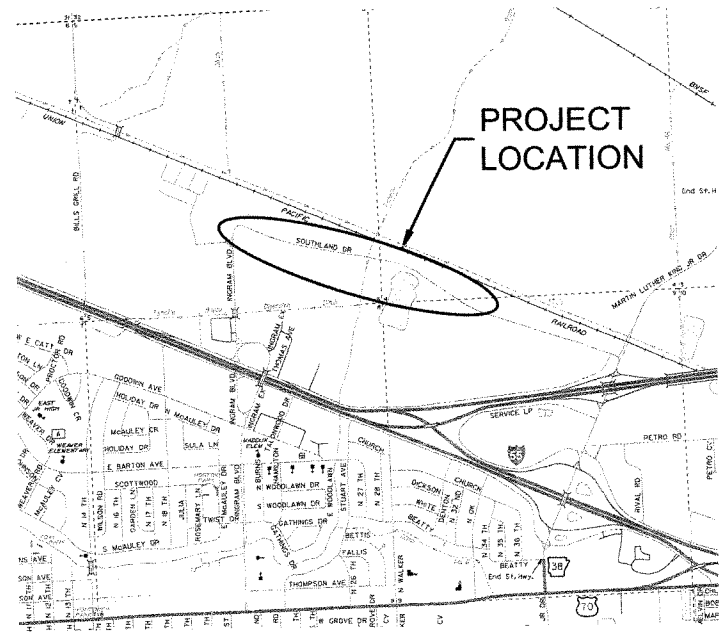


ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT
CONSTRUCTION PLANS FOR CITY STREET

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	110583	1 52
						INGRAM BLVD. - EAST (SOUTHLAND DR.) (WEST MEMPHIS)(S)		



VICINITY MAP

INGRAM BLVD. - EAST (SOUTHLAND DR.) (WEST MEMPHIS)(S)

CRITTENDEN COUNTY
AHTD JOB NO. 110583
FEDERAL AID PROJECT STPU-9448(36)



ARK. HWY. DIST. NO. 1

END SOUTHLAND DR.
STA. 46+55.22

BEGIN SOUTHLAND DR.
STA. 1+00.00

END INGRAM BLVD.
STA. 5+31.00

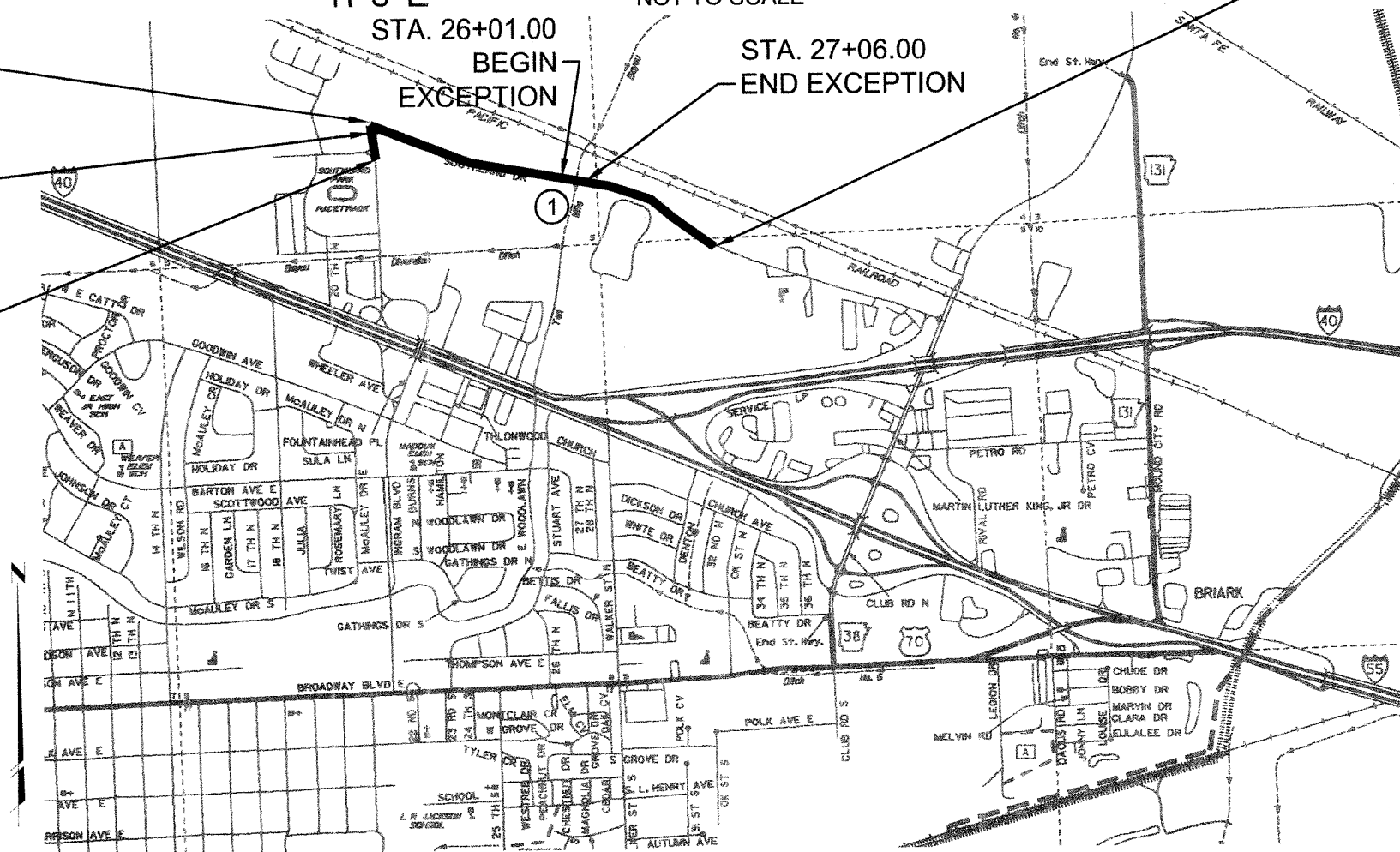
BEGIN INGRAM BLVD.
STA. 1+00.00

R-8-E
STA. 26+01.00
BEGIN EXCEPTION

NOT TO SCALE

STA. 27+06.00
END EXCEPTION

R-9-E



T
6
N

BRIDGE DATA

- ① IN PLACE
- BRIDGE END STA. 26+01.00
- BR. NO. 5866
- 105'-0" SPAN
- R.C. SLAB SPANS
- BRIDGE END STA. 27+06.00
- RETAIN

**DESIGN DATA
SOUTHLAND DRIVE**

DESIGN YEAR	-----	2043
2013 ADT	-----	3545
2043 ADT	-----	6430
2043 DHV	-----	707
DIRECTIONAL DISTRIBUTION	---	60%
TRUCKS	-----	30%
DESIGN SPEED	-----	40 MPH

**DESIGN DATA
INGRAM BLVD.**

DESIGN YEAR	-----	2043
2013 ADT	-----	5200
2043 ADT	-----	9400
2043 DHV	-----	1034
DIRECTIONAL DISTRIBUTION	---	55%
TRUCKS	-----	11%
DESIGN SPEED	-----	40 MPH

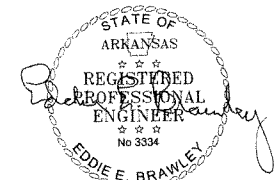
PROJECT LOCATION

BEGIN: LAT 35°09'56" N LONG 90°29'11" W
MID: LAT 35°09'47" N LONG 90°09'04" W
END: LAT 35°09'38" N LONG 90°08'39" W

LENGTH OF PROJECT CALCULATED ALONG CENTER LINE OF SOUTHLAND DRIVE

GROSS LENGTH OF PROJECT	4986.22 FEET OR	0.944 MILES
NET LENGTH OF ROADWAY	4881.22 FEET OR	0.924 MILES
NET LENGTH OF BRIDGES	105.00 FEET OR	0.020 MILES (EXCEPTION)
NET LENGTH OF PROJECTS	4881.22 FEET OR	0.924 MILES

P.E. JOB 110583
NON. PART.



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		2	52
				JOB NO.	110583			

INDEX OF SHEETS, GOV. SPECS., & GEN. NOTES

INDEX OF SHEETS

TITLE	DRWG.NO.	DATE
1 TITLE SHEET		
2 INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES		
3-4 TYPICAL SECTIONS OF IMPROVEMENT		
5 SPECIAL DETAILS		
6 SPECIAL DETAILS - EXISTING SECTION & SOIL INFORMATION		
7-8 TEMPORARY EROSION CONTROL DETAILS		
9-10 MAINTENANCE OF TRAFFIC DETAILS		
11-12 PERMANENT PAVEMENT MARKINGS		
13-14 QUANTITIES		
15 SUMMARY OF QUANTITIES AND REVISIONS		
16-17 SURVEY CONTROL DETAILS		
18 PLAN AND PROFILE - INGRAM BLVD.		
19-21 PLAN AND PROFILE - SOUTHLAND DR.		
22 DETAILS OF STANDARD TYPE 'PT' APPROACH GUTTERS (BRIDGES WITH CONCRETE PARAPET RAILING)	2091	7-14-10
23 TRANSVERSE & LONGITUDINAL JOINTS FOR CONCRETE PAVEMENT (NON-REINFORCED)	CPTJ-6A	5-25-06
24 DETAILS OF DROP INLET & JUNCTION BOX (TYPE ST)	FPC-9S	7-26-12
25 GUARD RAIL DETAILS	GR-8	7-14-10
26 GUARD RAIL DETAILS	GR-9	4-17-08
27 GUARD RAIL DETAILS	GR-9A	4-17-08
28 GUARD RAIL DETAILS	GR-10	7-14-10
29 GUARD RAIL DETAILS	GR-10A	7-14-10
30 GUARD RAIL DETAILS	GRT-1	7-14-10
31 PAVEMENT MARKING DETAILS	PM-1	9-12-13
32 TABLES AND METHOD OF SUPER ELEVATION FOR TWO-WAY TRAFFIC	SE-2	10-18-96
33 STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-1	12-15-11
34 STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-2	9-12-13
35 STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-3	10-15-09
36 TEMPORARY EROSION CONTROL DEVICES	TEC-1	12-15-11
37 TEMPORARY EROSION CONTROL DEVICES	TEC-2	6-2-94
38 TEMPORARY EROSION CONTROL DEVICES	TEC-3	11-3-94
39-52 CROSS SECTIONS		

NOTE: CROSS SECTIONS ARE NOT NORMALLY INCLUDED IN THE PLANS SOLD TO PROSPECTIVE BIDDERS BUT MAY BE HAD UPON REQUEST.

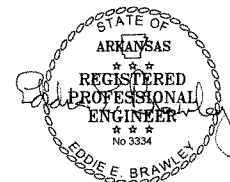
GENERAL NOTES

- GRADE LINE DENOTES FINISHED GRADE WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210 - UNCLASSIFIED EXCAVATION.

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2003, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS. IN CASE OF CONFLICT, THE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS SHALL GOVERN.

