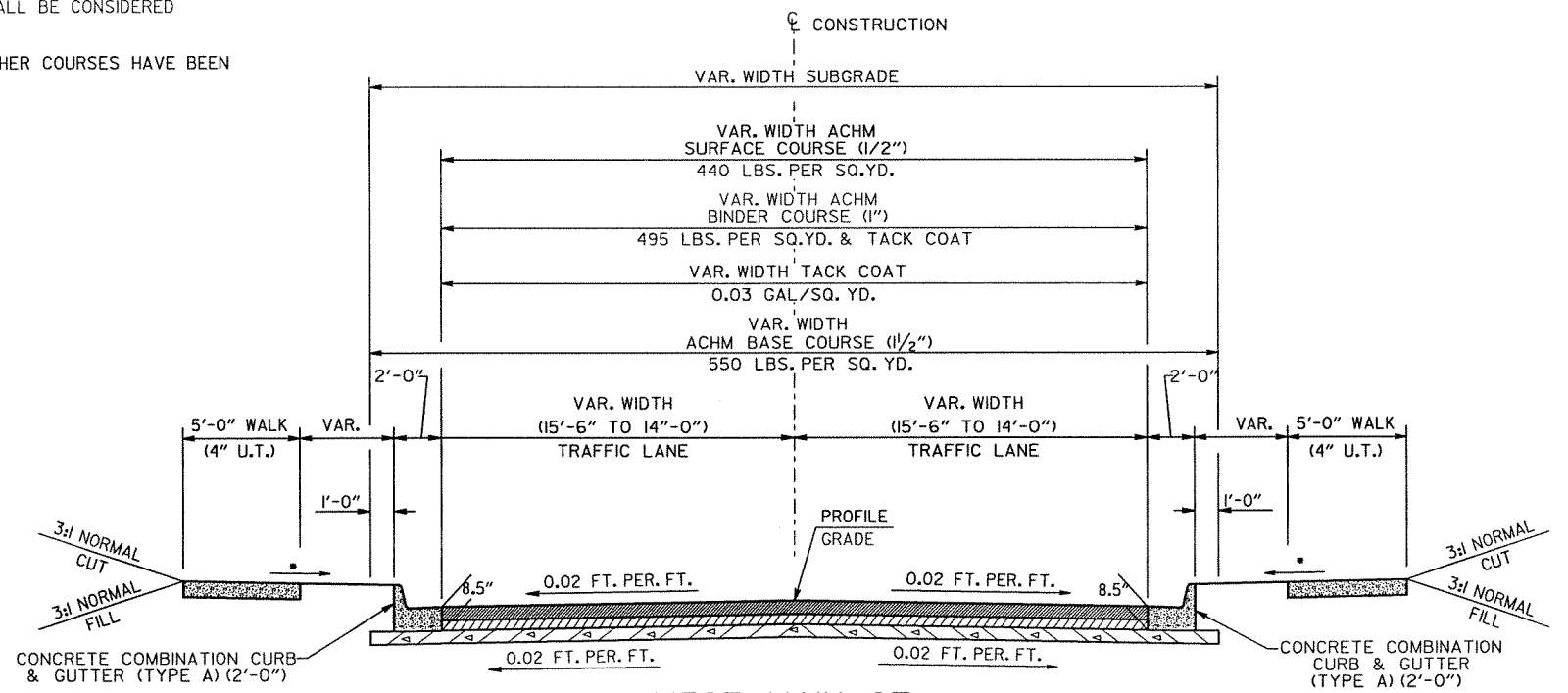
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				6	ARK.			
						JOB NO.	061267	4
						TYPICAL SECTIONS OF IMPROVEMENT		



**WEST MAIN ST.**  
**2 LANE WITH RAISED MEDIAN**  
**STA. 10+09.00 TO STA. 11+79.12**

**NOTES:**

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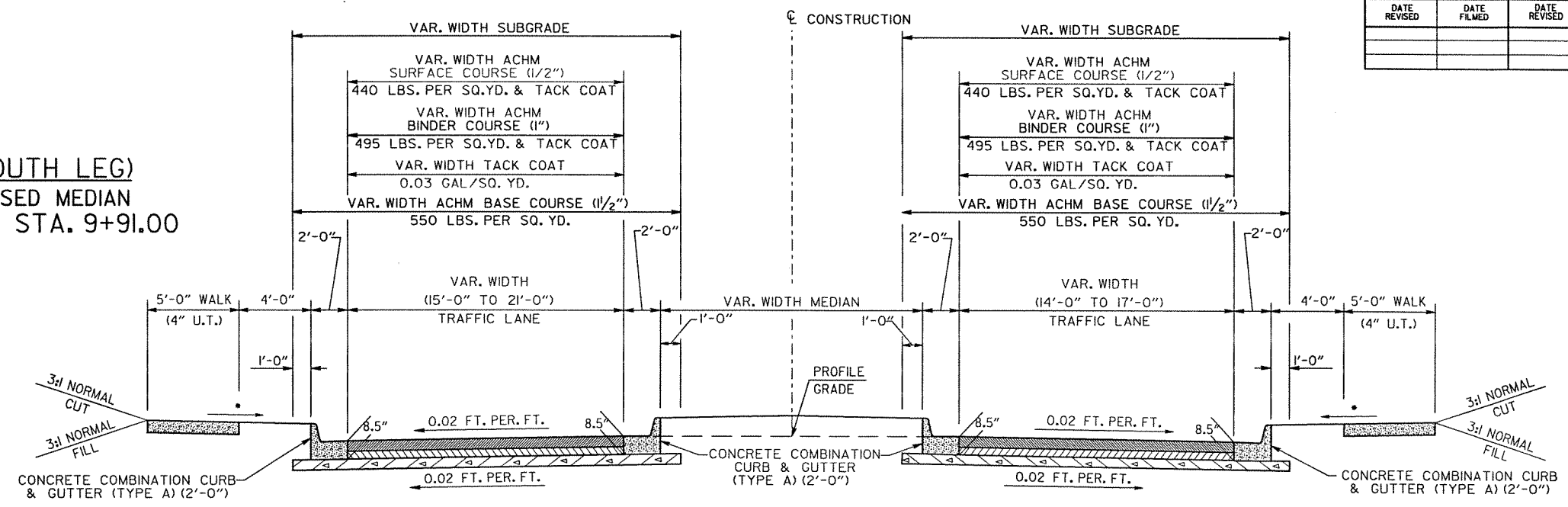


**WEST MAIN ST.**  
**STA. 11+79.12 TO STA. 13+60**

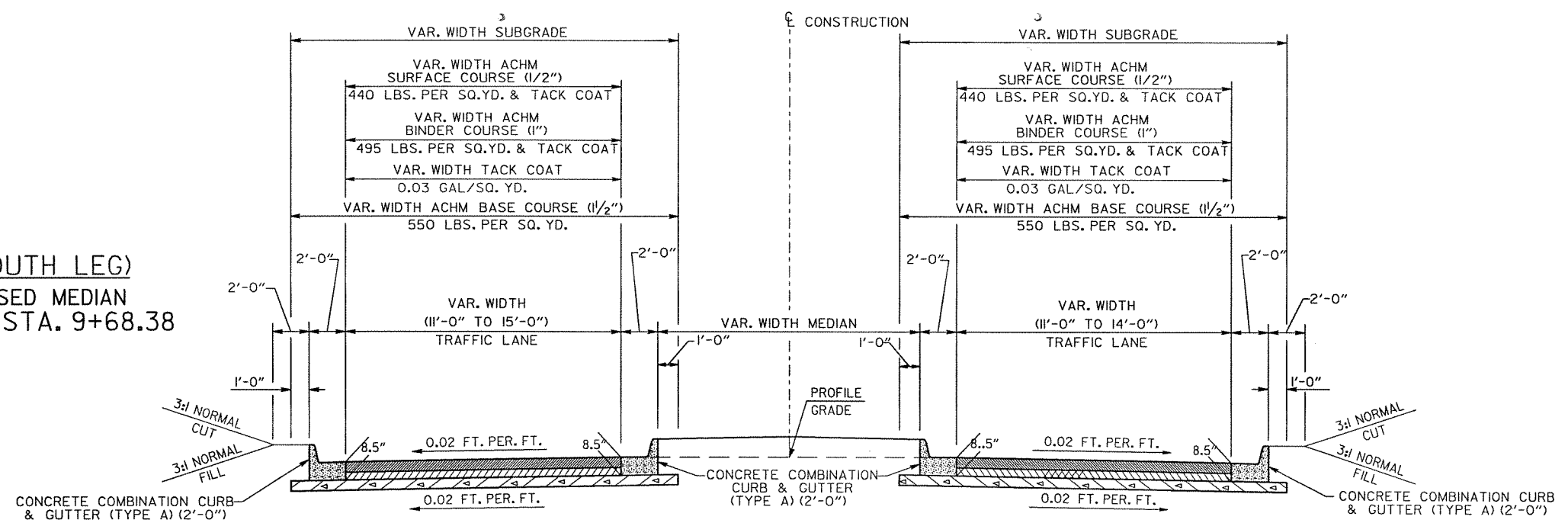
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				6	ARK.				
							JOB NO. 061267	5	56

2 TYPICAL SECTIONS OF IMPROVEMENT

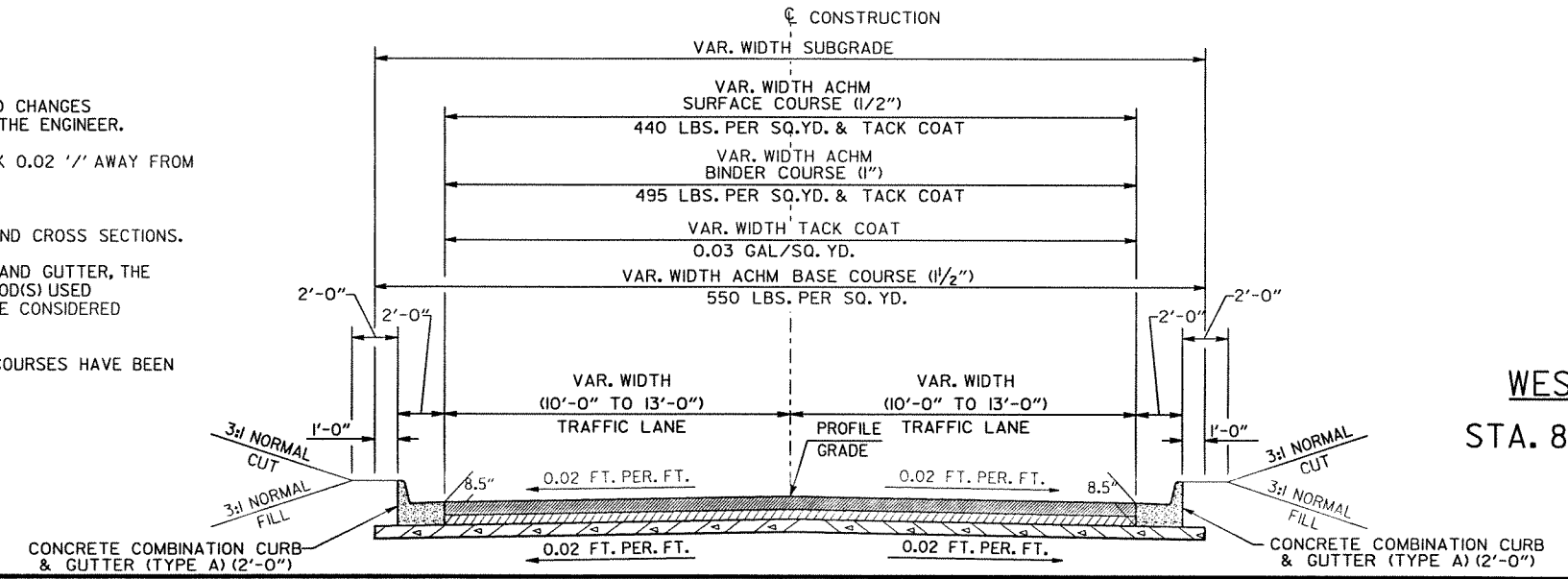
**WEST MAIN (SOUTH LEG)**  
2 LANE WITH RAISED MEDIAN  
STA. 9+68.38 TO STA. 9+91.00



**WEST MAIN (SOUTH LEG)**  
2 LANE WITH RAISED MEDIAN  
STA. 8+87.95 TO STA. 9+68.38



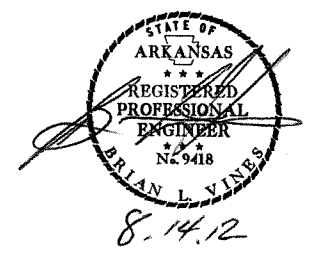
**WEST MAIN (SOUTH LEG)**  
STA. 8+35.00 TO STA. 8+87.95



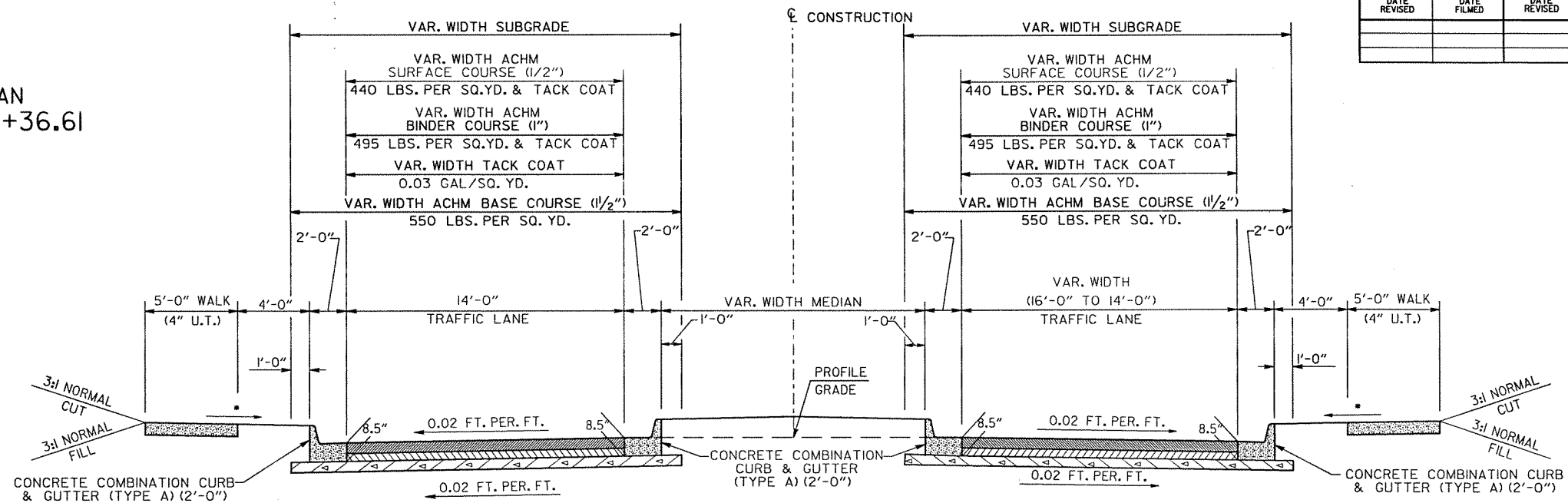
TYPICAL SECTIONS OF IMPROVEMENT

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HARRIS ROAD  
2 LANE WITH RAISED MEDIAN  
STA. 10+09.00 TO STA. 10+36.61

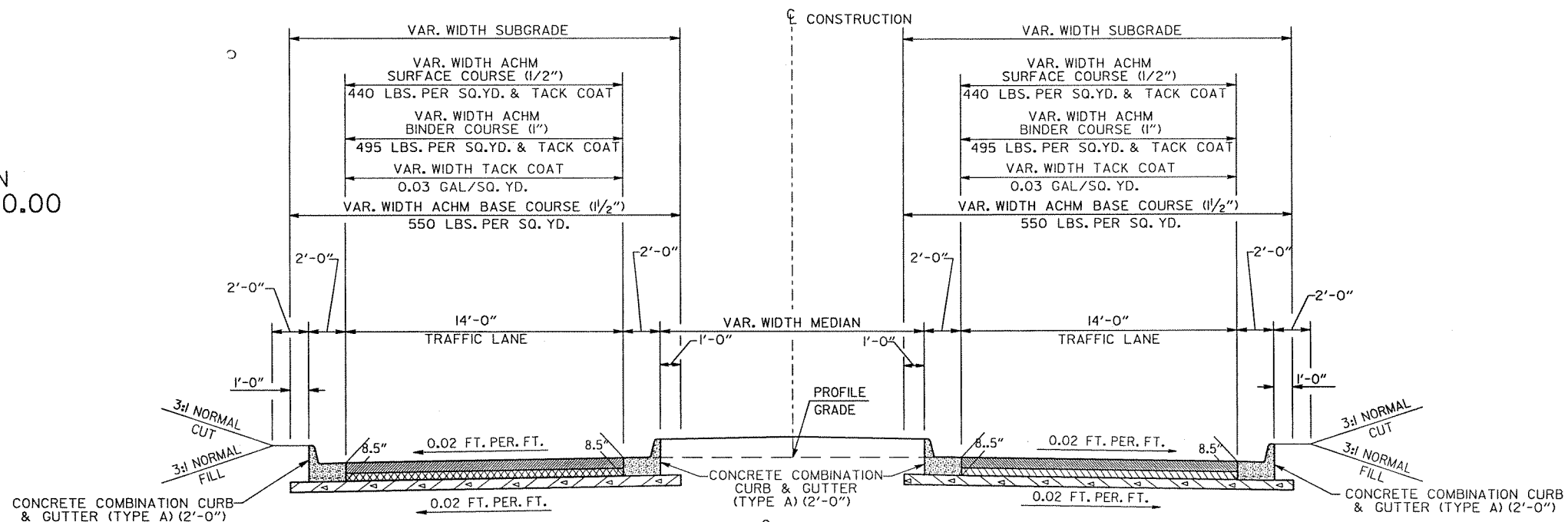


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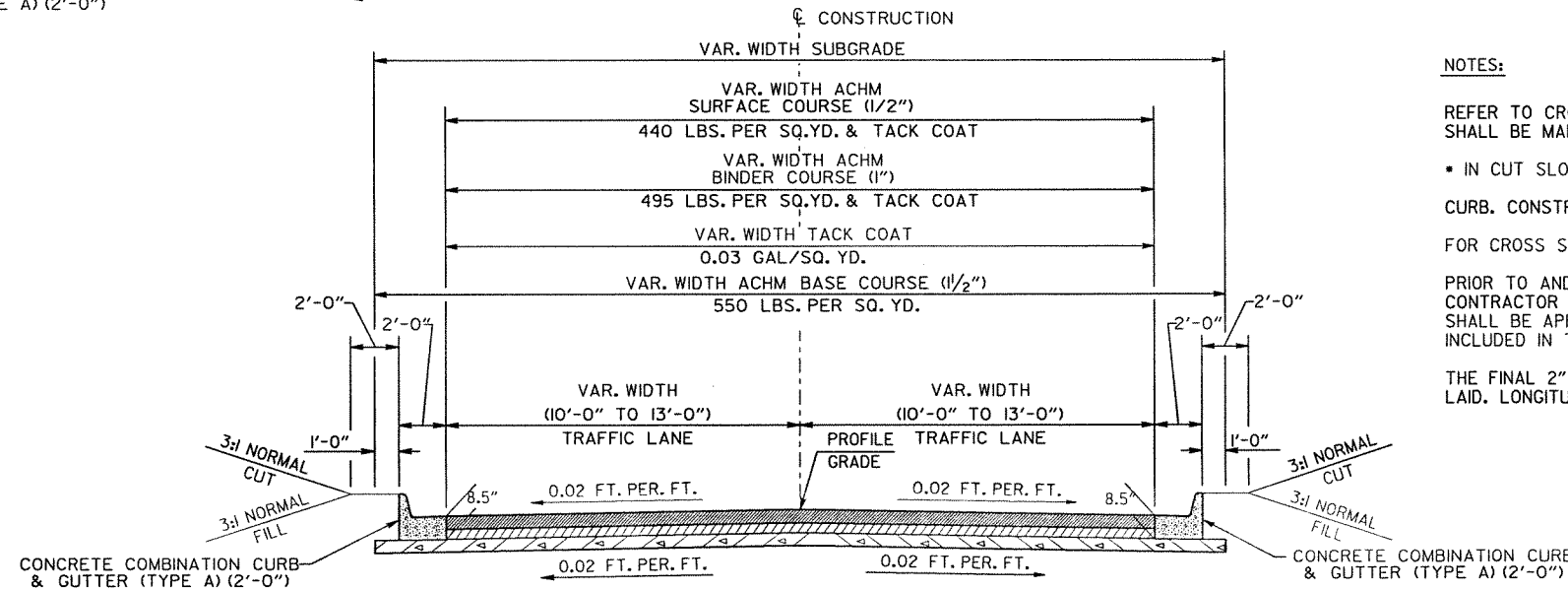
2 TYPICAL SECTIONS OF IMPROVEMENT



HARRIS ROAD  
2 LANE WITH RAISED MEDIAN  
STA. 10+36.61 TO STA. 10+50.00



HARRIS ROAD  
STA. 10+50.00 TO STA. 11+00.21

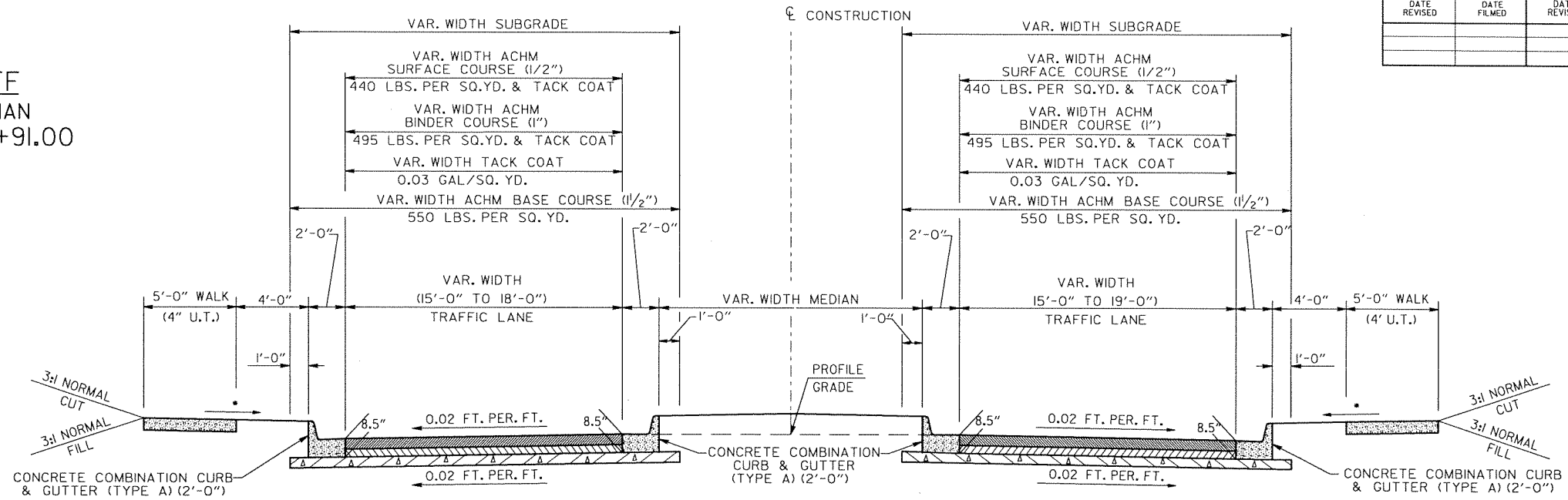


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TYPICAL SECTIONS OF IMPROVEMENT

**JACKSONVILLE CUTOFF**  
2 LANE WITH RAISED MEDIAN  
STA. 9+36.19 TO STA. 9+91.00

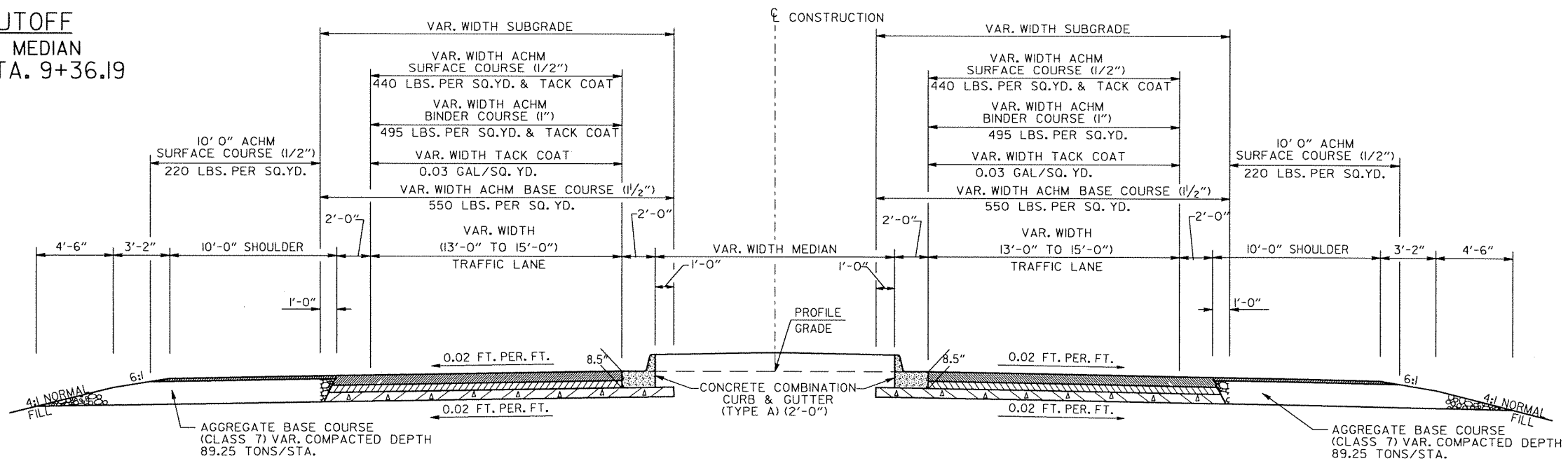


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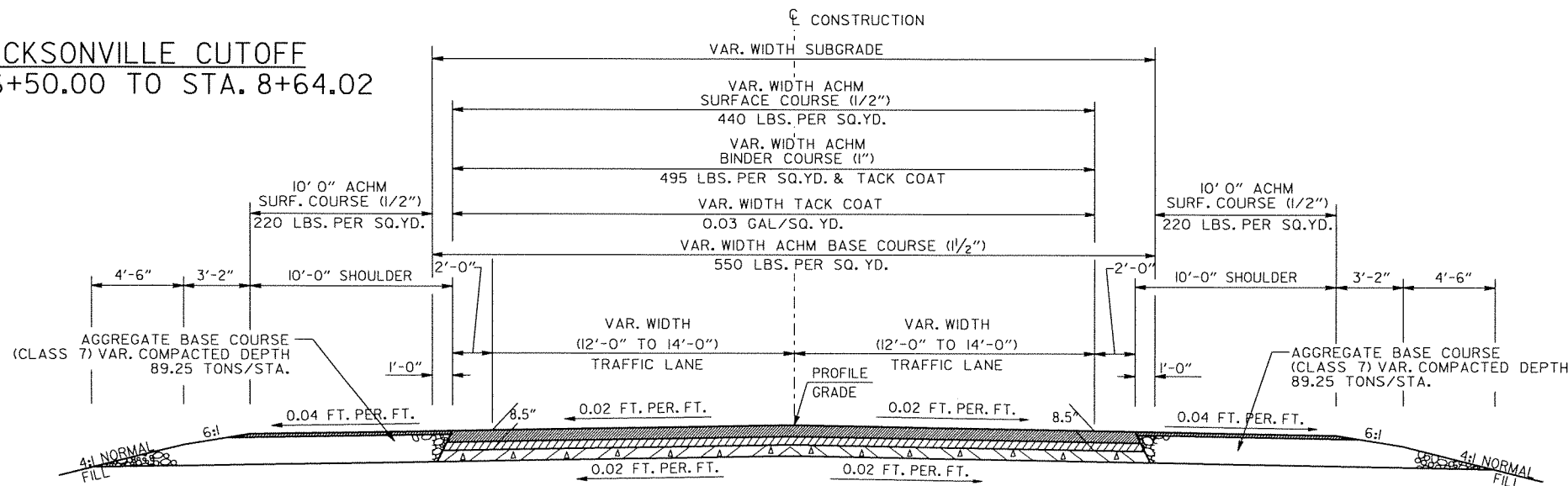
2 TYPICAL SECTIONS OF IMPROVEMENT



**JACKSONVILLE CUTOFF**  
2 LANE WITH RAISED MEDIAN  
STA. 8+64.02 TO STA. 9+36.19



**JACKSONVILLE CUTOFF**  
STA. 6+50.00 TO STA. 8+64.02



**NOTES:**

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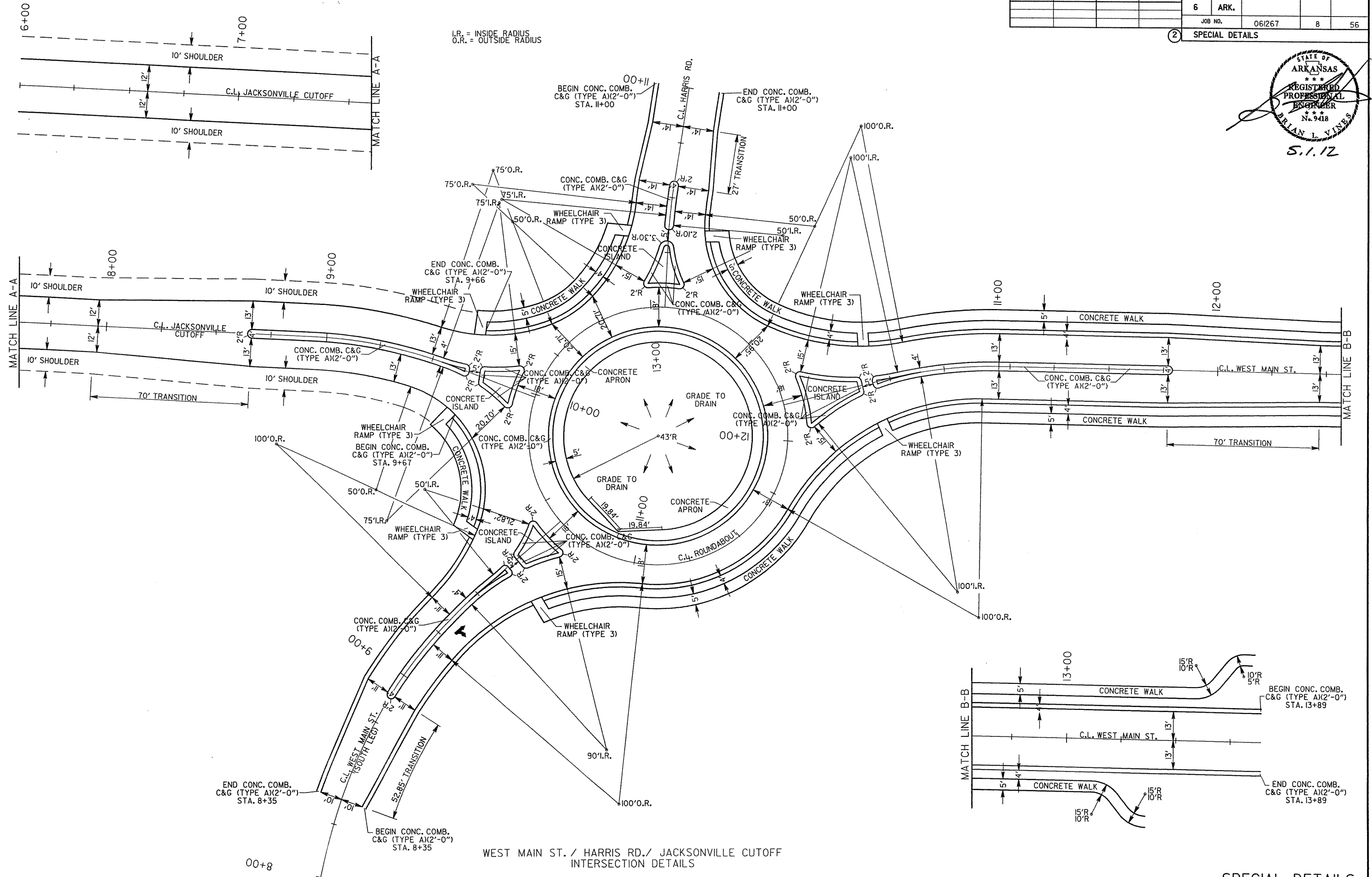
TYPICAL SECTIONS OF IMPROVEMENT

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				6	ARK.		8	56
				JOB NO.		061267		

2 SPECIAL DETAILS



I.R. = INSIDE RADIUS  
O.R. = OUTSIDE RADIUS



WEST MAIN ST. / HARRIS RD. / JACKSONVILLE CUTOFF INTERSECTION DETAILS

SPECIAL DETAILS

JACOBS

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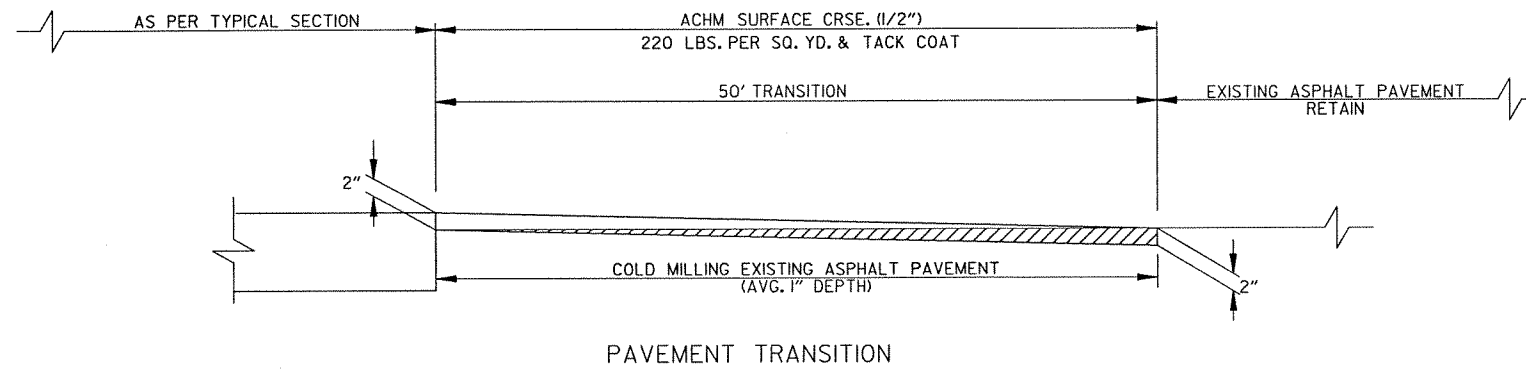
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2 SPECIAL DETAILS



8.14.12

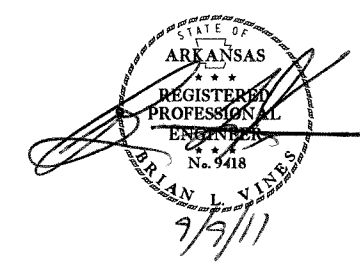


SPECIAL DETAILS

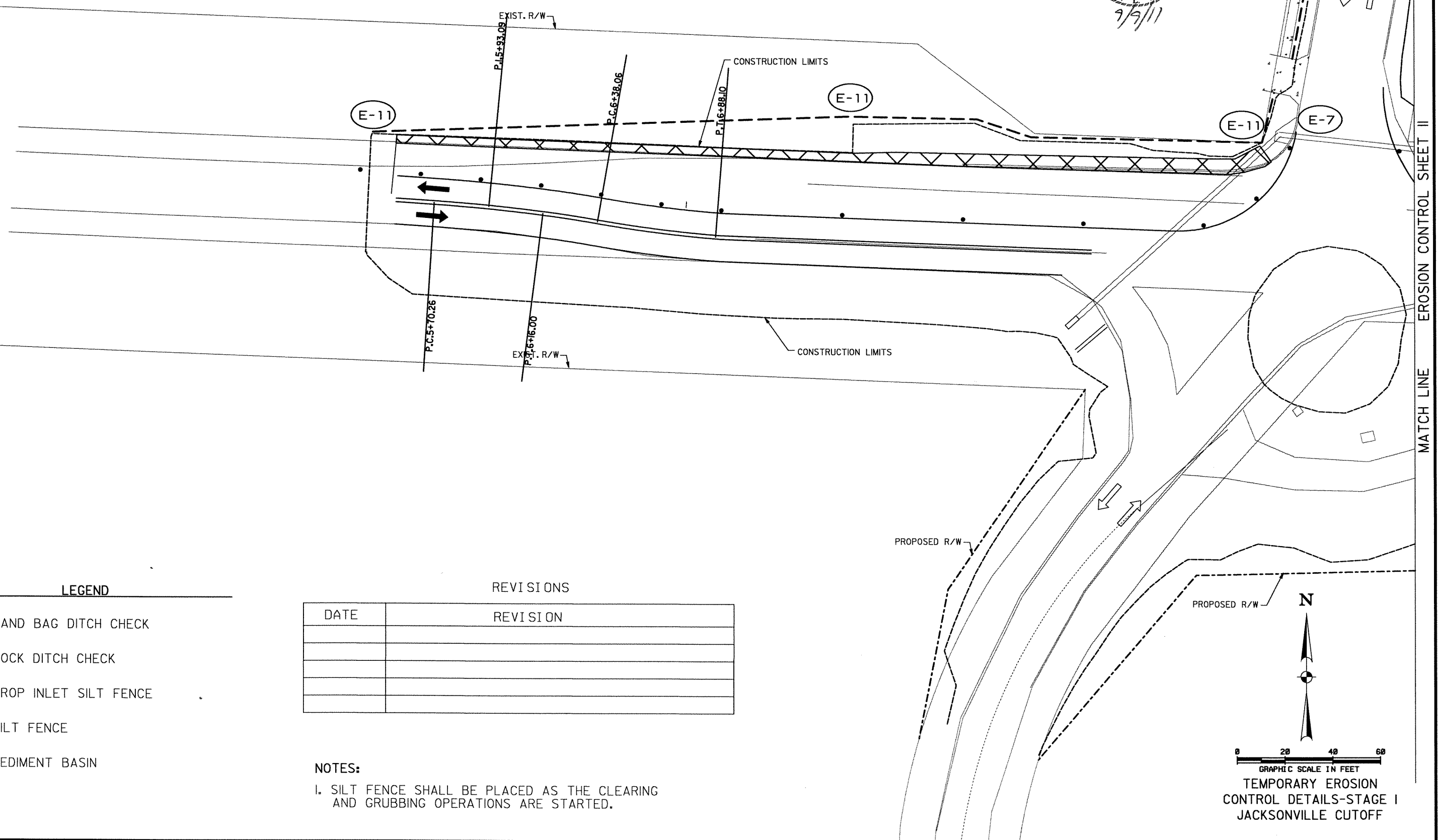
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				6	ARK.			
				JOB NO.	061267	10	56	



2 TEMPORARY EROSION CONTROL DETAILS



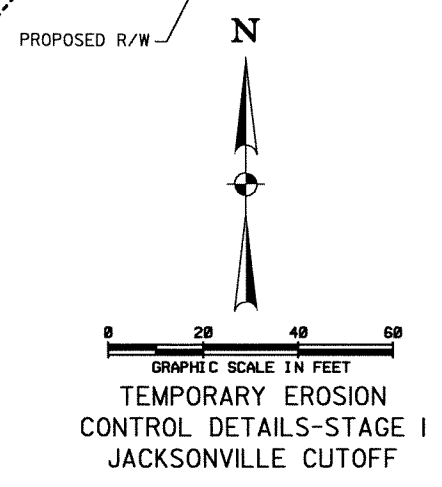
**LEGEND**

	SAND BAG DITCH CHECK
	ROCK DITCH CHECK
	DROP INLET SILT FENCE
	SILT FENCE
	SEDIMENT BASIN

**REVISIONS**

DATE	REVISION

**NOTES:**  
 1. SILT FENCE SHALL BE PLACED AS THE CLEARING AND GRUBBING OPERATIONS ARE STARTED.

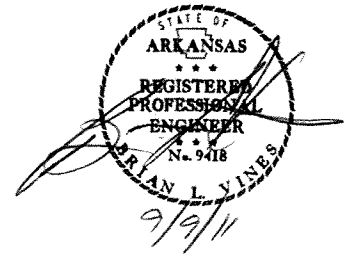


MATCH LINE EROSION CONTROL SHEET II

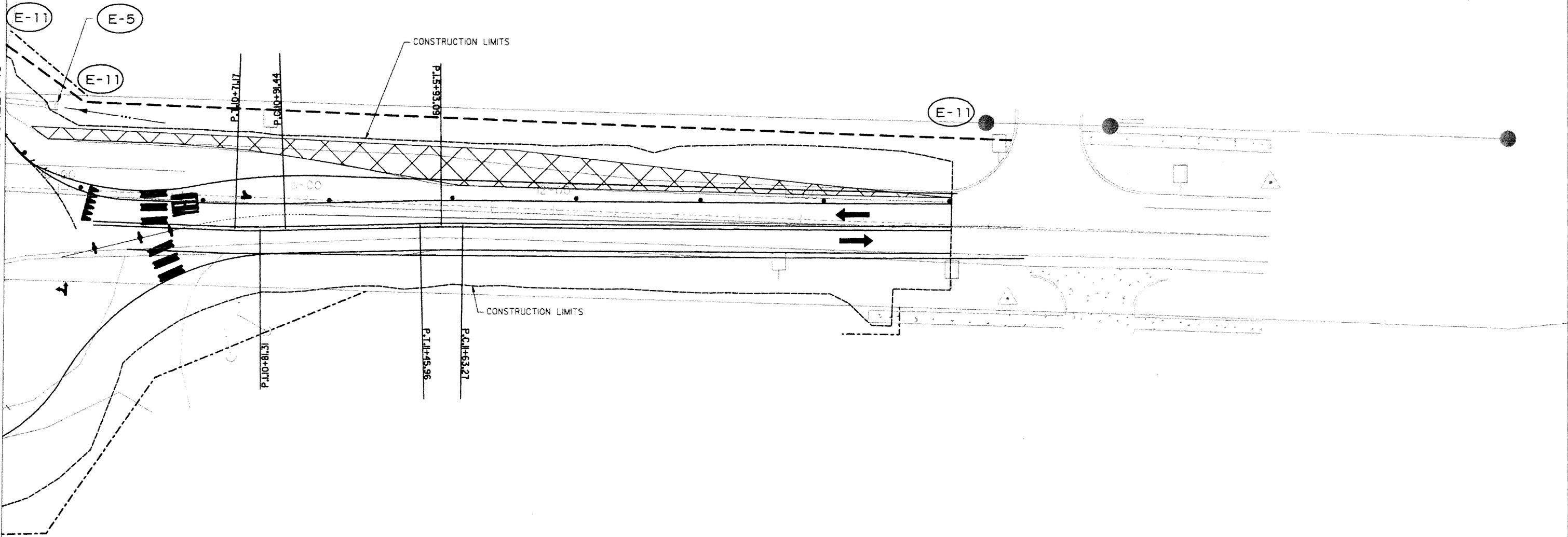
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 9/6/2011

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061267		11	56

② TEMPORARY EROSION CONTROL DETAILS



EROSION CONTROL SHEET 10



LEGEND

- (E-5) SAND BAG DITCH CHECK
- (E-6) ROCK DITCH CHECK
- (E-7) DROP INLET SILT FENCE
- (E-11) SILT FENCE
- (E-14) SEDIMENT BASIN

REVISIONS

DATE	REVISION

NOTES:

1. SILT FENCE SHALL BE PLACED AS THE CLEARING AND GRUBBING OPERATIONS ARE STARTED.

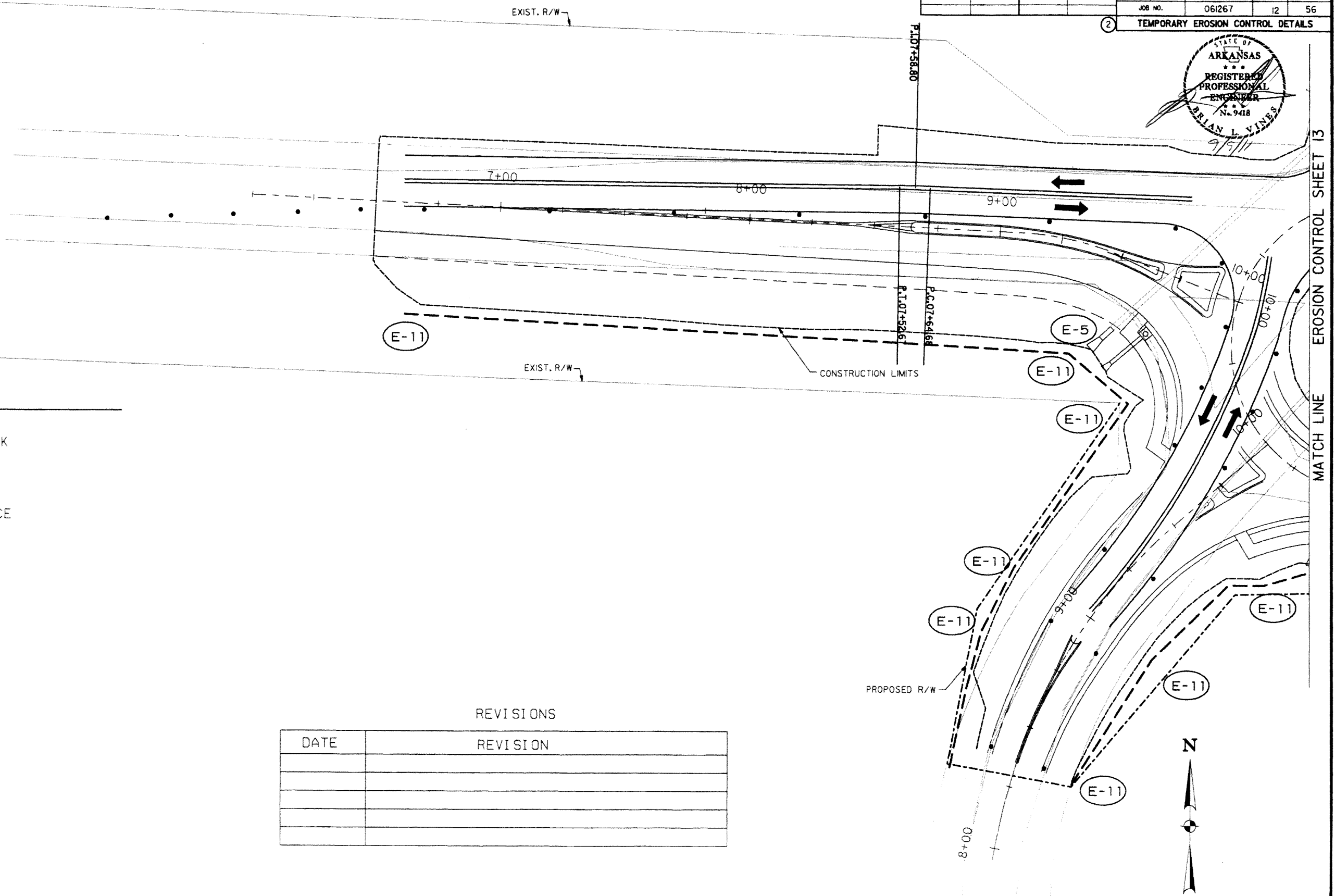
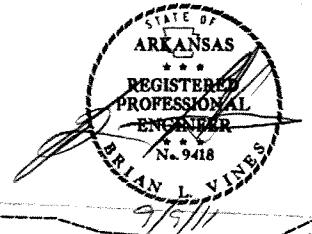