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-2	MANUAL FOR ASSESSING SAFETY HARDWARE (MASH)
102-1	BIDDING REQUIREMENTS AND CONDITIONS
103-1	DETERMINATION OF DBE PARTICIPATION
105-1	CONSTRUCTION CONTROL MARKINGS
105-2	EQUIPMENT AND MATERIAL STORAGE ON BRIDGE STRUCTURES
105-3	CONTROL OF WORK
107-1	WORKER VISIBILITY
108-1	LIQUIDATED DAMAGES
110-1	PROTECTION OF WATER QUALITY AND WETLANDS
303-1	AGGREGATE BASE COURSE
404-1	PRODUCTION VERIFICATION OF ASPHALT CONCRETE HOT MIX
404-2	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
409-1	MINERAL AGGREGATES
410-3	DENSITY TESTING FOR ACHM LEVELING COURSES AND BOND BREAKERS
501-1	INSTALLATION OF TIE BARS
600-1	WATER FOR VEGETATION
603-1	MAINTENANCE OF TRAFFIC
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-2	INSPECTION OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
606-1	PIPE CULVERTS FOR SIDE DRAINS
719-2	THERMOPLASTIC PAVEMENT MARKING MATERIAL
804-1	INSTALLATION OF DOWEL BARS AND TIE BARS
JOB 110583	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB 110583	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 110583	HIGH PERFORMANCE PAVEMENT MARKING
JOB 110583	INTERNET BIDDING
JOB 110583	PARTNERING REQUIREMENTS
JOB 110583	PLASTIC PIPE
JOB 110583	PROJECT DESCRIPTION
JOB 110583	REMOVAL AND DISPOSAL OF GUARDRAIL
JOB 110583	SHAPING ROADWAY SECTION
JOB 110583	STORM WATER POLLUTION PREVENTION PLAN
JOB 110583	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 110583	UTILITY ADJUSTMENTS
JOB 110583	VALUE ENGINEERING
JOB 110583	WARM MIX ASPHALT
JOB 110583	WATTLES

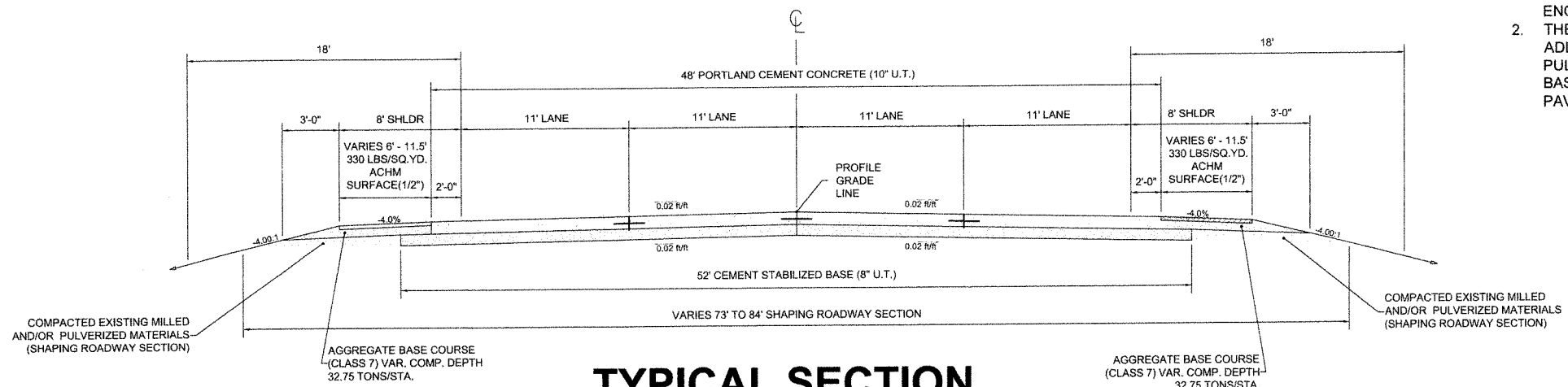


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				6	ARK.			
JOB NO. 110583							3	52

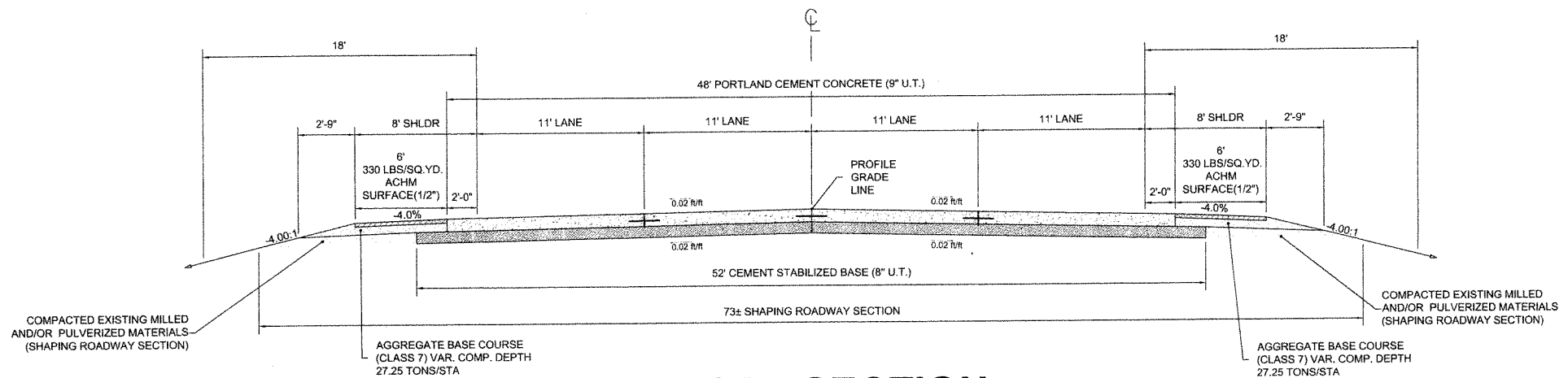
② TYPICAL SECTIONS OF IMPROVEMENT

NOTE:

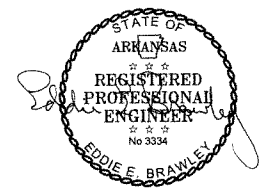
- REFER TO CROSS SECTIONS FOR DEVIATION FROM THE NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT APPROVAL OF THE ENGINEER.
- THE 8" CEMENT STABILIZED BASE COURSE CONSISTS OF ADDING CEMENT TO THE EXISTING MILLED AND/OR PULVERIZED AND COMPACTED ROADBED MATERIALS AS BASE COURSE FOR THE PORTLAND CEMENT CONCRETE PAVEMENT.



**TYPICAL SECTION
SOUTHLAND DR. - STA. 1+00.00 to STA. 46+55.22**



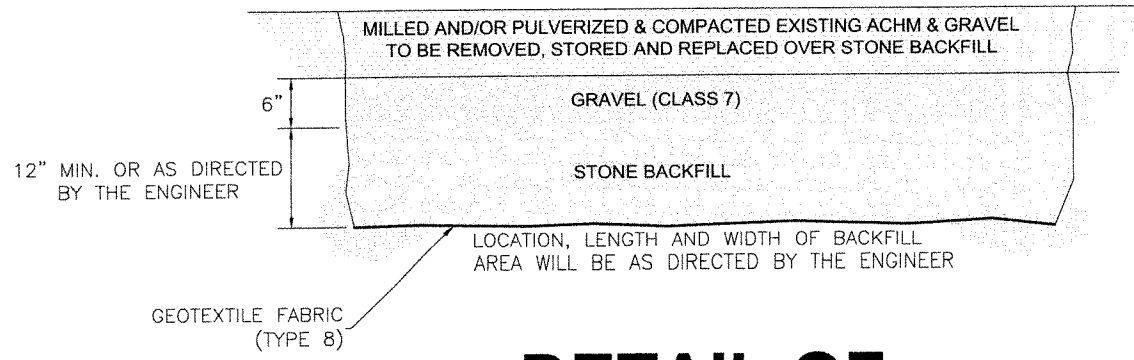
**TYPICAL SECTION
INGRAM BLVD. - STA. 1+00 to STA. 5+31**



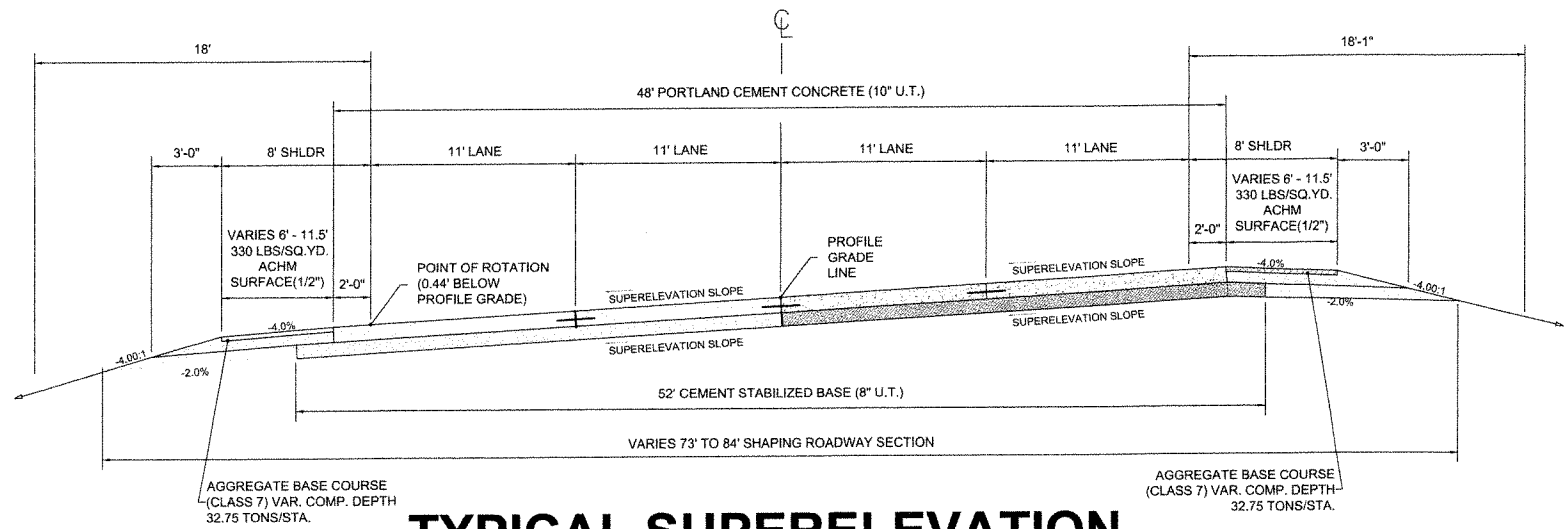
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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	110583	4 52

② TYPICAL SECTIONS OF IMPROVEMENT

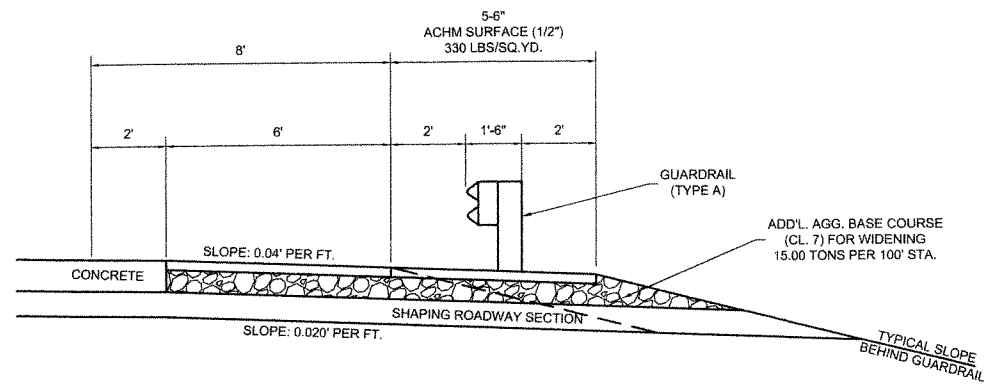


DETAIL OF STONE BACKFILL

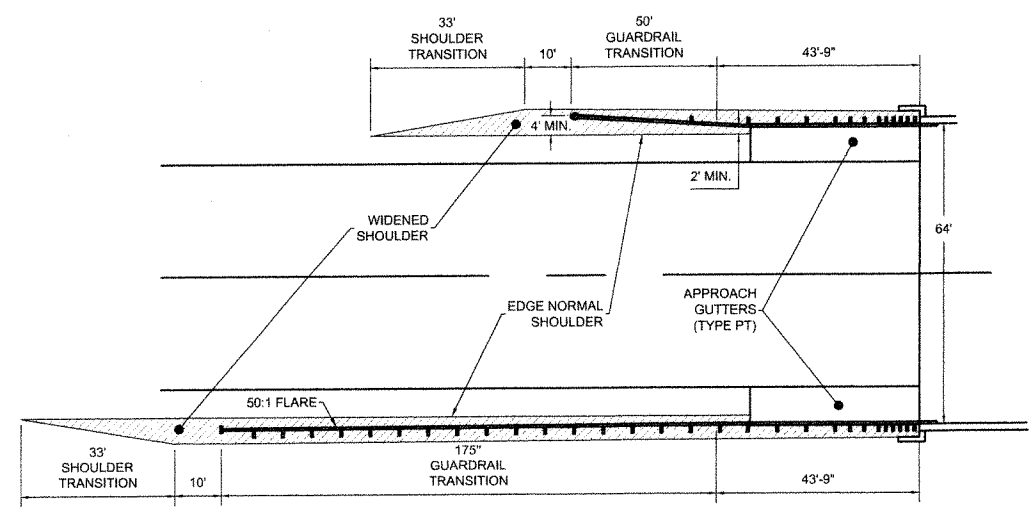


TYPICAL SUPERELEVATION SECTION OF IMPROVEMENT (REVERSE FOR CURVE RT.)