TEMPORARY EROSION CONTROL DETAILS -STAGE I  
W. MAIN STREET

I:\Job\WXL7000 - Jackson\file Main and Harris\700 CADD Files\777 Roadway Drawings\WXL7000-erosion control\stage 2\7:48 PM ...\wixi7000-erosion control\stage 2-04.dgn 9/6/2011

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 061267							12	56

2 TEMPORARY EROSION CONTROL DETAILS



**LEGEND**

- (E-5) SAND BAG DITCH CHECK
- (E-6) ROCK DITCH CHECK
- (E-7) DROP INLET SILT FENCE
- (E-11) SILT FENCE
- (E-14) SEDIMENT BASIN

**REVISIONS**

DATE	REVISION

**NOTES:**  
 1. SILT FENCE SHALL BE PLACED AS THE CLEARING AND GRUBBING OPERATIONS ARE STARTED.

N

0 20 40 60  
GRAPHIC SCALE IN FEET

**TEMPORARY EROSION CONTROL DETAILS -STAGE 2**  
**MAIN ST. & HARRIS RD. ROUNDABOUT**

MATCH LINE EROSION CONTROL SHEET 13

I:\Job\WI.XI.7000 - Jacksonville Main and Harris.700 CADD Files\717 Roadway\Drawings\71700-erosion\_control\stage 2-03.dgn  
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 9/6/2011

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061267	13	56	

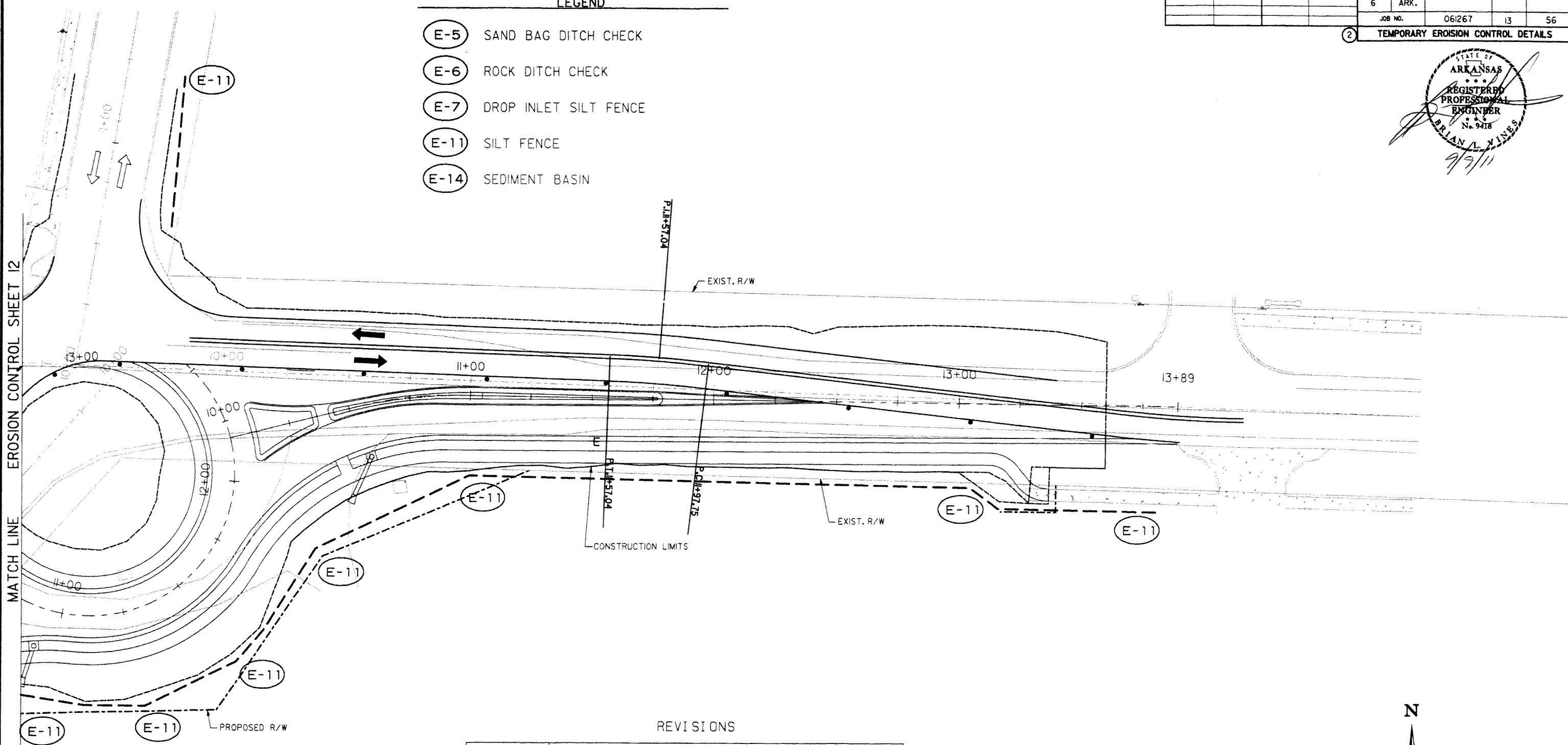
② TEMPORARY EROSION CONTROL DETAILS



LEGEND

- ⊖ E-5 SAND BAG DITCH CHECK
- ⊖ E-6 ROCK DITCH CHECK
- ⊖ E-7 DROP INLET SILT FENCE
- ⊖ E-11 SILT FENCE
- ⊖ E-14 SEDIMENT BASIN

EROSION CONTROL SHEET 12



REVISIONS

DATE	REVISION

NOTES:

1. SILT FENCE SHALL BE PLACED AS THE CLEARING AND GRUBBING OPERATIONS ARE STARTED.



TEMPORARY EROSION CONTROL DETAILS-STAGE 2  
MAIN ST. & HARRIS RD. ROUNDABOUT

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2:43:35 PM ...Drawings\1700-MOT1-01.dgn

9/6/2011

**NOTES:**

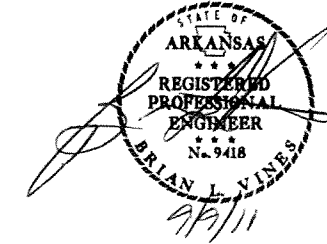
1. TYPE III BARRICADE SHALL PROHIBIT TRAFFIC FROM ENTERING WORK ZONE.
2. TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
3. THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/ BUSINESSES DURING CONSTRUCTION.

**CONSTRUCTION PAVEMENT MARKINGS**

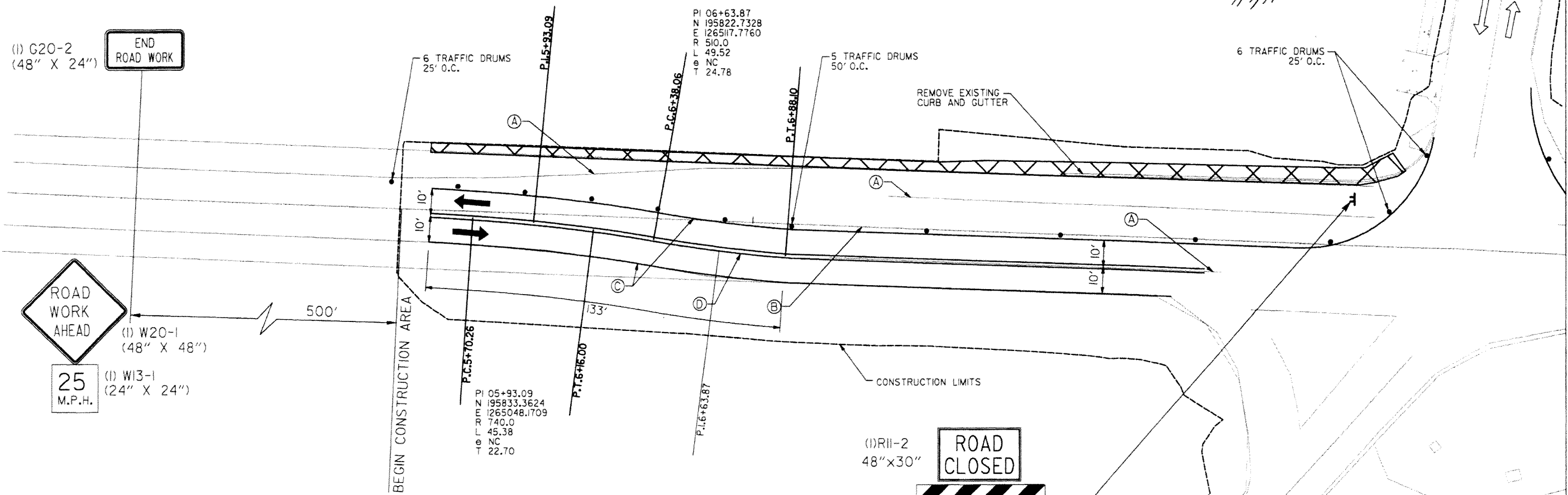
TRAFFIC DRUMS = 17 EACH

- (A) REMOVE PAVEMENT MARKINGS (4" WHITE) = 286 L.F.
- (B) REMOVE PAVEMENT MARKINGS (4" YELLOW) = 702 L.F.
- (C) CONSTRUCTION PAVEMENT MARKINGS (4" WHITE) = 1008 L.F.
- (D) CONSTRUCTION PAVEMENT MARKINGS (4" YELLOW) = 578 L.F.

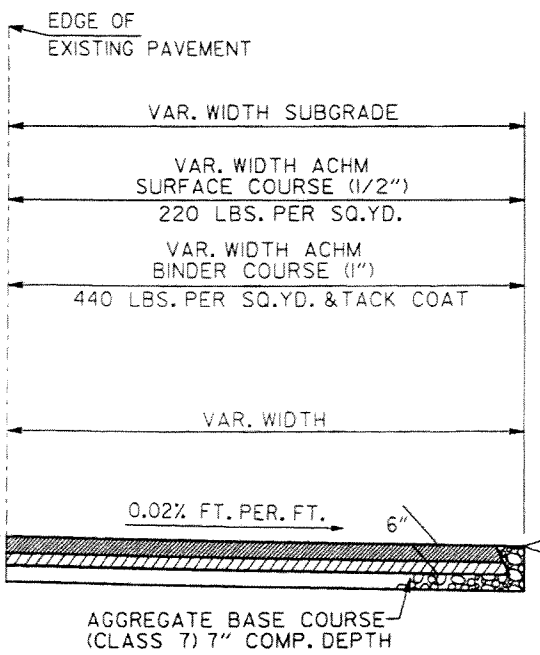
DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO. 061267	14
								56



**MAINTENANCE OF TRAFFIC**



**MAIN ST. JACKSONVILLE CUTOFF**  
STA. 9+17.00 TO STA. 5+52.00



**STAGE I CONSTRUCTION**

CONSTRUCT TEMPORARY WIDENING ON MAIN ST./ JACKSONVILLE CUTOFF WESTBOUND RIGHT TURN LANE TO HARRIS RD. TO PROVIDE A TOTAL WIDTH OF 20' PAVED TRAVELED WAY TO SERVE AS TEMPORARY DETOUR. STRIPE FOR 2-WAY TRAFFIC.

**LEGEND**

- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- ⇨ PROPOSED TRAFFIC FLOW

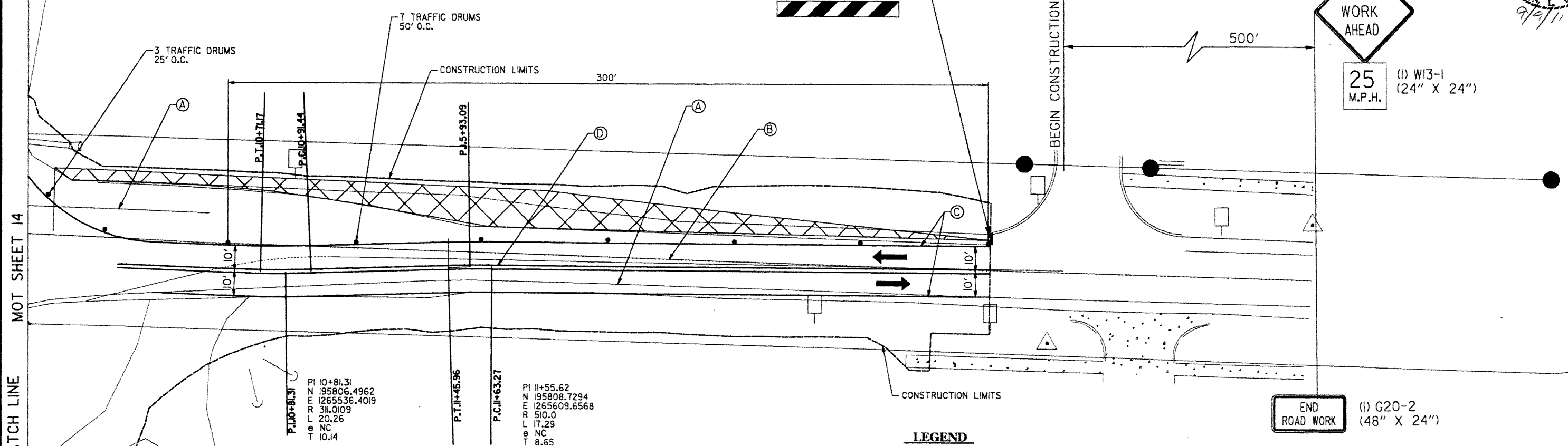
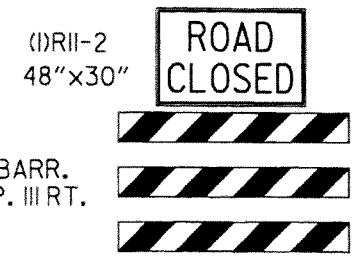
MAINTENANCE OF TRAFFIC-STAGE I  
JACKSONVILLE CUTOFF

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. PROJ. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						061267	15	56

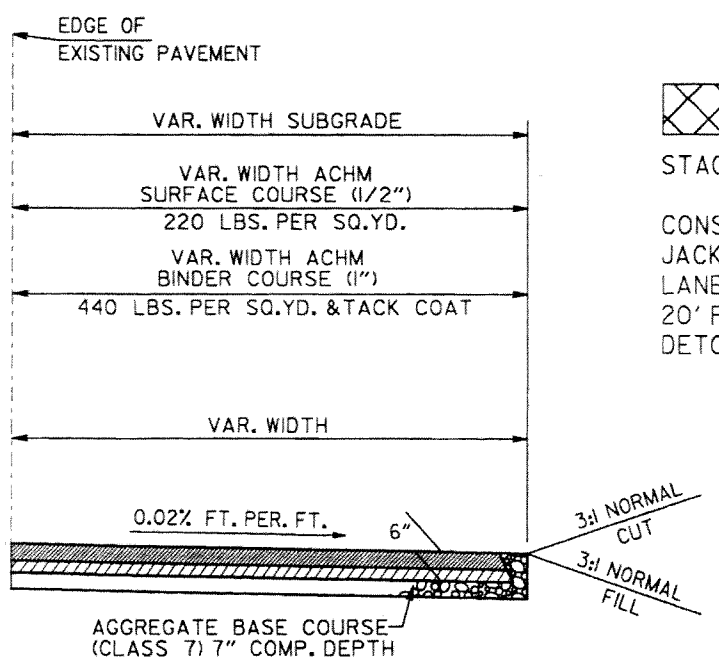
② MAINTENANCE OF TRAFFIC

**NOTES:**

1. TYPE III BARRICADE SHALL PROHIBIT TRAFFIC FROM ENTERING WORK ZONE.
2. TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
3. THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/BUSINESSES DURING CONSTRUCTION.



**MAIN ST/JACKSONVILLE CUTOFF  
STA. 13+30.00 TO STA. 09+88.00**



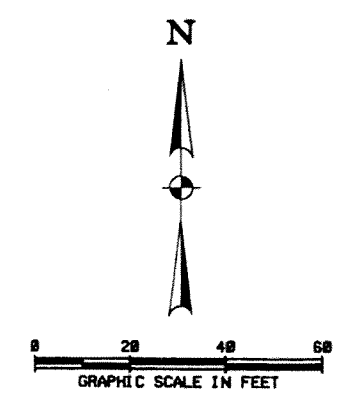
**STAGE I CONSTRUCTION**  
CONSTRUCT TEMPORARY WIDENING ON MAIN ST./ JACKSONVILLE CUTOFF WESTBOUND RIGHT TURN LANE TO HARRIS RD. TO PROVIDE A TOTAL WIDTH OF 20' PAVED TRAVELED WAY TO SERVE AS TEMPORARY DETOUR. STRIPE FOR 2-WAY TRAFFIC.

**LEGEND**

- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- PROPOSED TRAFFIC FLOW

**CONSTRUCTION PAVEMENT MARKINGS**

- TRAFFIC DRUMS = 12 EACH
- (A) REMOVE PAVEMENT MARKINGS (4" WHITE) = 694 L.F.
- (B) REMOVE PAVEMENT MARKINGS (4" YELLOW) = 764 L.F.
- (C) CONSTRUCTION PAVEMENT MARKINGS (4" WHITE) = 763 L.F.
- (D) CONSTRUCTION PAVEMENT MARKINGS (4" YELLOW) = 691 L.F.



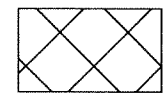
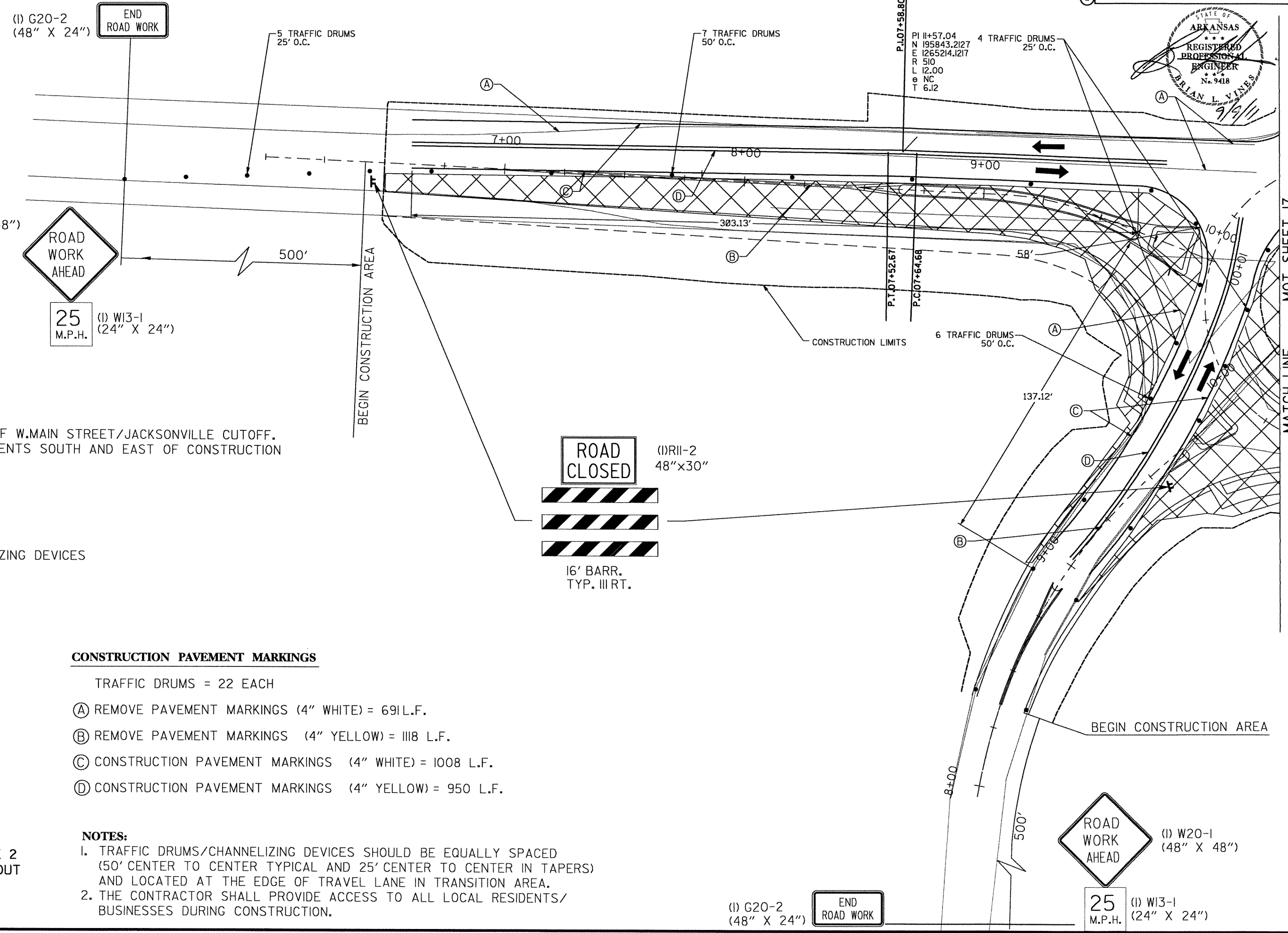
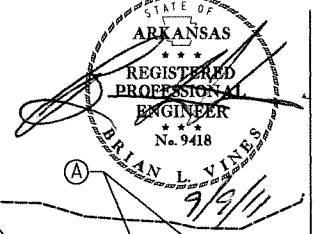
MAINTENANCE OF TRAFFIC-STAGE I  
MAIN STREET

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MATCH LINE  
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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						061267	16	56

② MAINTENANCE OF TRAFFIC



STAGE 2

SHIFT TRAFFIC TO NORTH SIDE OF W.MAIN STREET/JACKSONVILLE CUTOFF. CONSTRUCT PROPOSED IMPROVEMENTS SOUTH AND EAST OF CONSTRUCTION TRAFFIC.

**LEGEND**

- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- ➔ PROPOSED TRAFFIC FLOW



**MAINTENANCE OF TRAFFIC-STAGE 2  
MAIN ST. & HARRIS RD. ROUNDABOUT**

**CONSTRUCTION PAVEMENT MARKINGS**

- TRAFFIC DRUMS = 22 EACH
- Ⓐ REMOVE PAVEMENT MARKINGS (4" WHITE) = 691 L.F.
- Ⓑ REMOVE PAVEMENT MARKINGS (4" YELLOW) = 1118 L.F.
- Ⓒ CONSTRUCTION PAVEMENT MARKINGS (4" WHITE) = 1008 L.F.
- Ⓓ CONSTRUCTION PAVEMENT MARKINGS (4" YELLOW) = 950 L.F.

**NOTES:**

1. TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
2. THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/BUSINESSES DURING CONSTRUCTION.

(1) G20-2 (48" X 24") **END ROAD WORK**

**ROAD WORK AHEAD**  
25 M.P.H. (1) W13-1 (24" X 24")

**ROAD CLOSED** (1) R11-2 (48" X 30")  
16' BARR. TYP. III RT.



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				6	ARK.		17	56
JOB NO.						061267		

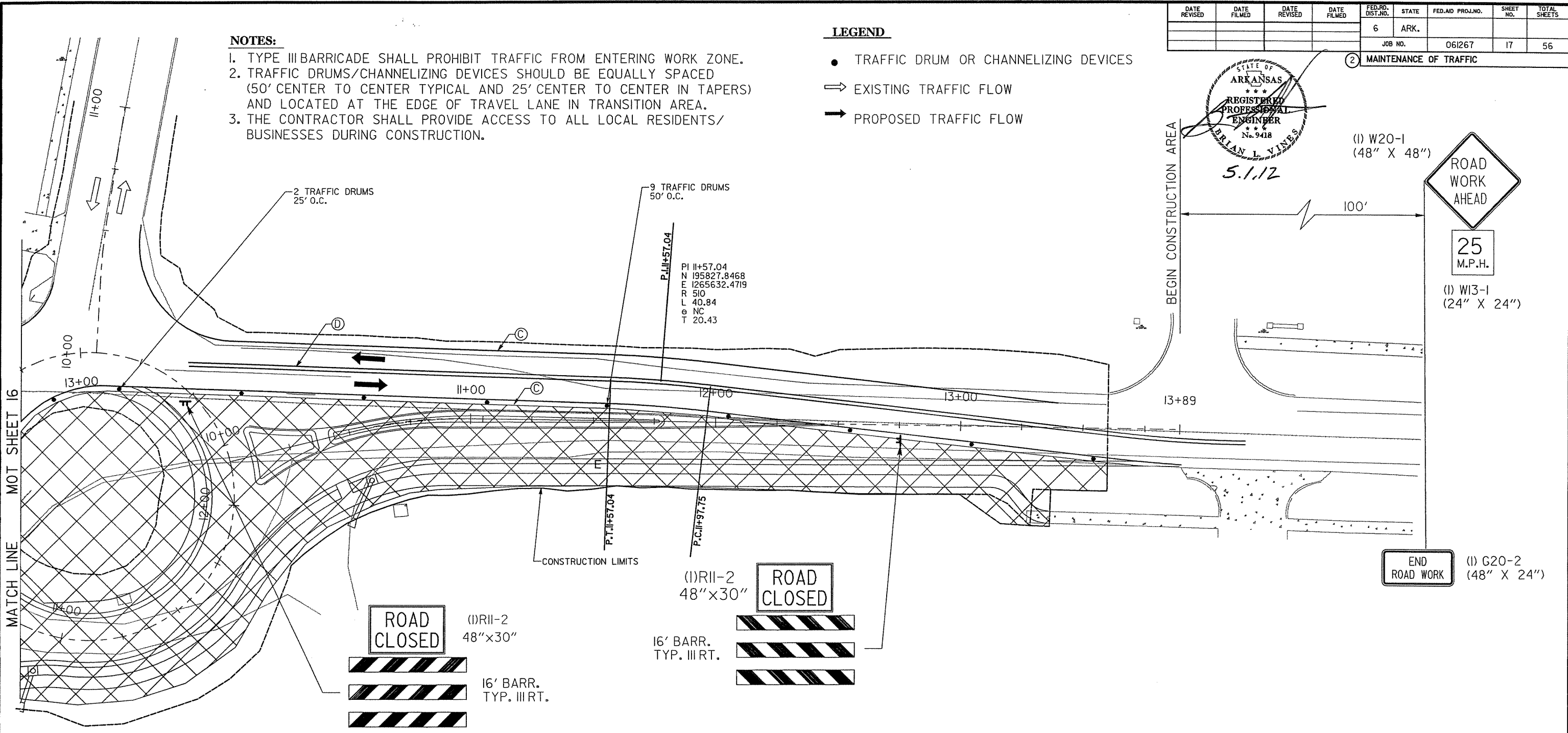
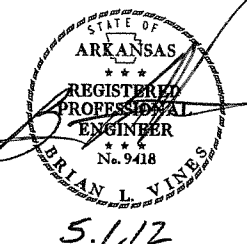
**NOTES:**

1. TYPE III BARRICADE SHALL PROHIBIT TRAFFIC FROM ENTERING WORK ZONE.
2. TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
3. THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/ BUSINESSES DURING CONSTRUCTION.

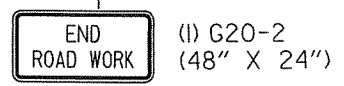
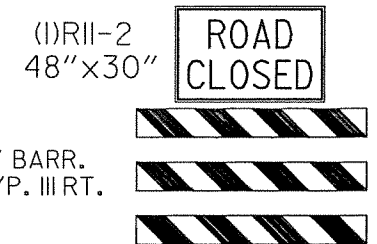
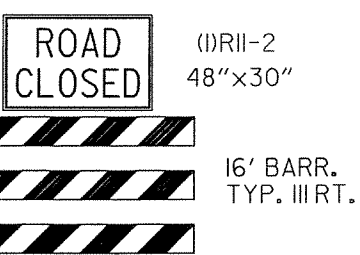
**LEGEND**

- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- ➔ PROPOSED TRAFFIC FLOW

**MAINTENANCE OF TRAFFIC**

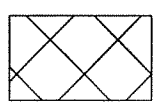


MOT SHEET 16  
 MATCH LINE



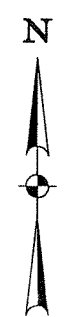
**CONSTRUCTION PAVEMENT MARKINGS**

- TRAFFIC DRUMS = 11 EACH
- (A) REMOVE PERMANENT MARKINGS (4" WHITE) = 0 L.F.
- (B) REMOVE PERMANENT MARKINGS (4" YELLOW) = 882 L.F.
- (C) CONSTRUCTION PAVEMENT MARKINGS (4" WHITE) = 1014 L.F.
- (D) CONSTRUCTION PAVEMENT MARKINGS (4" YELLOW) = 864 L.F.



STAGE 2

SHIFT TRAFFIC TO NORTH SIDE OF W.MAIN STREET/JACKSONVILLE CUTOFF. CONSTRUCT PROPOSED IMPROVEMENTS SOUTH AND EAST OF CONSTRUCTION TRAFFIC.



MAINTENANCE OF TRAFFIC-STAGE 2  
 MAIN ST. & HARRIS RD. ROUNDABOUT

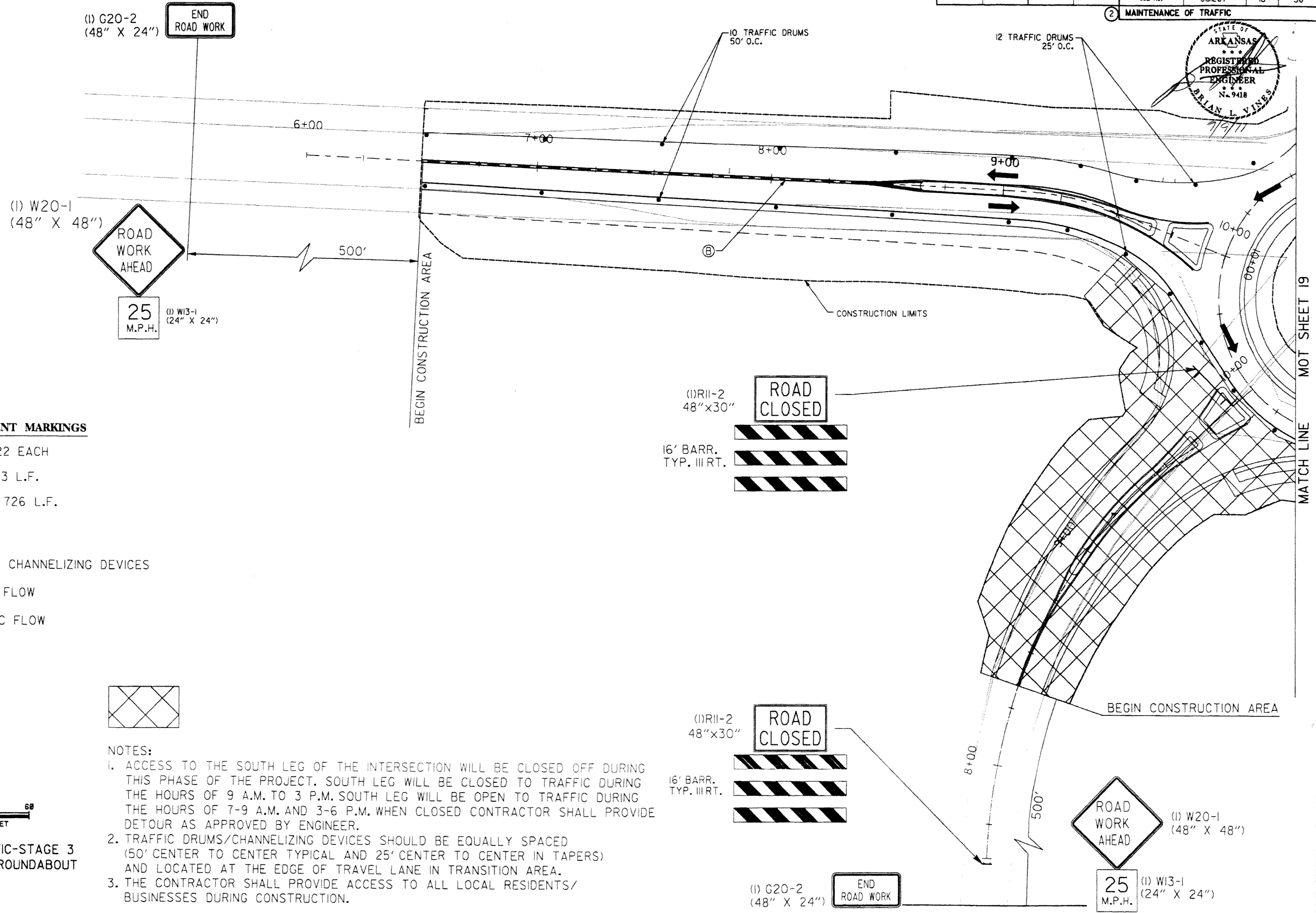
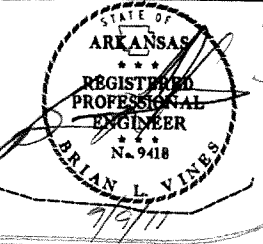
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				6	ARK.			
							JOB NO.	56
							061267	18

② MAINTENANCE OF TRAFFIC

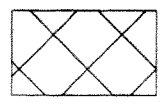


**CONSTRUCTION PAVEMENT MARKINGS**

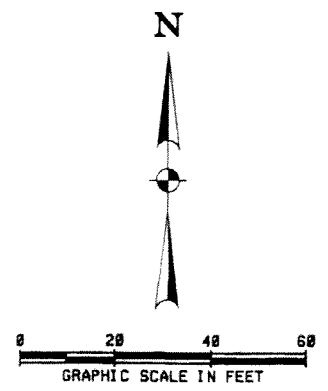
- TRAFFIC DRUMS = 22 EACH
- (A) CPM (4" WHITE) = 413 L.F.
- (B) CPM (4" YELLOW) = 726 L.F.

**LEGEND**

- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- PROPOSED TRAFFIC FLOW



- NOTES:**
- ACCESS TO THE SOUTH LEG OF THE INTERSECTION WILL BE CLOSED OFF DURING THIS PHASE OF THE PROJECT. SOUTH LEG WILL BE CLOSED TO TRAFFIC DURING THE HOURS OF 9 A.M. TO 3 P.M. SOUTH LEG WILL BE OPEN TO TRAFFIC DURING THE HOURS OF 7-9 A.M. AND 3-6 P.M. WHEN CLOSED CONTRACTOR SHALL PROVIDE DETOUR AS APPROVED BY ENGINEER.
  - TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
  - THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/ BUSINESSES DURING CONSTRUCTION.

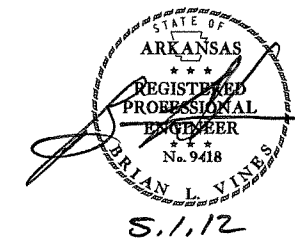


**MAINTENANCE OF TRAFFIC-STAGE 3  
MAIN ST. & HARRIS RD. ROUNDABOUT**

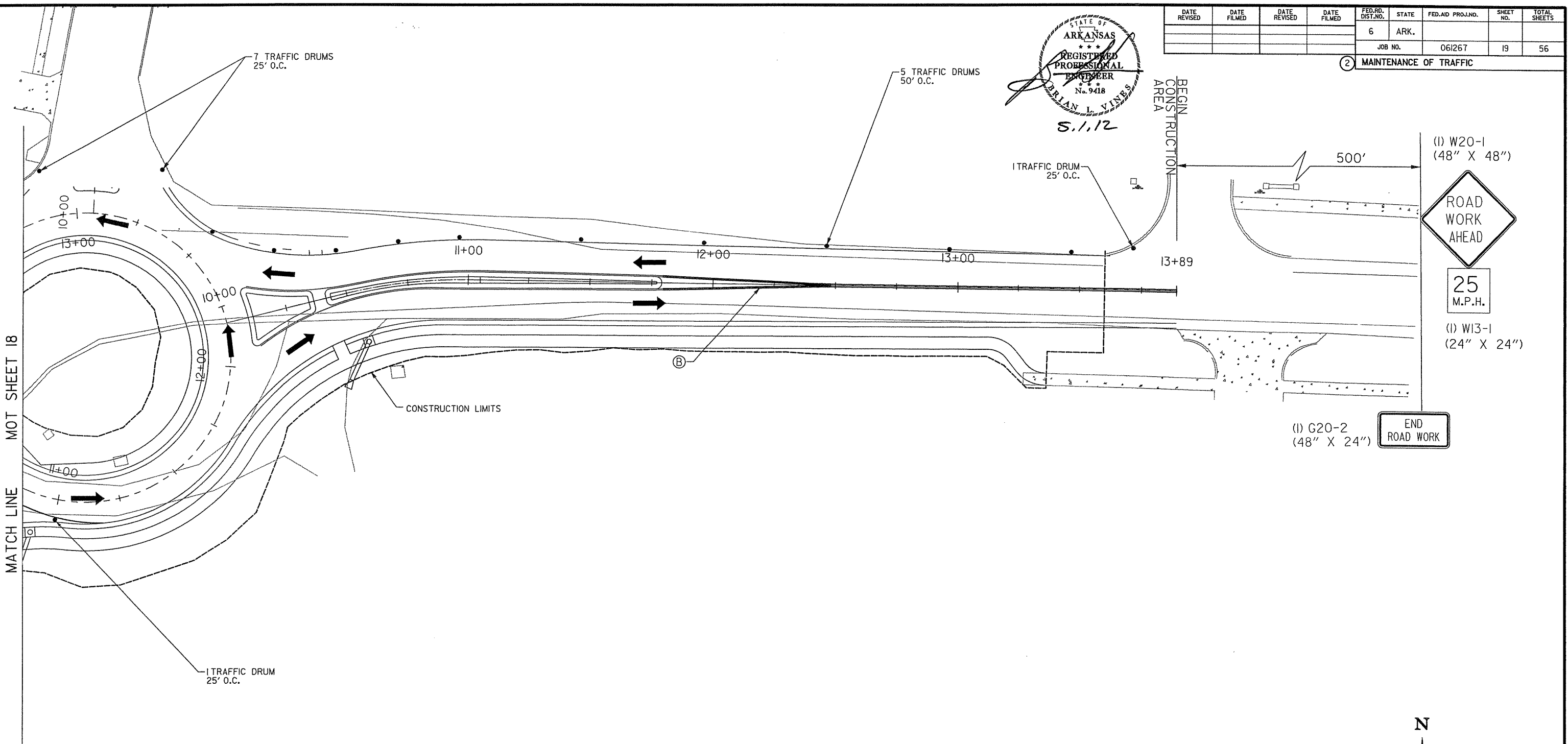
MATCH LINE MOT SHEET 19

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 5/1/2012

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061267	19	56	

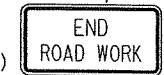


② MAINTENANCE OF TRAFFIC

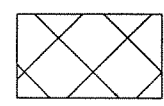


25 M.P.H.

(1) W13-1 (24" X 24")



(1) G20-2 (48" X 24")



STAGE 3

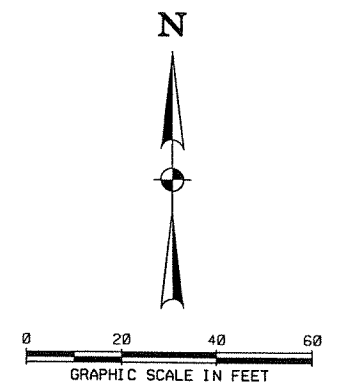
- NOTES:**
1. ACCESS TO THE SOUTH LEG OF THE INTERSECTION WILL BE CLOSED OFF DURING THIS PHASE OF THE PROJECT. SOUTH LEG WILL BE CLOSED TO TRAFFIC DURING THE HOURS OF 9 A.M. TO 3 P.M. SOUTH LEG WILL BE OPEN TO TRAFFIC DURING THE HOURS OF 7-9 A.M. AND 3-6 P.M. WHEN CLOSED CONTRACTOR SHALL PROVIDE DETOUR AS APPROVED BY ENGINEER.
  2. TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
  3. THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/BUSINESSES DURING CONSTRUCTION.

**LEGEND**

- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- ➔ PROPOSED TRAFFIC FLOW

**CONSTRUCTION PAVEMENT MARKINGS**

- TRAFFIC DRUMS = 14 EACH
- Ⓐ CPM (4" WHITE) = 0 L.F.
- Ⓑ CPM (4" YELLOW) = 277 L.F.



MAINTENANCE OF TRAFFIC-STAGE 3  
 MAIN ST. & HARRIS RD. ROUNDABOUT

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				6	ARK.			
				JOB NO.	061267		20	56

② MAINTENANCE OF TRAFFIC



(1) G20-2 (48" X 24")  
END ROAD WORK



25 M.P.H.

(1) W20-1 (48" X 48")

(1) W13-1 (24" X 24")

BEGIN CONSTRUCTION AREA

8+00

500'

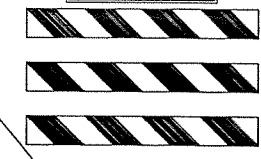
9+00

CONSTRUCTION LIMITS

CONSTRUCTION LIMITS

13 TRAFFIC DRUMS 25' O.C.

(1) R11-2 48" X 30"  
ROAD CLOSED



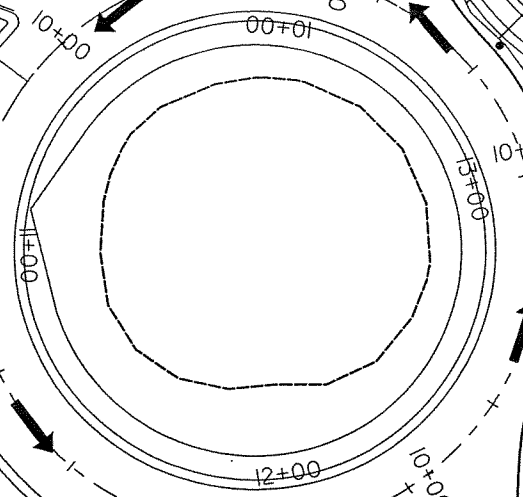
(1) W20-1 (48" X 48")



25 M.P.H.

(1) W13-1 (24" X 24")

16' BARR. TYP. III RT.



BEGIN CONSTRUCTION AREA

500'

**LEGEND**

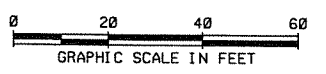
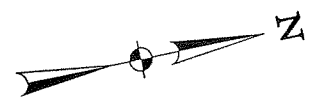
- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- ➔ PROPOSED TRAFFIC FLOW

**CONSTRUCTION PAVEMENT MARKINGS**

- TRAFFIC DRUMS = 13 EACH
- ⊗ STAGE 4
- Ⓐ CPM (4" WHITE) = 0 L.F.
- Ⓑ CPM (4" YELLOW) = 0 L.F.

**NOTES:**

- ACCESS TO THE NORTH LEG OF THE INTERSECTION WILL BE CLOSED OFF DURING THIS PHASE OF THE PROJECT. NORTH LEG WILL BE CLOSED TO TRAFFIC DURING THE HOURS OF 9 A.M. TO 3 P.M. SOUTH LEG WILL BE OPEN TO TRAFFIC DURING THE HOURS OF 7-9 A.M. AND 3-6 P.M. WHEN CLOSED CONTRACTOR SHALL PROVIDE DETOUR AS APPROVED BY ENGINEER.
- TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
- THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/BUSINESSES DURING CONSTRUCTION.



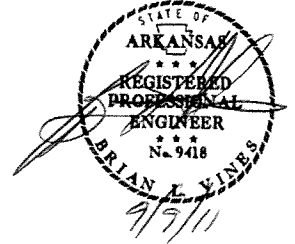
MAINTENANCE OF TRAFFIC-STAGE 4  
MAIN ST. & HARRIS RD. ROUNDABOUT

MATCH LINE MOT SHEET 21

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						061267	21	56

② MAINTENANCE OF TRAFFIC



STAGE 4

NOTES:

- ACCESS TO THE NORTH LEG OF THE INTERSECTION WILL BE CLOSED OFF DURING THIS PHASE OF THE PROJECT. NORTH LEG WILL BE CLOSED TO TRAFFIC DURING THE HOURS OF 9 A.M. TO 3 P.M. SOUTH LEG WILL BE OPEN TO TRAFFIC DURING THE HOURS OF 7-9 A.M. AND 3-6 P.M. WHEN CLOSED CONTRACTOR SHALL PROVIDE DETOUR AS APPROVED BY ENGINEER.
- TRAFFIC DRUMS/CHANNELIZING DEVICES SHOULD BE EQUALLY SPACED (50' CENTER TO CENTER TYPICAL AND 25' CENTER TO CENTER IN TAPERS) AND LOCATED AT THE EDGE OF TRAVEL LANE IN TRANSITION AREA.
- THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL LOCAL RESIDENTS/ BUSINESSES DURING CONSTRUCTION.

LEGEND

- TRAFFIC DRUM OR CHANNELIZING DEVICES
- ⇨ EXISTING TRAFFIC FLOW
- ➔ PROPOSED TRAFFIC FLOW

CONSTRUCTION PAVEMENT MARKINGS

TRAFFIC DRUMS = 13 EACH

- Ⓐ CPM (4" WHITE) = 0 L.F.
- Ⓑ CPM (4" YELLOW) = 0 L.F.

(1) G20-2 (48" X 24")

END ROAD WORK

BEGIN CONSTRUCTION AREA

13+89

12+00

13+00

5 TRAFFIC DRUMS 50' O.C.

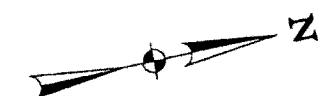
500'

ROAD WORK AHEAD

25 M.P.H.

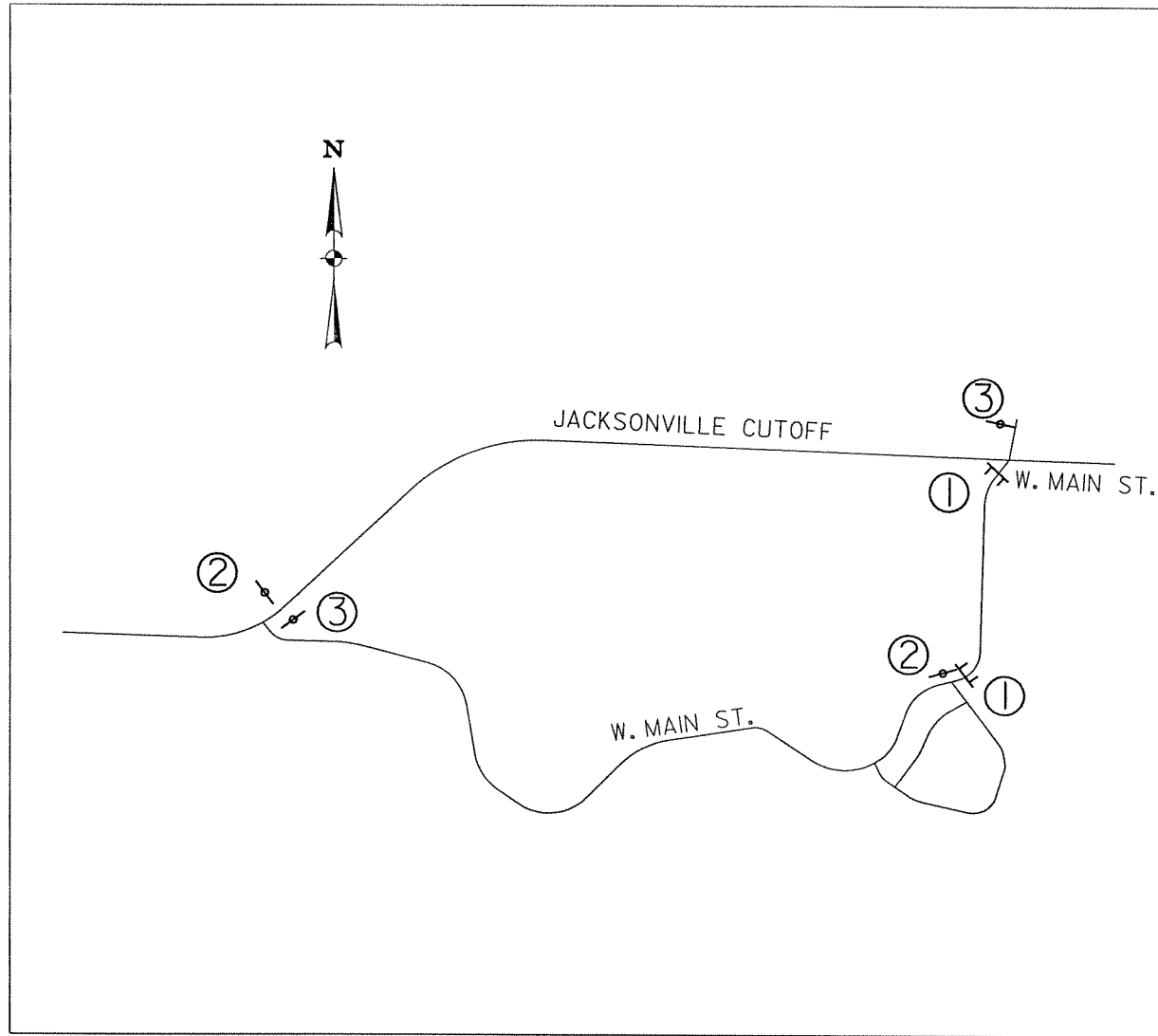
(1) W20-1 (48" X 48")

(1) W13-1 (24" X 24")



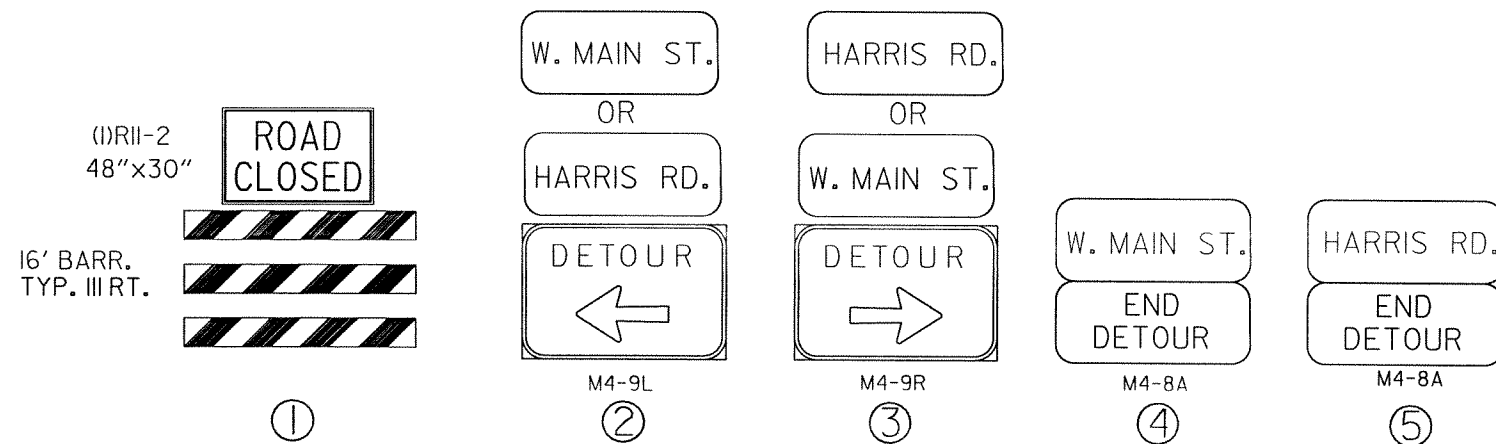
MAINTENANCE OF TRAFFIC-STAGE 4 MAIN ST. & HARRIS RD. ROUNDABOUT

### STAGE 3 DETOUR DETAILS

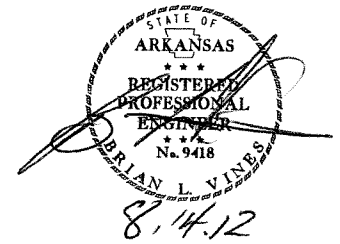


**NOTES:**

ACCESS TO THE SOUTH LEG (W. MAIN ST.) WILL BE CLOSED OFF DURING THIS PHASE OF THE PROJECT. W. MAIN ST. WILL BE CLOSED TO TRAFFIC DURING THE HOURS OF 9 A.M. TO 3 P.M. HARRIS RD. WILL BE OPEN TO TRAFFIC DURING THE HOURS OF 7-9 A.M. AND 3-6 P.M. WHEN CLOSED, CONTRACTOR SHALL PROVIDE DETOUR AS SHOWN ABOVE FOR ALL EASTBOUND AND WESTBOUND TRAFFIC ATTEMPTING TO TURN ONTO HARRIS RD.



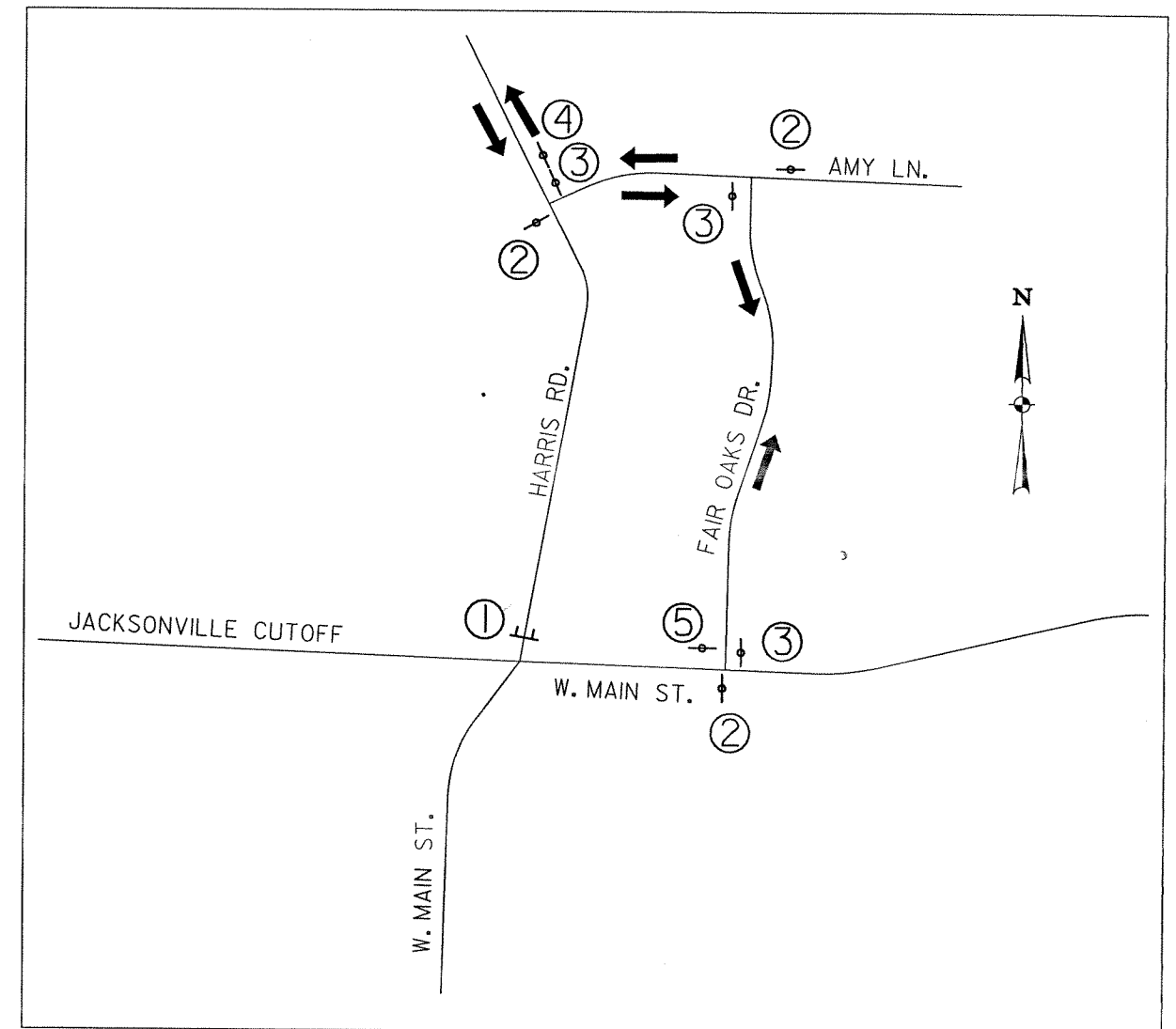
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				6	ARK.			
JOB NO.						061267	22	56
② MAINTENANCE OF TRAFFIC								



**NOTES:**

ACCESS TO HARRIS RD. WILL BE CLOSED OFF DURING THIS PHASE OF THE PROJECT. HARRIS RD. WILL BE CLOSED TO TRAFFIC DURING THE HOURS OF 9 A.M. TO 3 P.M. HARRIS RD. WILL BE OPEN TO TRAFFIC DURING THE HOURS OF 7-9 A.M. AND 3-6 P.M. WHEN CLOSED, CONTRACTOR SHALL PROVIDE DETOUR AS SHOWN ABOVE FOR ALL EASTBOUND AND WESTBOUND TRAFFIC ATTEMPTING TO TURN ONTO HARRIS RD.

### STAGE 4 DETOUR DETAILS



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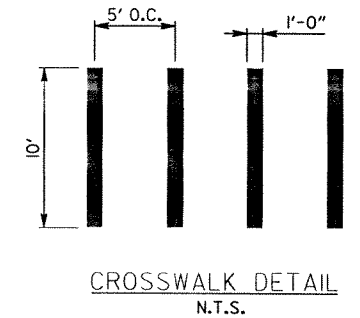
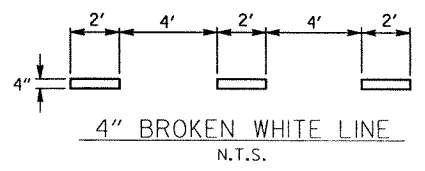
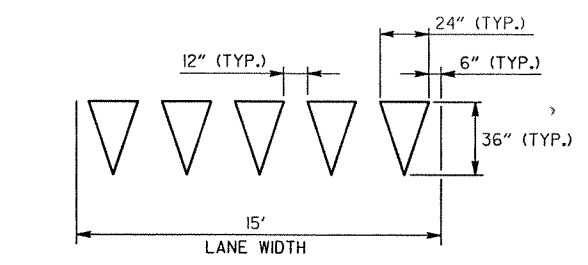
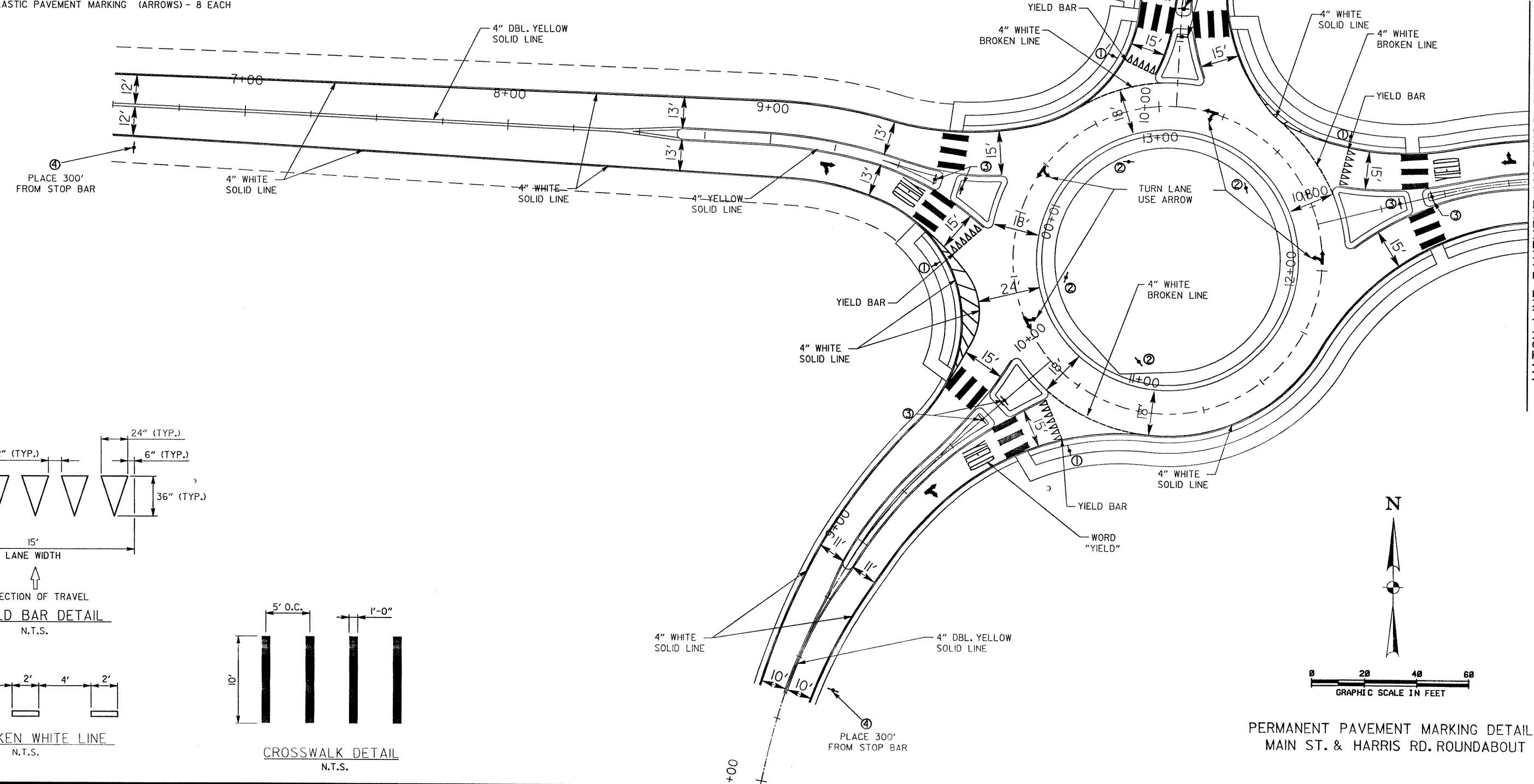
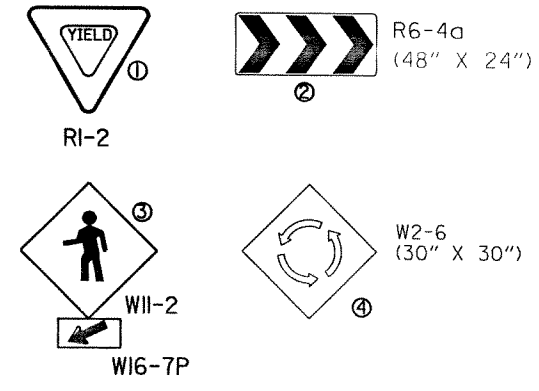
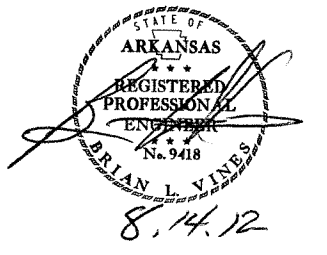
6/29/2012

### PERMANENT PAVEMENT MARKINGS

- THERMOPLASTIC PAVEMENT MARKING SGL. SOLID YELLOW (4") - 679 L.F.
- THERMOPLASTIC PAVEMENT MARKING DBL. SOLID YELLOW (4") - 427 L.F.
- THERMOPLASTIC PAVEMENT MARKING SGL. SOLID WHITE (4") - 1712 L.F.
- THERMOPLASTIC PAVEMENT MARKING SKIP WHITE (4") - 113 L.F.
- THERMOPLASTIC PAVEMENT MARKING SGL. SOLID WHITE (12") - 120 L.F.
- THERMOPLASTIC PAVEMENT MARKING SGL. SOLID WHITE (8") - 0 L.F.
- YIELD BAR PAVEMENT MARKING - 60 L.F.
- THERMOPLASTIC PAVEMENT MARKING (WORDS) - 4 EACH
- THERMOPLASTIC PAVEMENT MARKING (ARROWS) - 8 EACH

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 061267							23	56

PERMANENT PAVEMENT MARKING DETAILS

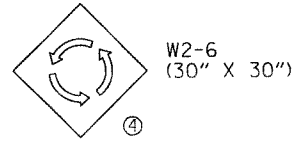


MATCH LINE PAVEMENT MARKINGS SHEET 24

PERMANENT PAVEMENT MARKING DETAILS  
MAIN ST. & HARRIS RD. ROUNDABOUT

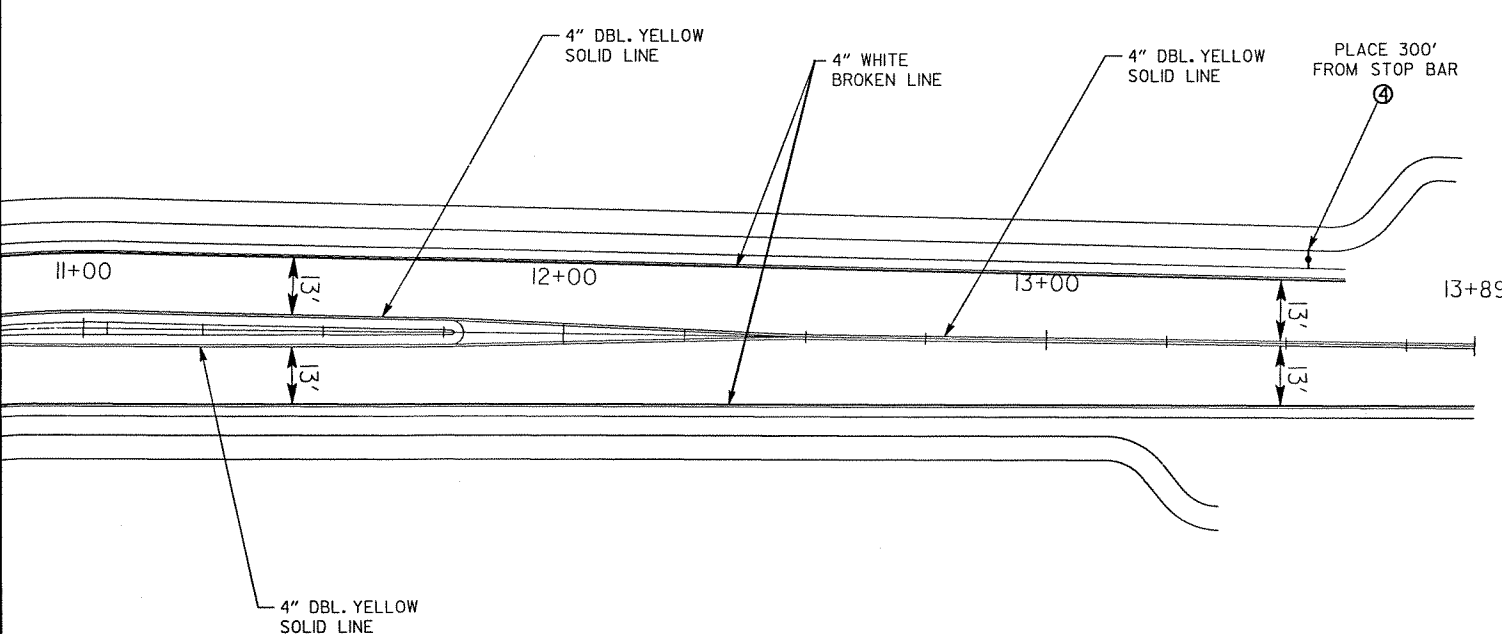
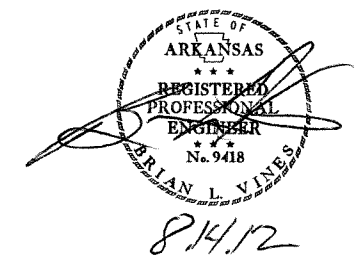
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MATCH LINE PAVEMENT MARKINGS SHEET 23



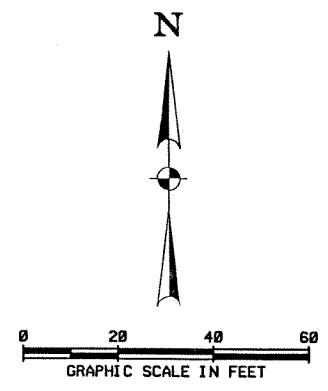
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061267	24	56	

PERMANENT PAVEMENT MARKING DETAILS



PERMANENT PAVEMENT MARKINGS

- THERMOPLASTIC PAVEMENT MARKING SGL. SOLID YELLOW (4")  
- 192 L.F.
- THERMOPLASTIC PAVEMENT MARKING DBL. SOLID YELLOW (4")  
- 282 L.F.
- THERMOPLASTIC PAVEMENT MARKING SGL. SOLID WHITE (4")  
- 590 L.F.

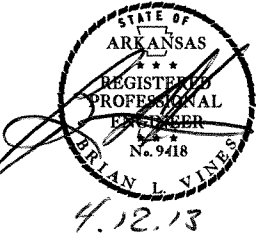


PERMANENT PAVEMENT MARKING DETAILS  
 MAIN ST. & HARRIS RD. ROUNDABOUT



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061267	25	56	

② QUANTITIES



**REMOVAL AND DISPOSAL OF ITEMS**

STATION	STATION	LOCATION	CURB AND GUTTER LIN. FT.	CONCRETE DRIVEWAYS SQ. YD.	WALKS SQ. YD.
8+96	10+00	LT. OF C.L. JACKSONVILLE CUTOFF	118		
10+08	10+36	LT. OF C.L. HARRIS RD.	39		
13+27	13+36	RT. OF C.L. WEST MAIN ST.			5
10+46		LT. OF HARRIS RD.		16	
<b>TOTALS:</b>			<b>157</b>	<b>16</b>	<b>5</b>

**ADVANCE WARNING SIGNS AND DEVICES**

SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE 1	STAGE 2	STAGE 3	STAGE 4	END OF JOB	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS EACH	BARRICADES (TYPE III) RIGHT LIN. FT.	
									NO.	SQ. FT.			
LIN. FT. - EACH													
W20-1	ROAD WORK AHEAD	48"x48"	4	4	4	4		4	4	64.0			
G20-2	END ROAD WORK	48"x24"	4	4	4	4		4	4	32.0			
W13-1	SPEED LIMIT (ADVISORY)	24"x24"	4	4	4	4		4	4	16.0			
R11-2	ROAD CLOSED	48"x30"	2	3	4	4		4	4	40.0			
M4-9L	DETOUR LEFT W/ STREET NAME	30"x36"			2	3		3	3	22.5			
M4-9R	DETOUR RIGHT W/ STREET NAME	30"x36"			2	3		3	3	22.5			
MR-8A	END DETOUR W/ STREET NAME	24"x30"				2		2	2	10.0			
TRAFFIC DRUMS			29	33	36	26		36			36		
TYPE III BARRICADE-RT. (16')			2	4	4	4		4				64	
<b>TOTALS:</b>											<b>207.0</b>	<b>36</b>	<b>64</b>

**EARTHWORK**

STATION	STATION	LOCATION / DESCRIPTION	UNCLASSIFIED EXCAVATION CU. YD.	COMPACTED EMBANKMENT CU. YD.	* SOIL STABILIZATION TON
6+75	10+00	JACKSONVILLE CUTOFF	750	255	
8+35	10+00	WEST MAIN ST. (SOUTH LEG)	87	260	
10+00	13+60	WEST MAIN ST.	453	318	
10+00	11+00	HARRIS RD. ROUNDBOUT	133	22	
			3	683	
ENTIRE PROJECT		TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER			50
<b>TOTALS:</b>			<b>1426</b>	<b>1538</b>	<b>50</b>

\* QUANTITY ESTIMATED.  
SEE SECTION 104.03 OF THE STD. SPECS.  
NOTE: EARTHWORK QUANTITIES SHOWN ABOVE SHALL BE PAID AS PLAN QUANTITY.

**CONSTRUCTION PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS**

DESCRIPTION	STAGE 1	STAGE 2	STAGE 3	END OF JOB	REMOVAL OF PERMANENT PAVEMENT MARKINGS LIN. FT.	CONSTRUCTION PAVEMENT MARKINGS LIN. FT.	THERMOPLASTIC PAVEMENT MARKINGS								
							4"		12" WHITE	WORDS	ARROWS	YIELD LINE			
							WHITE	YELLOW					LIN. FT.	EACH	LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS	2446	2691			5137										
CONSTRUCTION PAVEMENT MARKINGS	3040	3836	1416			8292									
THERMOPLASTIC PAVEMENT MARKINGS WHITE (4")				2415			2415								
THERMOPLASTIC PAVEMENT MARKINGS YELLOW (4")				1580			1580								
THERMOPLASTIC PAVEMENT MARKINGS YELLOW (12")				120					120						
THERMOPLASTIC PAVEMENT MARKINGS WORDS				4						4					
THERMOPLASTIC PAVEMENT MARKINGS ARROWS				8							8				
THERMOPLASTIC PAVEMENT MARKING (YIELD LINE)				60								60			
<b>TOTALS:</b>					<b>5137</b>	<b>8292</b>	<b>2415</b>	<b>1580</b>	<b>120</b>	<b>4</b>	<b>8</b>	<b>60</b>			

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2003 EDITION.

**DRIVEWAYS & TURNOUTS**

STATION	SIDE	LOCATION	WIDTH FEET	**MODIFIED CURB		PORTLAND CEMENT CONCRETE DRIVEWAY SQ. YD.
				STATION	STATION	
10+46	LT.	HARRIS RD.	16	10+24	10+68	33.80
<b>TOTAL:</b>						<b>33.80</b>

**CLEARING AND GRUBBING**

STATION	STATION	LOCATION	CLEARING STATION	GRUBBING STATION
10+00	12+00	C.L. ROUNDBOUT	2	2
<b>TOTALS:</b>				

**ACHM PATCHING OF EXISTING ROADWAY**

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	10
<b>TOTAL:</b>	<b>10</b>

NOTE: QUANTITY IS ESTIMATED  
SEE SECTION 104.03 OF THE STD. SPECS.

**BENCH MARKS**

STATION	LOCATION	BENCH MARKS EACH
ENTIRE PROJECT	- IN AND WHERE DIRECTED BY THE ENGINEER	2
<b>TOTAL:</b>		<b>2</b>

NOTE: SHOWN FOR INFORMATION ONLY BENCH MARKS SHALL BE FURNISHED AND PLACED BY STATE FORCES.

**REMOVAL AND DISPOSAL OF DROP INLETS**

STATION	LOCATION/DESCRIPTION	EACH
10+23	LT. OF C.L. HARRIS RD.	1
<b>TOTAL:</b>		<b>1</b>

**EROSION CONTROL**

STATION	STATION	LOCATION	PERMANENT EROSION CONTROL					TEMPORARY EROSION CONTROL					*SEDIMENT REMOVAL & DISPOSAL CU. YD.		
			SEEDING ACRE	LIME TON	MULCH COVER ACRE	WATER M.GAL.	SECOND SEEDING APPLICATION ACRE	TEMPORARY SEEDING ACRE	MULCH COVER ACRE	WATER M.GAL.	SAND BAG DITCH CHECKS (E-5) BAG	ROCK DITCH CHECKS (E-6) CU. YD.		DROP INLET SILT FENCE (E-7) LIN. FT.	SILT FENCE (E-11) LIN. FT.
ENTIRE PROJECT	PROJECT	STAGE 1						0.10	0.10	2.0	22		16	855	33
ENTIRE PROJECT	PROJECT	STAGE 2						0.20	0.20	4.1	22			1160	44
ENTIRE PROJECT	PROJECT		0.65	1.30	0.65	66.3	0.65								2
*ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.			0.25	0.50	0.25	25.5	0.25	0.10	0.10	2.0	22	9	16	504	19
<b>TOTALS:</b>			<b>0.90</b>	<b>1.80</b>	<b>0.90</b>	<b>91.8</b>	<b>0.90</b>	<b>0.40</b>	<b>0.40</b>	<b>8.1</b>	<b>66</b>	<b>9</b>	<b>32</b>	<b>2519</b>	<b>98</b>

BASIS OF ESTIMATE:  
LIME ..... 2 TONS / ACRE OF SEEDING  
WATER ..... 102.0 M.G. / ACRE OF SEEDING  
WATER ..... 20.4 M.G. / ACRE OF TEMPORARY SEEDING.  
SAND BAG DITCH CHECKS ..... 22 BAGS / LOCATION  
ROCK DITCH CHECKS ..... 3 CU. YD. / LOCATION

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

\*QUANTITIES ARE ESTIMATED.  
SEE SECTION 104.03 OF THE STD. SPECS.

**ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC**

LOCATION	TON	TACK COAT GALLON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	10	20
<b>TOTALS:</b>	<b>10</b>	<b>20</b>

NOTE: QUANTITIES ARE ESTIMATED.  
SEE SECTION 104.03 OF THE STD. SPECS.

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**SELECTED PIPE BEDDING**

LOCATION	SELECTED PIPE BEDDING CU.YD.
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	10
<b>TOTAL:</b>	<b>10</b>

NOTE: QUANTITY ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECS.

**COLD MILLING ASPHALT PAVEMENT**

STATION	STATION	LOCATION	AVG. WIDTH FEET	COLD MILLING ASPHALT PAVEMENT SQ. YD.
6+00	6+50	C.L. JACKSONVILLE CUTOFF	24	133.33
7+85	8+35	C.L. WEST MAIN ST. (SOUTH LEG)	20	111.11
13+60	14+10	C.L. WEST MAIN ST.	26	144.44
11+00	11+50	C.L. HARRIS RD.	28	155.56
<b>TOTAL:</b>				<b>544.44</b>

NOTE: AVERAGE MILLING DEPTH 1".

**STRUCTURES**

STATION	LOCATION/DESCRIPTION	REINFORCED CONCRETE PIPE CULVERT (CLASS III)		FLARED END SECTIONS FOR R.C. PIPE CULVERTS		DROP INLETS		SOLID SODDING SQ.YD.	WATER M.GAL.	STD. DWG. NOS.
		18"	24"	18"	24"	TYPE	EXT.			
		LIN. FT.		EACH		MO	4'			
9+70	LT. OF C.L. JACKSONVILLE CUTOFF / CONSTRUCT D.I. & PIPE OUTLET	6		1		1		5	0.06	FES-1, FES-2, FPC-9M, PCC-1
9+70	RT. OF C.L. JACKSONVILLE CUTOFF / EXTEND PIPE CULVERT		6		1			8	0.10	FES-1, FES-2, PCC-1
9+70	RT. OF C.L. JACKSONVILLE CUTOFF / CONSTRUCT D.I. & PIPE OUTLET	14		1		1		5	0.06	FES-1, FES-2, FPC-9M, PCC-1
10+54	RT. OF C.L. WEST MAIN ST. / CONSTRUCT D.I. & PIPE OUTLET	6		1		1	2	5	0.06	FES-1, FES-2, FPC-9M, PCC-1
10+54	LT. OF C.L. WEST MAIN ST. / CONSTRUCT D.I. & PIPE OUTLET	6		1		1	2	5	0.06	FES-1, FES-2, FPC-9M, PCC-1
10+93	RT. OF C.L. ROUNDABOUT / CONSTRUCT D.I. & PIPE OUTLET	8		1		1	2	5	0.06	FES-1, FES-2, FPC-9M, PCC-1
10+23	LT. OF C.L. HARRIS RD. / CONSTRUCT D.I.					1	2			FPC-9M
10+20	RT. OF C.L. HARRIS RD. / CONSTRUCT D.I.					1	2			FPC-9M
<b>TOTALS:</b>		<b>40</b>	<b>6</b>	<b>5</b>	<b>1</b>	<b>7</b>	<b>10</b>	<b>33</b>	<b>0.40</b>	

BASIS OF ESTIMATE:  
WATER..... 12.6 GAL. / SQ. YD. OF SOLID SODDING.

NOTE: FOR R.C. PIPE CULVERT INSTALLATIONS USE TYPE 3 BEDDING UNLESS OTHERWISE SPECIFIED.  
\* NOTE: REMOVAL OF EXISTING 24" R.C. PIPE CULVERT FOR THE CONSTRUCTION OF THE DROP INLET SHALL BE INCLUDED IN PRICE BID FOR "DROP INLETS (TYPE MO)".

**BASE AND SURFACING**

STATION	STATION	LOCATION	LENGTH FEET	AGGREGATE BASE COURSE (CLASS 7)		TACK COAT				ACHM BASE COURSE (1 1/2")				ACHM BINDER COURSE (1")				ACHM SURFACE COURSE (1/2")			
				TON / STATION	TON	TOTAL WID. FEET	SQ.YD.	GALLONS / SQ.YD.	GALLON	AVG. WID. FEET	SQ.YD.	POUND / SQ.YD.	PG 70-22 TON	AVG. WID. FEET	SQ.YD.	POUND / SQ.YD.	PG 70-22 TON	AVG. WID. FEET	SQ.YD.	POUND / SQ.YD.	PG 70-22 TON
<b>MAIN LANES</b>																					
6+00	6+50	C.L. JACKSONVILLE CUTOFF	50.0			24.0	133.3	0.10	13.3												
6+50	8+66	C.L. JACKSONVILLE CUTOFF - TRANSITION	216.0			82.2	1972.8	0.03	59.2	27.9	669.6	550	184.1	27.5	660.0	495	163.4	27.2	652.8	440	143.6
8+66	9+44	C.L. JACKSONVILLE CUTOFF	78.0			79.2	686.4	0.03	20.6	26.9	233.1	550	64.1	26.5	229.7	495	56.9	26.2	227.1	440	50.0
7+85	8+35	C.L. WEST MAIN ST. (SOUTH LEG)	50.0			20.0	111.1	0.10	11.1									20.0	111.1	220	12.2
8+35	8+90	C.L. WEST MAIN ST. (SOUTH LEG) - TRANSITION	55.0			70.2	429.0	0.03	12.9	23.9	146.1	550	40.2	23.5	143.6	495	35.5	23.2	141.8	440	31.2
8+90	9+45	C.L. WEST MAIN ST. (SOUTH LEG)	55.0			67.2	410.7	0.03	12.3	22.9	139.9	550	38.5	22.5	137.5	495	34.0	22.2	135.7	440	29.9
10+90	11+77	C.L. WEST MAIN ST.	87.0			79.2	765.6	0.03	23.0	26.9	260.0	550	71.5	26.5	256.2	495	63.4	26.2	253.3	440	55.7
11+77	12+48	C.L. WEST MAIN ST. - TRANSITION	71.0			85.2	672.1	0.03	20.2	28.9	228.0	550	62.7	28.5	224.8	495	55.6	28.2	222.5	440	49.0
12+48	13+60	C.L. WEST MAIN ST.	112.0			79.2	985.6	0.03	29.6	26.9	334.8	550	92.1	26.5	329.8	495	81.6	26.2	326.0	440	71.7
13+60	14+10	C.L. WEST MAIN ST.	50.0			26.0	144.4	0.10	14.4									26.0	144.4	220	15.9
10+45	10+84	C.L. HARRIS RD. - TRANSITION	39.0			91.2	395.2	0.03	11.9	30.9	133.9	550	36.8	30.5	132.2	495	32.7	30.2	130.9	440	28.8
10+84	11+00	C.L. HARRIS RD.	16.0			85.2	151.5	0.03	4.5	28.9	51.4	550	14.1	28.5	50.7	495	12.5	28.2	50.1	440	11.0
11+00	11+50	C.L. HARRIS RD.	50.0			28.0	155.6	0.10	15.6									28.0	155.6	220	17.1
		C.L. ROUNDABOUT	VAR.			VAR.	7485.9	0.03	224.6	VAR.	2840.5	550	781.1	VAR.	2495.3	495	617.6	VAR.	2495.3	440	549.0
<b>TOTALS:</b>						<b>566.8</b>			<b>473.2</b>				<b>1385.2</b>				<b>1153.2</b>				<b>1156.6</b>

BASIS OF ESTIMATE:  
ACHM SURFACE COURSE (1/2").....94.6% MIN. AGGR..... 5.4% ASPHALT BINDER  
ACHM BINDER COURSE (1").....95.6% MIN. AGGR..... 4.4% ASPHALT BINDER  
ACHM BASE COURSE (1 1/2").....96% MIN. AGGR.....4% ASPHALT BINDER  
MAXIMUM NUMBER OF GYRATIONS = 160 FOR PG 70-22

**WHEELCHAIR RAMPS**

STATION	LOCATION	TYPE 3 SQ.YD.
9+65	RT. OF C.L. JACKSONVILLE CUTOFF	3.3
10+42	RT. OF C.L. WEST MAIN ST.	3.3
9+65	LT. OF C.L. JACKSONVILLE CUTOFF	3.3
10+42	LT. OF C.L. WEST MAIN ST.	3.3
9+66	LT. OF C.L. WEST MAIN ST. (SOUTH LEG)	3.3
9+67	LT. OF C.L. WEST MAIN ST. (SOUTH LEG)	3.3
10+34	LT. OF C.L. HARRIS RD.	3.3
10+33	LT. OF C.L. HARRIS RD.	3.3
<b>TOTAL:</b>		<b>26.4</b>

**CONCRETE COMBINATION CURB AND GUTTER**

STATION	FROM LOCATION	STATION	TO LOCATION	TYPE A (2' 0")	
				STATION	TO LOCATION
9+68	RT. OF C.L. JACKSONVILLE CUTOFF	9+69	LT. OF C.L. WEST MAIN ST. (SOUTH LEG)		54
9+71	RT. OF C.L. WEST MAIN ST. (SOUTH LEG)	13+89	RT. OF C.L. WEST MAIN ST.		528
13+82	LT. OF C.L. WEST MAIN ST.	10+31	RT. OF C.L. HARRIS RD.		416
10+31	LT. OF C.L. HARRIS RD.	9+67	LT. OF C.L. JACKSONVILLE CUTOFF		83
10+00	LT. OF C.L. ROUNDABOUT	13+71	LT. OF C.L. ROUNDABOUT		371
8+64	C.L. JACKSONVILLE CUTOFF	9+67	C.L. JACKSONVILLE CUTOFF		214
9+72	C.L. JACKSONVILLE CUTOFF	9+90	C.L. JACKSONVILLE CUTOFF		68
8+88	C.L. WEST MAIN ST. (SOUTH LEG)	9+68	C.L. WEST MAIN ST. (SOUTH LEG)		171
9+73	C.L. WEST MAIN ST. (SOUTH LEG)	9+91	C.L. WEST MAIN ST. (SOUTH LEG)		65
10+06	C.L. WEST MAIN ST.	10+36	C.L. WEST MAIN ST.		89
10+41	C.L. WEST MAIN ST.	11+79	C.L. WEST MAIN ST.		284
10+09	C.L. HARRIS RD.	10+30	C.L. HARRIS RD.		67
10+35	C.L. HARRIS RD.	10+47	C.L. HARRIS RD.		32
<b>TOTAL:</b>					<b>2442</b>

**SOIL LOG**

STATION & OFFSET	LOCATION	DEPTH FEET	LIQUID LIMIT	PLASTICITY INDEX	AASHTO CLASSIFICATION
9+20, 10' RT	JACKSONVILLE CUTOFF	10	27	17	A-4
12+70, 10' RT	ROUNDABOUT	10	19	18	A-2-4
11+40, 10' RT	ROUNDABOUT	10	21	17	A-4
10+92, 10' RT	JACKSONVILLE CUTOFF	10	23	15	A-4

SOIL CHARACTERISTICS TABULATED ABOVE ARE REPRESENTATIVE AT THE LOCATION OF THE SAMPLE, AND FROM SURFACE INDICATIONS ARE TYPICAL FOR THE LIMITS SHOWN. THESE DATA ARE SHOWN FOR INFORMATION ONLY. THE STATE WILL NOT BE RESPONSIBLE FOR VARIATIONS IN THE SOIL CHARACTERISTICS AND/OR EXTENT OF SAME DIFFERING FROM THE ABOVE TABULATIONS.