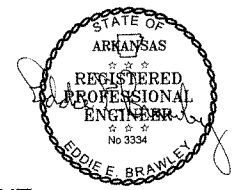
NOTE: REFER TO CROSS SECTIONS FOR DEVIATION FROM THE NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT APPROVAL OF THE ENGINEER.



GUARDRAIL WIDENING SECTION



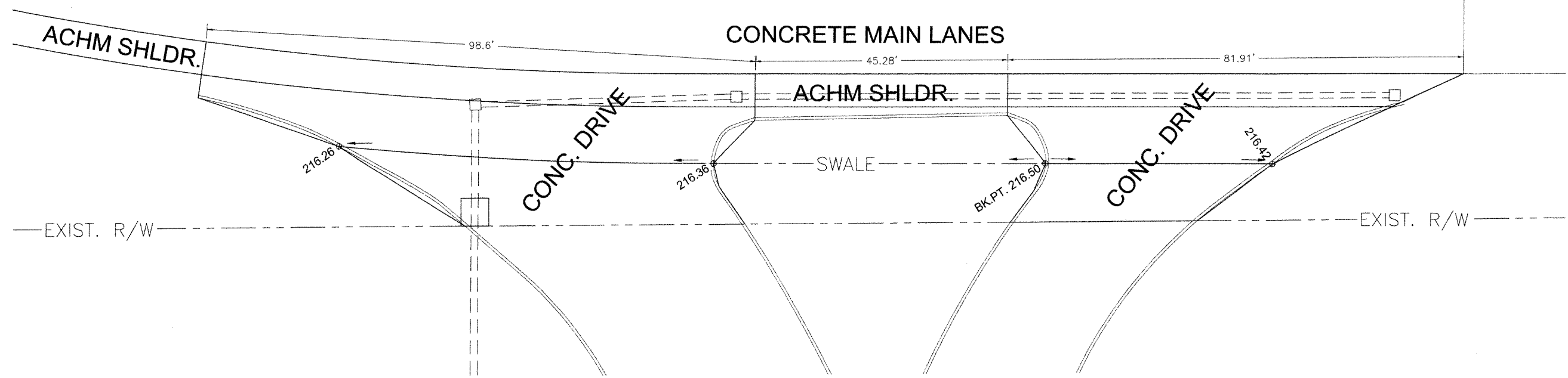
DETAILS OF ROADWAY WIDENING FOR GUARDRAIL



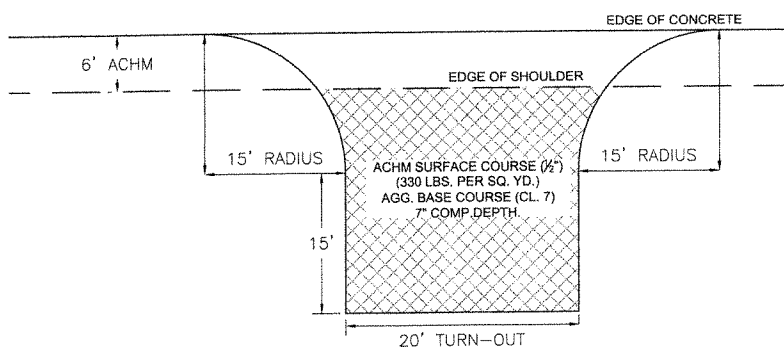
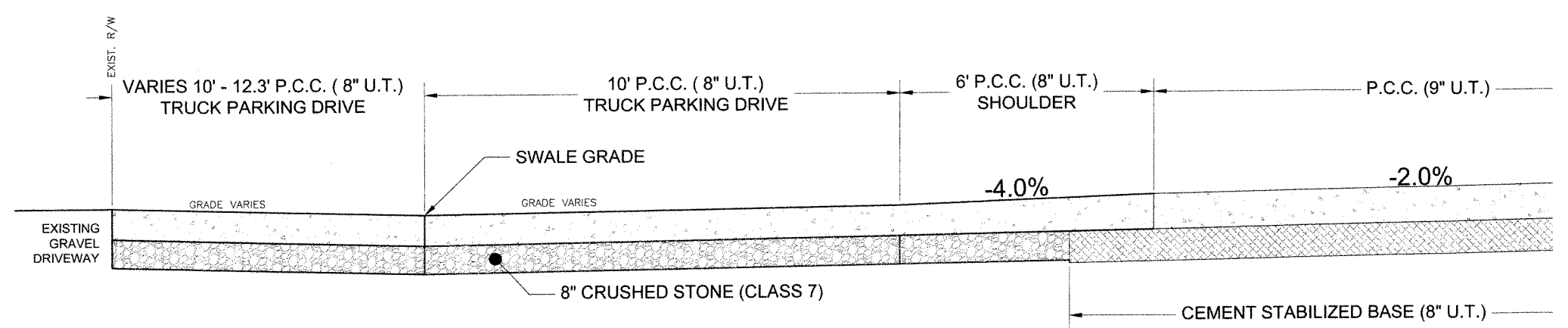
TYPICAL SECTIONS OF IMPROVEMENT

DATE REVISION	DATE FILMED	DATE REVIEWED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 110583							5	52

② SPECIAL DETAILS

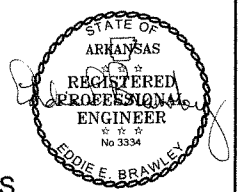


**SPECIAL DETAIL - TRUCK PARKING DRIVE PLAN
INGRAM BLVD. STA 1+00 TO STA 3+22.71**



**SPECIAL DETAIL - TRUCK PARKING DRIVE SECTION
INGRAM BLVD.**

**SPECIAL DETAIL
RURAL TURNOUT**



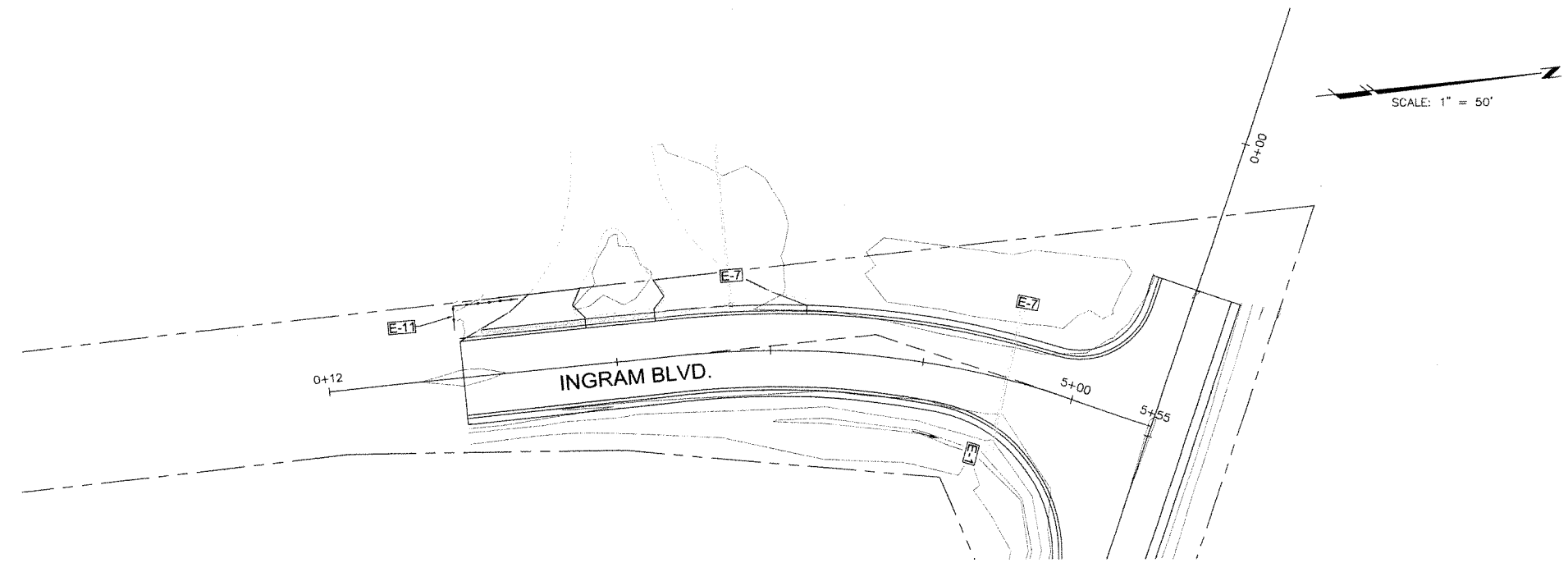
SPECIAL DETAILS

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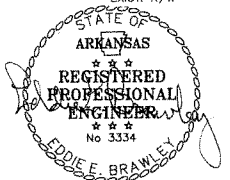
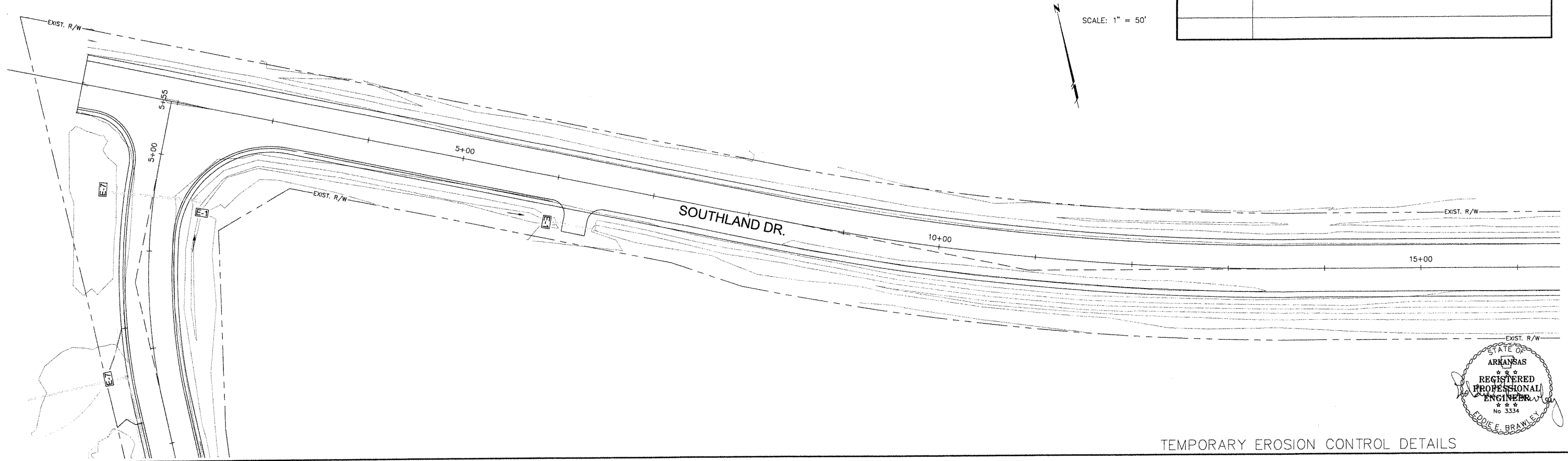
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				6	ARK.		7	52
JOB NO. 110583								