**CONCRETE ISLAND**

STATION	LOCATION	SQ.YD.
9+59	C.L. JACKSONVILLE CUTOFF	2.6
9+83	C.L. JACKSONVILLE CUTOFF	17.8
9+47	C.L. WEST MAIN ST. (SOUTH LEG)	9.9
9+83	C.L. WEST MAIN ST. (SOUTH LEG)	17.7
10+20	C.L. WEST MAIN ST.	30.8
11+04	C.L. WEST MAIN ST.	29.5
10+17	C.L. HARRIS RD.	15.7
10+40	C.L. HARRIS RD.	0.9
<b>TOTAL:</b>		<b>124.9</b>

**CONCRETE WALKS**

STATION	FROM LOCATION	STATION	TO LOCATION	CONCRETE WALKS	
				LENGTH LIN.FT.	SQ.YD.
9+60	RT. OF C.L. JACKSONVILLE CUTOFF	9+62	LT. OF C.L. WEST MAIN ST. (SOUTH LEG)	51	28
9+64	RT. OF C.L. WEST MAIN ST. (SOUTH LEG)	13+36	RT. OF C.L. WEST MAIN ST.	481	267
13+85	LT. OF C.L. WEST MAIN ST.	10+37	RT. OF C.L. HARRIS RD.	440	244
10+36	LT. OF C.L. HARRIS RD.	9+61	LT. OF C.L. JACKSONVILLE CUTOFF	129	72
<b>TOTAL:</b>					<b>611</b>

**STANDARD SIGN**

SIGN NUMBER	DESCRIPTION	SIGN SIZE	TOTAL SIGNS REQUIRED		CHANNEL POST SIGN SUPPORT (TYPE A)
			NO.	SQ. FT.	
R1-2	YIELD	36"x36"x36"	4	181.5	8.0
R6-4a	ROUNDABOUT DIRECTIONAL (3 CHEVRONS)	36"x12"	4	12.0	8.0
W11-2	PEDESTRIAN	30"x30"	8	50.0	16.0
W16-7P	DOWNWARD DIAGONAL ARROW	24"x12"	8	16.0	8.0
W2-6	CIRCULAR INTERSECTION	30"x30"	4	25.0	8.0
<b>TOTAL:</b>				<b>284.5</b>	<b>48.0</b>

**MAILBOXES**

LOCATION	MAILBOXES	
	MAILBOXES	MAILBOX SUPPORTS (SINGLE)
LT. OF HARRIS RD.	1	1
<b>TOTALS:</b>		
	1	1

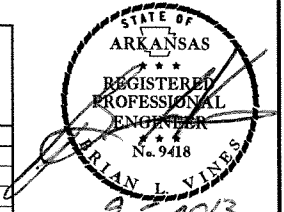
QUANTITIES

JACOBS

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9-5-13				6	ARK.		26	56
				JOB NO.	061267			

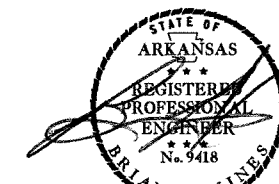
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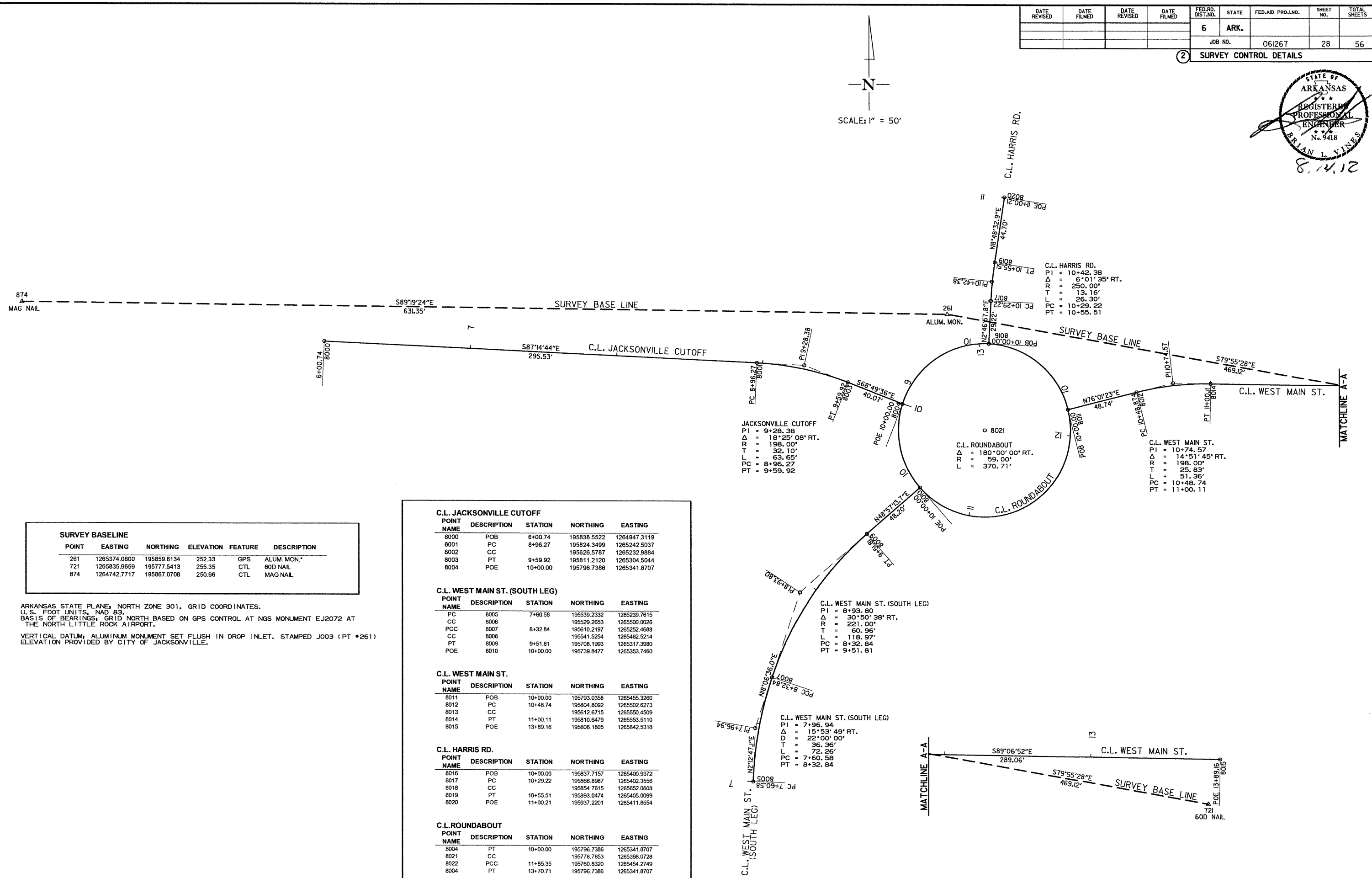
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				6	ARK.			
				JOB NO.		061267	28	56

2 SURVEY CONTROL DETAILS



8.14.12

SCALE: 1" = 50'



SURVEY BASELINE					
POINT	EASTING	NORTHING	ELEVATION	FEATURE	DESCRIPTION
261	1265374.0800	195859.6134	252.33	GPS	ALUM. MON.*
721	1265835.9659	195777.5413	255.35	CTL	60D NAIL
874	1264742.7717	195867.0708	250.96	CTL	MAG NAIL

ARKANSAS STATE PLANE, NORTH ZONE 301, GRID COORDINATES.  
 U.S. FOOT UNITS, NAD 83.  
 BASIS OF BEARINGS, GRID NORTH BASED ON GPS CONTROL AT NGS MONUMENT EJ2072 AT THE NORTH LITTLE ROCK AIRPORT.  
 VERTICAL DATUM, ALUMINUM MONUMENT SET FLUSH IN DROP INLET. STAMPED J003 (PT \*261) ELEVATION PROVIDED BY CITY OF JACKSONVILLE.

C.L. JACKSONVILLE CUTOFF					
POINT NAME	DESCRIPTION	STATION	NORTHING	EASTING	
8000	POB	6+00.74	195838.5522	1264947.3119	
8001	PC	8+96.27	195824.3499	1265242.5037	
8002	CC		195826.5787	1265232.9884	
8003	PT	9+59.92	195811.2120	1265304.5044	
8004	POE	10+00.00	195796.7386	1265341.8707	

C.L. WEST MAIN ST. (SOUTH LEG)					
POINT NAME	DESCRIPTION	STATION	NORTHING	EASTING	
PC	8005	7+60.58	195539.2332	1265239.7615	
CC	8006		195529.2653	1265500.0026	
PCC	8007	8+32.84	195610.2197	1265252.4688	
CC	8008		195541.5254	1265462.5214	
PT	8009	9+51.81	195708.1993	1265317.3980	
POE	8010	10+00.00	195739.8477	1265353.7460	

C.L. WEST MAIN ST.					
POINT NAME	DESCRIPTION	STATION	NORTHING	EASTING	
8011	POB	10+00.00	195793.0358	1265455.3260	
8012	PC	10+48.74	195804.8092	1265502.6273	
8013	CC		195612.6715	1265550.4509	
8014	PT	11+00.11	195810.6479	1265553.5110	
8015	POE	13+89.16	195806.1805	1265842.5318	

C.L. HARRIS RD.					
POINT NAME	DESCRIPTION	STATION	NORTHING	EASTING	
8016	POB	10+00.00	195837.7157	1265400.9372	
8017	PC	10+29.22	195866.8987	1265402.3556	
8018	CC		195854.7615	1265652.0608	
8019	PT	10+55.51	195893.0474	1265405.0099	
8020	POE	11+00.21	195937.2201	1265411.8554	

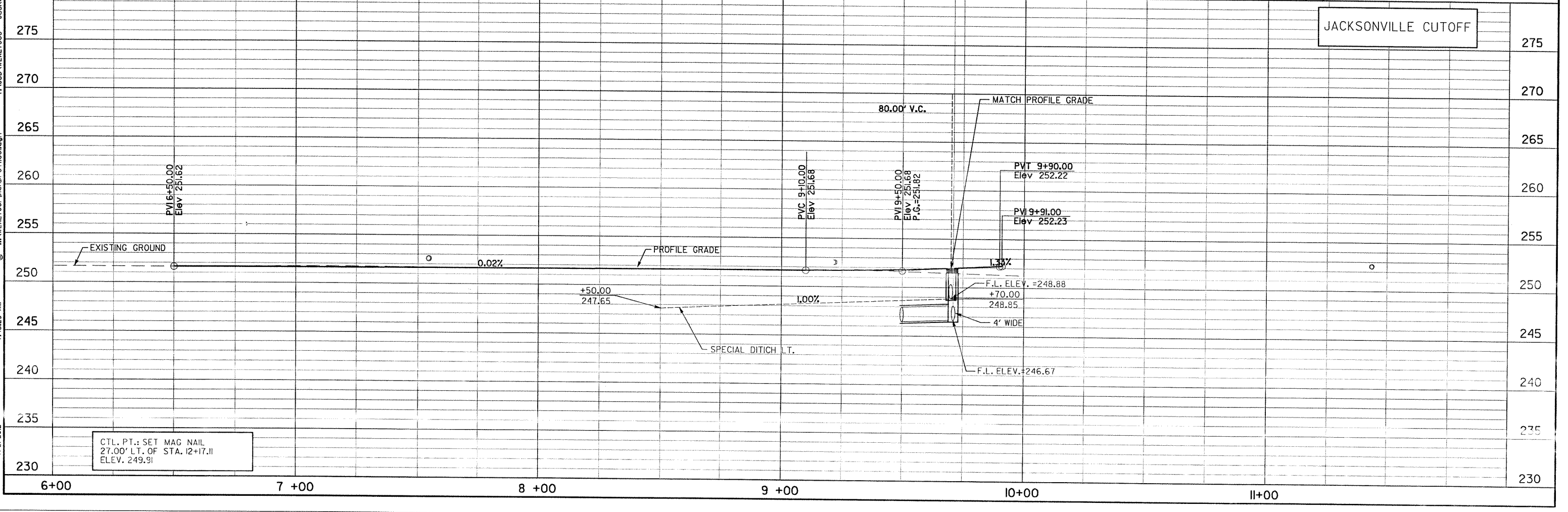
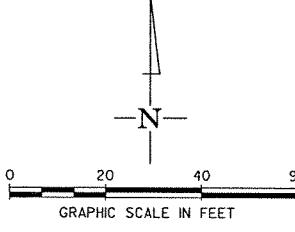
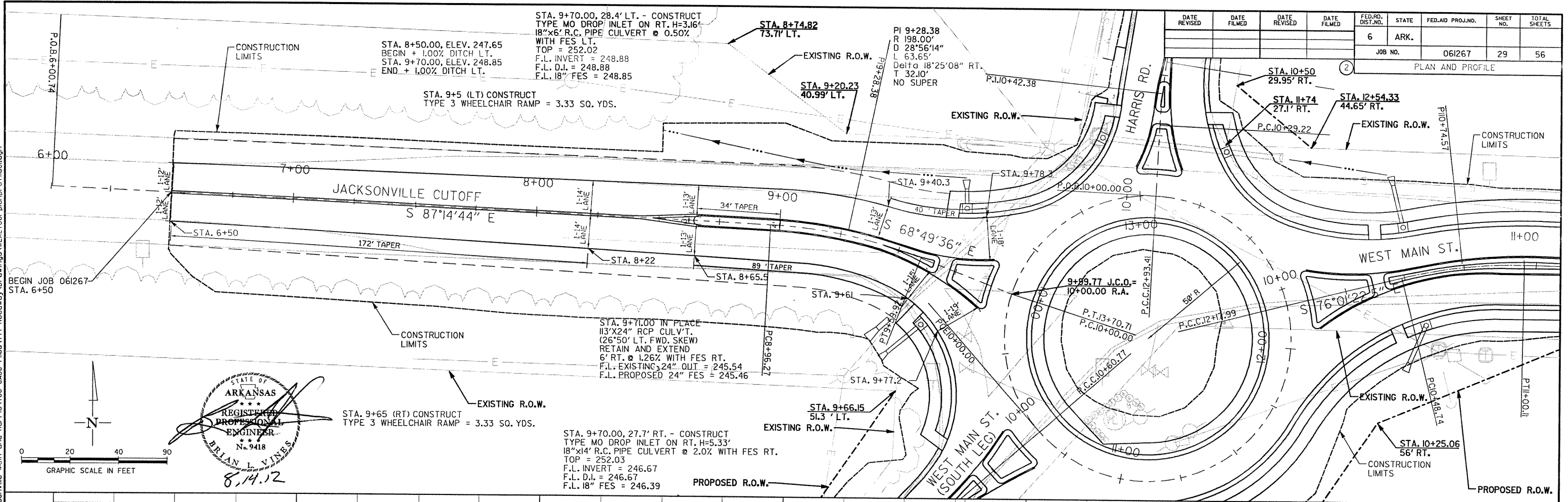
  

C.L. ROUNDABOUT					
POINT NAME	DESCRIPTION	STATION	NORTHING	EASTING	
8004	PT	10+00.00	195796.7386	1265341.8707	
8021	CC		195778.7853	1265398.0728	
8022	PCC	11+85.35	195760.8320	1265454.2749	
8004	PT	13+70.71	195796.7386	1265341.8707	

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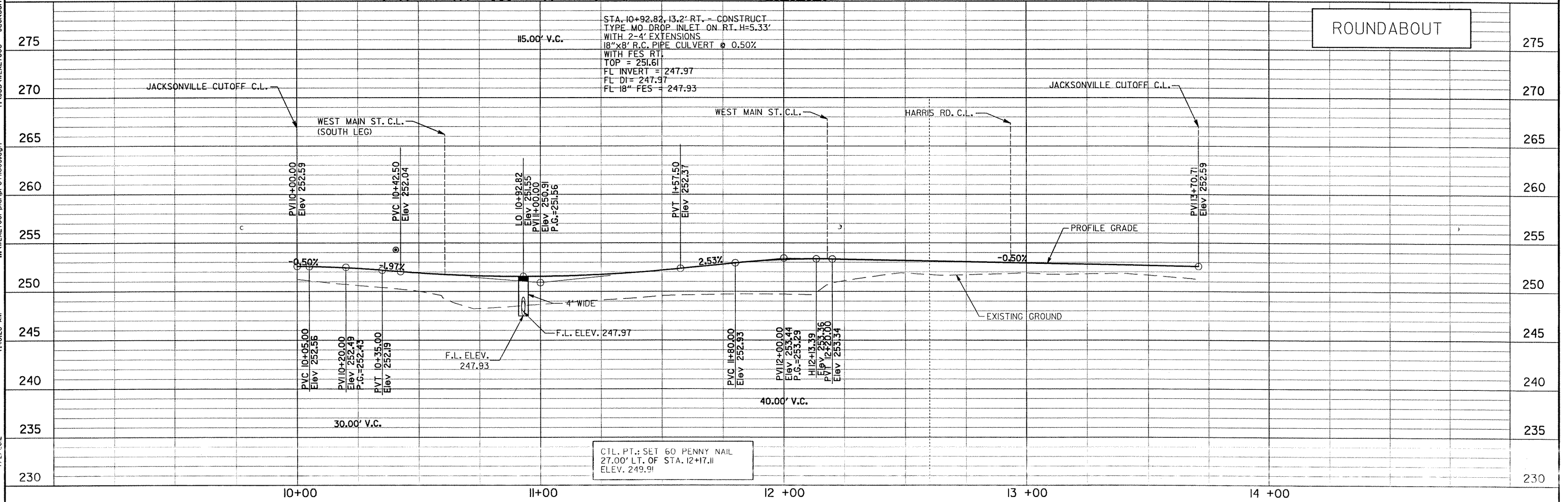
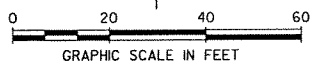
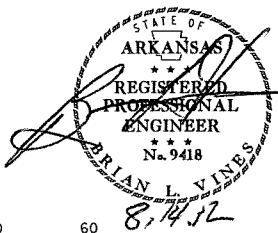
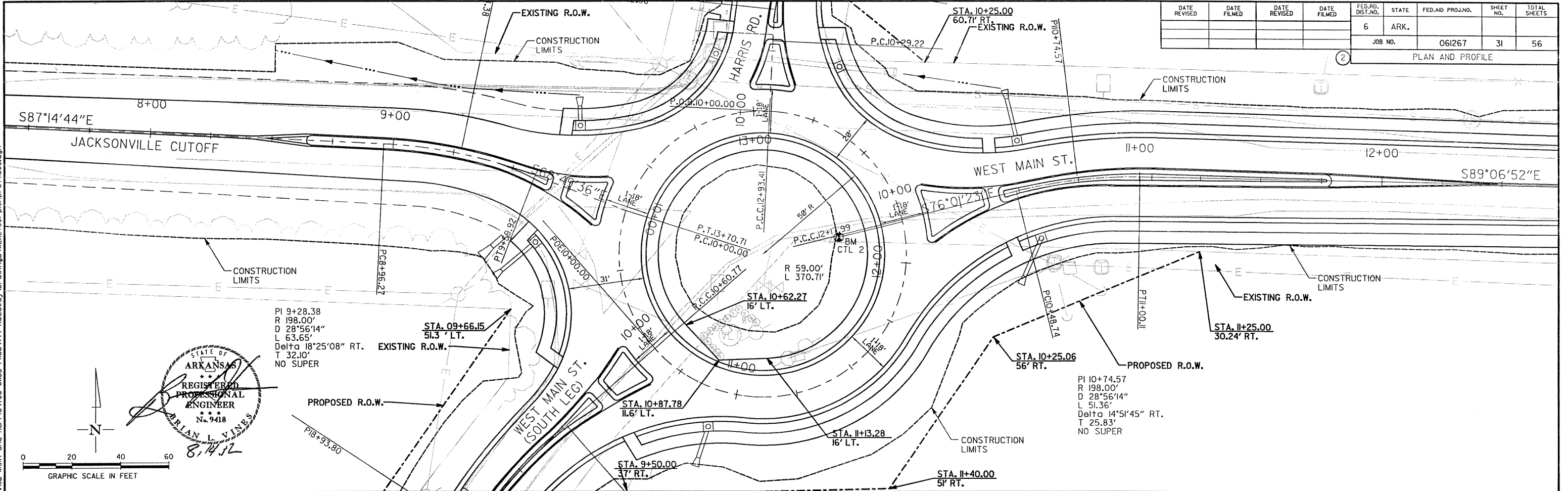
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				6	ARK.	061267	29	56
JOB NO. 061267							29	56





DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061267	31	56	

PLAN AND PROFILE



ROUNDABOUT

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STA. 9+66 (LT) CONSTRUCT  
 TYPE 3 WHEELCHAIR RAMP = 3.33 SQ. YDS.

PI 7+96.94  
 R 260.44'  
 D 22°00'00"  
 L 72.26'  
 Delta 15°53'49" RT.  
 T 36.36'  
 NO SUPER

PI 8+93.80  
 R 221.00'  
 D 25°55'32.5"  
 L 118.97'  
 Delta 30°50'37.7" RT.  
 T 60.96'  
 NO SUPER

STA. 09+66.03  
 51.31' LT.

STA. 10+23 IN PLACE  
 DROP INLET ON LT.  
 24" x 74" R.C. PIPE OUTLET  
 WITH F.E.S.  
 REMOVE DROP INLET &  
 RETAIN PIPE OUTLET

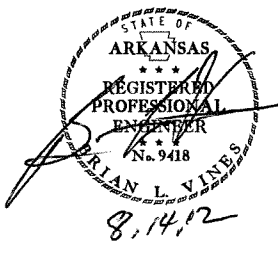
STA. 10+22.83, 22.76' LT. - CONSTRUCT  
 TYPE MO DROP INLET ON RT. H=5.75'  
 WITH 2-4' EXTENSIONS  
 TOP = 252.63  
 F.L. INVERT = 246.96  
 F.L. D.I. = 246.86

STA. 10+34 (LT) CONSTRUCT  
 TYPE 3 WHEELCHAIR RAMP = 3.33 SQ. YDS.

STA. 10+33 (RT) CONSTRUCT  
 TYPE 3 WHEELCHAIR RAMP = 3.33 SQ. YDS.

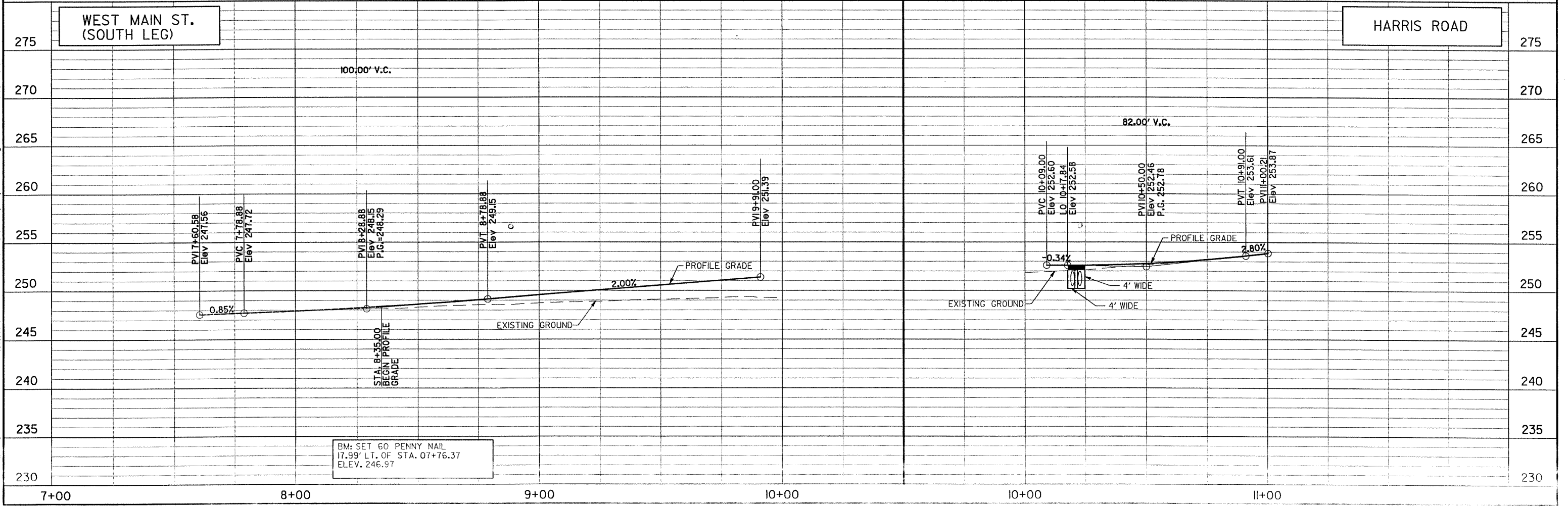
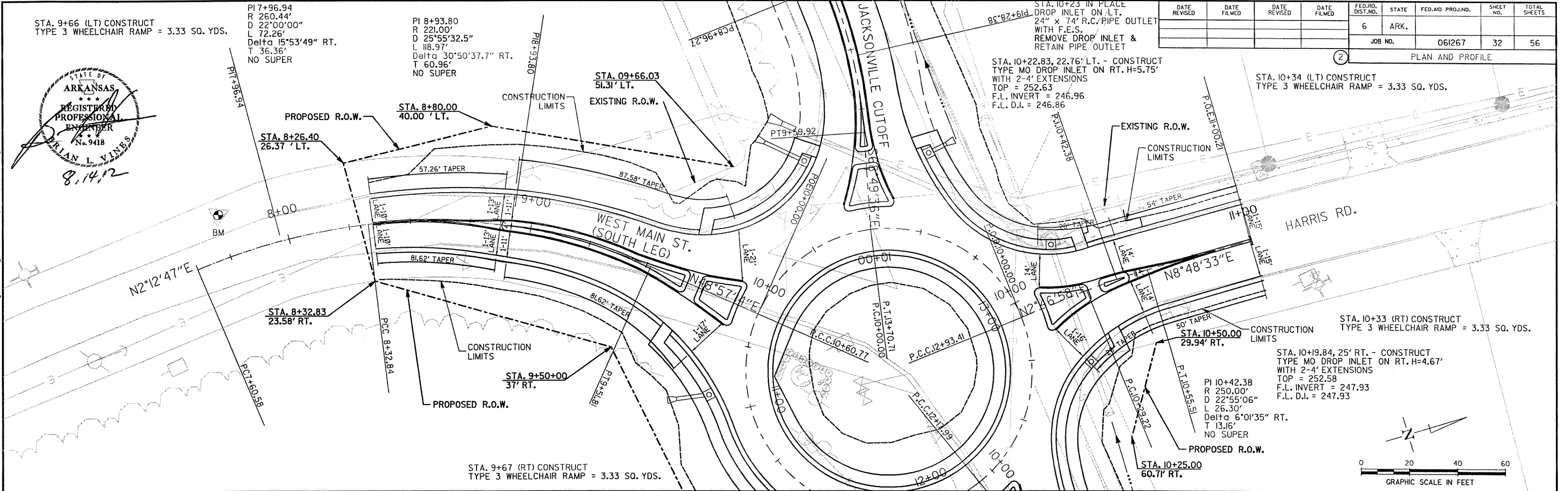
STA. 10+19.84, 25' RT. - CONSTRUCT  
 TYPE MO DROP INLET ON RT. H=4.67'  
 WITH 2-4' EXTENSIONS  
 TOP = 252.58  
 F.L. INVERT = 247.93  
 F.L. D.I. = 247.93

STA. 9+67 (RT) CONSTRUCT  
 TYPE 3 WHEELCHAIR RAMP = 3.33 SQ. YDS.



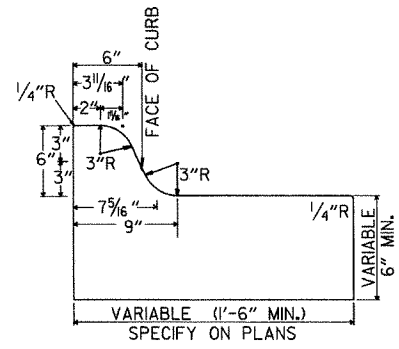
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				6	ARK.		32	56
				JOB NO.		061267		

PLAN AND PROFILE

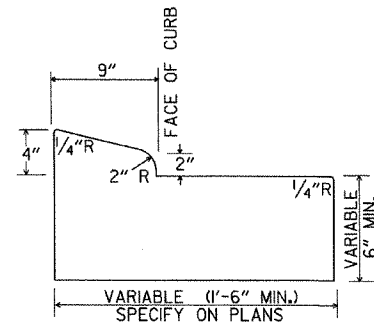


BM: SET 60 PENNY NAIL  
 17.99' LT. OF STA. 07+76.37  
 ELEV. 246.97

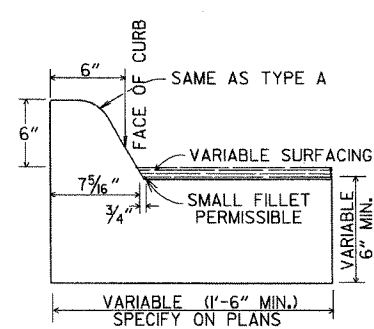




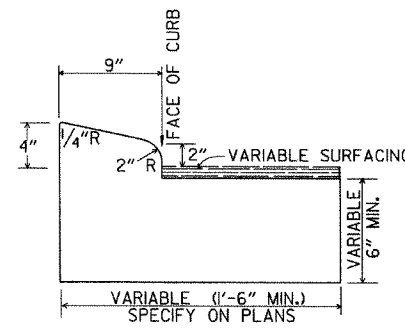
TYPE A



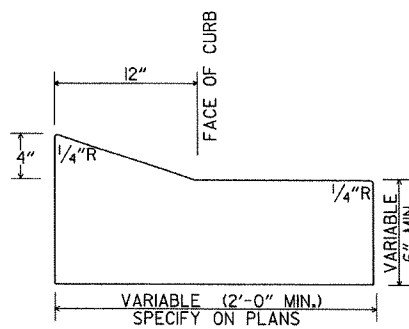
TYPE B-1



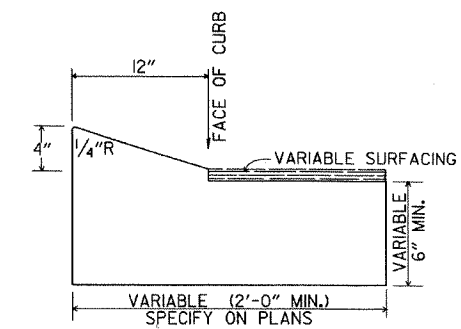
TYPE C



TYPE B-2

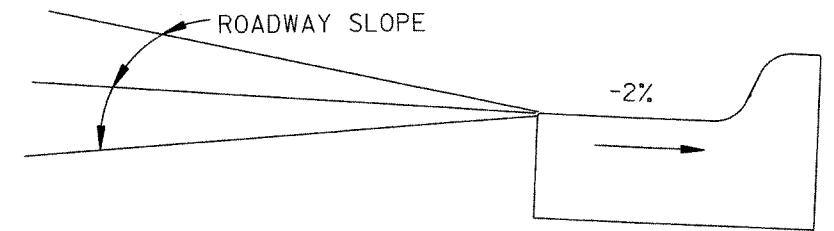


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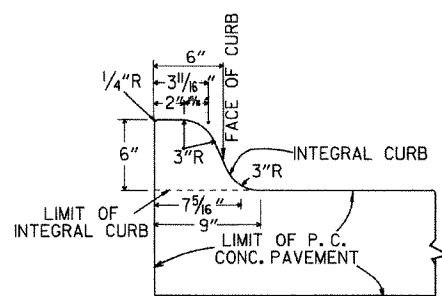


TYPE E-2

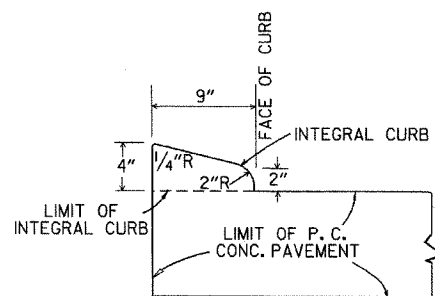
CONCRETE COMBINATION CURB AND GUTTER



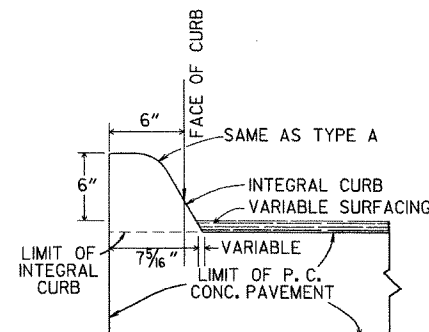
DETAIL OF GUTTER SLOPE  
GUTTER SHALL BE CONSTRUCTED ON 2% SLOPE AWAY FROM ROADWAY, REGARDLESS OF ROADWAY SLOPE.



TYPE A

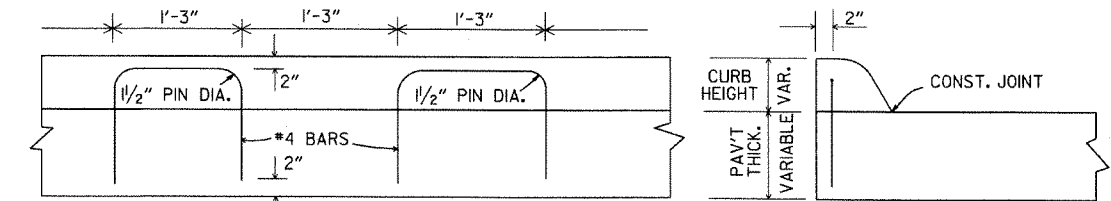


TYPE B



TYPE C

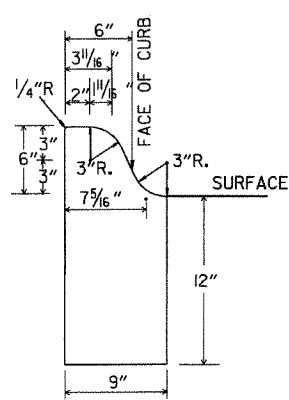
INTEGRAL CURB



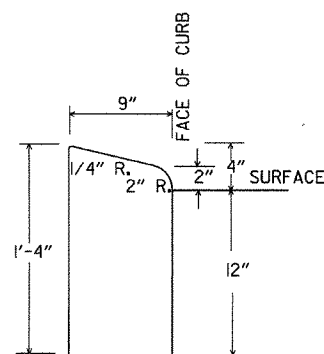
LONGITUDINAL SECTION

ELEVATION

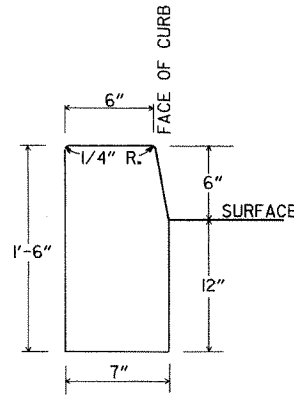
ALTERNATE CONSTRUCTION METHOD FOR INTEGRAL CURB



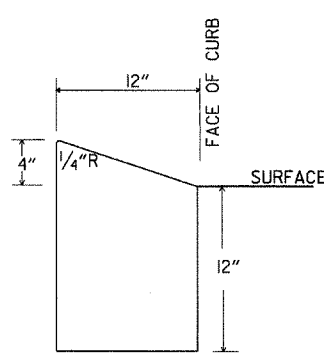
TYPE A



TYPE B

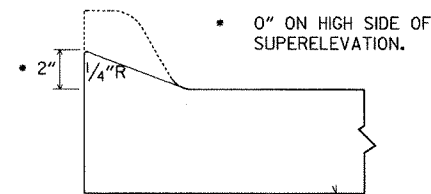


TYPE D



TYPE E

CONCRETE CURB



NOTE: USE MODIFIED CURB AS SPECIFIED ON STD. DR-1. COMPENSATION FOR MODIFIED CURB WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE TYPE OF CURB OR CURB AND GUTTER SPECIFIED.

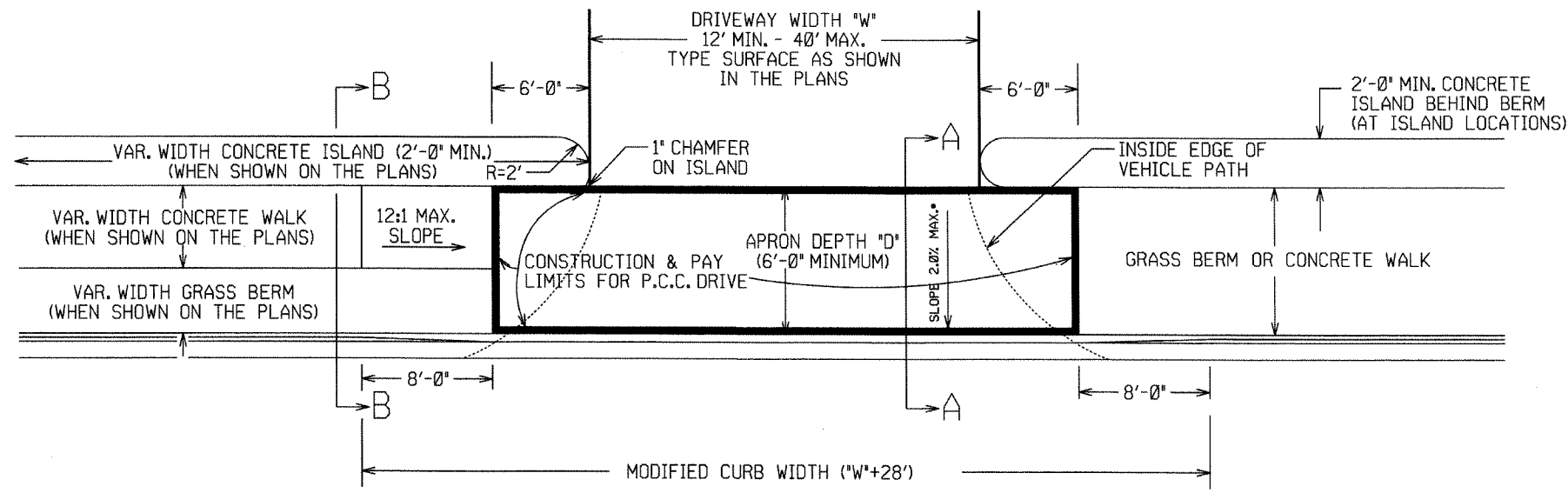
DETAILS OF MODIFIED CURB

DATE	REVISION	DATE FILMED
11-29-07	REVISED GUTTER SLOPE & MODIFIED CURB DETAILS	
11-10-05	ADDED DETAILS OF TYPE E CURBS	
11-16-01	REVISED CONCRETE CURB TYPE B	
11-18-98	REVISED MODIFIED CURB	
6-2-94	ADDED NOTE TO SPECIAL MODIFIED CURB	
8-5-93	CORRECTED GUTTER SLOPE	8-5-93
10-1-92	ADDED DETAILS OF GUTTER SLOPE	10-1-92
5-24-90	ADDED DETAILS OF MODIFIED CURB	5-24-90
11-30-89	VARIABLE DEPTH TYPE A & B 1	11-30-89
7-15-88	REVISED MODIFIED CURB	630-7-15-88
11-1-73	REVISED MODIFIED CURB	500-11-1-73
10-2-72	REVISED AND REDRAWN	512-10-2-72

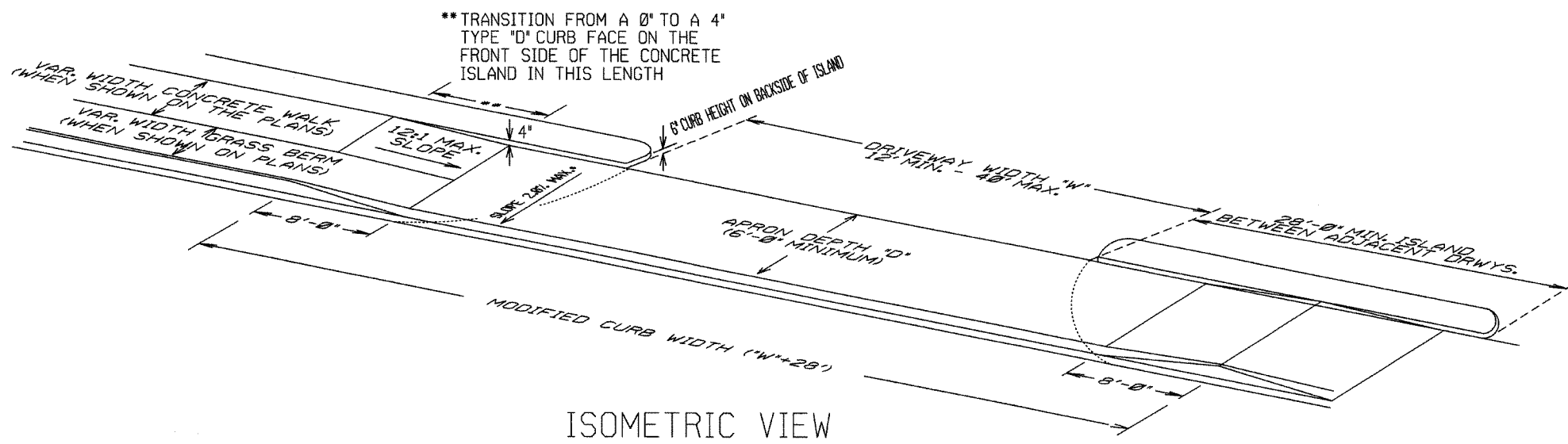
ARKANSAS STATE HIGHWAY COMMISSION

CURBING DETAILS

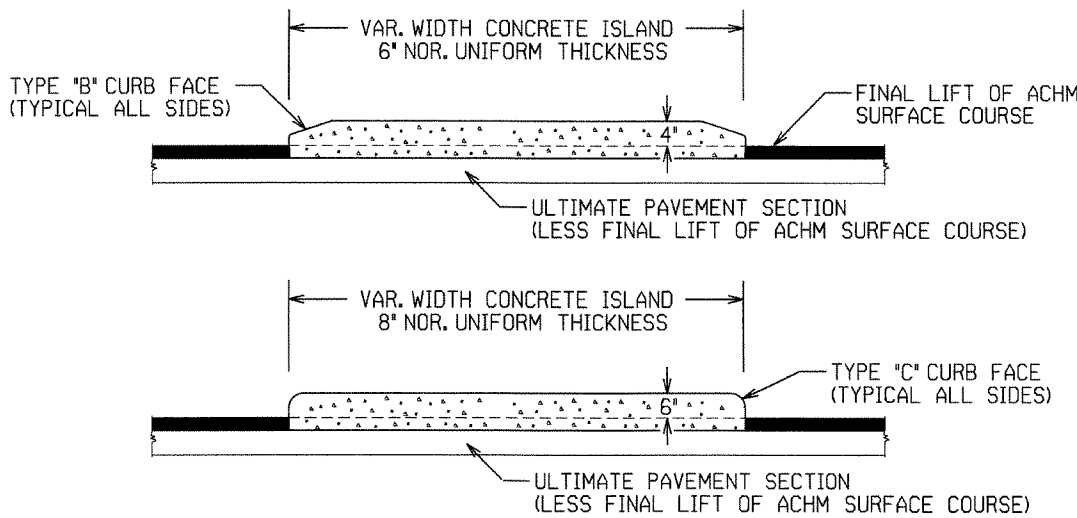
STANDARD DRAWING CG-1



PLAN VIEW

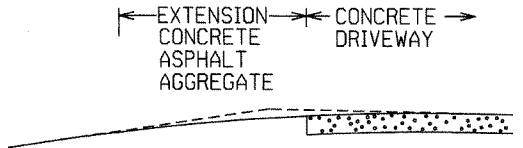


ISOMETRIC VIEW



CURBED ISLANDS FOR CHANNELIZATION

REFER TO PLANS FOR TYPE OF CURB FACE TO BE USED. NO DIRECT PAYMENT WILL BE MADE FOR THE CURB FACES SHOWN ON THE ISLAND DETAILS. PAYMENT FOR THE CURB FACE WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEM "CONCRETE ISLAND".

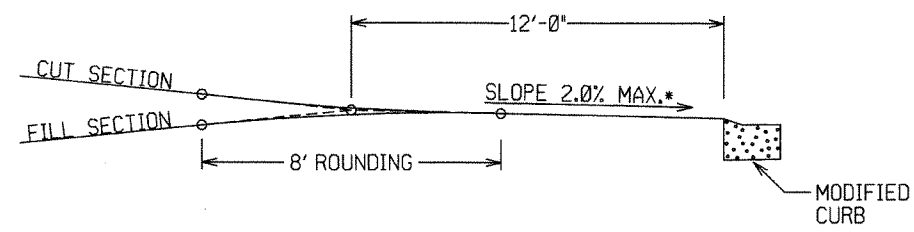


EXTENSION TYPICAL SECTIONS

- 1: CONCRETE - 6" P.C. CONCRETE DRIVEWAY
- 2: ASPHALT - 2" ACHM SURFACE COURSE (1/2")  
4" ACHM BINDER COURSE (1") OR  
4" ACHM BASE COURSE (1-1/2")
- 3: ASPHALT - 2" ACHM SURFACE COURSE (1/2")  
7" AGGREGATE BASE COURSE
- 4: AGGREGATE - 6" AGGREGATE BASE COURSE

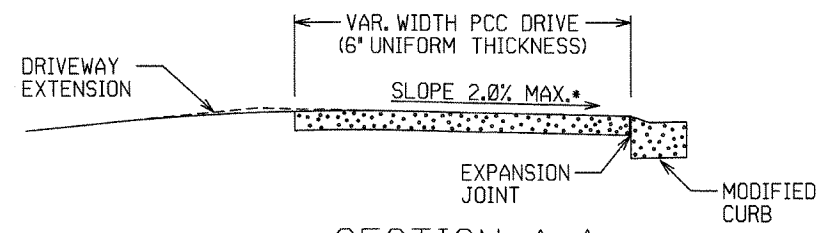
THE TYPE OF EXTENSION SHALL BE AS SHOWN IN THE PLANS. THE CONTRACTOR MAY, WITH THE APPROVAL OF THE ENGINEER, SUBSTITUTE A LOWER NUMBERED TYPE OF EXTENSION IN LIEU OF THE TYPE SPECIFIED IN THE PLANS, BUT AT NO ADDITIONAL COST TO THE DEPARTMENT.

DRIVEWAY EXTENSION DETAILS

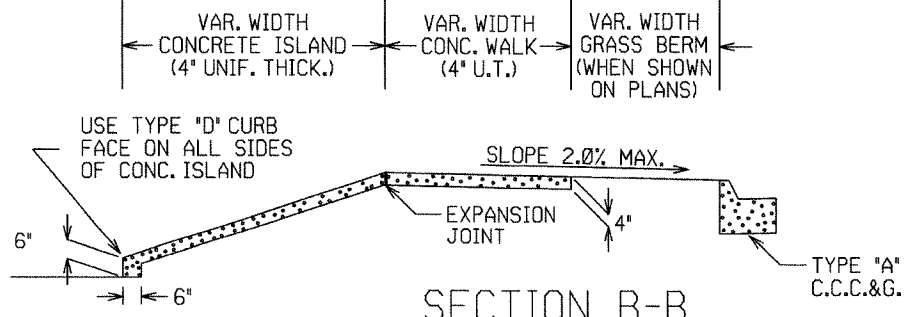


DRIVEWAY VERTICAL ALIGNMENT DETAILS

\* NOTE: DRIVEWAYS MAY NOT BE SLOPED AWAY FROM THE ROADWAY UNLESS APPROVED BY THE ENGINEER.