TEMPORARY EROSION CONTROL DETAILS

LEGEND	
E-1	WATTLE DITCH CHECK
E-7	DROP INLET SILT FENCE
E-11	SILT FENCE



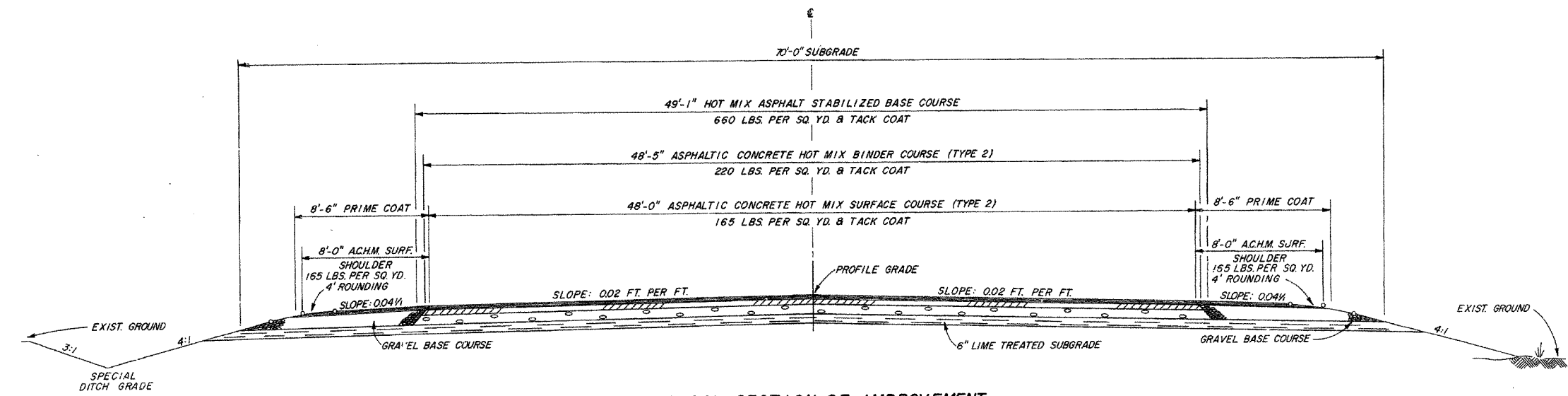
REVISION BOX	
DATE	REVISION



TEMPORARY EROSION CONTROL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110583		6	52

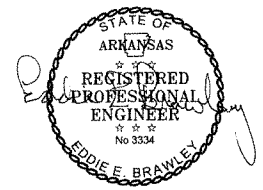
② SPECIAL DETAILS - EXIST. SECTION & SOIL INFO.



TYPICAL SECTION OF IMPROVEMENT
AS PER ORIGINAL DESIGN (1980)

SOIL LOG						
ALIGNMENT	LOCATION	SIDE	ACHM SURFACE	ACHM BASE	ASPHALTIC BASE & LIME	SUBGRADE
INGRAM BLVD.	2+00	15' RT.	0'-2"	0'-1 3/4"	0'-9 1/2"	CLAY
SOUTHLANDR.	22+50	CENTER	0'-1 1/2"	0'-3 1/2"	0'-7 1/2"	CLAY

NOTE: SOIL CHARACTERISTICS TABULATED ABOVE ARE REPRESENTATIVE AT THE LOCATION OF THE SAMPLE AND FROM SURFACE INDICATIONS ARE TYPICAL FOR THE LIMITS SHOWN. THESE DATA ARE SHOWN FOR INFORMATION ONLY. THE OWNER WILL NOT BE RESPONSIBLE FOR VARIATIONS IN THE SOIL CHARACTERISTICS AND/OR EXTENT OF SAME DIFFERING FROM THE ABOVE TABULATIONS.

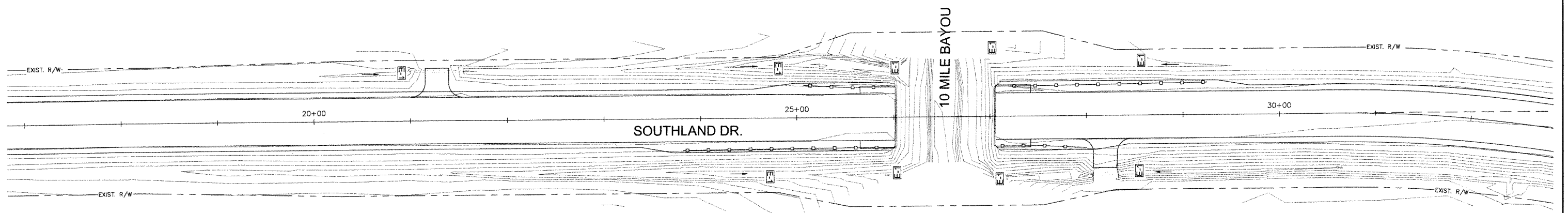


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				6	ARK.		8	52

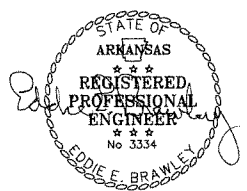
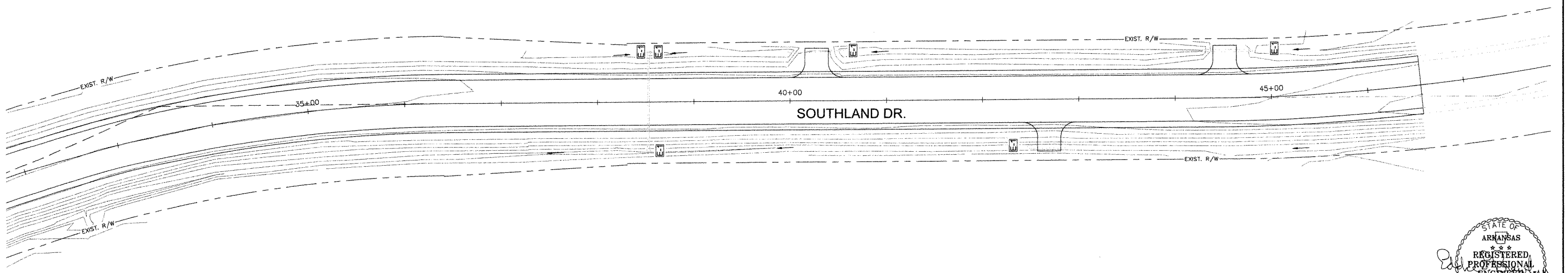
TEMPORARY EROSION CONTROL DETAILS

SCALE: 1" = 50'



REVISION BOX		LEGEND	
DATE	REVISION		
		E-1	WATTLE DITCH CHECK
		E-7	DROP INLET SILT FENCE
		E-11	SILT FENCE

SCALE: 1" = 50'



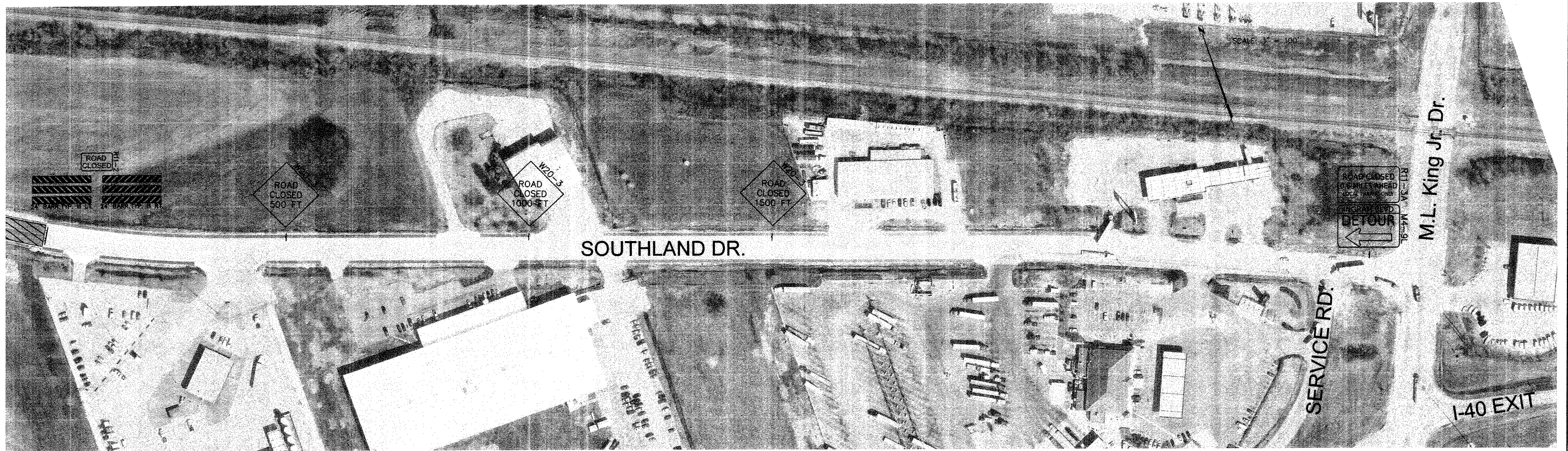
TEMPORARY EROSION CONTROL DETAILS

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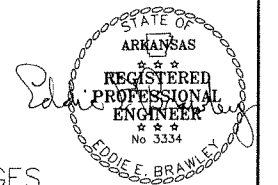
STAGE I:
 CLOSE SOUTHLAND DR. AND RECONSTRUCT.
 CLOSE INGRAM BLVD. EXCEPT FOR SOUTHBOUND LANES FROM STA 1+00 TO STA 2+00 FOR
 SOUTHLAND GAMING AND RACING TRUCK PARKING ACCESS. RECONSTRUCT REMAINING INGRAM
 BLVD.