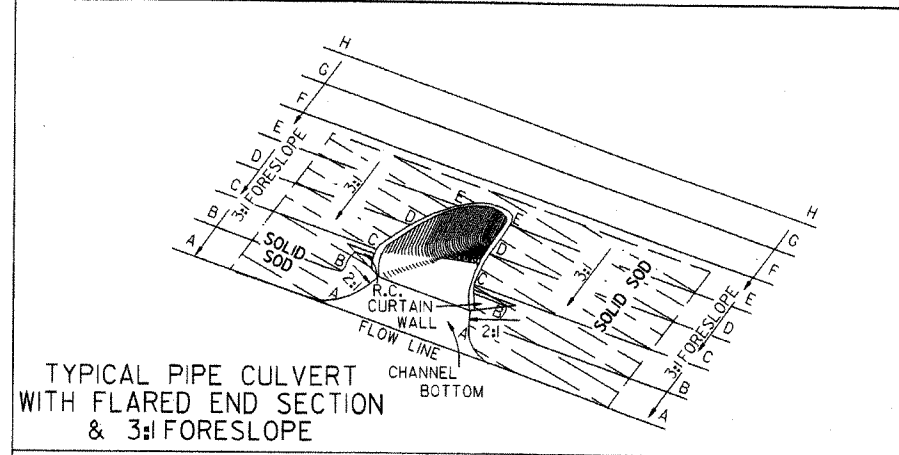
SECTION A-A



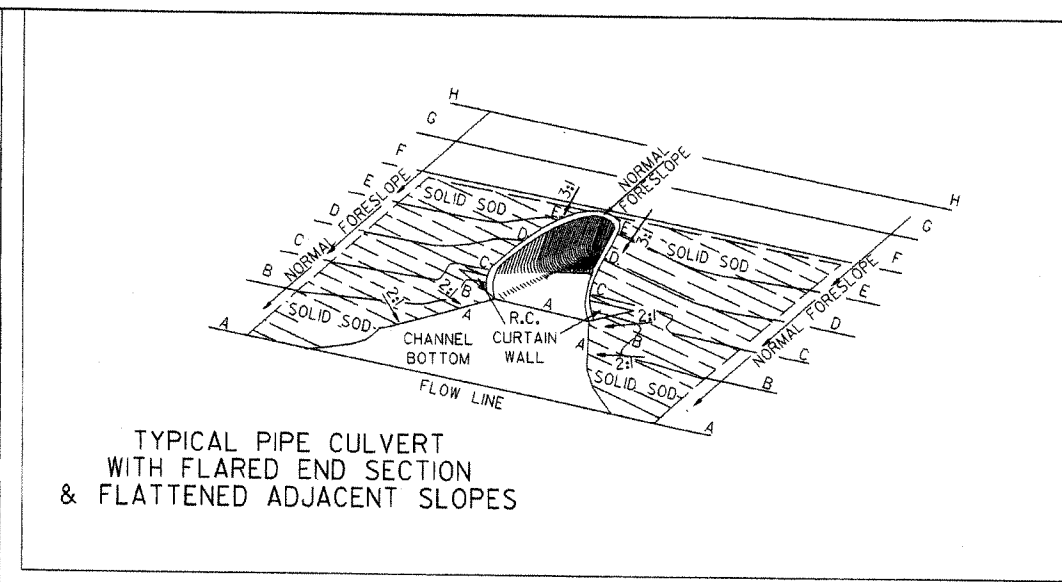
SECTION B-B  
CURBED ISLAND BEHIND WALK

DATE	REV	DATE FILMED	DESCRIPTION
11-29-07			ADDED CHANNELIZATION ISLAND WITH TYPE C CURB FACE & REVISED DRIVEWAY SLOPE NOTE & VERTICAL ALIGNMENT DETAIL
11-10-05			REV. APRON SLOPE & DEPTH OF AGG. BASE.
8-22-02			ADDED ISLAND DETAILS & NOTES
3-30-00			REV. MOD. CURB WIDTH & TRANS. NOTE
11-19-98			REVISED NOTES
11-18-98			REDRAWN AND REISSUED

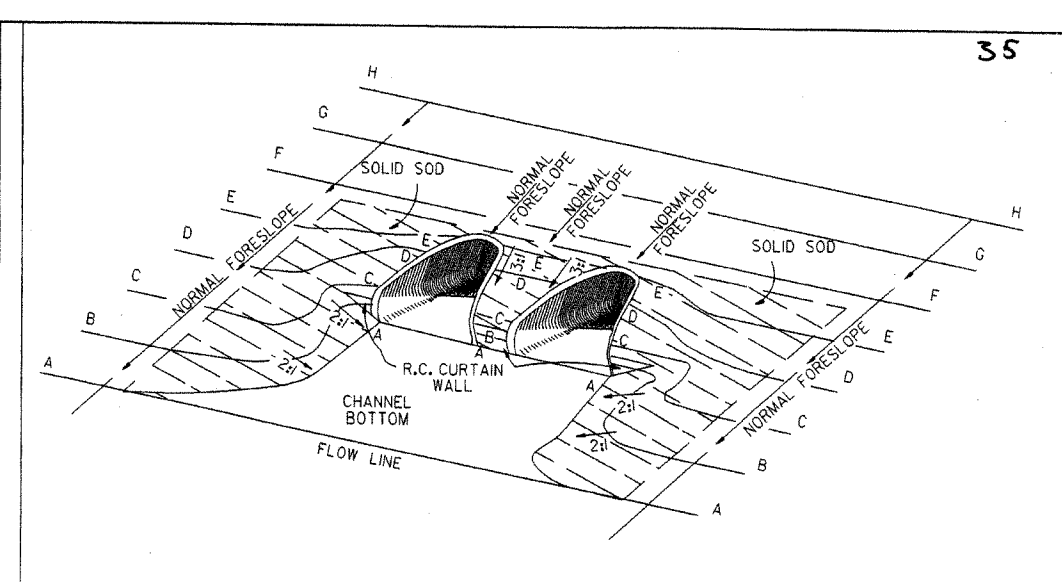
ARKANSAS STATE HIGHWAY COMMISSION  
DETAILS OF DRIVEWAYS & ISLANDS  
STANDARD DRAWING DR-1



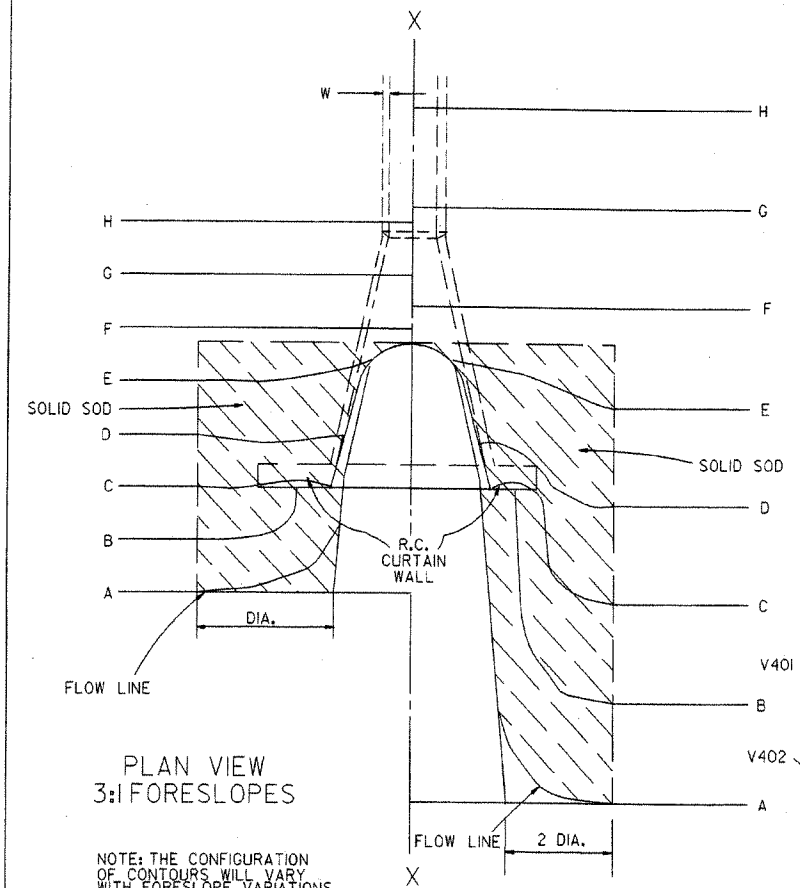
TYPICAL PIPE CULVERT WITH FLARED END SECTION & 3:1 FORESLOPE



TYPICAL PIPE CULVERT WITH FLARED END SECTION & FLATTENED ADJACENT SLOPES



TYPICAL MULTIPLE PIPE CULVERT WITH FLARED END SECTIONS & FLATTENED ADJACENT SLOPES



PLAN VIEW 3:1 FORESLOPES

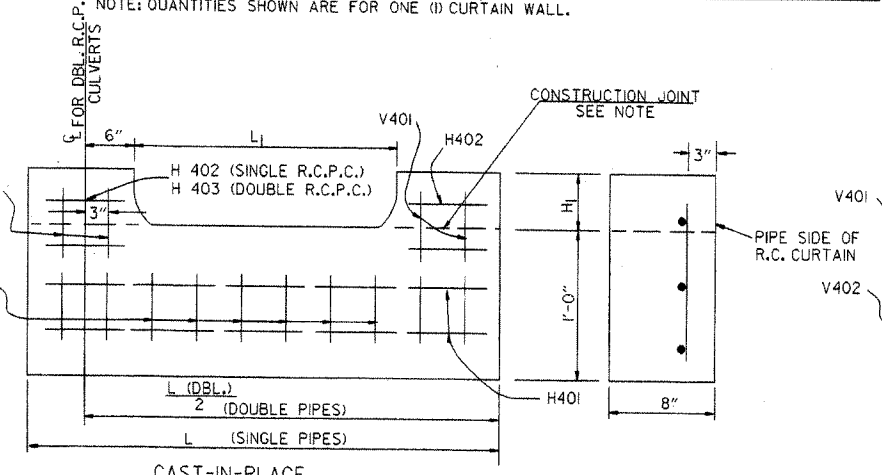
NOTE: THE CONFIGURATION OF CONTOURS WILL VARY WITH FORESLOPE VARIATIONS.

PLAN VIEW FLATTENED FORESLOPES

R.C. CURTAIN WALL DIMENSIONS & QUANTITIES

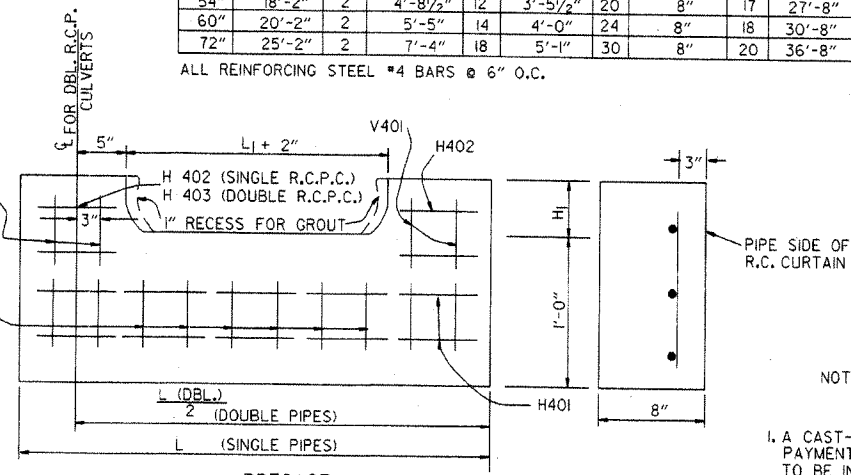
PIPE DIA.	H <sub>1</sub>	L <sub>1</sub>	L	L (DBL.) 2	SINGLE R.C.P.C.		DOUBLE R.C.P.C.	
					CONC.	REINF. STEEL	CONC.	REINF. STEEL
					CU. YDS.	LBS.	CU. YDS.	LBS.
18"	11 1/2"	3'-5"	8'-0"	6'-3"	0.31	27.7	0.45	39.5
24"	1'-0 1/2"	4'-6"	9'-6"	7'-6"	0.37	33.4	0.53	48.0
30"	1'-3 1/2"	5'-7"	11'-0"	9'-0"	0.45	39.0	0.67	59.0
36"	1'-7"	6'-8"	13'-0"	10'-6"	0.58	52.6	0.83	73.9
42"	2'-1 1/2"	7'-3"	15'-6"	12'-0"	0.82	77.1	1.10	100.7
48"	2'-5"	7'-10"	17'-0"	13'-0"	0.98	94.9	1.27	120.4
54"	2'-9 1/2"	8'-5"	18'-6"	14'-0"	1.16	115.8	1.47	143.7
60"	3'-4"	9'-0"	20'-6"	15'-6"	1.47	149.7	1.84	180.3
72"	4'-5"	10'-2"	25'-6"	18'-6"	2.31	232.6	2.73	271.0

NOTE: QUANTITIES SHOWN ARE FOR ONE (1) CURTAIN WALL.



R.C. CURTAIN WALL DETAILS

NOTE: THE PORTION OF THE R.C. CURTAIN WALL BENEATH THE FLARED END SECTION (LOWER 1'-0") SHALL BE PLACED MONOLITHICALLY. THE FLARED END SECTION SHALL THEN BE SET IN PLACE & THE REMAINING PORTIONS OF THE R.C. CURTAIN WALL PLACED.



NOTE: THE PRECAST CURTAIN WALL WILL BE SET AND BACKFILLED WITH COMPACTED MATERIAL. THE FLARED END SECTION SHALL THEN BE SET IN PLACE AND THE 1" RECESS FILLED WITH GROUT. WHERE "L" EXCEEDS 11' THE CURTAIN WALL MAY BE CAST IN TWO (2) OR MORE SECTIONS. THE METHOD OF JOINING THE SECTIONS FOR INSTALLATION SHALL BE APPROVED BY THE ENGINEER.

REINFORCING STEEL SCHEDULE

PIPE DIA.	SINGLE R.C. PIPE CULVERT								DOUBLE R.C. PIPE CULVERT									
	H401		H402		V401		V402		H401		H402		H403		V401		V402	
	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.
18"	7'-8"	2	1'-11 1/2"	4	1'-7 1/2"	8	8"	8	12'-2"	2	1'-11 1/2"	4	8"	2	1'-7 1/2"	10	8"	14
24"	9'-2"	2	2'-2"	4	1'-8 1/2"	10	8"	9	14'-8"	2	2'-2"	4	8"	2	1'-8 1/2"	12	8"	18
30"	10'-8"	2	2'-4 1/2"	4	1'-11 1/2"	10	8"	12	17'-8"	2	2'-4 1/2"	4	8"	2	1'-11 1/2"	14	8"	22
36"	12'-8"	2	2'-10"	6	2'-3"	12	8"	14	20'-8"	2	2'-10"	6	8"	3	2'-3"	14	8"	28
42"	15'-2"	2	3'-9 1/2"	8	2'-9 1/2"	16	8"	15	23'-8"	2	3'-9 1/2"	8	8"	4	2'-9 1/2"	18	8"	30
48"	16'-8"	2	4'-3"	10	3'-1"	18	8"	16	25'-8"	2	4'-3"	10	8"	5	3'-1"	20	8"	32
54"	18'-2"	2	4'-8 1/2"	12	3'-5 1/2"	20	8"	17	27'-8"	2	4'-8 1/2"	12	8"	6	3'-5 1/2"	22	8"	34
60"	20'-2"	2	5'-5"	14	4'-0"	24	8"	18	30'-8"	2	5'-5"	14	8"	7	4'-0"	26	8"	36
72"	25'-2"	2	7'-4"	18	5'-1"	30	8"	20	36'-8"	2	7'-4"	18	8"	9	5'-1"	33	8"	40

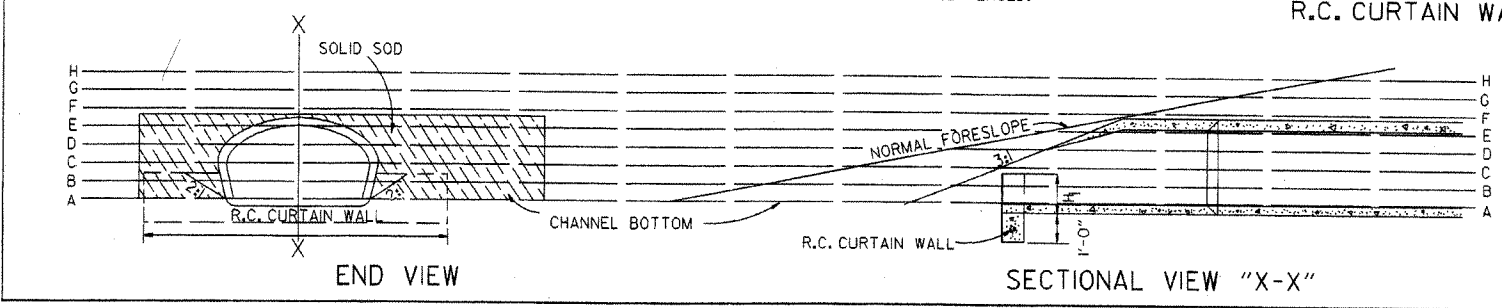
ALL REINFORCING STEEL #4 BARS @ 6" O.C.

SOLID SODDING

PIPE DIA.	SINGLE R.C.P.C.						DOUBLE R.C.P.C.					
	3:1		4:1		6:1		3:1		4:1		6:1	
	SQ. YDS.						SQ. YDS.					
18"	5	7	12	6	8	13	5	7	12	6	8	13
24"	8	12	19	9	13	20	8	12	19	9	13	20
30"	13	18	29	14	19	30	13	18	29	14	19	30
36"	17	26	41	18	28	43	17	26	41	18	28	43
42"	23	35	55	25	37	57	23	35	55	25	37	57
48"	29	46	68	31	48	70	29	46	68	31	48	70
54"	35	57	85	37	59	87	35	57	85	37	59	87
60"	45	62	104	48	65	107	45	62	104	48	65	107
72"	64	92	156	67	95	159	64	92	156	67	95	159

NOTE: QUANTITIES SHOWN ABOVE ARE FOR ONE (1) END OF F.E.S.

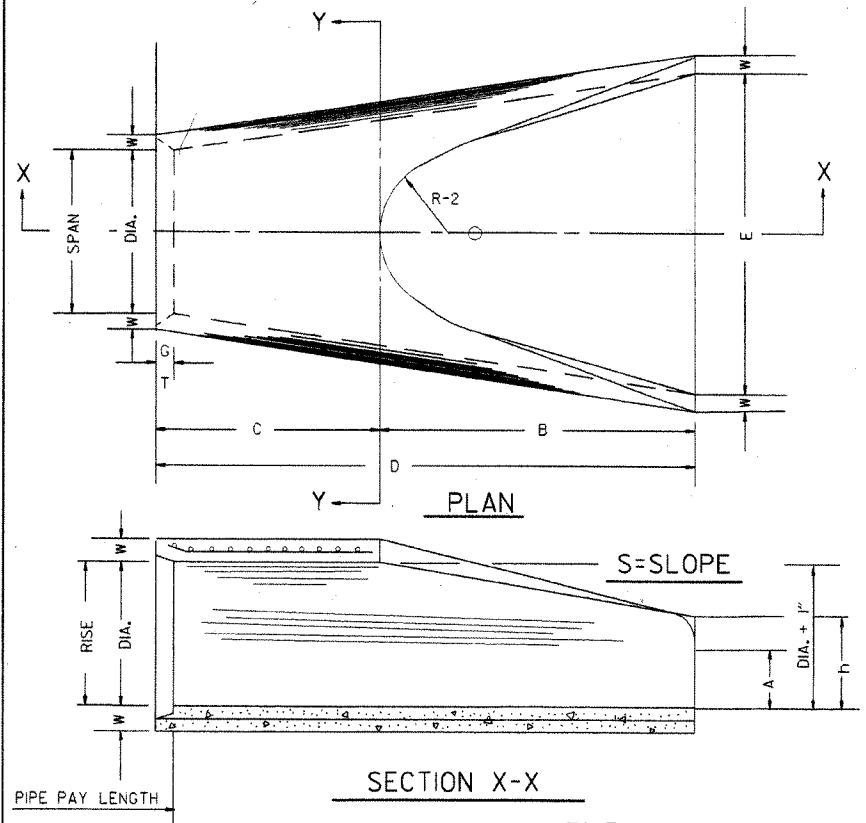
- GENERAL NOTES
1. A CAST-IN-PLACE OR PRECAST CURTAIN WALL MAY BE USED. PAYMENT FOR THE CURTAIN WALL SHALL BE CONSIDERED TO BE INCLUDED IN THE UNIT PRICE BID EACH FOR FLARED END SECTIONS OF THE SEVERAL SIZES, WHICH PRICE SHALL BE FULL COMPENSATION FOR FURNISHING ALL MATERIALS INCLUDING REINFORCING STEEL AND CONCRETE; FOR FORMS, MIXING AND PLACING; FOR EXCAVATION AND BACKFILL; AND FOR ALL LABOR, TOOLS, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE WORK.
  2. ALL EXPOSED EDGES SHALL BE CHAMFERED 3/4".
  3. CONCRETE FOR CURTAIN WALL SHALL MEET THE REQUIREMENTS FOR CLASS A OR S CONCRETE AS PROVIDED IN SECTION 802 OF THE STANDARD SPECIFICATIONS OR FOR PAVING CONCRETE AS PROVIDED IN SECTION 501 OF THE STANDARD SPECIFICATIONS.
  4. WELDED WIRE MESH 3 x 3 W/10 x W10 MAY BE USED IN LIEU OF REINFORCING BARS.



END VIEW

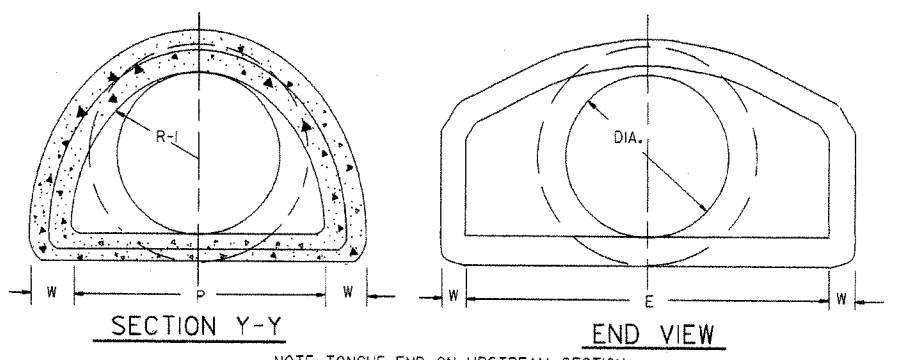
SECTIONAL VIEW "X-X"

10-18-96 ADDED NOTE TO SOLID SODDING		ARKANSAS STATE HIGHWAY COMMISSION
10-12-95 CORRECTED SPELLING		
11-3-94 ADDED GENERAL NOTE NO. 4		
8-15-91 REV. CURTAIN WALL QUANT. STEEL SCH. & SOLID SOD QUANT.		
3-2-81 ALLOW PRECAST IN 2 OR MORE PIECES CHAMFER EDGES		
5-15-80 ADDED PRECAST WALL & GENERAL NOTES		
10-2-72 REVISED AND REDRAWN		
DATE	REVISION	FILMED
		STANDARD DRAWING FES-1



**TABLE OF DIMENSIONS**

DIA.	WALL	A	B	C	D	E	S	DIA. + 1"	P	R-1	R-2	G-T	WT.	h
18"	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	3:1	19"	29"	15 1/2"	12"	2"	1000	1'-0 1/2"
24"	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3:1	25"	33 3/8"	16 1/8"	14"	2 1/2"	1600	1'-1 1/2"
30"	3 1/2"	1'-0"	4'-6"	1'-7 1/4"	6'-1 3/4"	5'-0"	3:1	31"	37"	18 1/2"	15"	3 1/4"	1940	1'-4 5/8"
36"	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	3:1	37"	47 1/8"	24 5/8"	20"	3 1/2"	4100	1'-8"
42"	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	3:1	43"	53 1/2"	27 1/2"	22"	3 1/2"	5380	2'-2 1/2"
48"	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	3:1	49"	56 1/2"	28 1/2"	22"	3 1/2"	6550	2'-6"
54"	5 1/2"	2'-4"	6'-6"	1'-10"	8'-4"	7'-6"	3:1	55"	65 1/2"	33 3/8"	24"	4"	8750	2'-10 1/2"
60"	6"	2'-0"	6'-6"	1'-10"	8'-4"	8'-0"	3:1	61"	72 1/2"	36 1/8"	24"	4"	9270	3'-5"
72"	7"	3'-10"	6'-6"	1'-10"	8'-4"	9'-0"	3:1	73"	77 1/8"	38 1/8"	24"	5"	13250	4'-6"

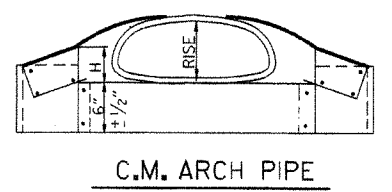
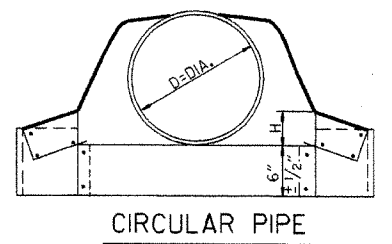
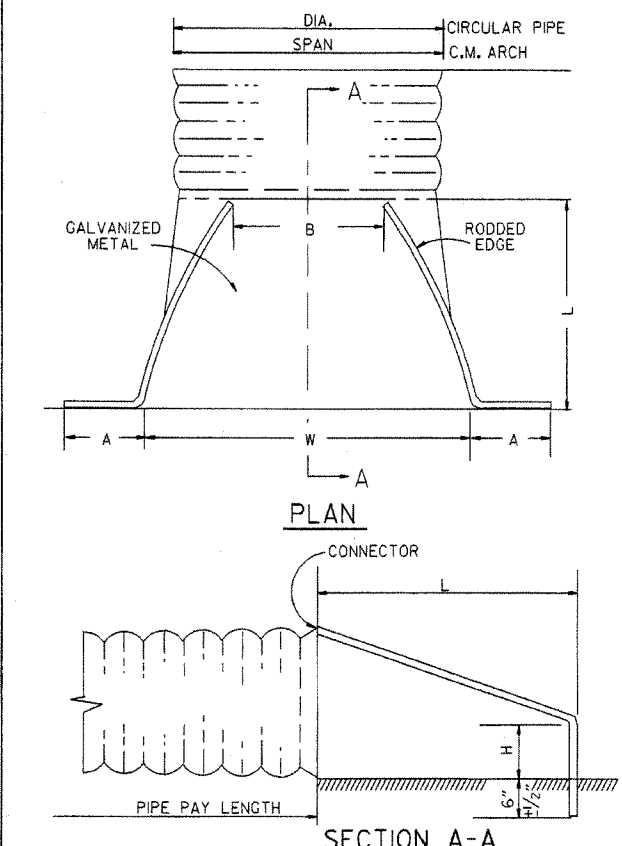
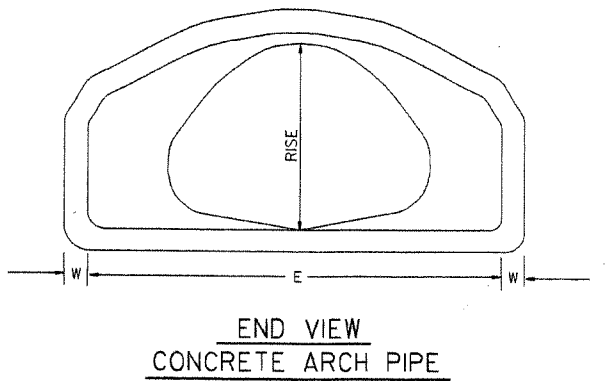


NOTE: TONGUE END ON UPSTREAM SECTION  
GROOVE END ON DOWNSTREAM SECTION

**ARCH PIPE**

EQUIV. DIA.	• SPAN		• RISE		W	A	B	C	D	E	P	R2	G-T	S
	AASHTO M 206	AHD NOMINAL	AASHTO M 206	AHD NOMINAL										
INCHES														
15	18	18	11	11	2"	4"	2'-0"	4'-0"	6'-0"	3'-0"	29"	12"	1 1/2"	2 1/2:1
18	22	22	13 1/2	14	2 1/2"	5"	2'-0"	4'-1"	6'-1"	3'-6"	32 3/8"	13"	2 1/2"	2 1/2:1
21	26	26	15 1/2	16	2 3/4"	7"	2'-3"	3'-10"	6'-1"	4'-0"	34 1/8"	14"	2 1/2"	2 1/2:1
24	28 1/2	29	18	18	3"	9"	2'-3"	3'-10"	6'-1"	5'-0"	36 1/8"	15"	2 1/2"	2 1/2:1
30	36 1/4	36	22 1/2	23	3 1/2"	10"	3'-1"	3'-0 1/2"	6'-1 1/2"	6'-0"	47 1/8"	20"	3"	2 1/2:1
36	43 3/4	44	26 3/8	27	4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	6'-6"	54 3/8"	22"	3 1/2"	2 1/2:1
42	51 1/8	51	31 3/8	31	4 1/2"	11 1/2"	4'-7"	1'-10 1/4"	6'-5 1/4"	7'-2"	59 3/8"	23"	3 3/4"	2 1/2:1
48	58 1/2	59	36	36	5"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	7'-10"	70 5/8"	24"	4 1/4"	2 1/2:1
54	65	65	40	40	5 1/2"	1'-7"	5'-3"	2'-11"	8'-2"	8'-6"	72 1/8"	24"	4 3/4"	2 1/2:1
60	73	73	45	45	6"	1'-10"	5'-6"	2'-8"	8'-2"	9'-0"	77 1/8"	24"	5"	2 1/2:1

• THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PER CENT FROM THE VALUES SPECIFIED BY AASHTO M 206.

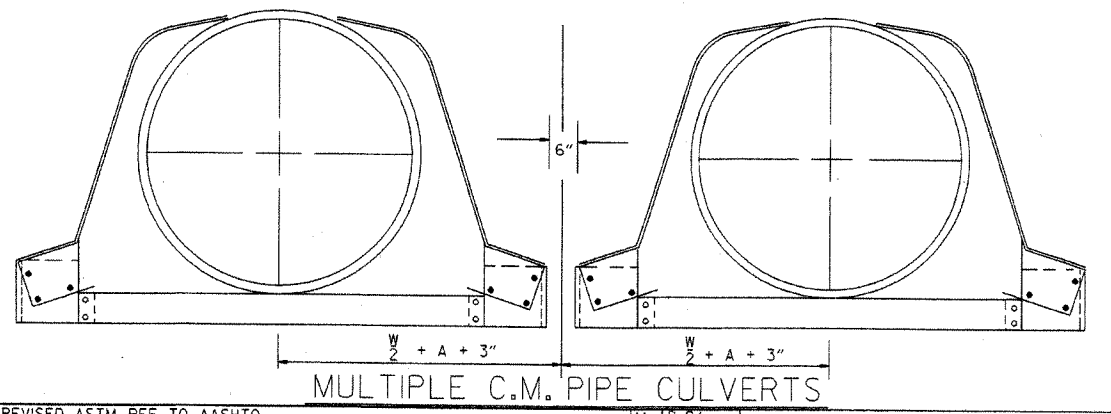
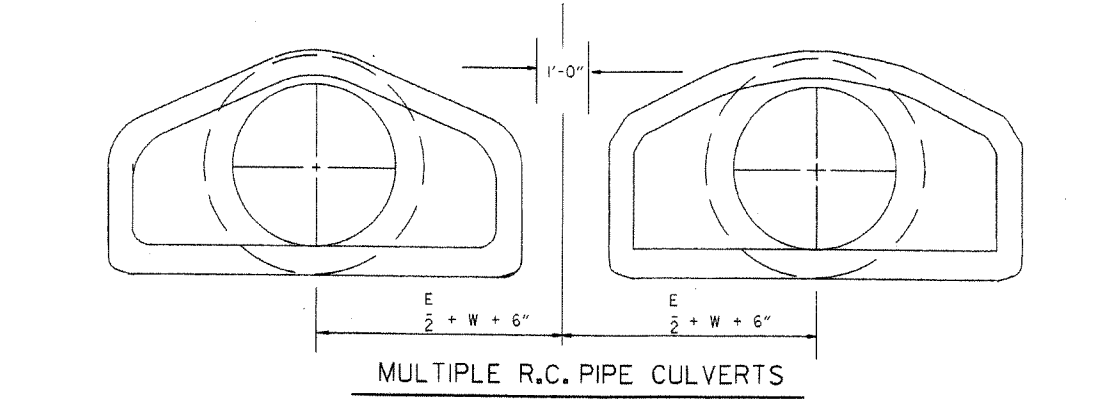


**CIRCULAR PIPE**

D. DIA.	GAUGE	A 1" ±	B. MAX.	H 1" ±	L 1 1/2" ±	W 2" ±	S
12	16	6	6	6	21	24	2 1/2:1
15	16	7	8	6	26	30	2 1/2:1
18	16	8	10	6	31	36	2 1/2:1
21	16	9	12	6	36	42	2 1/2:1
24	16	10	13	6	41	48	2 1/2:1
30	14	12	16	8	51	60	2 1/2:1
36	14	14	19	9	60	72	2 1/2:1
42	12	16	22	11	69	84	2 1/2:1
48	12	18	27	12	78	90	2 1/2:1
54	12	18	30	12	84	102	2:1
60	12	18	33	12	87	114	1 1/2:1
66	12	18	36	12	87	120	1 1/2:1
72	12	18	39	12	87	126	1 1/3:1

**C.M. ARCH PIPE**

EQUIV. DIA.	SPAN	RISE	A 1" ±	B. MAX.	H 1" ±	L 1 1/2" ±	W 2" ±	S	GAUGE
15"	17	13	7	9	6	19	30	2 1/2:1	16
18"	21	15	7	10	6	23	36	2 1/2:1	16
21"	24	18	8	12	6	28	42	2 1/2:1	16
24"	28	20	9	14	6	32	48	2 1/2:1	16
30"	35	24	10	16	6	39	60	2 1/2:1	14
36"	42	29	12	18	8	46	75	2 1/2:1	14
42"	49	33	13	21	9	53	85	2 1/2:1	12
48"	57	38	18	26	12	63	90	2 1/2:1	12
54"	64	43	18	30	12	70	102	2 1/2:1	12
60"	71	47	18	33	12	77	114	2 1/4:1	12



NOTE: ALTERNATE CONNECTIONS TO THE PIPE CULVERTS, IN ACCORDANCE WITH MANUFACTURER'S STANDARD PRACTICES, MAY BE MADE SUBJECT TO THE APPROVAL OF THE ENGINEER.

**END SECTIONS FOR CORRUGATED METAL PIPE CULVERTS**

10-18-96	REVISED ASTM REF. TO AASHTO	10-18-96	ARKANSAS STATE HIGHWAY COMMISSION <b>FLARED END SECTION</b> STANDARD DRAWING FES-2
5-15-80	REVISED DISTANCE BETWEEN MULTIPLE R.C.P. F.E.S.	664-5-15-80	
7-14-78	C.M. ARCH SIZES TO CONFORM WITH AASHTO SIZES	752-7-14-78	
8-22-75	ADDED MULTIPLE PIPE CULVERTS	517-8-22-75	
12-5-74	REMOVED NOTE RE REINF. FOR R.C. F.E.S.	500-12-5-74	
5-24-73	CMP END SECTION, SHOW PIPE PAY LENGTH	627-5-24-73	
10-2-72	REVISED AND REDRAWN	760-10-2-72	
DATE	REVISION	FILMED	

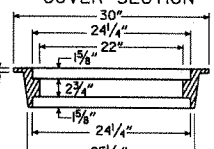
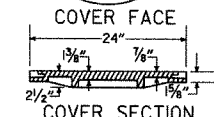
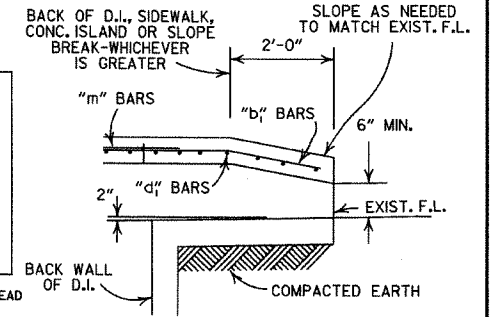
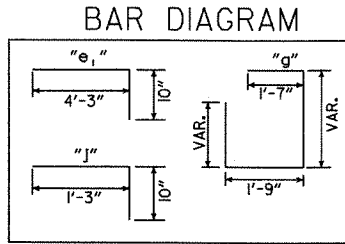
4'-0" LENGTH DROP INLET DROP INLET EXTENSION 37

PIPE SIZE	MIN. WIDTH	HEIGHT 5'-0"		PLUS OR MINUS PER LIN. FT. OF HEIGHT		4'-0"		8'-0"	
		CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS
18"	2'-6"	1.77	156	0.28	22	0.58	38	0.87	72
24"	2'-6"	1.79	156	0.28	22				
30"	3'-2"	2.39	205	0.30	26				
36"	3'-8"	2.63	236	0.32	28				
42"	4'-4"	2.95	250	0.34	30				
48"	4'-10"	3.21	265	0.36	32				
						DEDUCT FROM QUANTITY COMPUTED FOR EACH EXTENSION ADDED.			
						0.04	3		

NOTE: QUANTITIES ARE APPROXIMATE AND ARE SHOWN FOR BIDDER INFORMATION ONLY.

DEDUCT FROM QUANTITY COMPUTED FOR EACH PIPE ENTERING INLET

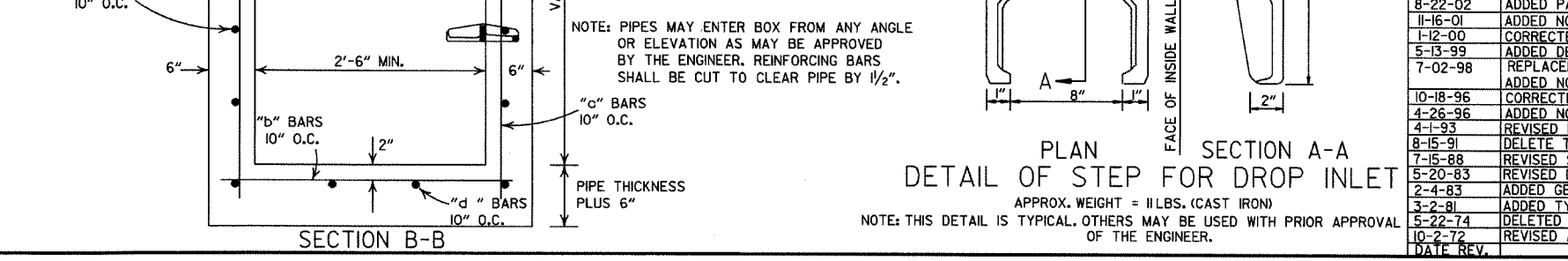
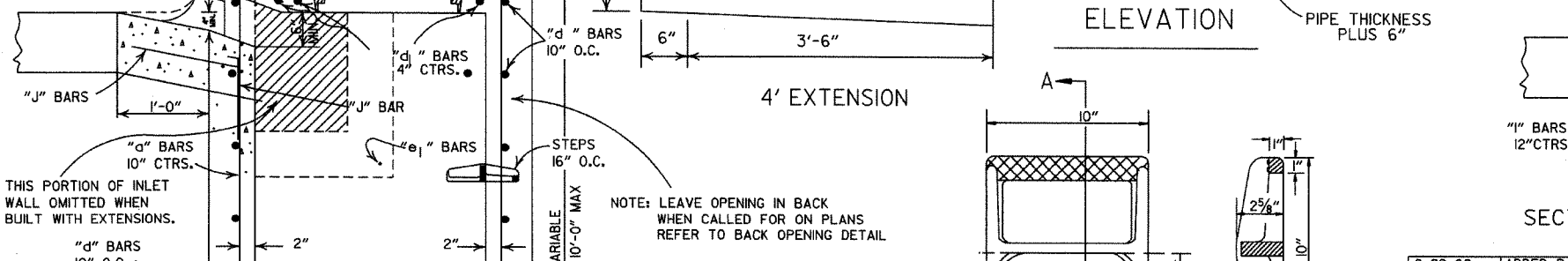
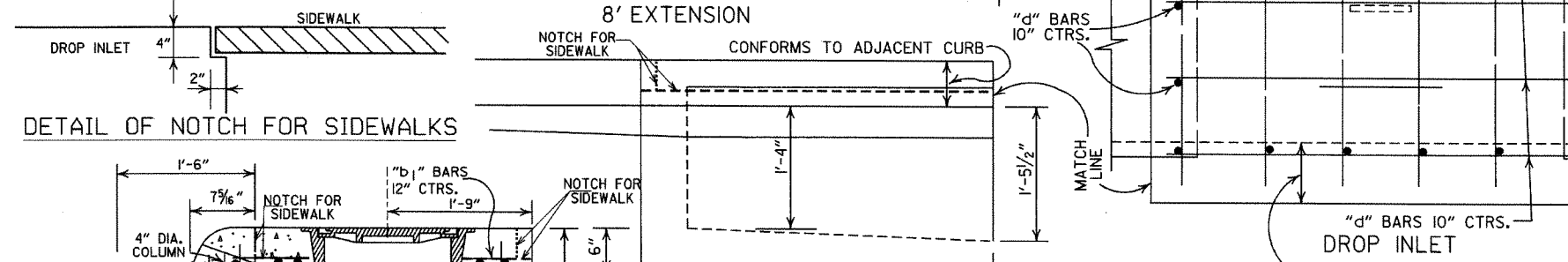
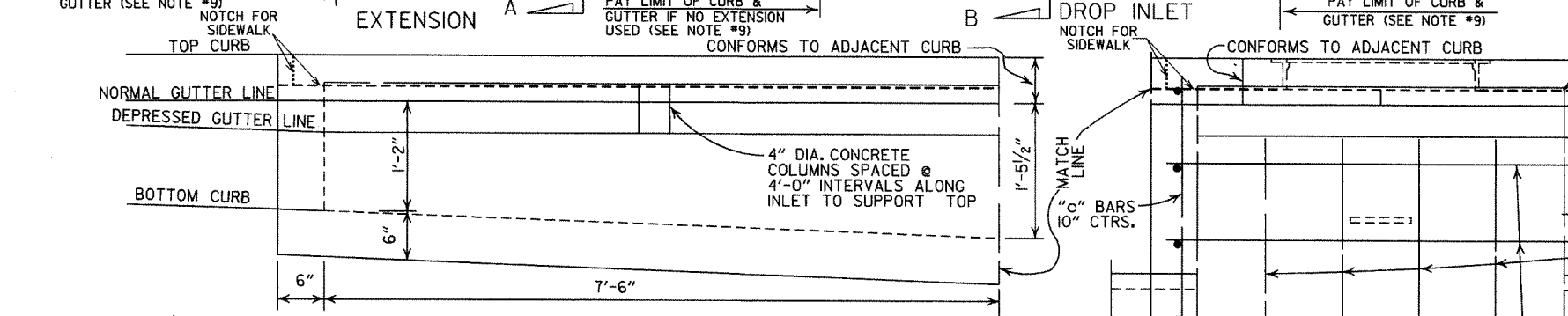
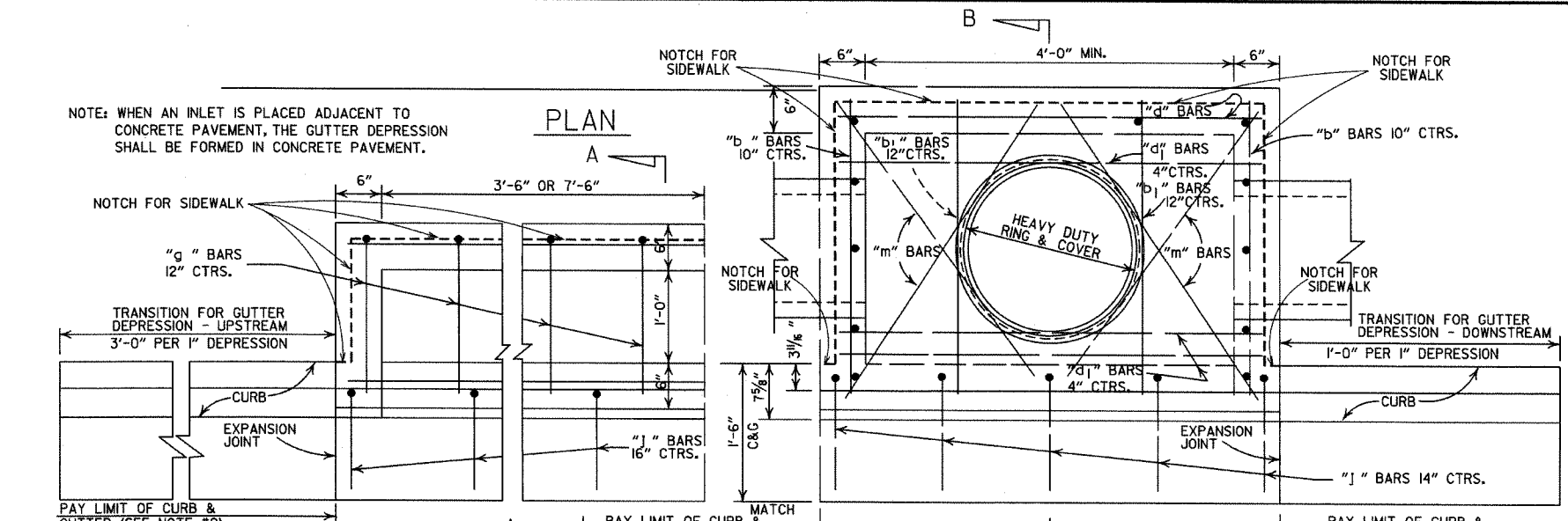
INSIDE DIA. PIPE INCHES	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS
18	0.05	2
24	0.09	3
30	0.13	4
42	0.24	8



APPROXIMATE TOTAL WEIGHT = 333 LBS.

HEAVY DUTY RING & COVER

- GENERAL NOTES:
1. ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER.
  2. STEPS SHALL BE INSTALLED IN ALL INLETS 4'-0" HIGH AND OVER OF AS APPROVED BY THE ENGINEER.
  3. ALL REINF. BARS SHALL BE #4 AND HAVE 1/2" COVER.
  4. DROP INLETS AND EXTENSION ON CURVED SECTIONS SHALL CONFORM TO THE CURVATURE OF THE CURB.
  5. THIS DROP INLET MAY BE CONSTRUCTED ON NEW OR EXISTING R.C. BOX CULVERT AS SHOWN ON F.P.C.-9.
  6. WHEN PLANS CALL FOR DROP INLET OVER 10'-0" HIGH, FLOOR AND WALLS SHALL BE CONSTRUCTED AS SHOWN FOR TYPE "RM" DROP INLET (F.P.C.-9D).
  7. HEAVY DUTY RING SHALL ALWAYS BE INSTALLED WITH FLANGE ON TOP.
  8. DURING CONSTRUCTION OF THE ROADWAY THE CONTRACTOR SHALL MAINTAIN DRAINAGE INTO OR AROUND THE DROP INLET AS APPROVED BY THE ENGINEER.
  9. PAYMENT FOR CURB AND/OR CURB AND GUTTER WITHIN THE LIMITS OF DROP INLETS AND DROP INLET EXTENSIONS SHALL BE CONSIDERED INCLUDED IN PAYMENT MADE FOR DROP INLETS AND/OR DROP INLET EXTENSIONS.
  10. HEAVY DUTY RING AND COVER SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M105 CLASS 35B & AASHTO M306.
  11. HEAVY DUTY RING AND COVER SHALL NOT BE PAINTED.
  12. 4"x2" NOTCH SHALL BE FORMED IN ALL DROP INLETS TO SUPPORT SIDEWALK CONSTRUCTION. REFER TO DETAIL OF NOTCH FOR SIDEWALKS.
  13. DIMENSIONS SHOWN FOR RING AND COVER ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR CASTINGS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR CASTING DESIGNS MAY BE MADE BY REFERRING TO PREVIOUSLY APPROVED DRAWINGS.