STAGE II:
 SOUTHLAND DR. REMAINS CLOSED.
 CLOSE INGRAM BLVD. SOUTHBOUND LANES STA 1+00 TO STA 2+00. MAINTAIN SOUTHLAND
 GAMING AND RACING TRUCK PARKING ACCESS USING THE NORTHBOUND LANES OF INGRAM
 BLVD. RECONSTRUCT CLOSED PORTION OF INGRAM BLVD.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	110583	9 52
② MAINTENANCE OF TRAFFIC DETAILS								



ALL STAGES

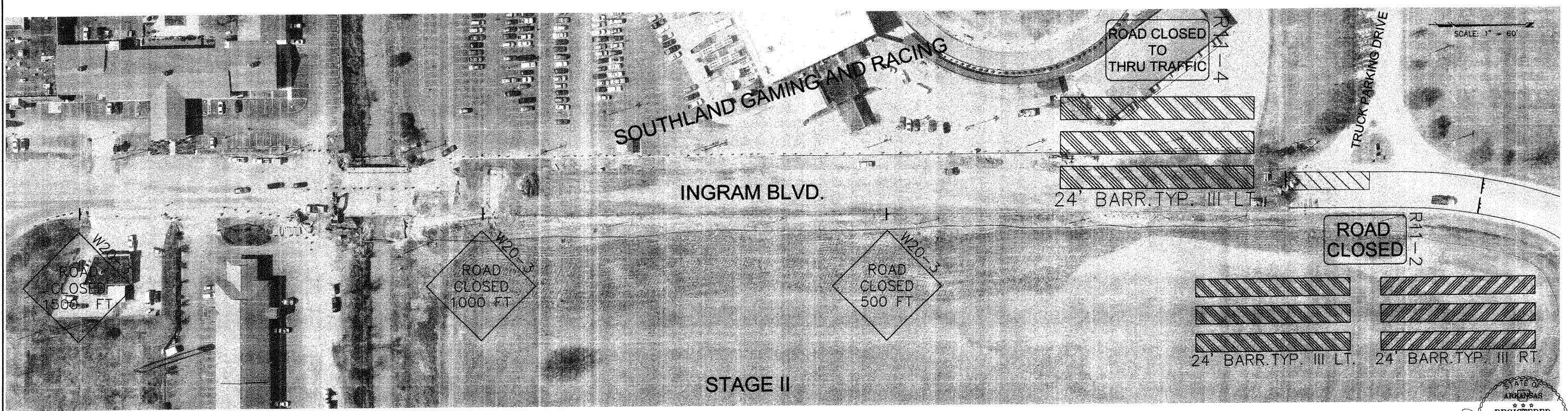
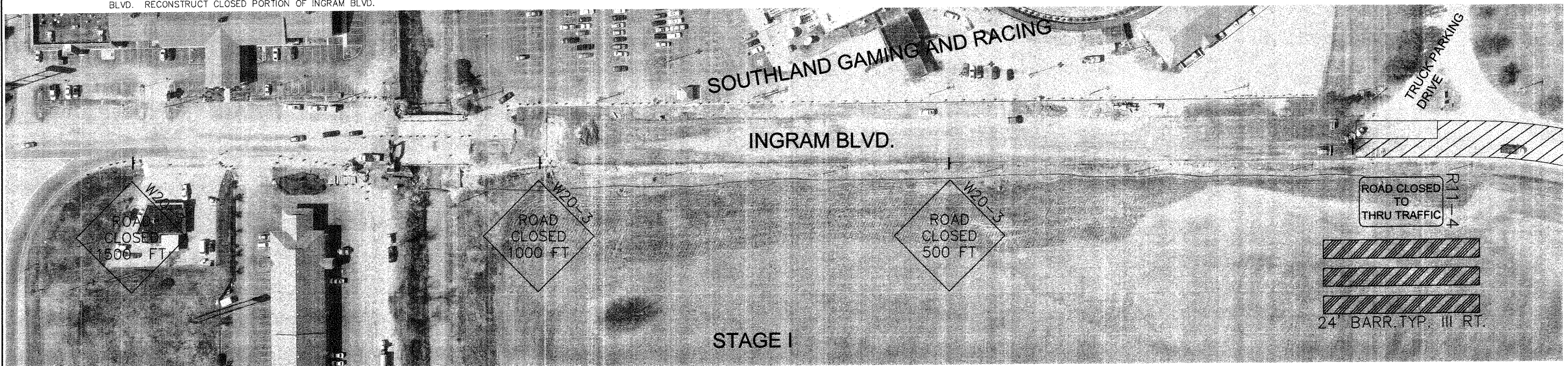


STAGE I:
 CLOSE SOUTHLAND DR. AND RECONSTRUCT.
 CLOSE INGRAM BLVD. EXCEPT FOR SOUTHBOUND LANES FROM STA 1+00 TO STA 2+00 FOR
 SOUTHLAND GAMING AND RACING TRUCK PARKING ACCESS. RECONSTRUCT REMAINING INGRAM
 BLVD.

STAGE II:
 SOUTHLAND DR. REMAINS CLOSED.
 CLOSE INGRAM BLVD. SOUTHBOUND LANES STA 1+00 TO STA 2+00. MAINTAIN SOUTHLAND
 GAMING AND RACING TRUCK PARKING ACCESS USING THE NORTHBOUND LANES OF INGRAM
 BLVD. RECONSTRUCT CLOSED PORTION OF INGRAM BLVD.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	110583	10 52
MAINTENANCE OF TRAFFIC DETAILS								

SCALE: 1" = 60'



STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 3334
 EDDIE E. BRAWLEY

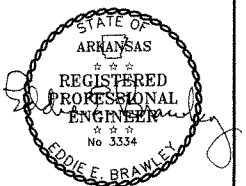
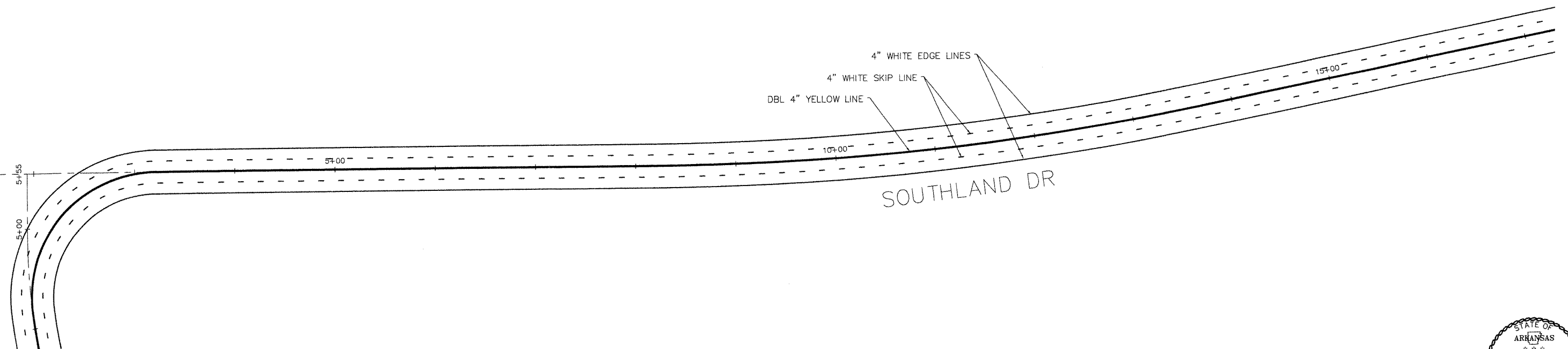
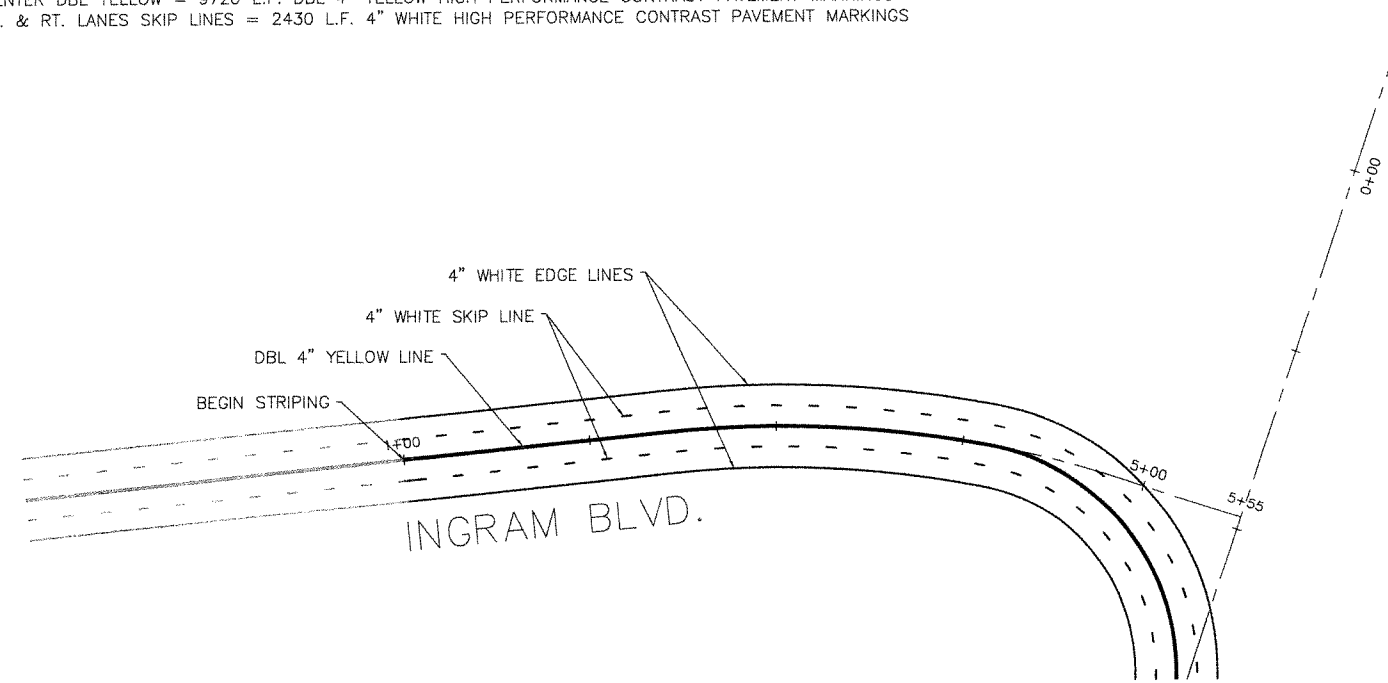
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THERMOPLASTIC PAVEMENT MARKINGS
 INGRAM BLVD. STA 1+00 TO SOUTHLAND DR. STA 46+55
 LT. & RT. LANES OUTSIDE EDGE LINES = 9720 L.F. 4" WHITE

HIGH PERFORMANCE CONTRAST PAVEMENT MARKINGS
 INGRAM BLVD. STA 1+00 TO SOUTHLAND DR. STA 46+55
 CENTER DBL YELLOW = 9720 L.F. DBL 4" YELLOW HIGH PERFORMANCE CONTRAST PAVEMENT MARKINGS
 LT. & RT. LANES SKIP LINES = 2430 L.F. 4" WHITE HIGH PERFORMANCE CONTRAST PAVEMENT MARKINGS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							110583	11	52

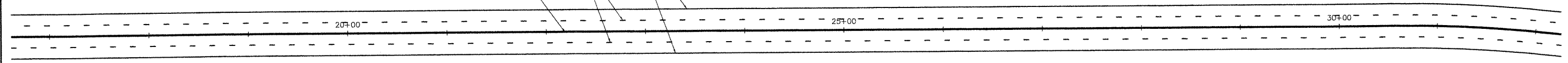
PERMANENT PAVEMENT MARKINGS



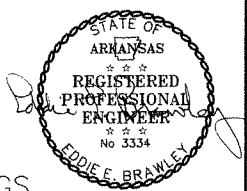
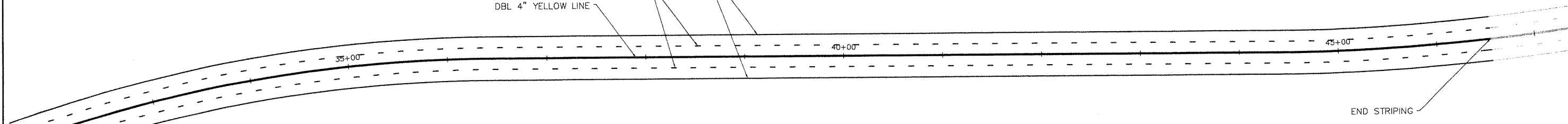
PERMANENT PAVEMENT MARKINGS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	110583	12	52	
PERMANENT PAVEMENT MARKINGS								