PLAN SECTION A-A  
DETAIL OF STEP FOR DROP INLET

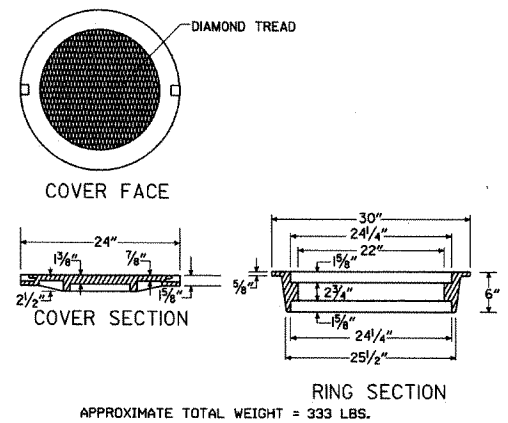
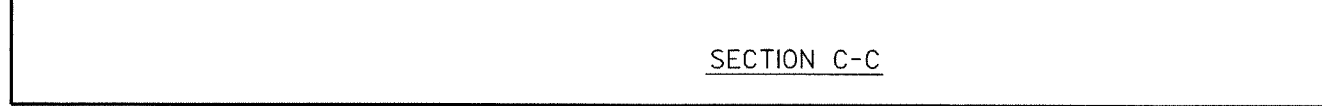
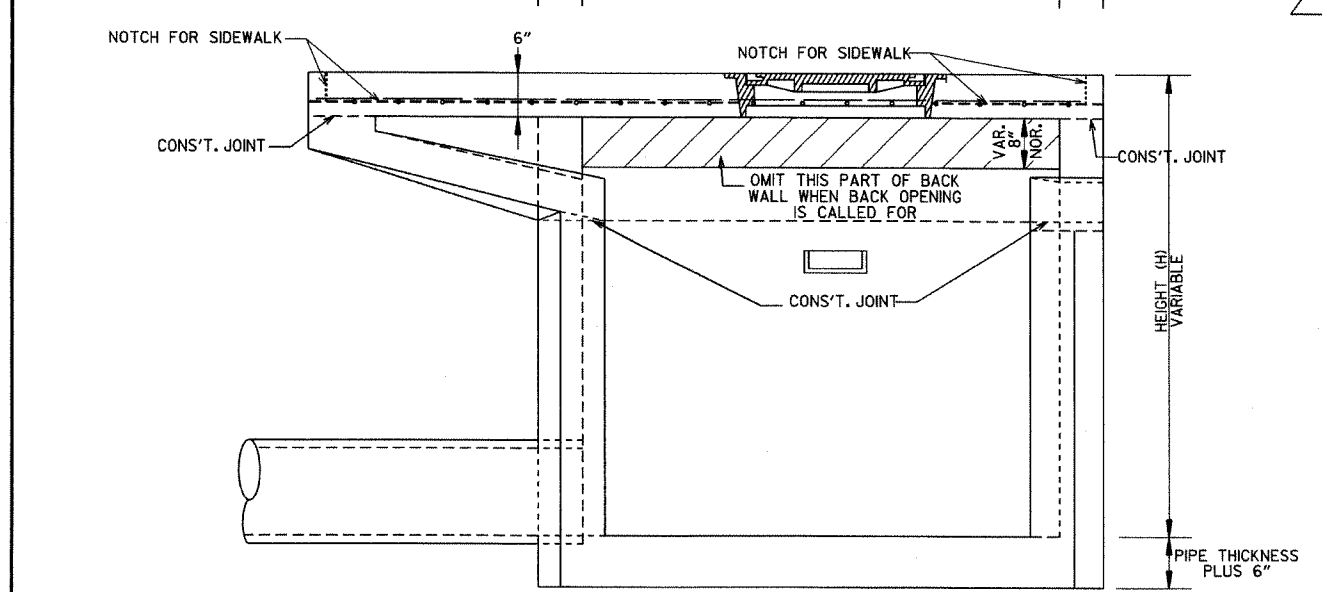
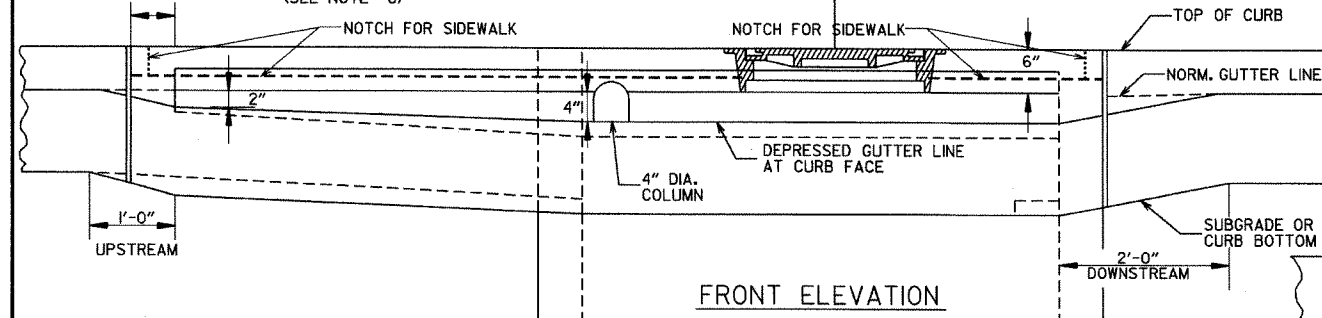
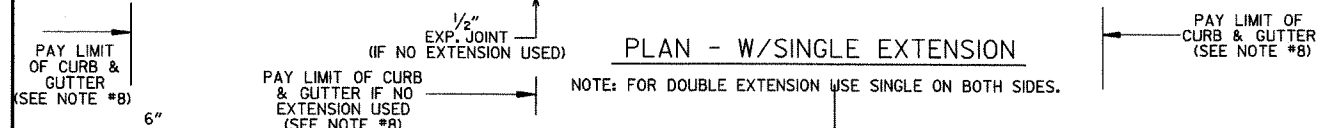
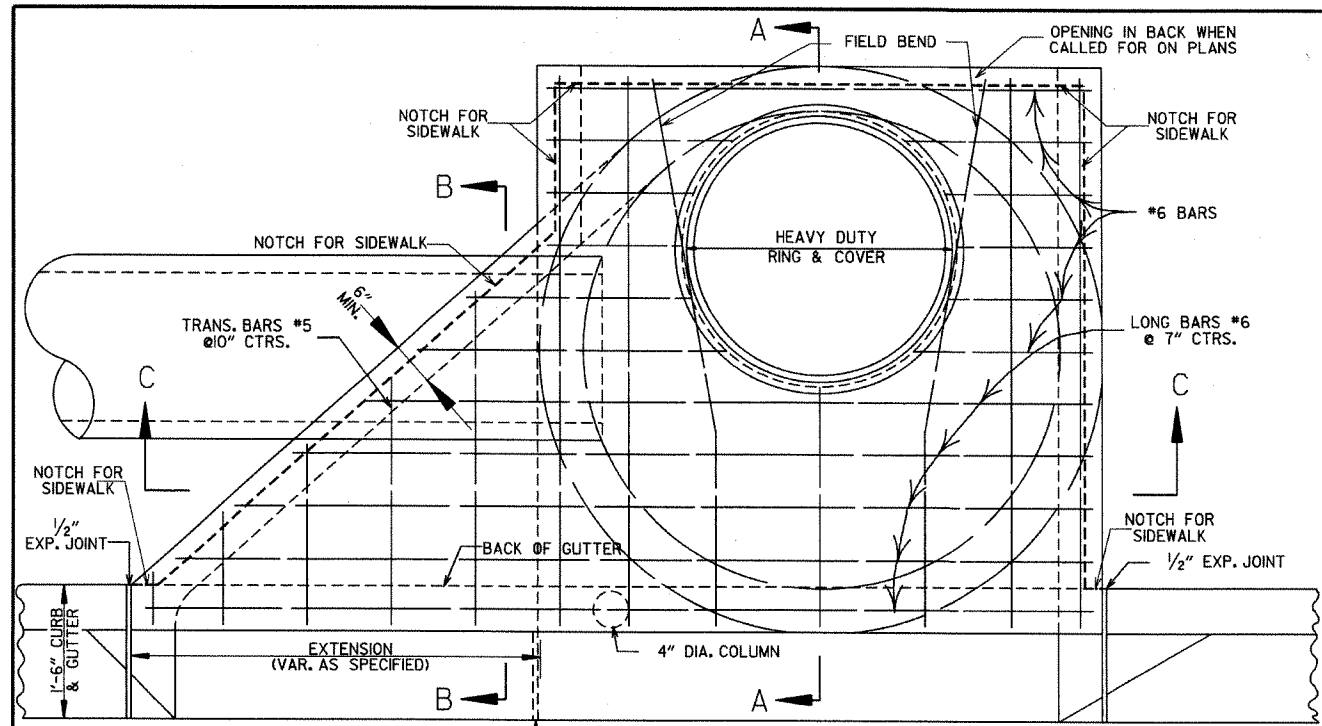
APPROX. WEIGHT = 11 LBS. (CAST IRON)  
NOTE: THIS DETAIL IS TYPICAL. OTHERS MAY BE USED WITH PRIOR APPROVAL OF THE ENGINEER.

DATE	REVISION	DATE FILMED
8-22-02	ADDED PAY LIMIT CURB NOTES TO SECTIONS A-A & B-B	
11-16-01	ADDED NOTE 13; REVISED SECTION B-B	
1-12-00	CORRECTED DIMENSION ON SECTION B-B & REVISED RING & COVER	
5-13-99	ADDED DETAIL OF NOTCH FOR SIDEWALKS	
7-02-98	REPLACED RING & COVER W/HEAVY DUTY RING & COVER ADDED NOTES 9,10,&11	
10-18-96	CORRECTED SPELLING	
4-26-96	ADDED NOTE 8 & REVISED (4'x8') EXTENSION TITLES	10-18-96
4-1-93	REVISED BACK OPENING & NOTE	
8-15-91	DELETE TYPE IV GRATE	
7-15-88	REVISED STEP DETAIL	
5-20-83	REVISED DETAILS OF GRATES (TYPE IV & IV-A)	
2-4-83	ADDED GENERAL NOTE NO. 4	
3-2-81	ADDED TYPE IV-A GRATE	
5-22-74	DELETED INLET (TYPE F) & GRATE (TYPE III)	
10-2-72	REVISED AND REDRAWN	

ARKANSAS STATE HIGHWAY COMMISSION

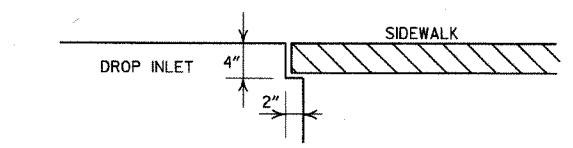
DETAILS OF DROP INLETS  
(TYPE C)

STANDARD DRAWING FPC-9E

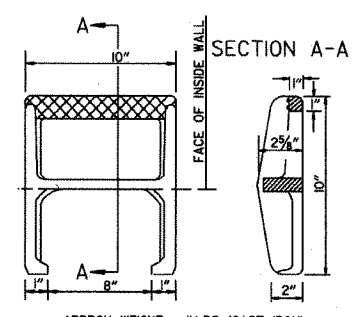


HEAVY DUTY RING & COVER

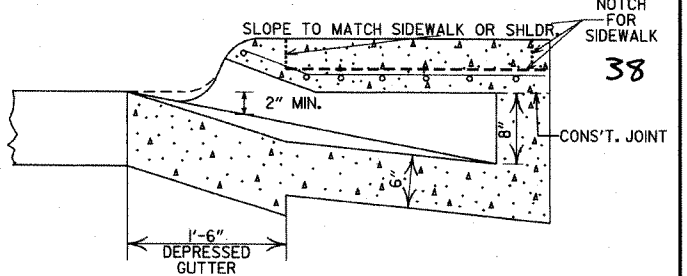
1. HEAVY DUTY RING AND COVER SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M105 CLASS 35B & AASHTO M306.
2. HEAVY DUTY RING AND COVER SHALL NOT BE PAINTED.
3. HEAVY DUTY RING SHALL ALWAYS BE INSTALLED WITH FLANGE ON TOP.



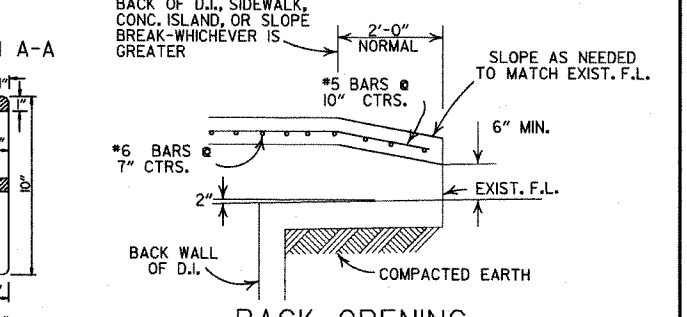
DETAIL OF NOTCH FOR SIDEWALKS



DETAIL OF STEP FOR DROP INLET



SECTION B-B



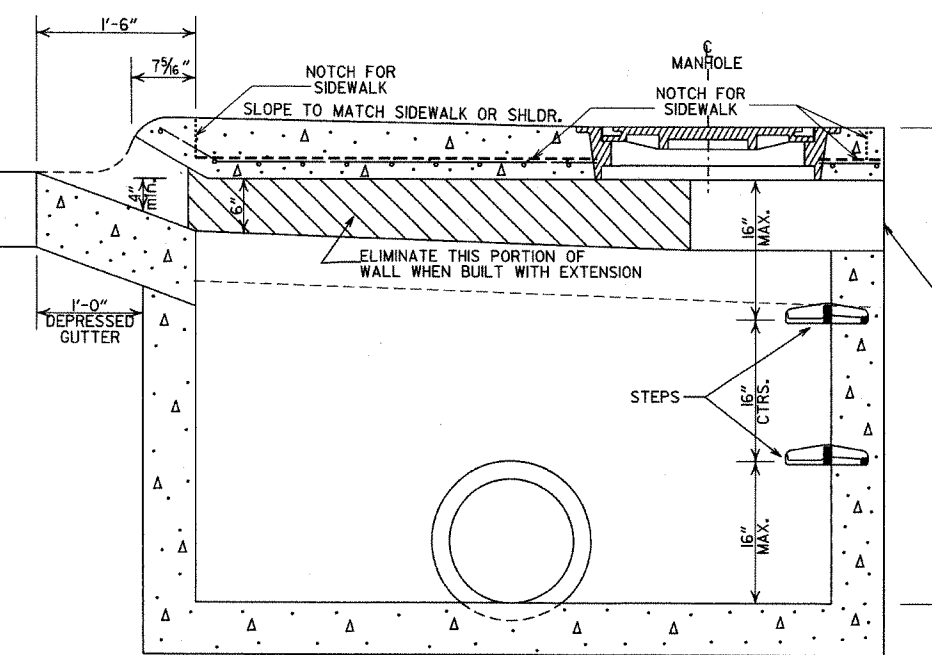
BACK OPENING

WHEN OPENING IN BACK IS CALLED FOR ON PLANS EXTEND OPENING AS SHOWN IN DETAIL. PAYMENT TO BE INCLUDED IN PRICE BID FOR DROP INLET (TYPE MO).

- GENERAL NOTES:
1. ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER.
  2. STEPS SHALL BE INSTALLED IN ALL INLETS 4'-0" HIGH AND OVER OR AS DIRECTED BY THE ENGINEER.
  3. ALL REINFORCING BARS SHALL BE GRADE 60 AND HAVE MIN. 1/2" COVER.
  4. DROP INLETS AND EXTENSION ON CURVED SECTIONS SHALL CONFORM TO THE CURVATURE OF THE CURB.
  5. 4" DIA. COLUMNS SPACED AT MAX. 4'-0" INTERVALS SHALL BE INSTALLED ALONG INLET AND EXTENSION TO SUPPORT TOP.
  6. BASE AND INLET WALLS SHALL BE CAST MONOLITHICALLY.
  7. THE THROAT SHALL BE CAST INTEGRALLY WITH THE GUTTER.
  8. PAYMENT FOR CURB AND/OR CURB AND GUTTER EXTENSIONS WITHIN THE LIMITS OF DROP INLETS AND DROP INLET EXTENSIONS SHALL BE CONSIDERED INCLUDED IN PAYMENT MADE FOR DROP INLETS AND/OR DROP INLET EXTENSIONS.
  9. PIPES MAY ENTER DROP INLET FROM ANY ANGLE OR ELEVATION AS MAY BE APPROVED BY THE ENGINEER.
  10. APPROPRIATE SIZE TYPE C DROP INLETS MAY BE SUBSTITUTED FOR TYPE MO DROP INLETS AS APPROVED BY THE ENGINEER. PAYMENT TO BE AS DROP INLET (TYPE MO).
  11. DURING CONSTRUCTION OF THE ROADWAY THE CONTRACTOR SHALL MAINTAIN DRAINAGE INTO OR AROUND THE DROP INLET AS APPROVED BY THE ENGINEER.
  12. 4"x2" NOTCH SHALL BE FORMED IN ALL DROP INLETS TO SUPPORT SIDEWALK CONSTRUCTION. REFER TO DETAIL OF NOTCH FOR SIDEWALKS.
  13. DIMENSIONS SHOWN FOR RING AND COVER ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR CASTINGS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR CASTING DESIGNS MAY BE MADE BY REFERRING TO PREVIOUSLY APPROVED DRAWINGS.

LEAVE OPENING IN BACK WHEN CALLED FOR ON PLANS REFER TO BACK OPENING DETAIL

MINIMUM WALL THICKNESS			
DIA. OF D.I.	DIA. OF OUTLET PIPE	CAST IN PLACE	PRECAST
4" I.D.	12" THRU 27"	6"	5"
5" I.D.	30" THRU 42"	8"	6"
6" I.D.	48" THRU 54"	8"	7"



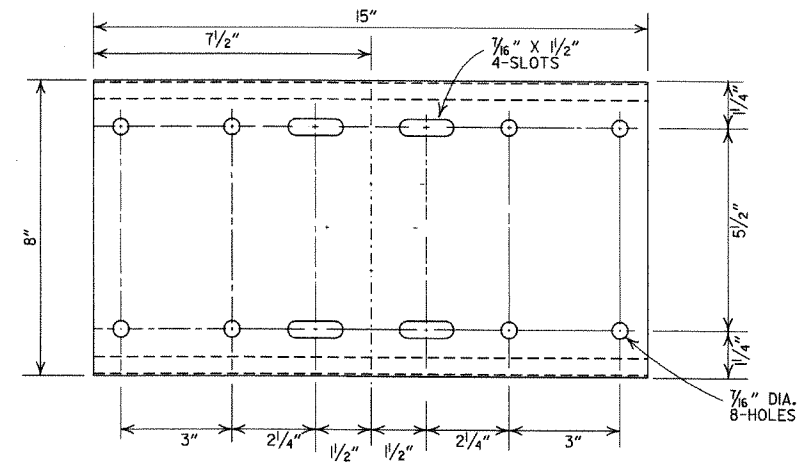
SECTION A-A

DATE	ISSUED	REVISIONS	DATE FILMED
8-22-02	ADDED PAY LIMIT CURB NOTES TO SECTIONS A-A & B-B		
11-16-01	ADDED NOTE 13		
1-12-00	REVISED HEAVY DUTY RING & COVER		
5-13-99	ADDED NOTCH DETAIL FOR SIDEWALKS		
7-02-98	REF. NOTE B, REV. PLAN DET. REV. PICTURE FOR NEW RING & COVER, ADDED HEAVY DUTY RING & COVER AND DETAIL OF STEP FOR DROP INLET		
4-26-96	ADDED NOTE 11, ADJ. OPENING DIMENSION		
10-12-95	CORRECTED #5 BAR SPACING		
1-20-95	CORRECTED DIAMETER OF IN IN BOX		
12-2-95	TYPE C TO MO (OPEN BACK DETAIL)		
11-1-93	REVISED GENERAL NOTES		11-3-94
11-1-93	REV. BACK OPEN DETAIL & NOTE		11-3-94
8-15-91	REVISED NOTES 1, 2 & ADDED BR. OPEN DETAIL		8-18-91
11-30-89	ADDED NOTE NO. 12		11-30-89
11-21-89	ADDED NOTE & MINIMUM WALL THICKNESS		11-21-89
11-12-89	ADDED EXTEND NOTE TO SECTION A-A		11-12-89
11-11-87	MODIFIED WALL THICKNESS		11-11-87
10-12-87	ISSUED		10-12-87

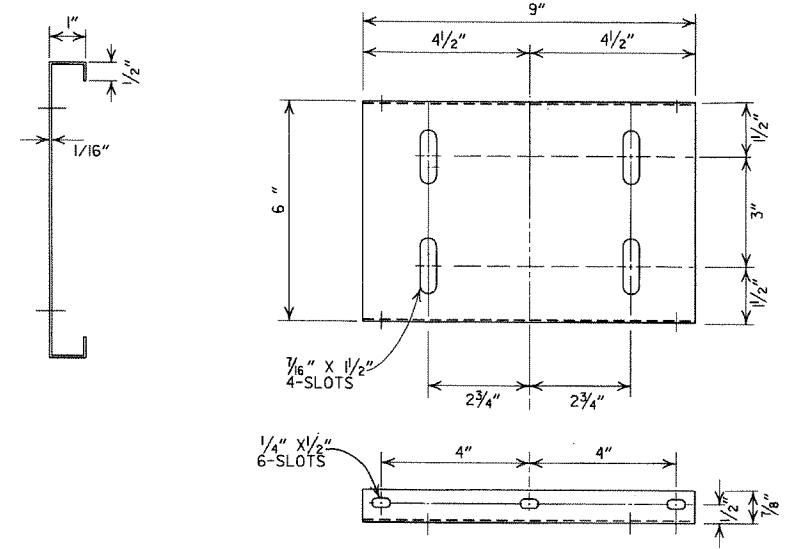
ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF DROP INLET (TYPE MO)

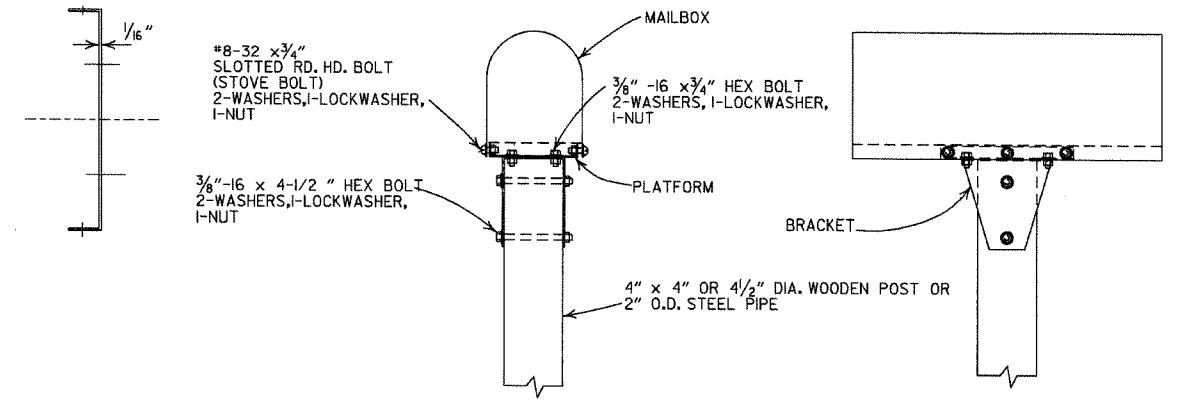
STANDARD DRAWING FPC-9M



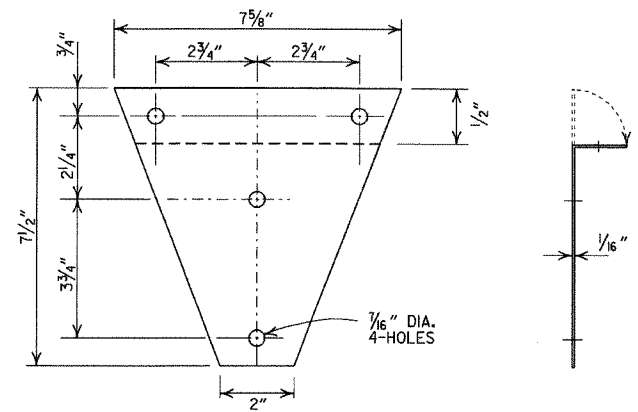
SHELF



PLATFORM

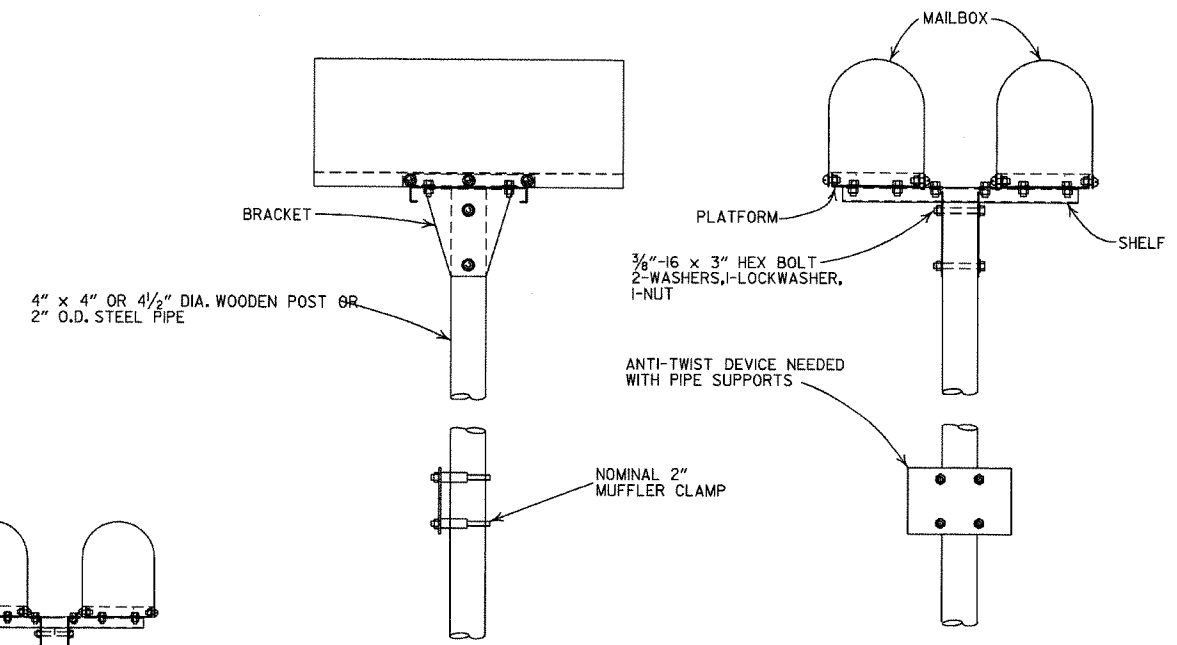


SINGLE INSTALLATION

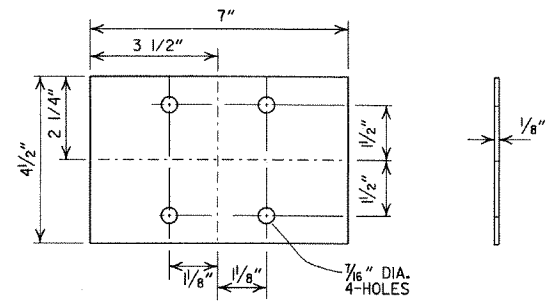


BRACKET

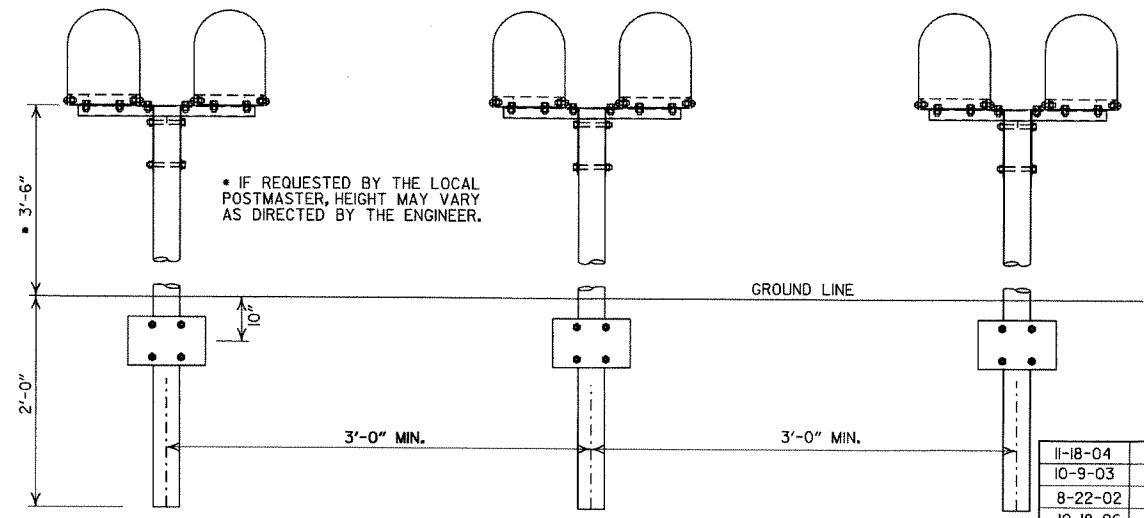
- GENERAL NOTES
1. MAILBOX POSTS MAY BE WOOD OR METAL. WOOD POSTS SHALL BE PRESSURE TREATED FOR GROUND CONTACT IN ACCORDANCE WITH SECTION 637.02 OF THE STANDARD SPECIFICATIONS.
  2. ANTI-TWIST PLATES SHALL BE USED ONLY ON METAL POSTS.
  3. MAILBOX SHELF, BRACKET & PLATFORM SHALL BE GALVANIZED OR PAINTED STEEL, HOWEVER TREATED WOOD MAY BE USED WITH WOODEN POSTS. THE WOODEN SHELF, BRACKET & PLATFORM SHALL BE A MINIMUM OF 3/4" THICK AND SHALL BE ASSEMBLED WITH BOLTS OF THE APPROPRIATE LENGTH WITH SIX 8 x 3/4" FLATHEAD WOOD SCREWS USED TO ATTACH THE MAILBOX TO THE PLATFORM.
  4. THE MAILBOX SHELF AND PLATFORM THAT IS SHOWN IS FOR STANDARD SIZE MAILBOXES. THE SHELF AND PLATFORM SIZE SHALL BE MODIFIED TO FIT MAILBOXES OF A DIFFERENT SIZE.
  5. METAL PIPE FOR MAILBOX SUPPORT SHALL BE 2" OUTSIDE DIAMETER STEEL WITH A WALL THICKNESS OF 0.145" AND A WEIGHT OF 2.72 LBS PER FT. OUTSIDE DIAMETER AND WEIGHT SHALL HAVE A TOLERANCE OF +/- 5% ACCORDING TO AASHTO M 181.
  6. MAILBOX SUPPORT SYSTEM DIFFERING FROM THOSE SHOWN MAY BE USED, PROVIDED THEY ARE ON THE AHTD QUALIFIED PRODUCTS LIST FOR MAILBOX SUPPORTS.



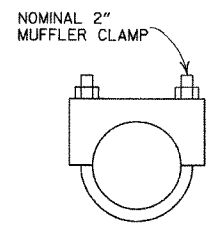
DOUBLE INSTALLATION



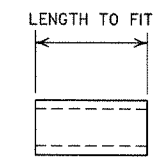
ANTI-TWIST PLATE



SPACING FOR MULTIPLE POST INSTALLATION



CLAMP



SPACER

DATE	ISSUED	REVISION
11-18-04		REVISED NOTES
10-9-03		REVISED NOTE 6
8-22-02		REVISED NOTE 6
10-18-96		CORRECTED AASHTO
10-1-92		CORRECTED SPELLING
9-26-91		NEW PHONE NUMBER
8-15-91		ADDED NOTE
11-30-89		ADJUSTED HEIGHT & ADDED NOTE
2-16-89		DELETED SLOTS FROM SHELF & PLTF
11-17-88	10-1-92	ADJUSTED DIMENSIONS OF STEEL POSTS
7-15-88	120-7-15-88	ISSUED
		FILMED
		REVISION

ARKANSAS STATE HIGHWAY COMMISSION

MAILBOX DETAILS

STANDARD DRAWING MB-1

REINFORCED CONCRETE ARCH PIPE DIMENSIONS

EQUIV. DIA.	SPAN		RISE	
	AASHTO M 206	AHTD NOMINAL	AASHTO M 206	AHTD NOMINAL
INCHES	INCHES			
15	18	18	11	11
18	22	22	13 1/2	14
21	26	26	15 1/2	16
24	28 1/2	29	18	18
30	36 1/4	36	22 1/2	23
36	43 3/8	44	26 3/8	27
42	51 1/8	51	31 1/8	31
48	58 1/2	59	36	36
54	65	65	40	40
60	73	73	45	45
72	88	88	54	54
84	102	102	62	62
90	115	115	72	72
96	122	122	77 1/2	77
108	138	138	87 1/8	87
120	154	154	96 1/8	97
132	168 3/4	169	106 1/2	107

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M206.

REINFORCED CONCRETE HORIZONTAL ELLIPTICAL PIPE DIMENSIONS

EQUIV. DIA.	AASHTO M 207	
	SPAN	RISE
INCHES	INCHES	
18	23	14
24	30	19
27	34	22
30	38	24
33	42	27
36	45	29
39	49	32
42	53	34
48	60	38
54	68	43
60	76	48
66	83	53
72	91	58
78	98	63
84	106	68

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M207.

CONSTRUCTION SEQUENCE

1. PLACE STRUCTURAL BEDDING MATERIAL TO GRADE. DO NOT COMPACT.
2. INSTALL PIPE TO GRADE.
3. COMPACT STRUCTURAL BEDDING OUTSIDE THE MIDDLE THIRD OF THE PIPE.
4. PLACE AND COMPACT THE HAUNCH AREA UP TO THE MIDDLE OF THE PIPE.
5. COMPLETE BACKFILL ACCORDING TO SUBSECTION 606.03.(f)(ii).

NOTE: HAUNCH AND STRUCTURAL BEDDING MATERIAL WILL NOT BE PAID FOR SEPARATELY, BUT COMPENSATION WILL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF CONCRETE PIPE.

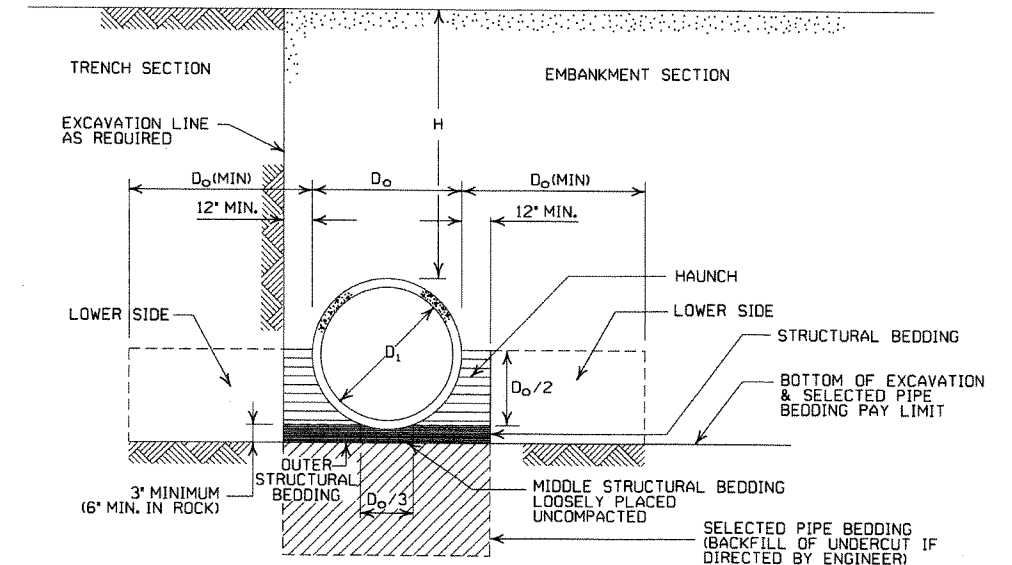
- LEGEND -

- D<sub>1</sub> = NORMAL INSIDE DIAMETER OF PIPE
- D<sub>o</sub> = OUTSIDE DIAMETER OF PIPE
- H = FILL COVER HEIGHT OVER PIPE (FEET)
- MIN. = MINIMUM
- [Symbol] = UNDISTURBED SOIL

INSTALLATION TYPE	MATERIAL REQUIREMENTS FOR HAUNCH AND STRUCTURAL BEDDING
TYPE 1	AGGREGATE BASE COURSE (CLASS 5 OR CLASS 7)
TYPE 2	SELECTED MATERIALS (CLASS SM-1, SM-2, OR SM-4) OR TYPE 1 INSTALLATION MATERIAL*
TYPE 3**	AASHTO CLASSIFICATION A-1 THRU A-6 SOIL OR TYPE 1 OR 2 INSTALLATION MATERIAL

\* SM-3 WILL NOT BE ALLOWED.

\*\* MATERIALS SHALL NOT INCLUDE ORGANIC MATERIALS OR STONES LARGER THAN 3 INCHES.



EMBANKMENT AND TRENCH INSTALLATIONS

1. MATERIAL IN THE HAUNCH AND OUTER STRUCTURAL BEDDING SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.
2. FOR TRENCHES WITH WALLS OF NATURAL SOIL, THE DENSITY OF THE SOIL IN THE LOWER SIDE ZONE SHALL BE AS FIRM AS THE 95% DENSITY REQUIRED FOR THE HAUNCH. IF THE EXISTING SOIL DOES NOT MEET THIS CRITERIA, IT SHALL BE REMOVED AND RECOMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OF MATERIAL USED.
3. FOR EMBANKMENTS, THE MATERIAL IN THE LOWER SIDE ZONE SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.

GENERAL NOTES

1. CONCRETE PIPE CULVERT CONSTRUCTION SHALL CONFORM TO ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2003 EDITION), WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS, UNLESS OTHERWISE NOTED IN THE PLANS, SECTION AND SUBSECTION REFER TO THE STANDARD CONSTRUCTION SPECIFICATIONS.
2. CONCRETE PIPE CULVERT DESIGN SHALL CONFORM TO AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, FIFTH EDITION (2010) WITH 2010 INTERIMS.
3. ALL PIPE SHALL CONFORM TO SECTION 606. CIRCULAR R.C. PIPE CULVERTS SHALL CONFORM TO AASHTO M170, R.C. ARCH PIPE CULVERTS SHALL CONFORM TO AASHTO M206 AND HORIZONTAL ELLIPTICAL PIPE CULVERTS SHALL CONFORM TO AASHTO M207.
4. ALL PIPE SHALL BE PROTECTED DURING CONSTRUCTION BY A COVER SUFFICIENT TO PREVENT DAMAGE FROM PASSAGE OF EQUIPMENT.
5. THE MINIMUM TRENCH WIDTH SHALL BE THE OUTSIDE DIAMETER OF THE PIPE PLUS 24 INCHES. THE MAXIMUM ALLOWABLE TRENCH WIDTH SHALL BE THE MINIMUM WIDTH PRACTICABLE FOR WORKING CONDITIONS.
6. MULTIPLE PIPE CULVERTS SHALL BE INSTALLED WITH A MINIMUM CLEARANCE OF 24 INCHES BETWEEN STRINGS OF PIPE. REFER TO STD. DWG. FES-2 FOR MINIMUM CLEARANCE WHERE FLARED END SECTIONS ARE USED.
7. IMPERVIOUS MATERIAL SHOULD BE PLACED AS DIRECTED BY THE ENGINEER AT THE ENDS OF THE CULVERT TO PREVENT LOSS OF STRUCTURAL BEDDING WHEN PERVIOUS MATERIAL IS USED FOR STRUCTURAL BEDDING AND/OR BACKFILL.
8. NOT MORE THAN ONE LIFTING HOLE MAY BE PROVIDED IN CONCRETE PIPE TO FACILITATE HANDLING. HOLE MAY BE CAST IN PLACE, CUT INTO THE FRESH CONCRETE AFTER FORMS ARE REMOVED, OR DRILLED. THE HOLE SHALL NOT BE MORE THAN TWO INCHES IN DIAMETER OR TWO INCHES SQUARE. CUTTING OR DISPLACEMENT OF REINFORCEMENT WILL NOT BE PERMITTED. SPALLED AREAS AROUND THE HOLE SHALL BE REPAIRED IN A WORKMANLIKE MANNER. LIFTING HOLE SHALL BE FILLED WITH MORTAR, CONCRETE, OR OTHER METHOD AS APPROVED BY THE ENGINEER.
9. WHEN DIRECTED BY THE ENGINEER, UNSUITABLE MATERIAL THAT IS ENCOUNTERED AT THE BOTTOM OF THE EXCAVATED TRENCH (BELOW THE AREA IDENTIFIED AS "STRUCTURAL BEDDING" ABOVE) WILL BE EXCAVATED AND REPLACED WITH SELECTED PIPE BEDDING. THE QUANTITY OF MATERIAL REQUIRED TO BACKFILL THE UNDERCUT AREA UP TO THE SELECTED PIPE BEDDING PAY LIMIT DESIGNATED ABOVE WILL BE MEASURED AND PAID FOR AS "SELECTED PIPE BEDDING."
10. WHEN THE EXISTING MATERIAL EXCAVATED FOR THE PIPE TRENCH IS DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR BACKFILLING THE PIPE (ABOVE THE AREA IDENTIFIED ABOVE AS THE HAUNCH), BORROW MATERIAL OR MATERIAL FROM THE ROADWAY EXCAVATION WILL BE USED TO BACKFILL THE PIPE. IF SUITABLE MATERIAL IS NOT AVAILABLE, THE ENGINEER MAY AUTHORIZE THE USE OF "SELECTED PIPE BACKFILL."

MINIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE			
	CLASS III		CLASS IV	CLASS V
	TYPE 1 OR 2	TYPE 3	ALL	ALL
PIPE ID (IN.)	FEET			
12-15	2	2.5	2	1
18-24	2.5	3	2	1
27-33	3	4	2	1
36-42	3.5	5	2	1
48	4.5	5.5	2	1
54-60	5	7	2	1
66-78	6	8	2	1
84-108	7.5	8	2	1

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE		
	CLASS III	CLASS IV	CLASS V
	FEET		
TYPE 1	21	32	50
TYPE 2	16	25	39
TYPE 3	12	20	30

NOTE: IF FILL HEIGHT EXCEEDS 50 FEET, A SPECIAL DESIGN CONCRETE PIPE WILL BE REQUIRED USING TYPE 1 INSTALLATION.

MINIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
	FEET	
TYPE 2 OR TYPE 3	2.5	1.5

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
	FEET	
TYPE 2	13	21
TYPE 3	10	16

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

DATE	REVISION	DATE FILMED
12-15-11	REVISED FOR LRFD DESIGN SPECIFICATIONS	
5-18-00	REVISED TYPE 3 BEDDING & ADDED NOTE	
3-30-00	REVISED INSTALLATIONS	
11-06-97	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

CONCRETE PIPE CULVERT FILL HEIGHTS & BEDDING

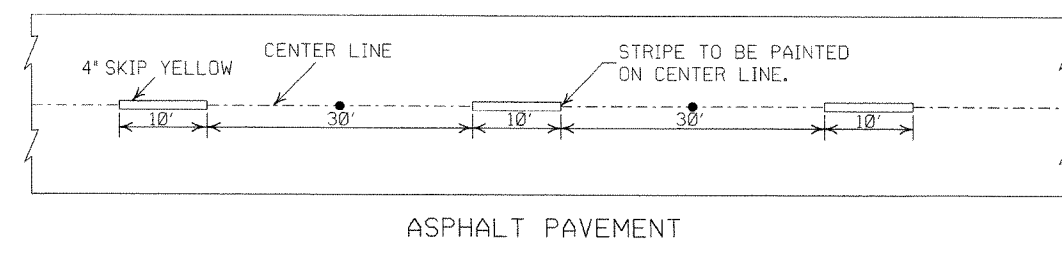
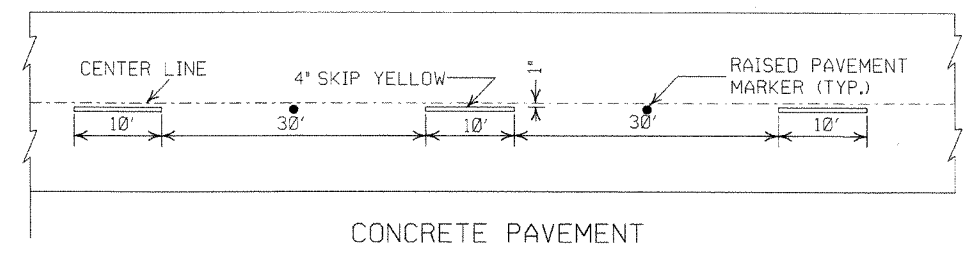
STANDARD DRAWING PCC-1





NOTES:

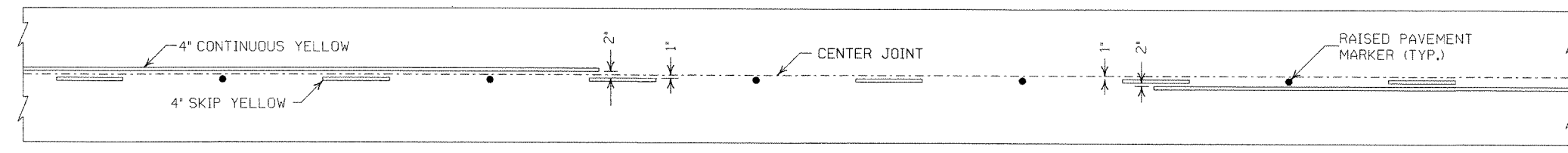
1. ALL LINES SHALL HAVE A WIDTH OF 4 INCHES.
2. THE THICKNESS AND RATE OF PAINT APPLICATION SHALL BE AS SPECIFIED IN SECTION 718 OF THE STANDARD SPECIFICATIONS.
3. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. RAISED PAVEMENT MARKERS SHALL BE CENTERED BETWEEN SKIP LINES ON 40 FEET SPACING UNLESS OTHERWISE SHOWN ON THE PLANS.