4" WHITE EDGE LINES
 4" WHITE SKIP LINE
 DBL 4" YELLOW LINE



4" WHITE EDGE LINES
 4" WHITE SKIP LINE
 DBL 4" YELLOW LINE



PERMANENT PAVEMENT MARKINGS

ADVANCE WARNING SIGNS AND DEVICES AND PERMANENT PAVEMENT MARKINGS

SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE 1	STAGE 2	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		BARRICADES (TYPE III)	THERMO. PAVEMENT MARKING	HIGH PERFORMANCE CONTRAST PAVEMENT MARKINGS			
						NO.	SQ. FT.			LIN. FT.	4" WHITE	4" YELLOW	4" WHITE
SQ. FT.-LIN. FT.-EACH													
M4-9L	INGRAM BLVD DETOUR	30"x24"	1	1	1	1	5						
R11-2	ROAD CLOSED	48"x30"	1	2	2	2	20						
R11-3A	ROAD CLOSED 0.6 MILES AHEAD LOCAL TRAFFIC ONLY	60"x30"	1	1	1	1	12.5						
R11-4	ROAD CLOSED TO THRU TRAFFIC	60"x30"	1	1	1	1	12.5						
W20-3	ROAD CLOSED 1500 FT	48"x48"	2	2	2	2	32						
W20-3	ROAD CLOSED 1000 FT	48"x48"	2	2	2	2	32						
W20-3	ROAD CLOSED 500 FT	48"x48"	2	2	2	2	32						
	TYPE III BARRICADE (24') LT.		1	3	3			72					
	TYPE III BARRICADE (24') RT.		2	2	2			48					
	THERMOPLASTIC PAVEMENT MARKINGS - WHITE (4")								9720				
	HIGH PERFORMANCE CONTRAST PAVEMENT MARKINGS - YELLOW (4")									9720			
	HIGH PERFORMANCE CONTRAST PAVEMENT MARKINGS - WHITE (4")										2430		
TOTALS							146.0	120	9720	9720	2430		

THIS IS A HIGH VOLUME ROAD AS DEFINED IN SECTION 604.03 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2003.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 110583							14	52
QUANTITIES								

STRUCTURES				
STATION	SIDE	DESCRIPTION	DROP INLET (TYPE ST)	STD. DWG. NO.
			EACH	
2+74.6	LT.	DROP INLET (TYPE ST) CONNECT TO EXIST. 15" CMP OUTLET	1	FPC-9S

PIPE BEDDING	
LOCATION	SELECTED PIPE BEDDING
	CU. YD.
*ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	10
TOTAL	10

* NOTE: QUANTITY ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECS.

APPROACH GUTTER (TYPE PT)					
STATION	STATION	LOCATION	APPROACH GUTTER (TYPE PT)	REINFORCING STEEL RDWY. (GRADE 60)	AGG. BASE CRS. (CLASS 7)
			CU. YD.	POUNDS	TON
25+64.5	26+01	LT. & RT.	33.22	3106	26.0
27+06	27+42.5	LT. & RT.	33.22	3106	26.0
TOTALS			66.44	6212	52.0

EROSION CONTROL																
STATION	STATION	LOCATION	TEMPORARY EROSION CONTROL									PERMANENT EROSION CONTROL				
			TEMPORARY SEEDING	MULCH COVER	WATER	WATTLE (12")	DROP INLET SILT FENCE	SILT FENCE	SEDIMENT BASIN	OBLITERATION OF SEDIMENT BASIN	SEDIMENT REMOVAL & DISPOSAL	SEEDING	LIME	MULCH COVER	WATER	SECOND SEEDING APPL.
												ACRE	TON	ACRE	M.GAL.	ACRE
			(E-1)	(E-7)	(E-11)	(E-14)	CU. YD.			ACRE	TON	ACRE	M.GAL.	ACRE		
1+00	5+31	INGRAM BLVD. LT. & RT.	0.20	0.20	4.08	10	40	60			5	0.20	0.40	0.20	20.40	0.20
1+00	26+01	SOUTHLAND DR. LT & RT.	1.72	1.72	35.09	60				6	1.72	3.44	1.72	175.44	1.72	
27+06	46+55.22	SOUTHLAND DR. LT & RT.	1.34	1.34	27.34	100				10	1.34	2.68	1.34	136.68	1.34	
*ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER									100	100	100					
TOTALS			3.26	3.26	66.51	170	40	60	100	100	121	3.26	6.52	3.26	332.52	3.26

* NOTE: QUANTITIES ARE ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECS.

BASIS OF ESTIMATE:
LIME - 2 TONS/ACRE OF SEEDING
WATER - 102 M.G. PER ACRE SEEDING
WATER - 20.4 M.G. PER ACRE TEMPORARY SEEDING

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.



QUANTITIES

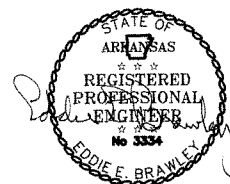
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				6	ARK.		15	52
				JOB NO.	110583			

2 SUMMARY OF QUANTITIES AND REVISIONS

SUMMARY OF QUANTITIES			
ITEM NO.	ITEM	QUANTITY	UNIT
202	REMOVAL AND DISPOSAL OF CURB AND GUTTER	202	LIN. FT.
202	REMOVAL AND DISPOSAL OF APPROACH GUTTERS	4	EACH
202	REMOVAL AND DISPOSAL OF DROP INLETS	3	EACH
202	REMOVAL AND DISPOSAL OF PIPE CULVERTS	3	EACH
SP & 202	REMOVAL AND DISPOSAL OF GUARDRAIL	1470	LIN. FT.
207	STONE BACKFILL	5000	TON
210	UNCLASSIFIED EXCAVATION	1873	CU. YD.
210	COMPACTED EMBANKMENT	970	CU. YD.
SP & 213	SHAPING ROADWAY SECTION	48.81	STATION
SS & 303	AGGREGATE BASE COURSE (CLASS 7)	3466	TON
307	PROCESSING CEMENT TREATED BASE COURSE (8" UNIFORM THICKNESS)	28594	SQ. YD.
307	CEMENT IN TREATED BASE COURSE	806	TON
SP,SS & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")	1075	TON
SP,SS & 407	ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (1/2")	61	TON
SS & 501	PORTLAND CEMENT CONCRETE PAVEMENT (9" UNIFORM THICKNESS)	2635	SQ. YD.
SS & 501	PORTLAND CEMENT CONCRETE PAVEMENT (10" UNIFORM THICKNESS)	23735	SQ. YD.
504	APPROACH GUTTERS (TYPE PT)	66.44	CU. YD.
505	PORTLAND CEMENT CONCRETE DRIVEWAY	387.53	SQ. YD.
601	MOBILIZATION	1.00	LUMP SUM
SS & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	146	SQ. FT.
SS & 604	BARRICADES	120	LIN. FT.
SP,SS & 606	36" SIDE DRAIN	45	LIN. FT.
606	SELECTED PIPE BEDDING	10	CU. YD.
609	DROP INLETS (TYPE ST)	1	EACH
SS & 617	GUARDRAIL (TYPE A)	450	LIN. FT.
SS & 617	GUARDRAIL TERMINAL (TYPE 2)	2	EACH
SS & 617	TERMINAL ANCHOR POST (TYPE 1)	2	EACH
SS & 617	THRIE BEAM GUARDRAIL TERMINAL	4	EACH
620	LIME	7	TON
620	SEEDING	3.26	ACRE
620	MULCH COVER	6.52	ACRE
SS & 620	WATER	399.0	M.G.
621	TEMPORARY SEEDING	3.26	ACRE
621	SILT FENCE	60	LIN. FT.
621	DROP INLET SILT FENCE	40	LIN. FT.
621	SEDIMENT BASIN	100	CU. YD.
621	OBLITERATION OF SEDIMENT BASIN	100	CU. YD.
621	SEDIMENT REMOVAL AND DISPOSAL	121	CU. YD.
SP	WATTLE (12")	170	LIN. FT.
623	SECOND SEEDING APPLICATION	3.26	ACRE
625	GEOTEXTILE FABRIC (TYPE 8)	3200	SQ. YD.
635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
SS & 719	THERMOPLASTIC PAVEMENT MARKING WHITE (4")	9720	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC CONTRAST PAVEMENT MARKING WHITE (4") (ALTERNATE NO. 1)	2430	LIN. FT.
* SP	HIGH PERFORMANCE CONTRAST MARKING TAPE WHITE (4") (ALTERNATE NO. 2)	2430	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC CONTRAST PAVEMENT MARKING YELLOW (4") (ALTERNATE NO. 1)	9720	LIN. FT.
* SP	HIGH PERFORMANCE CONTRAST MARKING TAPE YELLOW (4") (ALTERNATE NO. 2)	9720	LIN. FT.
SS & 804	REINFORCING STEEL - ROADWAY (GRADE 60)	6212	POUND

* DENOTES ALTERNATE BID ITEMS

REVISIONS		
DATE	DESCRIPTION	PAGE NO.(S)



SUMMARY OF QUANTITIES AND REVISIONS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		16	52
				JOB NO.		110583		

2 SURVEY CONTROL DETAILS

SURVEY CONTROL COORDINATES
 DATE: 7/1/2013
 COORDINATE SYSTEM: ARKANSAS STATE PLANE - NORTH ZONE
 BASED ON STATIC GPS OBSERVATION
 USING OPUS TO SOLVE FOR POSITION.
 PROJECTED TO GROUND COORDINATES.
 UNITS: U.S. SURVEY FOOT
 USE CAF = 1.0 FOR STAKEOUT FOR THIS PROJECT.
 A PROJECT CAF OF 0.9999620703350 HAS BEEN USED TO
 COMPUTE THE ABOVE GROUND COORDINATES.
 THIS CAF IS INTENDED FOR USE WITHIN THE PROJECT LIMITS.
 GRIC DISTANCE = GROUND DISTANCE x CAF.
 HORIZONTAL DATUM: NAD 83 (CORS96)
 VERTICAL DATUM: NAVD 88
 BASIS OF BEARING - GRID BASED ON GPS OBSERVATION AT
 CONTROL POINT 2