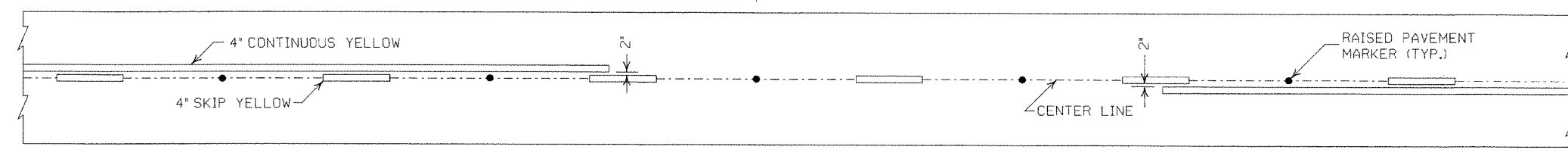
CONCRETE PAVEMENT

ASPHALT PAVEMENT

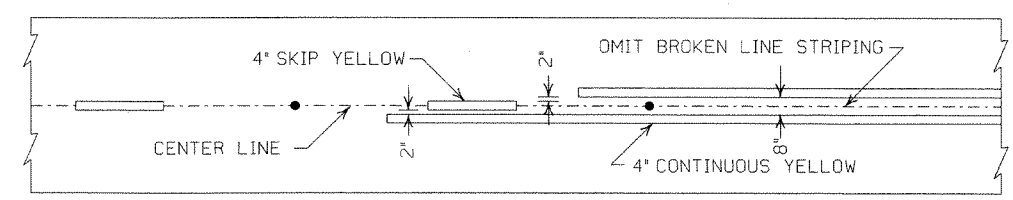
BROKEN LINE STRIPING



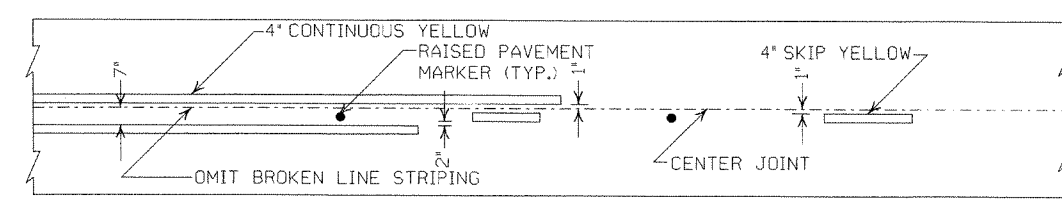
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

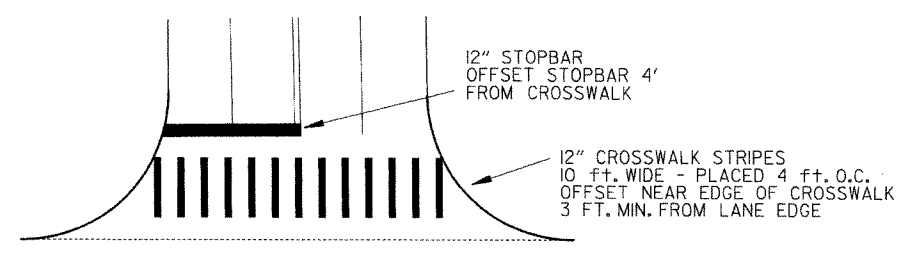


ASPHALT PAVEMENT

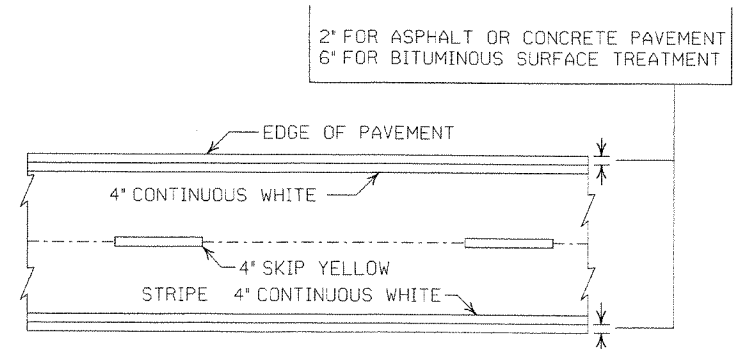


CONCRETE PAVEMENT

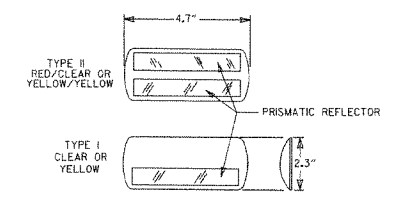
STRIPING AT ADJACENT NO PASSING LANES



CROSSWALK AND STOPBAR DETAILS



PAVEMENT EDGE LINE MARKING



NOTE: THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

DETAIL OF STANDARD RAISED PAVEMENT MARKERS

GENERAL NOTES:

THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND RAISED PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.


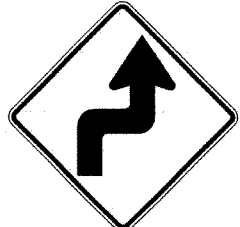
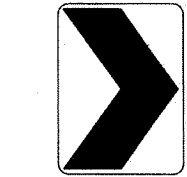



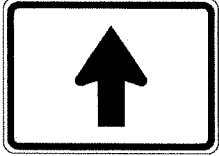
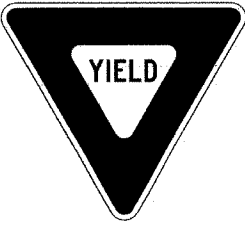

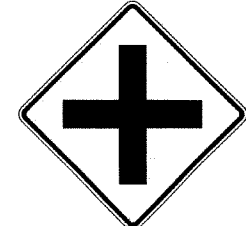

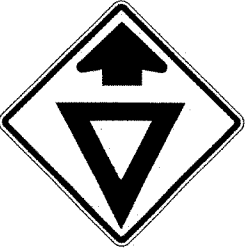

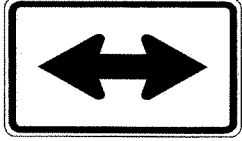
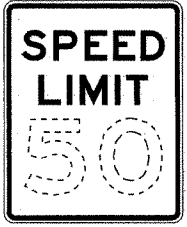

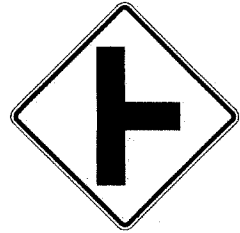




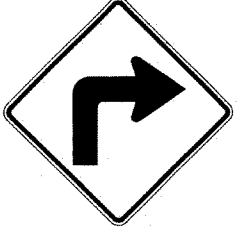

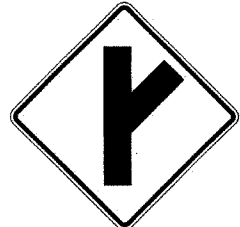

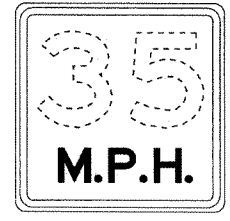

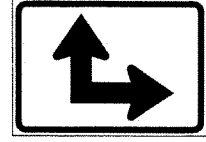
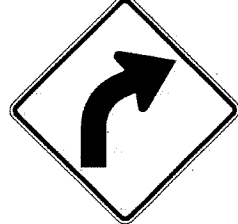
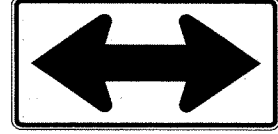
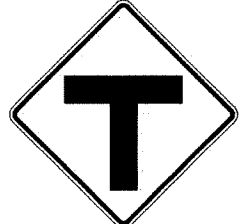

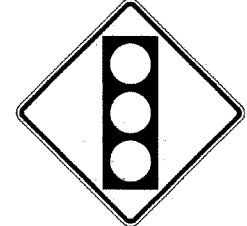
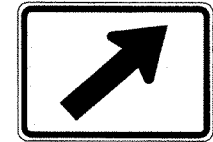

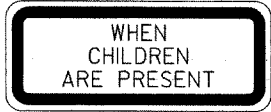
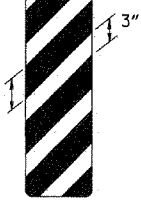
THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

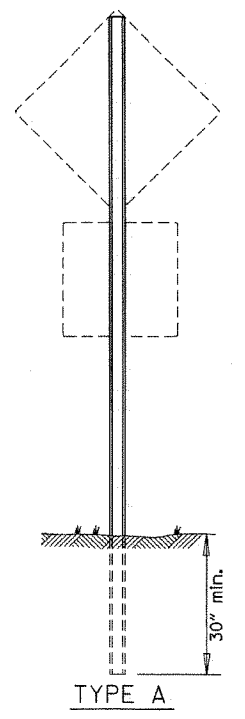
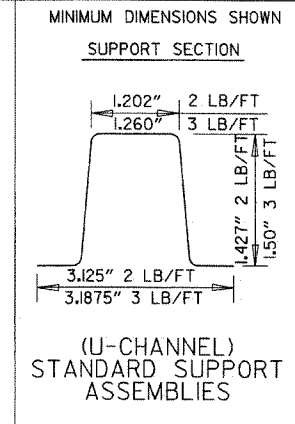
NOTE:

DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER, REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

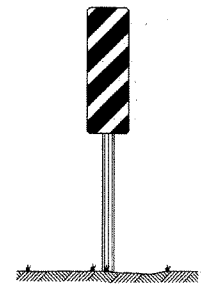
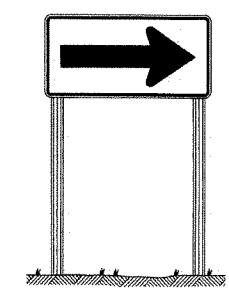
DATE	REVISION	FILMED
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80

ARKANSAS STATE HIGHWAY COMMISSION	
PAVEMENT MARKING DETAILS	
STANDARD DRAWING PM-1	

 RI-1 30"X30"	 WI-3 30"X30" (LT. OR RT.)	 WI-8 18"X24"	 W2-5 30"X30"	 W3-1 36"X36"	 W5-1 36"X36"	 M6-3 21"X15"
 RI-2 36"X36"X36"	 WI-4 30"X30" (LT. OR RT.)	 W2-1 30"X30"	 SI-1 36"X36"	 W3-2 36"X36"	 County Route Marker MI-5 24"X24"	 M6-4 21"X15"
 R2-1 24"X30"	 WI-5 30"X30" (LT. OR RT.)	 W2-2 30"X30"	 W5-2 36"X36"	 W8-3 36"X36"	<p>NOTE: REFLECTORIZED YELLOW LEGEND (COUNTY NAME, ROUTE LETTER &amp; NUMBER) &amp; BORDER ON A BLUE BACKGROUND.</p>  RI-3 12"X6"	 M6-5 21"X15"
 WI-1 30"X30" (LT. OR RT.)	 WI-6 48"X24"	 W2-3 30"X30" (LT. OR RT.)	 W5-3 36"X36"	 WI3-1 18"X18"	 M6-1 21"X15"	 M6-6 21"X15"
 WI-2 30"X30" (LT. OR RT.)	 WI-7 48"X24"	 W2-4 30"X30"	 W10-1 36" DIAMETER	 W3-3 36"X36"	 M6-2 21"X15"	 S4-3 24"X8"   S4-2 24"X10"
						 OM-3 12"X36" (LT. OR RT.)



NOTE: LENGTH OF SIGN POSTS SHALL BE DETERMINED SO AS TO PROVIDE FOR MINIMUM VERTICAL CLEARANCES AS CALLED FOR IN THE SPECIFICATIONS PLUS A MINIMUM VERTICAL PENETRATION OF 30" IN THE SOIL.

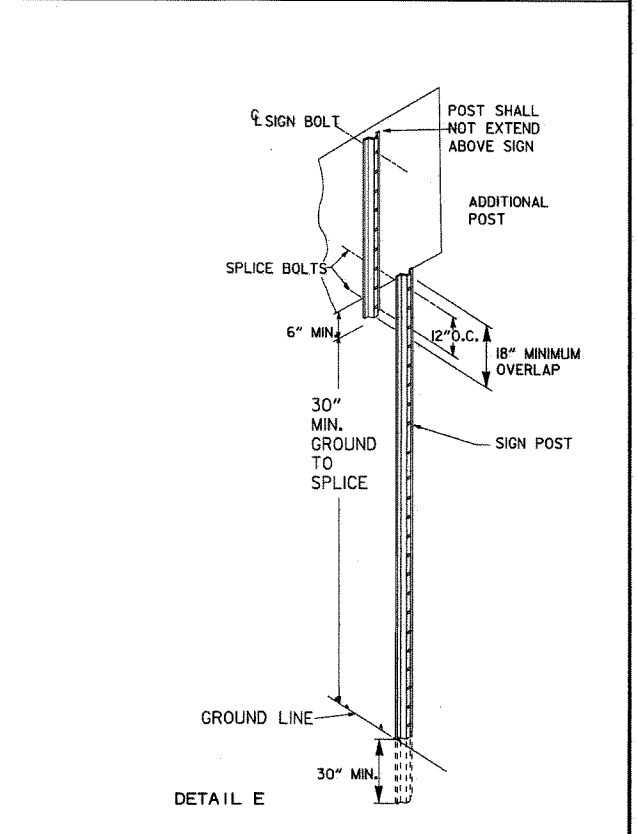
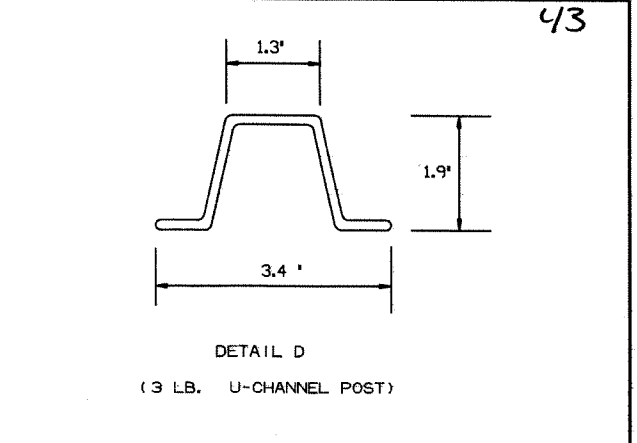
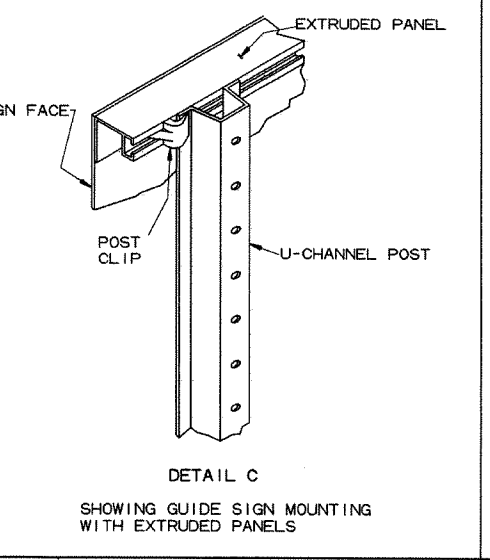
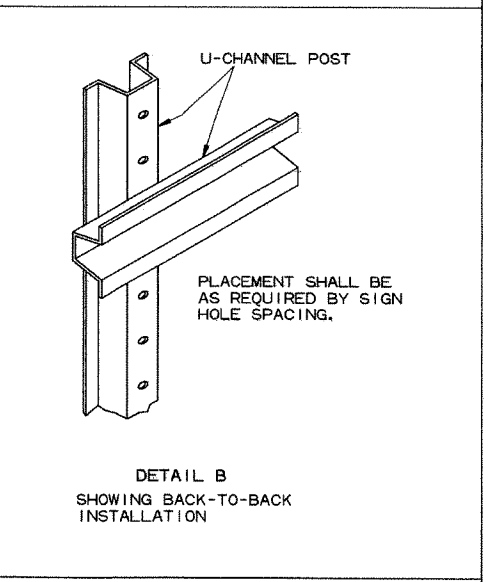
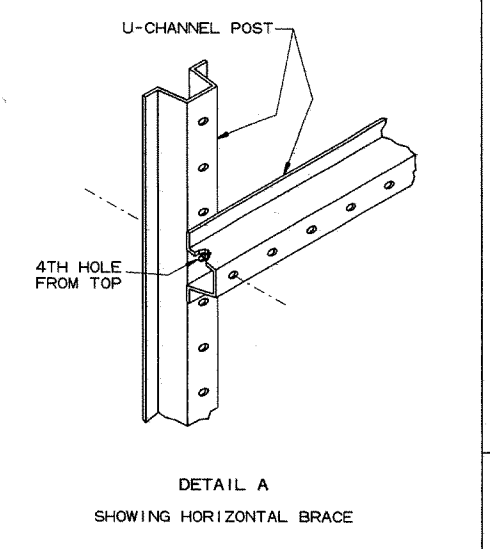
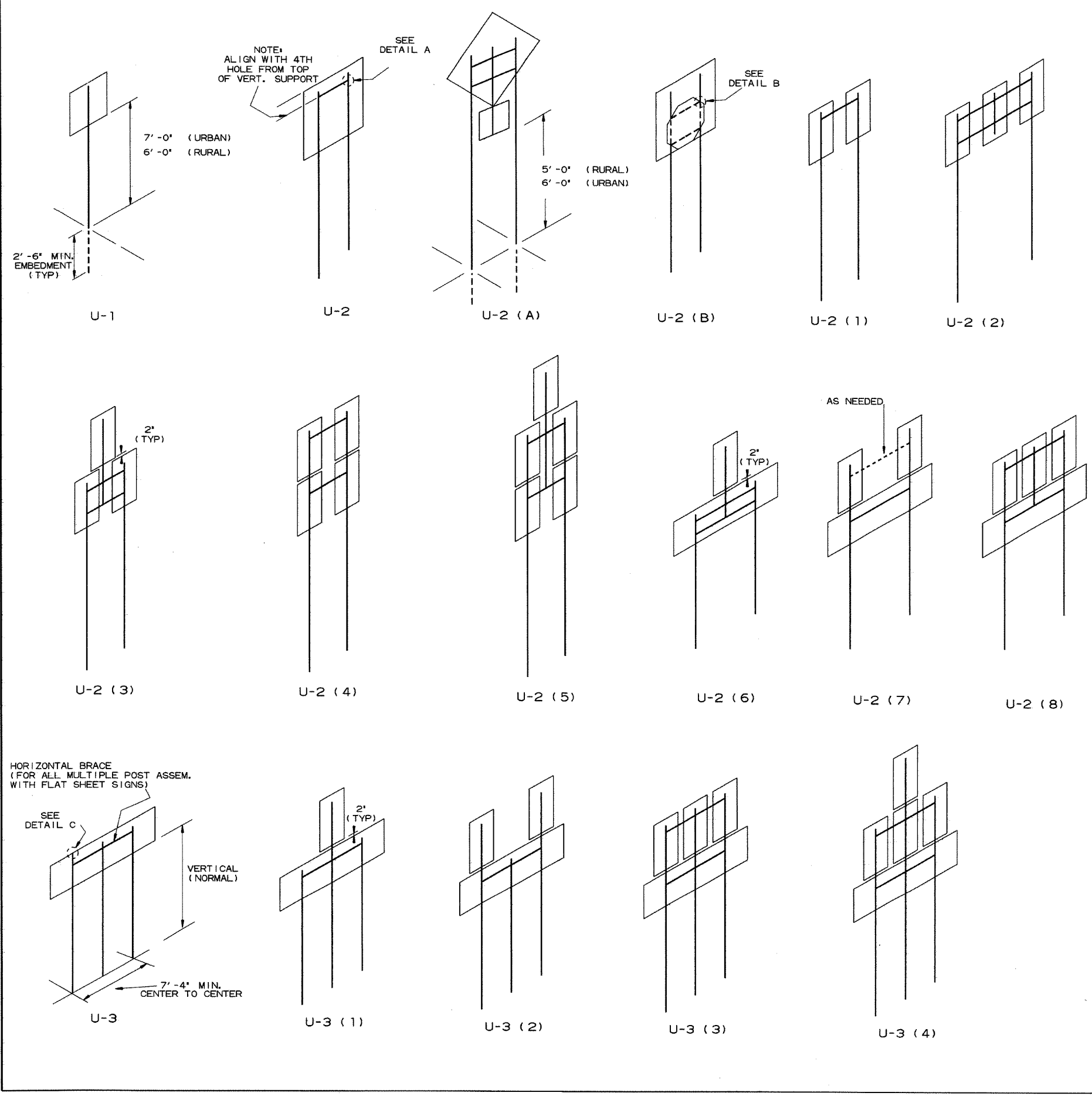


MINIMUM WEIGHT  
TYPE A & B = 3 LBS./FT.  
TYPE C = 2 LBS./FT.

STANDARD HIGHWAY SIGNS


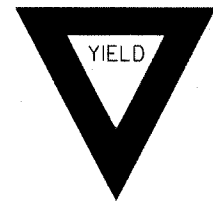
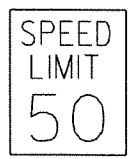
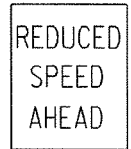



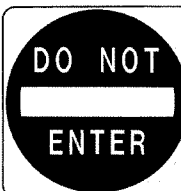

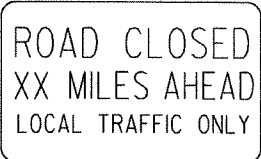
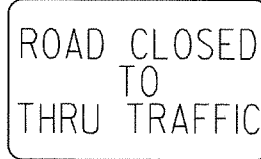
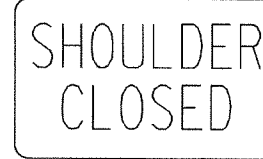
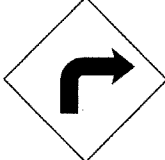



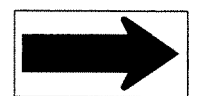

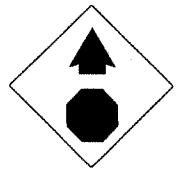
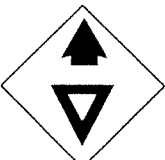
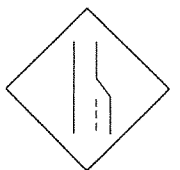



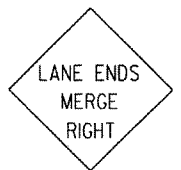


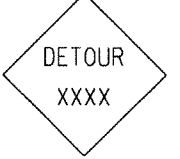


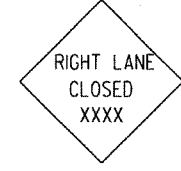
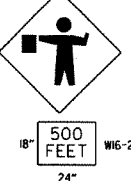



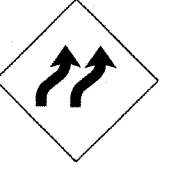


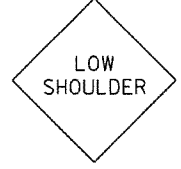
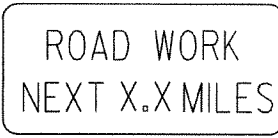
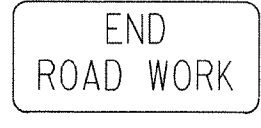
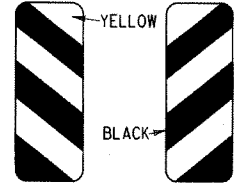
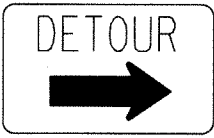

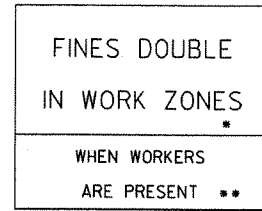
DATE	REVISION	DATE FILMED
4-17-08	REVISED SIGN DESIGNATION - W3-1 & W3-2	
4-10-03	REVISED W5-2, W8-3, OM-3; ADDED WI-8	
1-5-81	REDRAWN	960-1-15-81
9-15-78	ADDED W14-3	877-9-15-78
9-2-76	POST WT.	623-9-3-76
5-3-76	STEEL POST WT. FROM 2" - 3"	504-5-3-76
8-12-74	ADDED S4-2 & S4-3	500-8-21-74
12-21-72	REV. HT. TYPE "C" ASSEMBLY	500-12-21-72
12-1-72	ADDED M6-2, 3, 4, 5, 6	562-12-1-72
	ISSUED	

SUPPORT ASSEMBLIES  
ARKANSAS STATE HIGHWAY COMMISSION  
STANDARD HIGHWAY SIGNS  
AND SUPPORT ASSEMBLIES  
STANDARD DRAWING SHS-1



NOTES:  
 SIGNS AT LEAST 8' IN LENGTH MAY BE INSTALLED ON THREE 3 LB. POST. IN NO CASE SHALL THERE BE MORE THAN TWO 3 LB. POSTS WITHIN A 7' PATH.  
 SPLICES NECESSARY TO ATTAIN PROPER MOUNTING HEIGHT SHALL BE AS SHOWN IN DETAIL (E).  
 NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. CARRIAGE BOLTS TO MOUNT SIGNS TO POST AND 3/16" DIA. CARRIAGE BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS.  
 ALL SIGN POSTS SHALL BE PLUMB.

ARKANSAS STATE HIGHWAY COMMISSION		
U-CHANNEL POST ASSEMBLIES		
STANDARD DRAWING SHS-2		
10-9-03	REMOVED ROUND POST & REVISED SPACING	10-9-03
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL	6-8-95
2-2-95	REDRAWN	2-2-95
DATE	REVISION	FILMED

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>WHEN WORKERS ARE PRESENT **</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>

ADVANCE DISTANCES (XXXX)

500 FT 1/2 MILE  
1000 FT 3/4 MILE  
1500 FT 1 MILE AHEAD

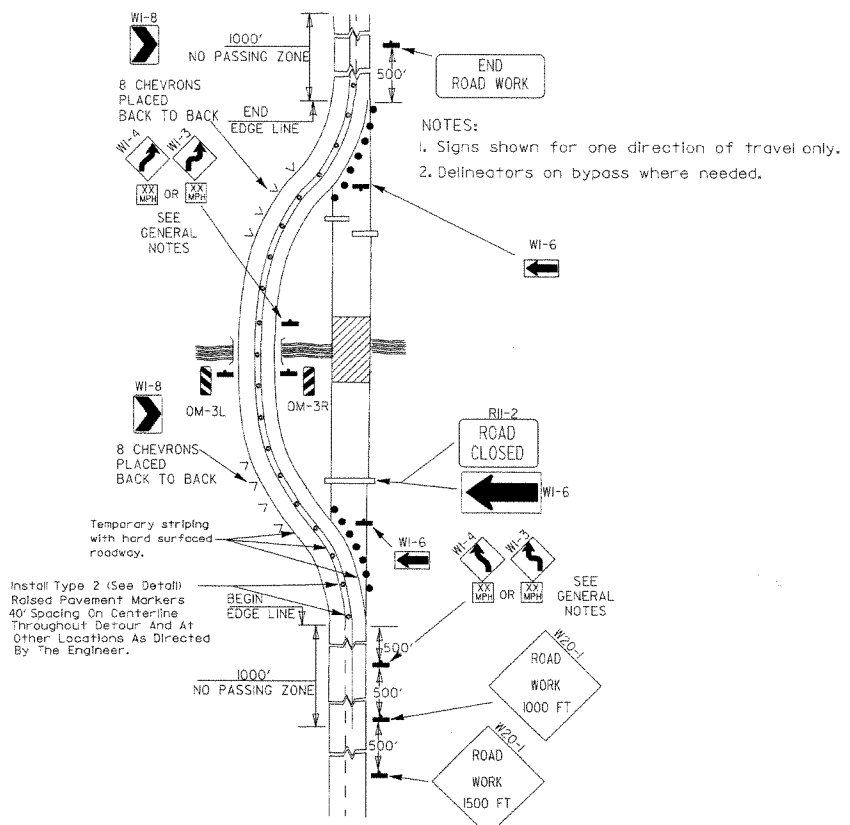
GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

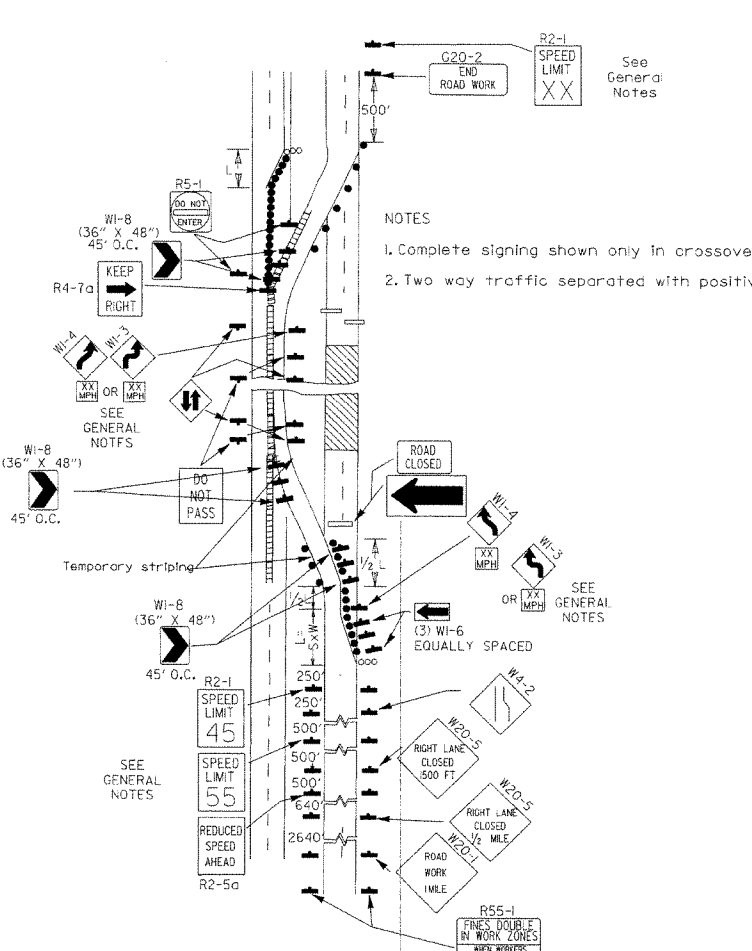
NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

12-15-8	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

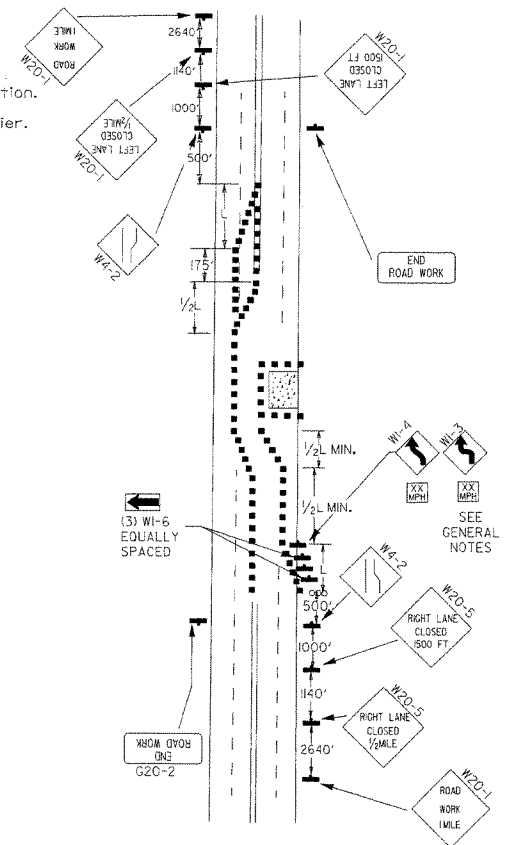
ARKANSAS STATE HIGHWAY COMMISSION  
STANDARD TRAFFIC CONTROLS  
FOR HIGHWAY CONSTRUCTION  
STANDARD DRAWING TC-1



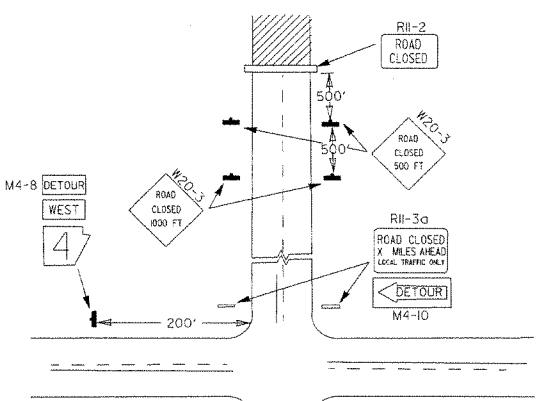
(A) Typical application of traffic control devices on a 2-lane highway where the entire roadway is closed and a bypass detour is provided.



(B) Typical application - 4-lane divided roadway where one roadway is closed.



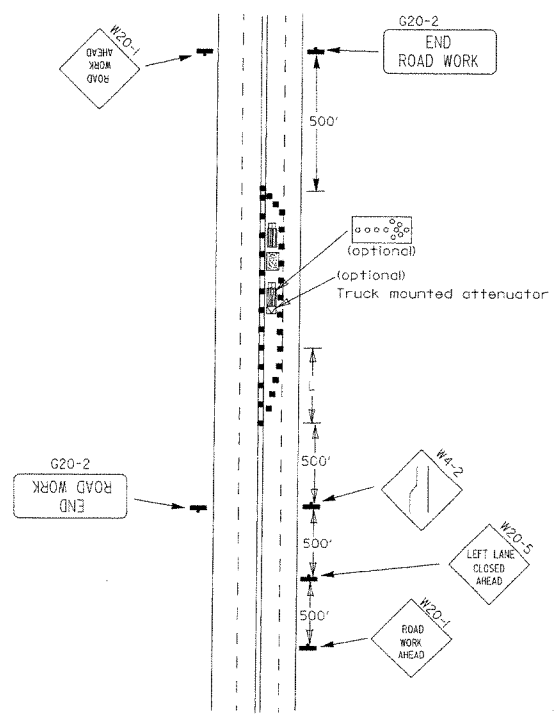
(C) Typical application - 4-lane undivided roadway where half of the roadway is closed.



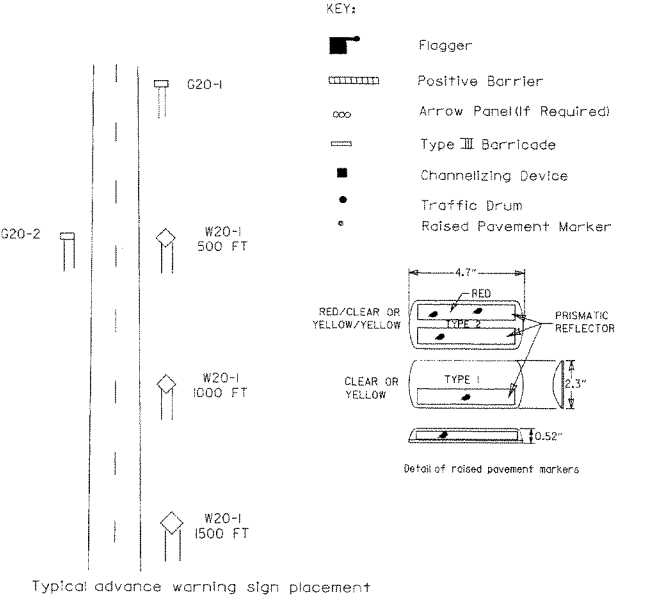
(D) Typical application - roadway closed beyond detour point.

- NOTES:
1. Flood lights should be provided to mark flagger stations at night as needed.
  2. If entire work area is visible from one station, a single flagger may be used.
  3. Channelizing devices are to be extended to a point where they are visible to approaching traffic.
  4. Automated Flagger Assistance Device (AFAD) optional. Refer to MUTCD.

(E) Typical application of traffic control devices on 2-lane highway where one lane is closed and flagging is provided.



(F) Typical application - 4-lane undivided roadway with inside lane closed.



Taper formulae:

$L = S \times W$  for speeds of 45mph or more.

$L = \frac{W \times S^2}{60}$  for speeds of 40mph or less.

Where:

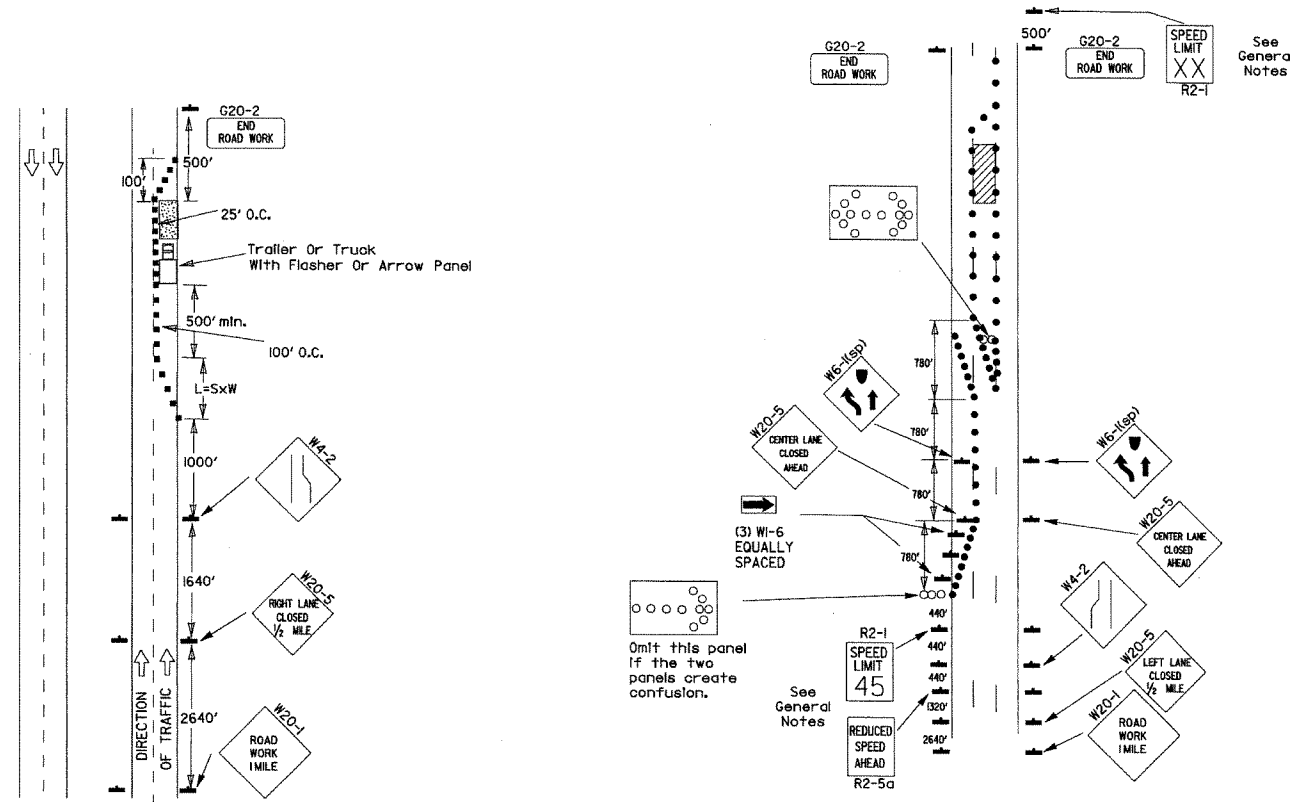
- L = Minimum length of taper.
- S = Numerical value of posted speed limit prior to work or 85th percentile speed.
- W = Width of offset.

- GENERAL NOTES:
1. Advisory speed posted on W1-3 or W1-4 curve warning signs to be determined at site. Use W1-4 when speed is greater than 30mph and W1-3 when 30mph or less.
  2. When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-(K55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-(Kxx) shall be installed to match original speed limit.
  3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-(K45) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-(Kxx) shall be installed to match original speed limit.
  4. The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit, or as directed by the Engineer.
  5. Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
  6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
  7. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

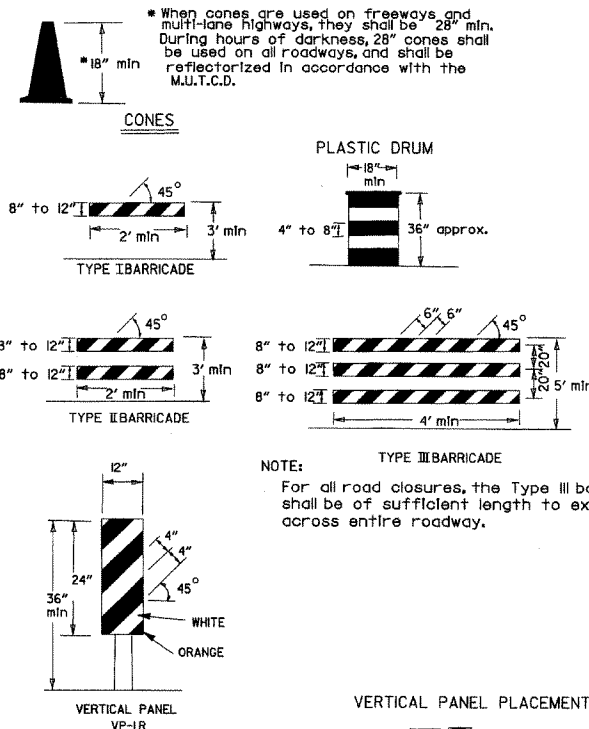
DATE	REVISION	FILMED
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION  
STANDARD TRAFFIC CONTROLS  
FOR HIGHWAY CONSTRUCTION  
STANDARD DRAWING TC-2

Channelizing devices



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

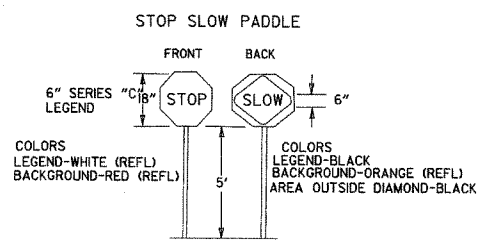
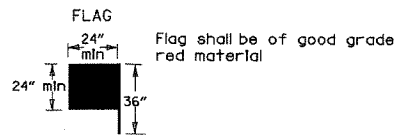


(B) Typical application - 3-lane oneway roadway where center lane is closed.

TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

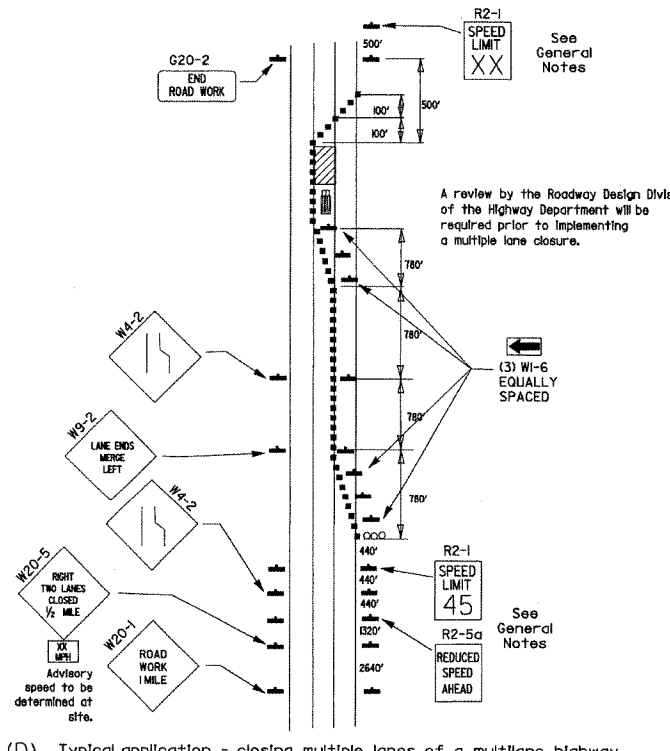
VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-1 and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

\* When shown on the plans concrete barrier will be used.  
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



- KEY:
- Arrow Panel (if Required)
  - Channelizing Device
  - Traffic drum

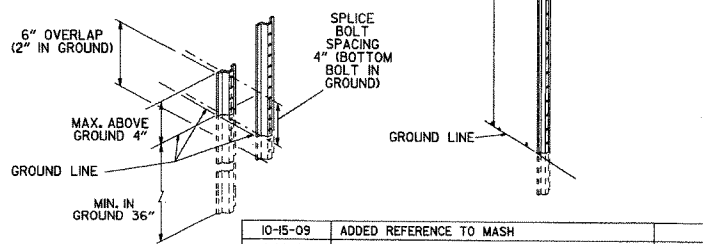
- GENERAL NOTES:
- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
  - When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
  - When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(45) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
  - The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
  - Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
  - Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
  - The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1(1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
  - Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
  - All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
  - Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.