COORDINATES LISTED BELOW ARE GROUND COORDINATES

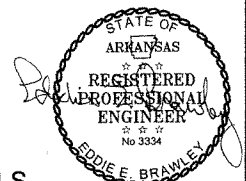
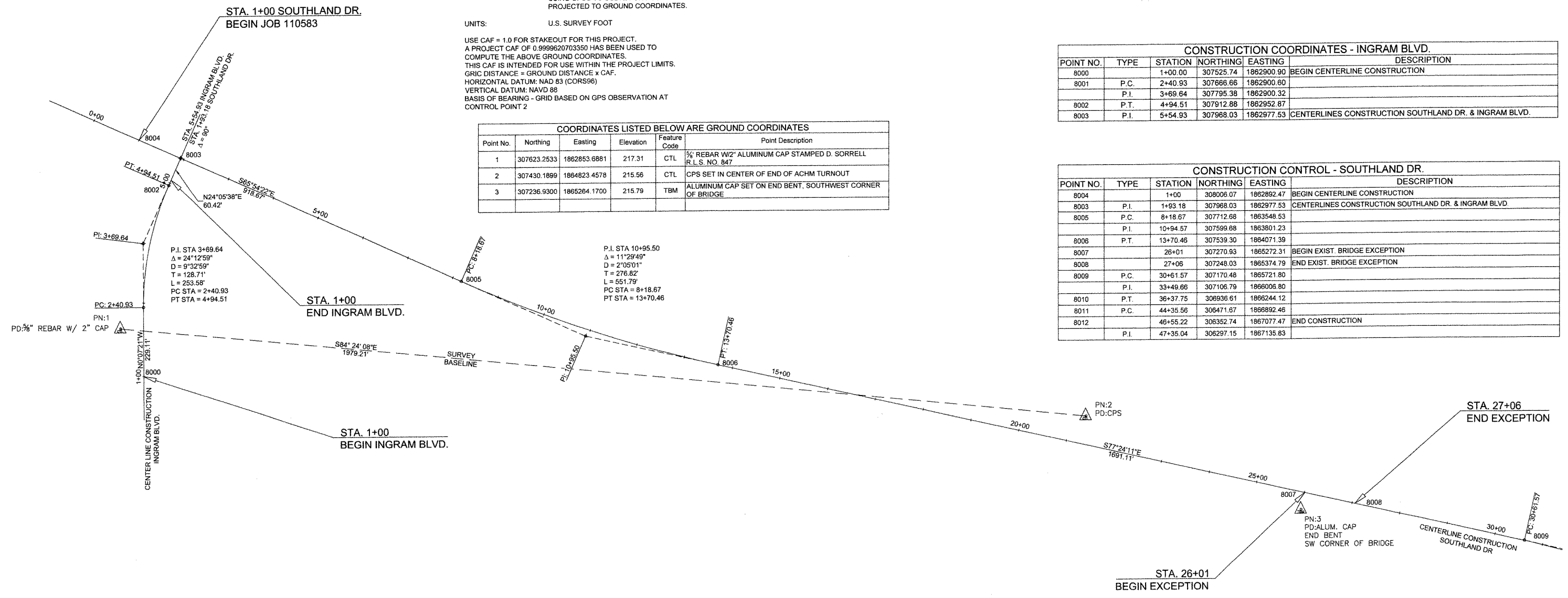
Point No.	Northing	Easting	Elevation	Feature Code	Point Description
1	307623.2533	1862853.6881	217.31	CTL	5/8" REBAR W/2" ALUMINUM CAP STAMPED D. SORRELL R.L.S. NO. 847
2	307430.1899	1864823.4578	215.56	CTL	CPS SET IN CENTER OF END OF ACHM TURNOUT
3	307236.9300	1865264.1700	215.79	TBM	ALUMINUM CAP SET ON END BENT, SOUTHWEST CORNER OF BRIDGE

CONSTRUCTION COORDINATES - INGRAM BLVD.

POINT NO.	TYPE	STATION	NORTHING	EASTING	DESCRIPTION
8000		1+00.00	307525.74	1862900.90	BEGIN CENTERLINE CONSTRUCTION
8001	P.C.	2+40.93	307666.66	1862900.60	
	P.I.	3+69.64	307795.38	1862900.32	
8002	P.T.	4+94.51	307912.88	1862952.87	
8003	P.I.	5+54.93	307968.03	1862977.53	CENTERLINES CONSTRUCTION SOUTHLAND DR. & INGRAM BLVD.

CONSTRUCTION CONTROL - SOUTHLAND DR.

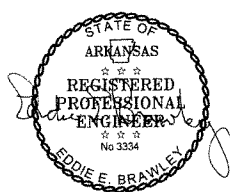
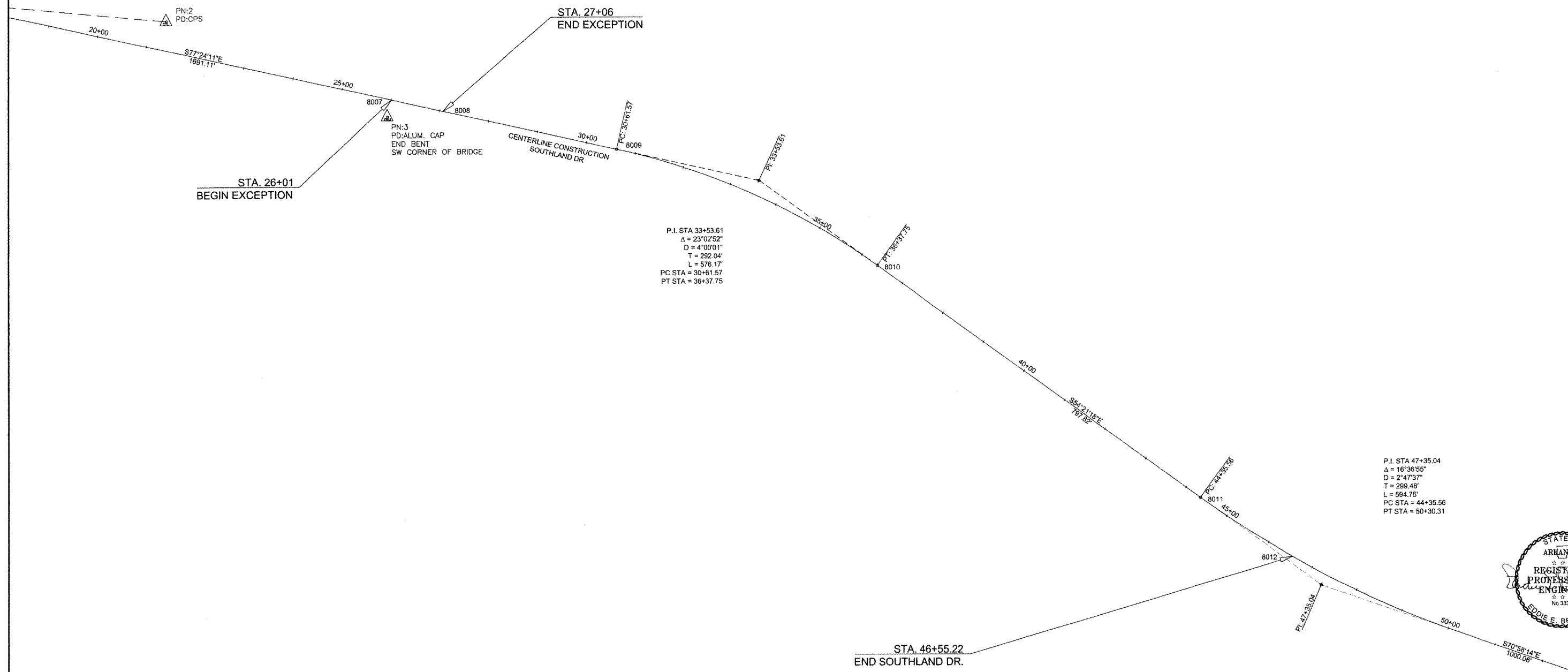
POINT NO.	TYPE	STATION	NORTHING	EASTING	DESCRIPTION
8004		1+00	308006.07	1862892.47	BEGIN CENTERLINE CONSTRUCTION
8003	P.I.	1+93.19	307968.03	1862977.53	CENTERLINES CONSTRUCTION SOUTHLAND DR. & INGRAM BLVD
8005	P.C.	8+18.67	307712.68	1863548.53	
	P.I.	10+94.57	307599.88	1863801.23	
8006	P.T.	13+70.46	307539.30	1864071.39	
8007		26+01	307270.93	1865272.31	BEGIN EXIST. BRIDGE EXCEPTION
8008		27+06	307248.03	1865374.79	END EXIST. BRIDGE EXCEPTION
8009	P.C.	30+61.57	307170.48	1865721.80	
	P.I.	33+49.66	307106.79	1866006.80	
8010	P.T.	36+37.75	306936.61	1866244.12	
8011	P.C.	44+35.56	306471.67	1866892.46	
8012		46+55.22	306352.74	1867077.47	END CONSTRUCTION
	P.I.	47+35.04	306297.15	1867135.83	



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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. 110583	17	52

② SURVEY CONTROL DETAILS

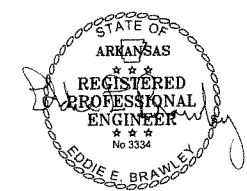
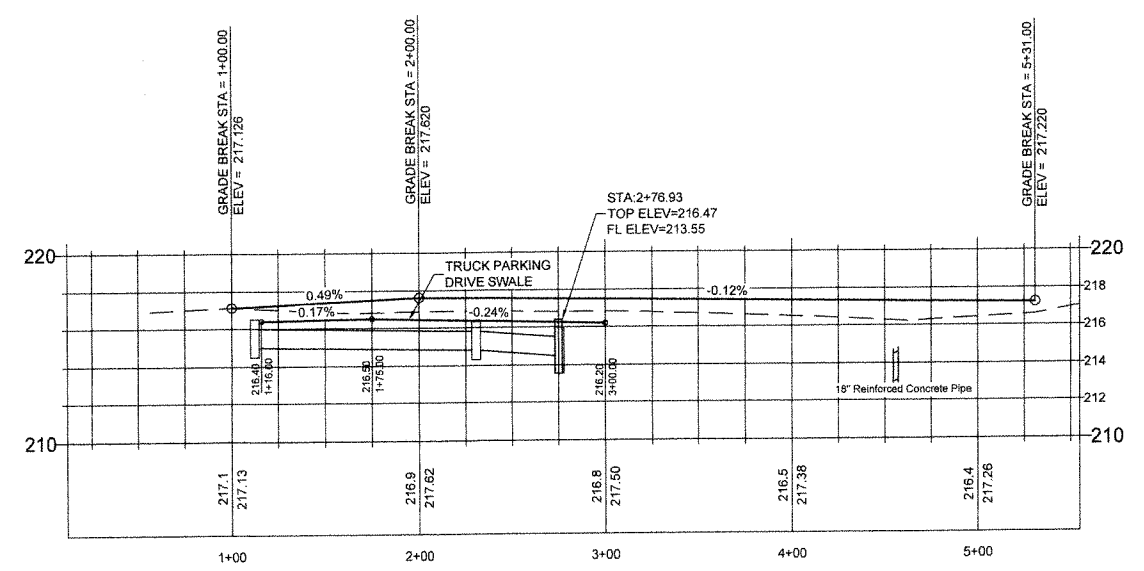
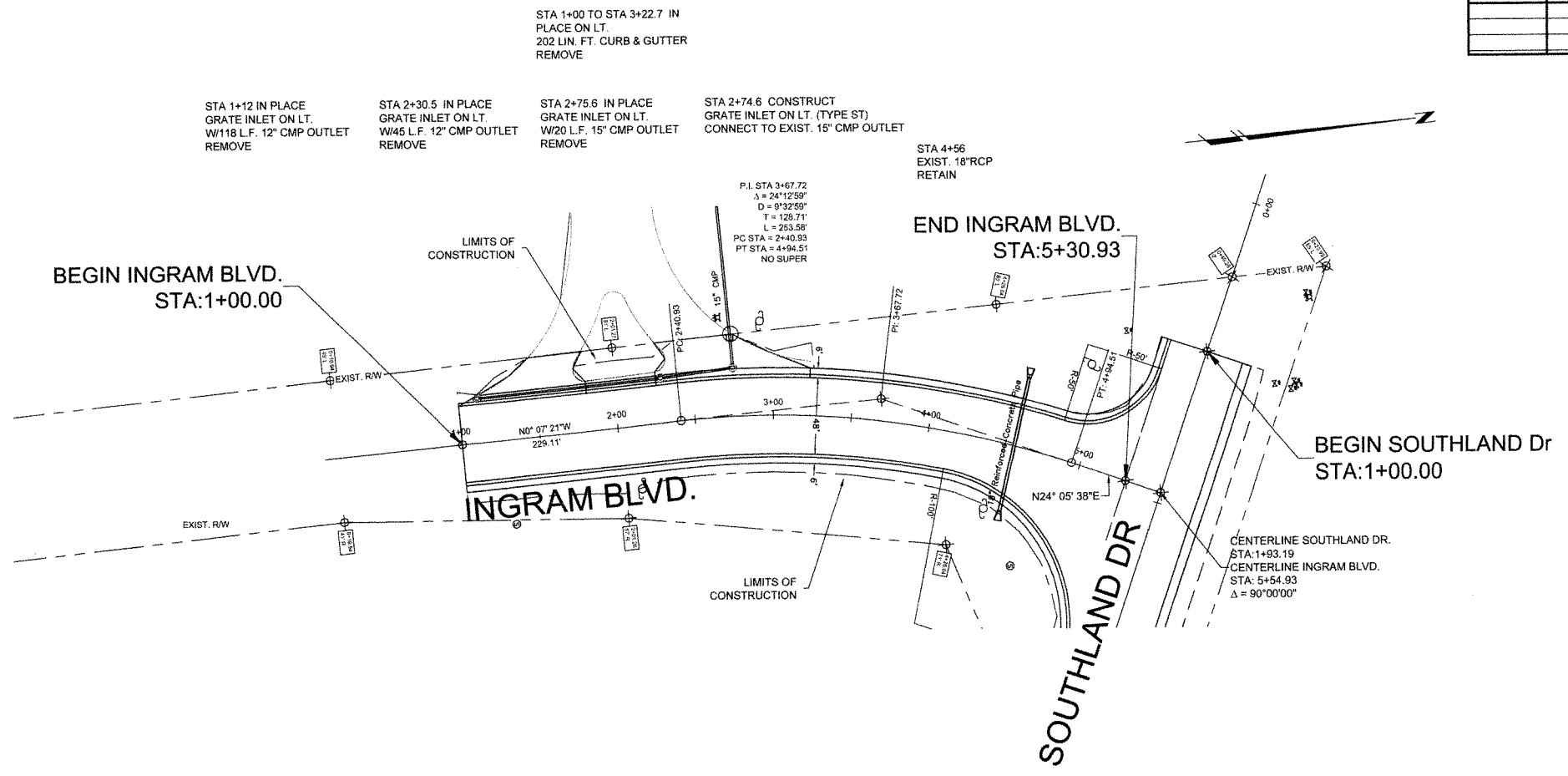


SURVEY CONTROL DETAILS

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				6	ARK.			
JOB NO. 110583							18	52

② PLAN & PROFILE - INGRAM BLVD.