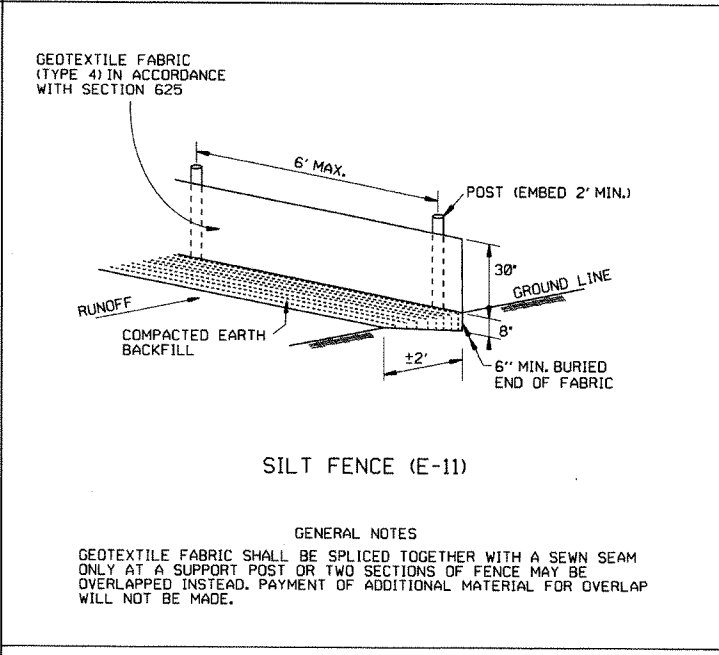
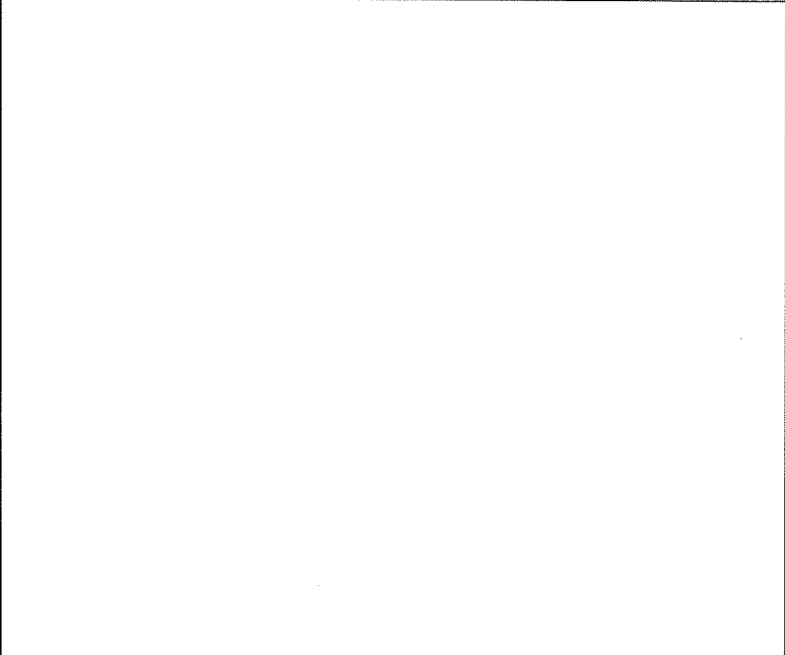
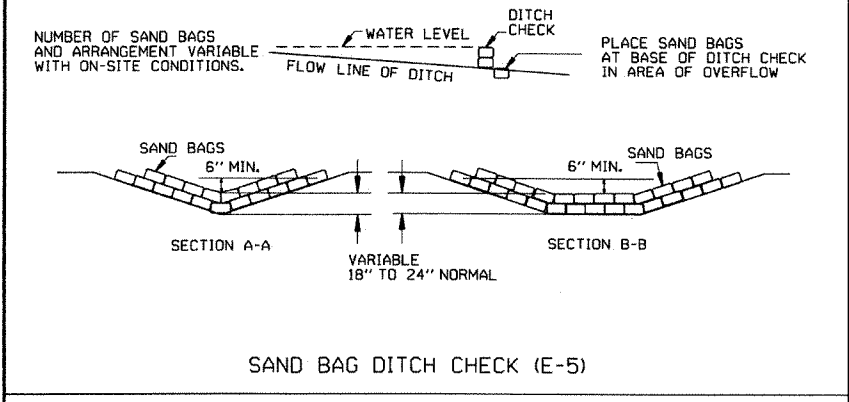
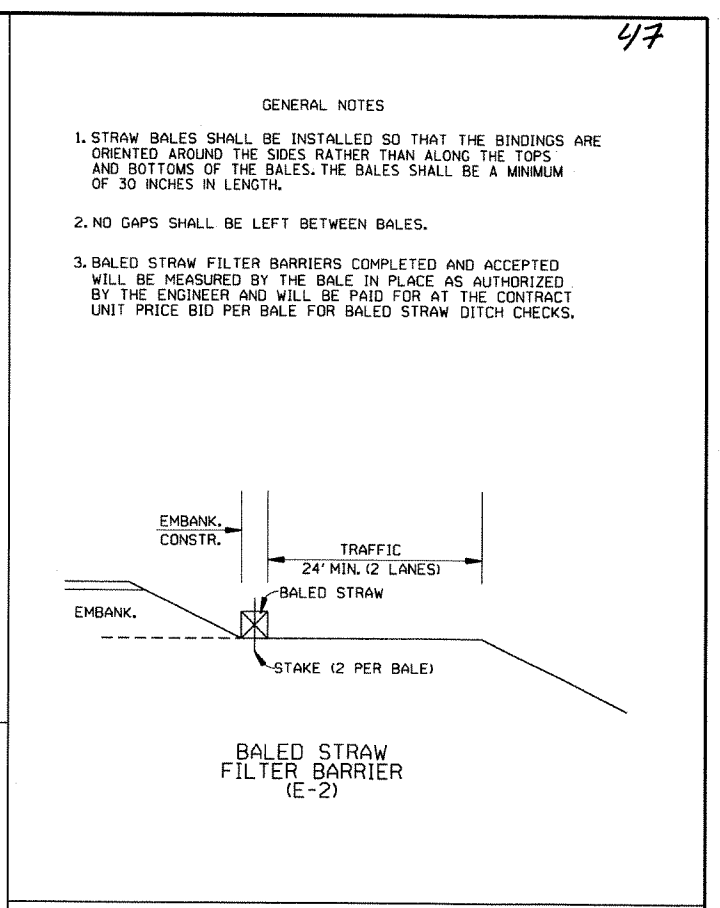
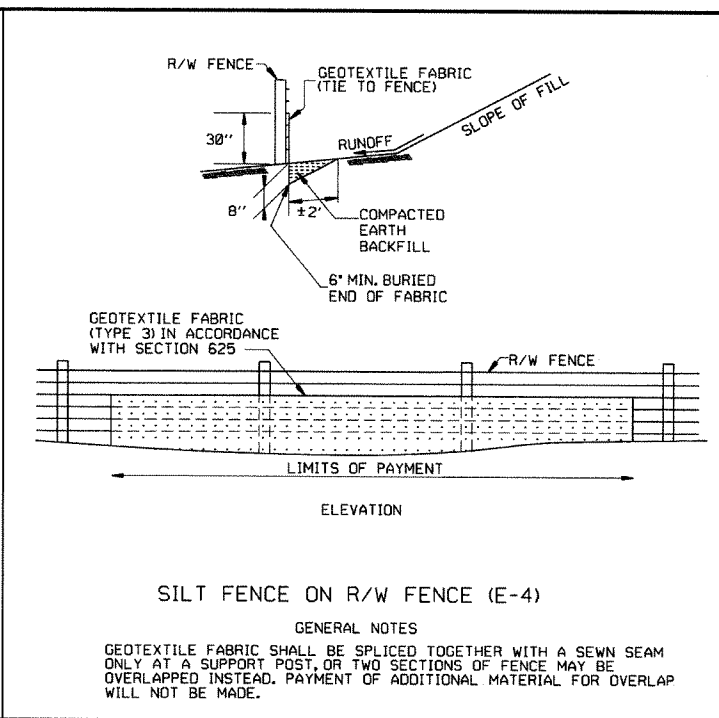
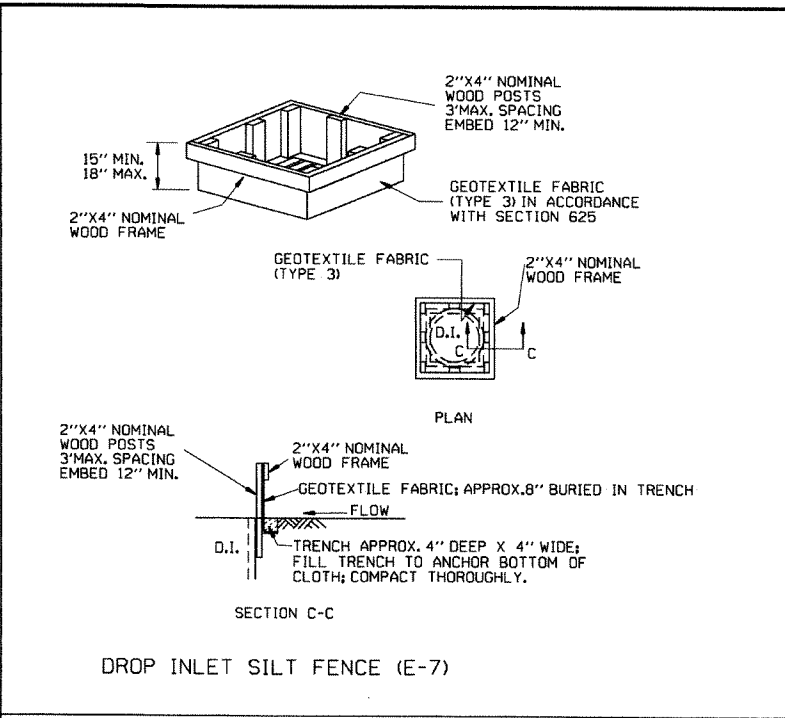
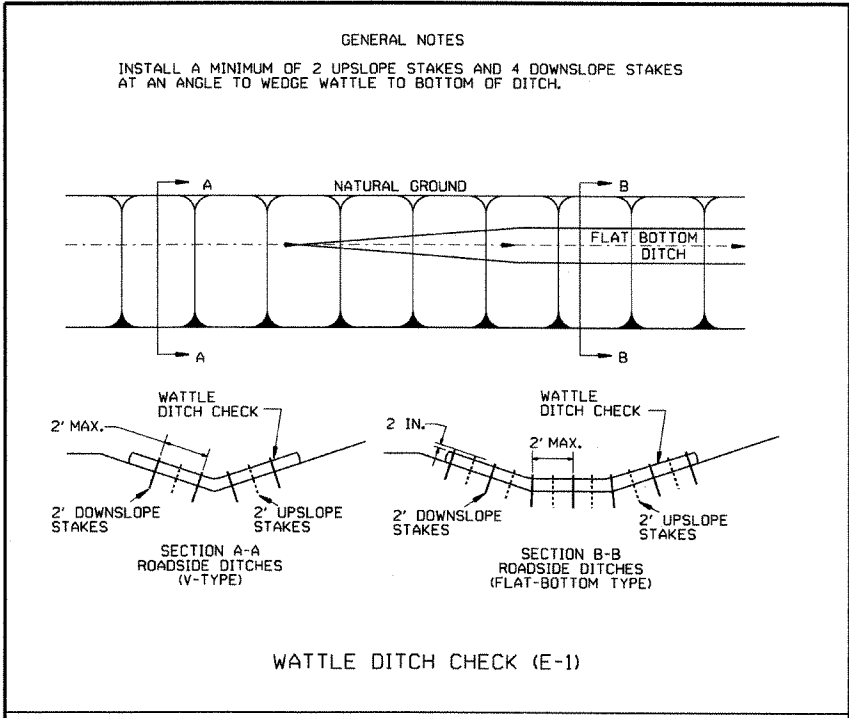
(C) Typical application - construction operations of Intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

NOTES:

- USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2)
- NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS, EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.
- SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



DATE	REVISION	FILMED
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	



12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK			ARKANSAS STATE HIGHWAY COMMISSION
11-18-98	ADDED NOTES			
7-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)			
7-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95		
7-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC			
6-2-94	REVISED E-1, 4, 7 & 11; DELETED E-2 & 3	6-2-94		
4-1-93	REDRAWN			
10-1-92	REDRAWN			
8-2-76	ISSUED R.D.M.	298-7-28-76		
DATE	REVISION	FILMED		

TEMPORARY EROSION CONTROL DEVICES

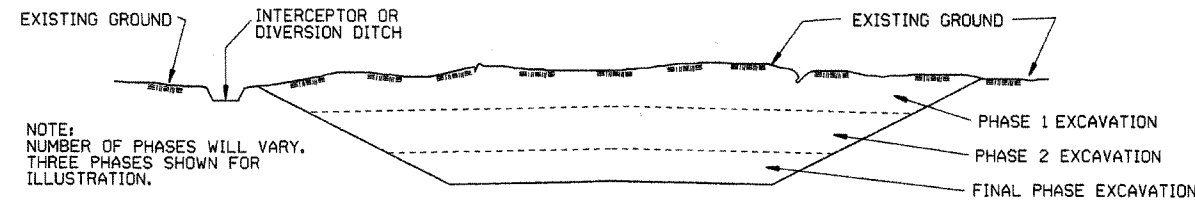
STANDARD DRAWING TEC-1

# CLEARING AND GRUBBING

## CONSTRUCTION SEQUENCE

1. PLACE PERIMETER CONTROLS (I.E. SILT FENCES, DIVERSION DITCHES, SEDIMENT BASINS, ETC.)
2. PERFORM CLEARING AND GRUBBING OPERATION.

# EXCAVATION



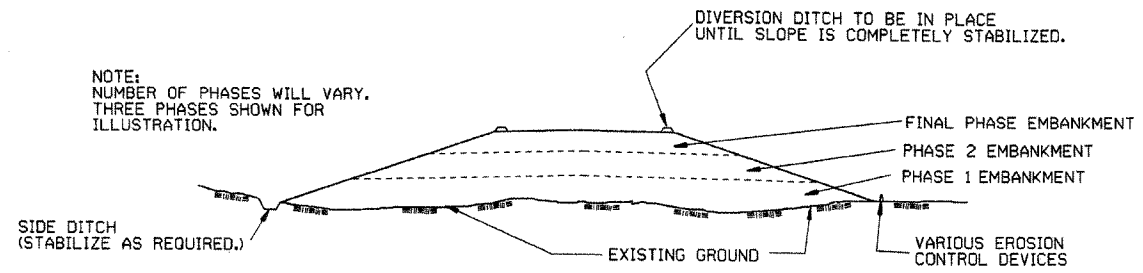
## GENERAL NOTE

ALL CUT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE EXCAVATED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

## CONSTRUCTION SEQUENCE

1. EXCAVATE AND STABILIZE INTERCEPTOR AND/OR DIVERSION DITCHES.
2. PERFORM PHASE 1 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
3. PERFORM PHASE 2 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
4. PERFORM FINAL PHASE OF EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING. STABILIZE DITCHES. CONSTRUCT DITCH CHECKS, DIVERSION DITCHES, SEDIMENT BASINS, OR OTHER EROSION CONTROL DEVICES AS REQUIRED.

# EMBANKMENT



## GENERAL NOTE

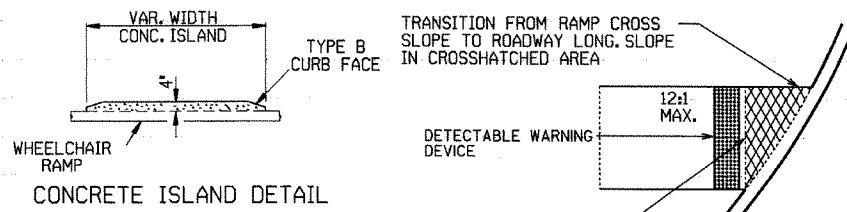
ALL EMBANKMENT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE CONSTRUCTED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

## CONSTRUCTION SEQUENCE

1. CONSTRUCT DIVERSION DITCHES, DITCH CHECKS, SEDIMENT BASINS, SILT FENCES, OR OTHER EROSION CONTROL DEVICES AS SPECIFIED.
2. PLACE PHASE 1 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
3. PLACE PHASE 2 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
4. PLACE FINAL PHASE OF EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PLACE DIVERSION DITCHES AND SLOPE DRAINS AND MAINTAIN UNTIL ENTIRE SLOPE IS STABILIZED.

			ARKANSAS STATE HIGHWAY COMMISSION
			TEMPORARY EROSION CONTROL DEVICES
11-03-94	CORRECTED SPELLING		STANDARD DRAWING TEC-3
6-2-94	Drawn & Issued		
DATE	REVISION	6-2-94 FILMED	



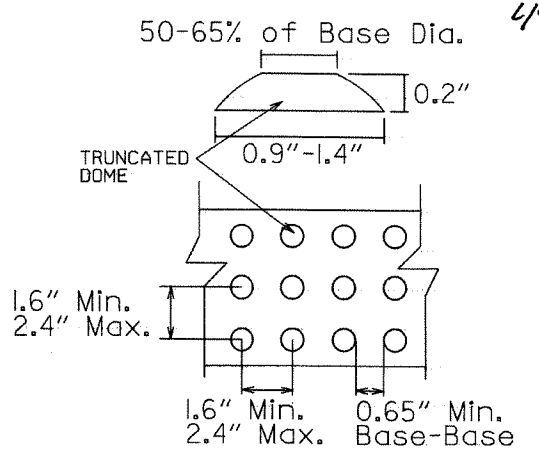


VAR. WIDTH CONC. ISLAND  
TYPE B CURB FACE  
WHEELCHAIR RAMP  
CONCRETE ISLAND DETAIL  
TRANSITION FROM RAMP CROSS SLOPE TO ROADWAY LONG SLOPE IN CROSSHATCHED AREA  
DETECTABLE WARNING DEVICE  
BEGIN RAMP SLOPE ON A LINE PERPENDICULAR TO TRAVEL DIRECTION

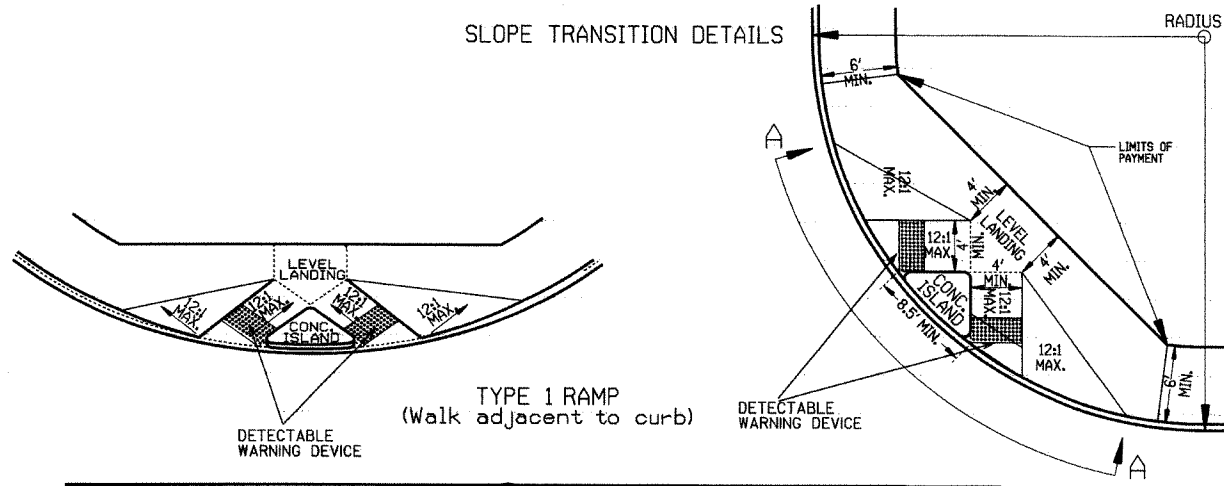
TYPE 1 RAMP DIMENSIONS AND QUANTITIES

RADIUS 'R'	DISTANCE 'X'	DISTANCE 'Y'	LENGTH 'L'	RAMP AREA 'A'
FEET	FEET	FEET	FEET	SQ. YD.
15	11.67	18.82	32.18	26.21
20	11.52	22.28	35.46	30.07
25	11.43	26.60	38.77	33.80
30	11.37	30.26	40.93	36.90
35	11.33	33.51	43.11	39.77
40	11.30	36.45	45.26	42.45
45	11.27	39.16	47.34	44.97
50	11.25	41.69	49.36	47.35
55	11.24	44.07	51.31	49.63
60	11.22	46.33	53.21	51.80

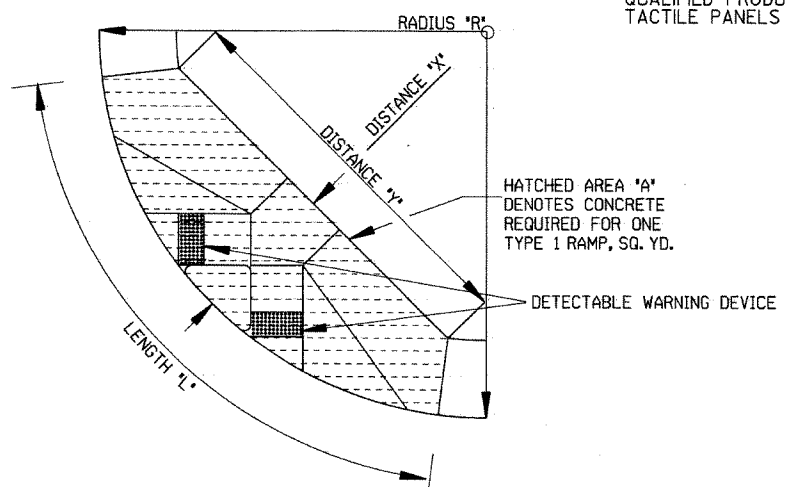
GENERAL NOTES FOR DETECTABLE WARNING DEVICES  
THE DETECTABLE WARNING DEVICE SHALL BE LOCATED SO THAT THE NEAREST EDGE OF THE DEVICE IS 6 TO 8 INCHES FROM THE FACE OF THE CURB. TRUNCATED DOMES IN THE DETECTABLE WARNING SURFACE SHALL MEET THE REQUIREMENTS OF THE GEOMETRIC CONFIGURATION SHOWN. DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES. DETECTABLE WARNING DEVICE SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE. DETECTABLE WARNING DEVICE SHALL BE ON THE AHT QUALIFIED PRODUCTS LIST FOR CAST-IN-PLACE TACTILE PANELS (ADA DETECTABLE WARNING).



DETECTABLE WARNING DEVICE DETAIL



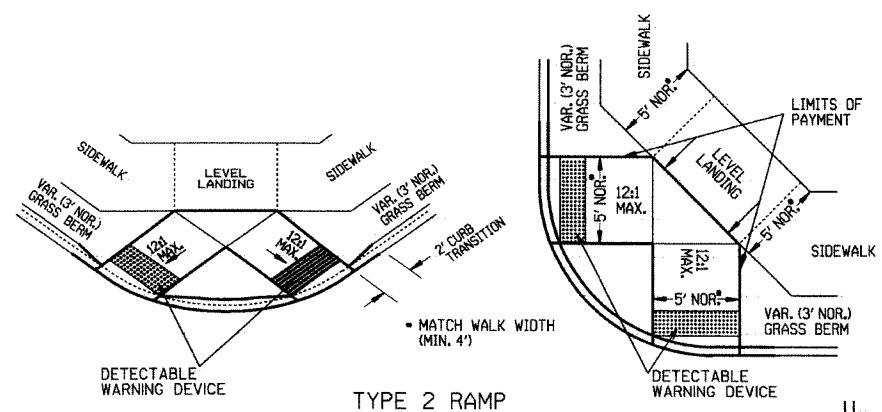
TYPE 1 RAMP (Walk adjacent to curb)



HATCHED AREA 'A' DENOTES CONCRETE REQUIRED FOR ONE TYPE 1 RAMP, SQ. YD.  
DETECTABLE WARNING DEVICE

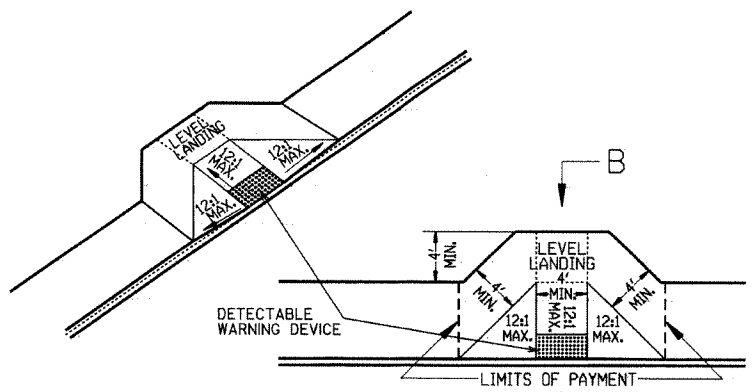


SECTION A-A

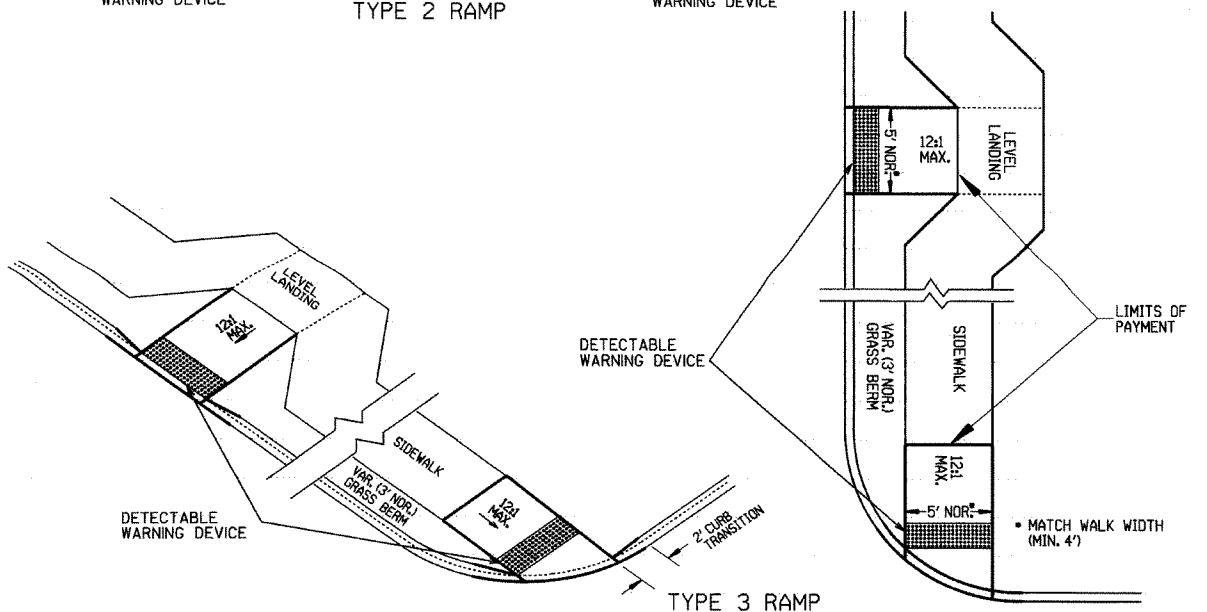


TYPE 2 RAMP

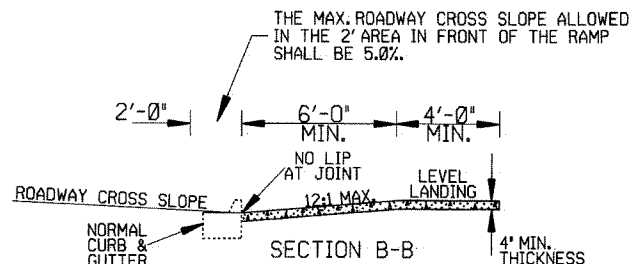
NOTE: THE CROSS SLOPE OF THE RAMPS, LEVEL LANDINGS, AND SIDEWALKS SHALL NOT EXCEED 2.0% UNLESS REQUIRED TO MATCH STREET LONGITUDINAL GRADE.



TYPE 4 RAMP (Walk adjacent to curb)



TYPE 3 RAMP



SECTION B-B

RAMP SELECTION CRITERIA

CHOICE	TYPE	DESCRIPTION
FIRST CHOICE	TYPE 1	CORNER LOCATIONS WITH THE WALK ADJACENT TO THE CURB (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 2	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE INSUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 3	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE SUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
SECOND CHOICE	TYPE 4	TANGENT LOCATIONS (BOTH NEW CONSTRUCTION AND ALTERATIONS).
THIRD CHOICE	TYPE 5	TANGENT LOCATIONS (ALTERATIONS ONLY).
FOURTH CHOICE	TYPE 6	CORNER LOCATIONS (ALTERATIONS ONLY). THIS RAMP MAY BE USED ONLY IF THE TYPE 5 RAMPS CANNOT BE PLACED AT THE ENDS OF THE RADIUS.
		IF SITE CONSTRAINTS PREVENT THE CONSTRUCTION OF ANY OF THE TYPES LISTED, THEN AND ONLY THEN CAN THE 12:1 MAX. SLOPE ON THE RAMP BE EXCEEDED TO PROVIDE ACCESS TO THE STREET LEVEL (ALTERATIONS ONLY). THE SLOPE CAN BE STEEPENED TO A 10:1 MAX. FOR A MAX. LENGTH OF 5' OR A 8:1 MAX. FOR A MAX. LENGTH OF 2'. SLOPES STEEPER THAN 8:1 ARE NOT ALLOWED UNDER ANY CIRCUMSTANCES.

NOTE: IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED SHALL BE BASED ON THE AMOUNT OF RIGHT-OF-WAY AVAILABLE, AND ON THE PRESENCE OF OTHER SITE CONSTRAINTS (UTILITIES, BUILDINGS, ETC.). THE TABLE ABOVE LISTS THE ORDER IN WHICH THE RAMPS ARE TO BE CONSIDERED. AN ALTERATION IS DEFINED AS A PROJECT THAT CHANGES OR AFFECTS THE USE OF A PEDESTRIAN PATHWAY (OVERLAYS, SIGNALIZATION PROJECTS, ETC.) BUT DOES NOT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY. ALL PROJECTS THAT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY WILL USUALLY BE CONSIDERED NEW CONSTRUCTION FOR THE PURPOSES OF THE CHART ABOVE.

DATE	ISSUED-P.H.D.	299-7-28-76
6-02-76	ISSUED-P.H.D.	299-7-28-76
7-14-88	INCLUD. CONC. ISLD. IN PAY ITEM	652-7-15-88
7-15-88	ADJUSTED MAX. SLOPE	652-7-15-88
5-24-90	FROM 10:1 MAX. SLOPES	5-24-90
10-18-95	CORRECTED DIMENSIONS	10-18-95
7-02-98	REBORN & REISSUED	
8-12-98	REVISED TEXTURE	
1-18-98	REVISED NOTES	
3-30-00	ADD. SLOPE TRANS. & REV. ISL. DIMS.	
8-22-02	ADD. DETECTABLE WARNING DEVICES	
4-10-03	REV. DETECTABLE WARNING DEVICES	
10-9-03	REVISED GEN. NOTES & ADDED NOTE	
10-05	REVISED TO NEW SIDEWALK POLICY	

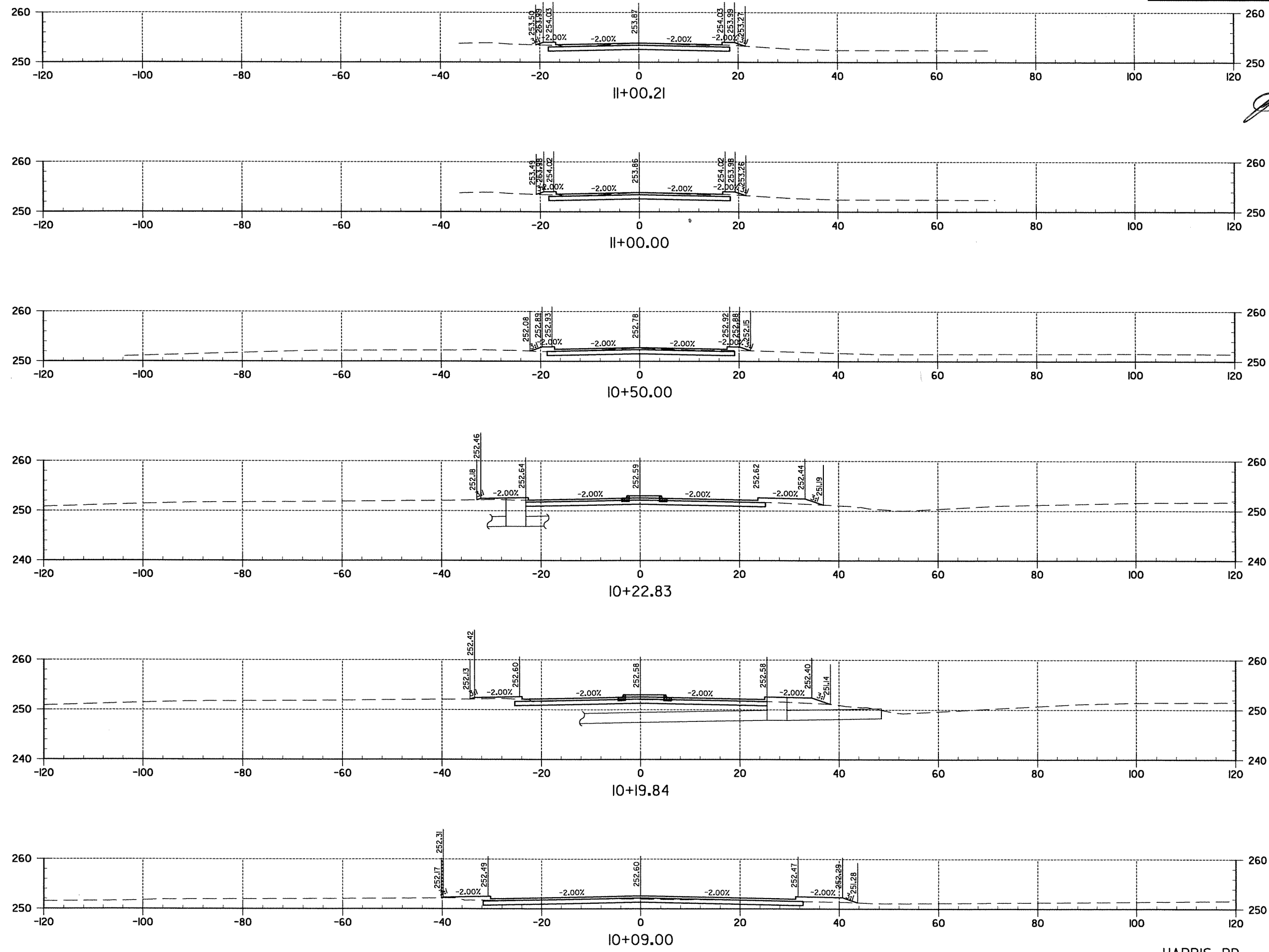
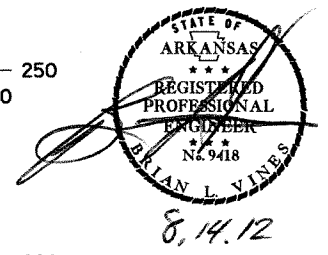
ARKANSAS STATE HIGHWAY COMMISSION

WHEELCHAIR RAMPS NEW CONSTRUCTION AND ALTERATIONS

STANDARD DRAWING WR-1

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		061267	50	56

② HARRIS RD. STA. 10+09.00 TO 11+00.21



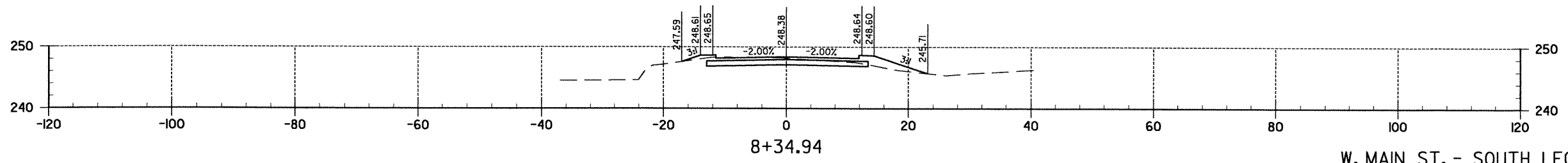
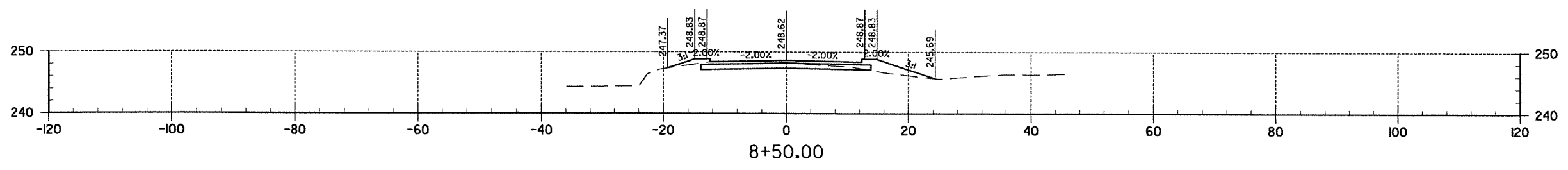
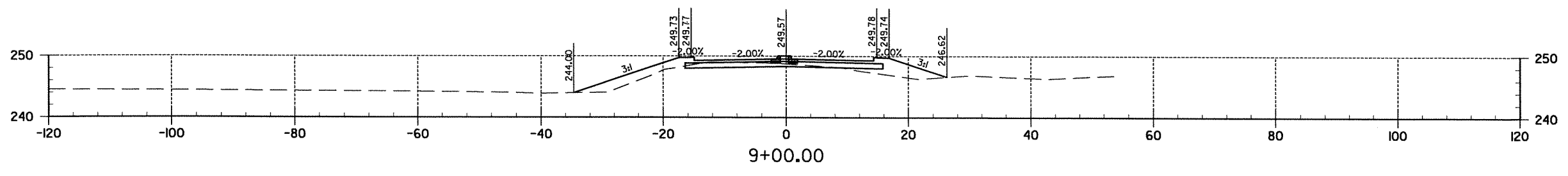
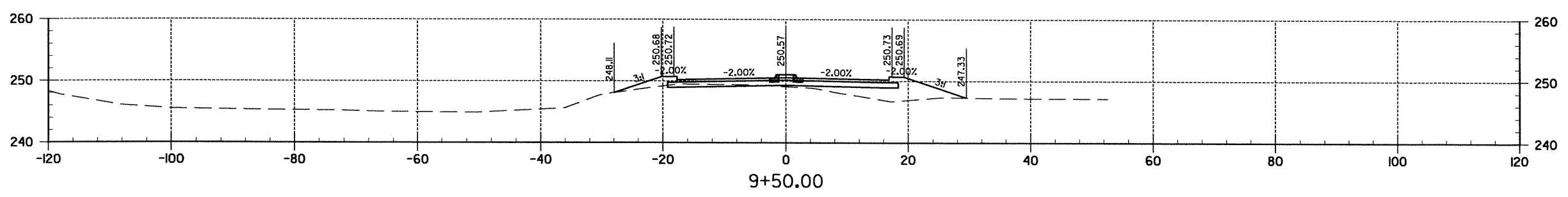
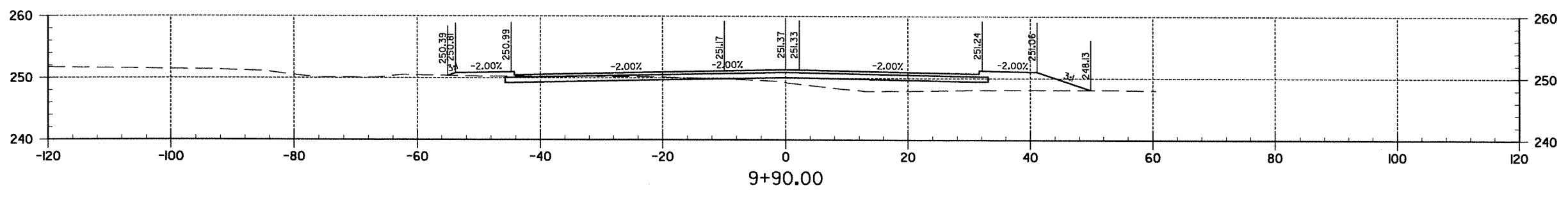
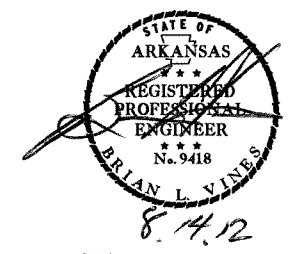
END AREA (S.F.)	VOLUME (C.Y.)	
	CUT	FILL
40	2	0
40	2	0
37	4	71
40	14	39
40	14	9
42	14	5
50	13	18
		5
		0
		0

HARRIS RD.

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 061267							51	56

② W. MAIN ST. STA. 8+34.94 TO 9+90.00



W. MAIN ST. - SOUTH LEG

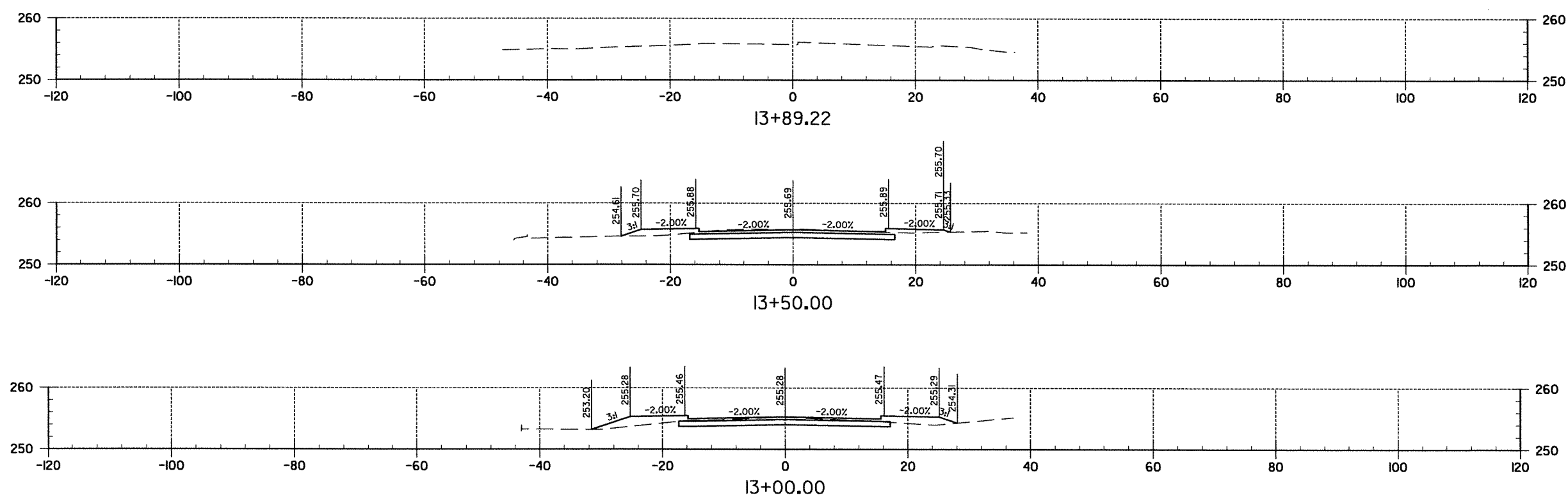
CUT	END AREA (S.F.)		VOLUME (C.Y.)	
	CUT	FILL	CUT	FILL
22		97		
			20	109
5		50		
			18	88
14		45		
			35	56
24		15		
			14	7
27		10		
			0	0

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		061267	53	56

② W. MAIN ST. STA. 13+00.00 TO 13+89.22



END AREA (S.F.)		VOLUME (C.Y.)	
CUT	FILL	CUT	FILL
0	0		
44	14	73	38
35	27	65	44
		8	3

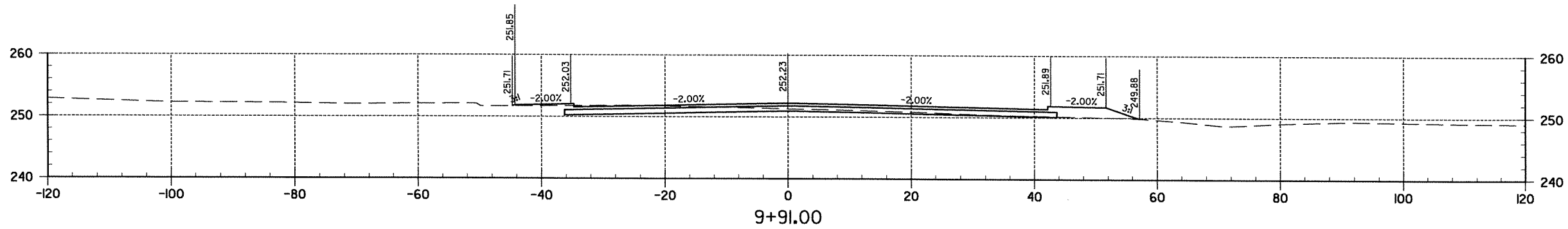
MAIN ST. - EAST

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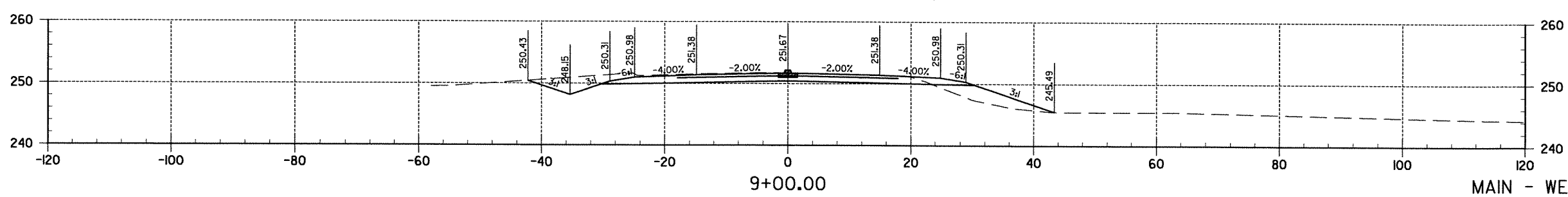
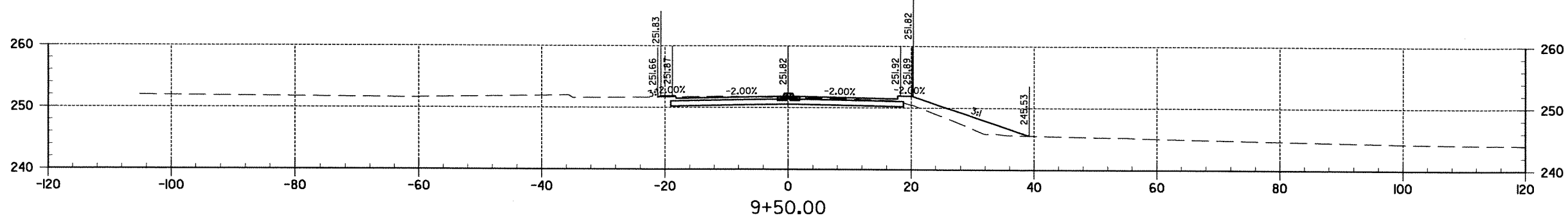
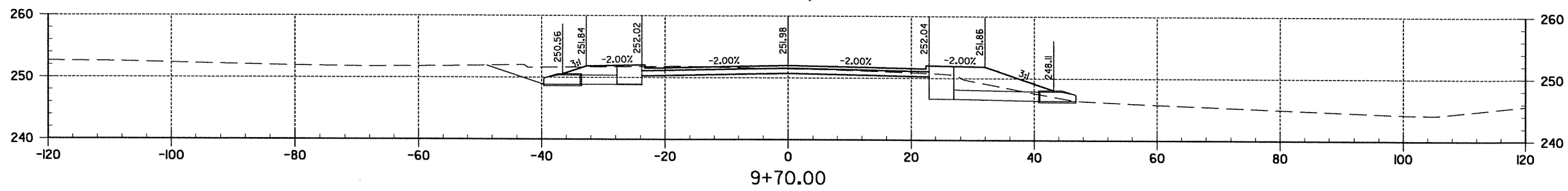
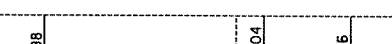


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061267		55	56

2 J.C.O. STA. 9+00.00 TO 9+91.00



9+71, 26°50' LT. FWD. SKEW



MAIN - WEST

END AREA (S.F.)	VOLUME (C.Y.)	
	CUT	FILL
47	20	35
43	43	33
43	43	33
45	30	126
91	28	144

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