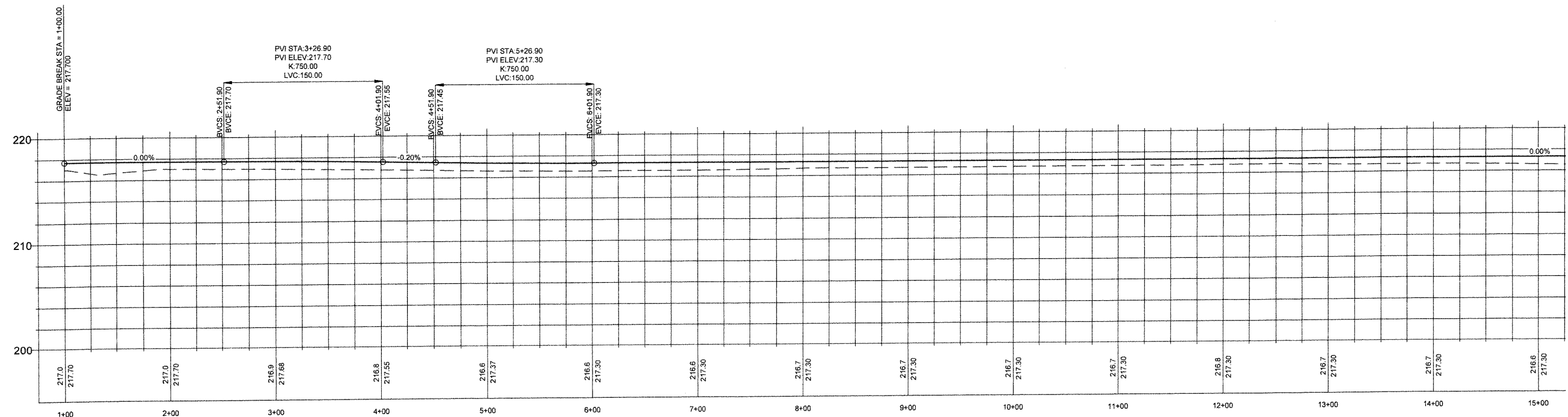
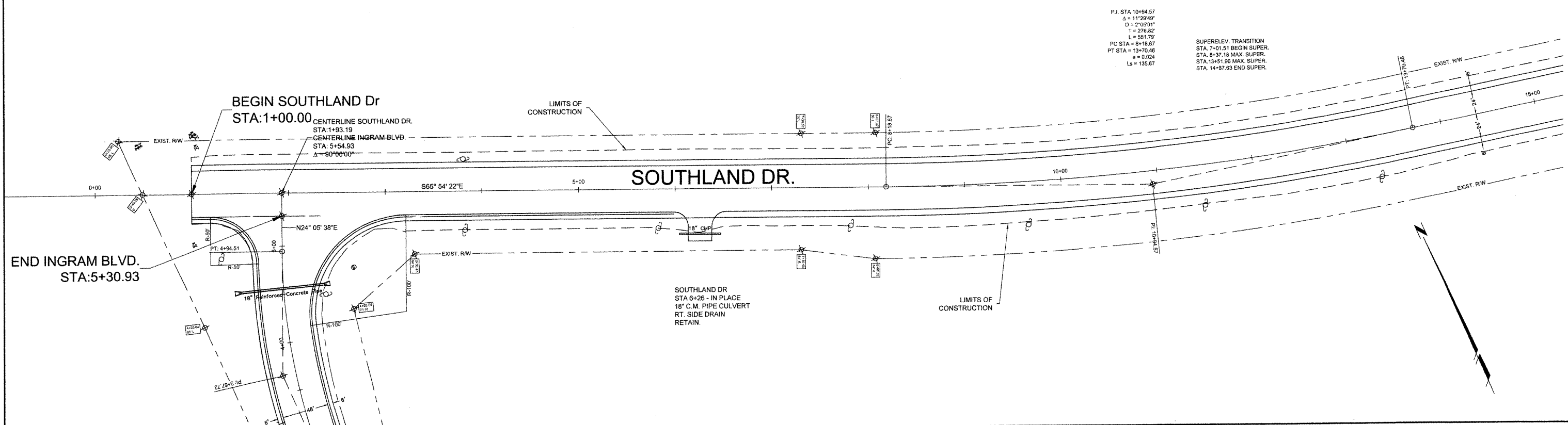


PLAN & PROFILE - INGRAM BLVD. STA 1+00 - STA 5+31

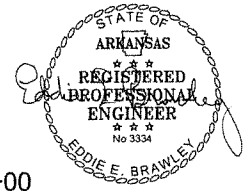
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				6	ARK.			
						JOB NO. 110583	19	52

2 PLAN & PROFILE - SOUTHLAND DR.



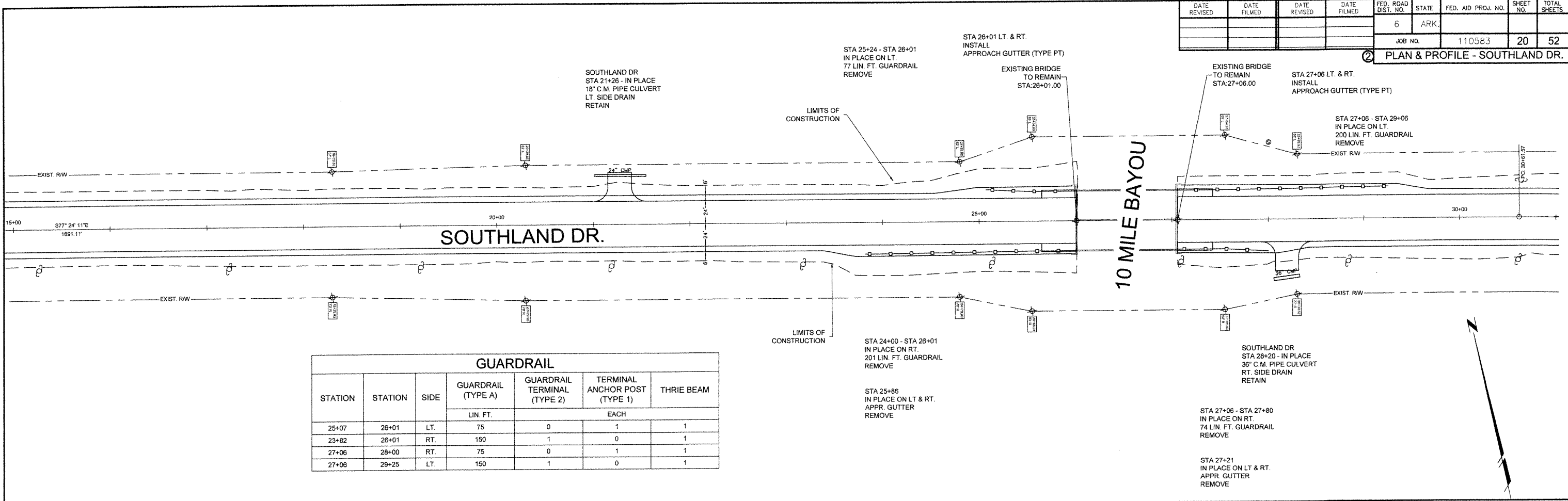
PLAN & PROFILE - SOUTHLAND DR. STA 1+00 - STA 15+00



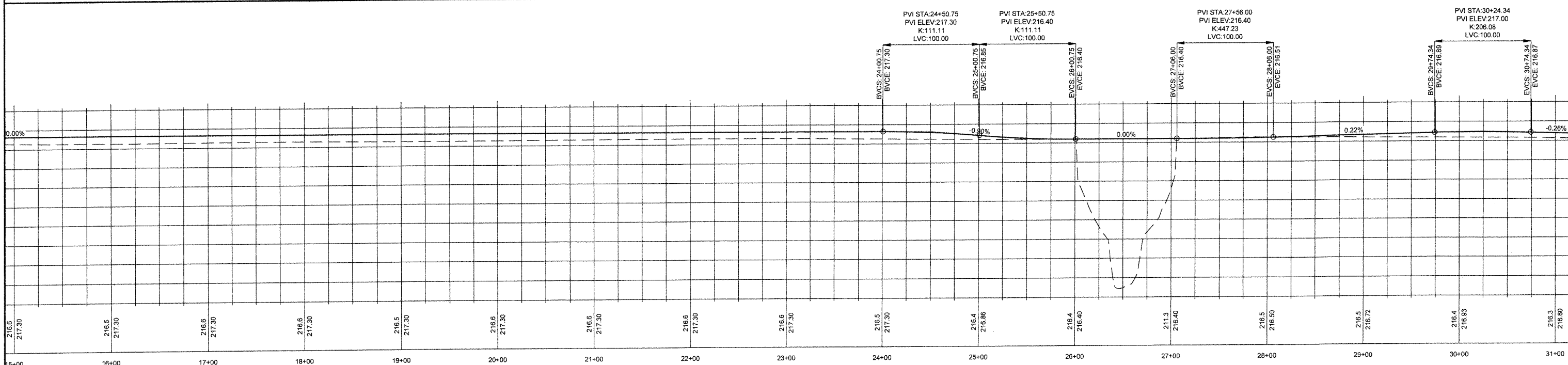
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				6	ARK.			
JOB NO. 110583							20	52

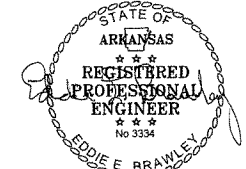
PLAN & PROFILE - SOUTHLAND DR.



GUARDRAIL						
STATION	STATION	SIDE	GUARDRAIL (TYPE A)	GUARDRAIL TERMINAL (TYPE 2)	TERMINAL ANCHOR POST (TYPE 1)	THRIE BEAM
			LIN. FT.		EACH	
25+07	26+01	LT.	75	0	1	1
23+82	26+01	RT.	150	1	0	1
27+06	28+00	RT.	75	0	1	1
27+06	29+25	LT.	150	1	0	1



PN-4 - ALUMINUM CAP ON END BENT
35' RT. STA. 26+00.5
ELEVATION = 215.79

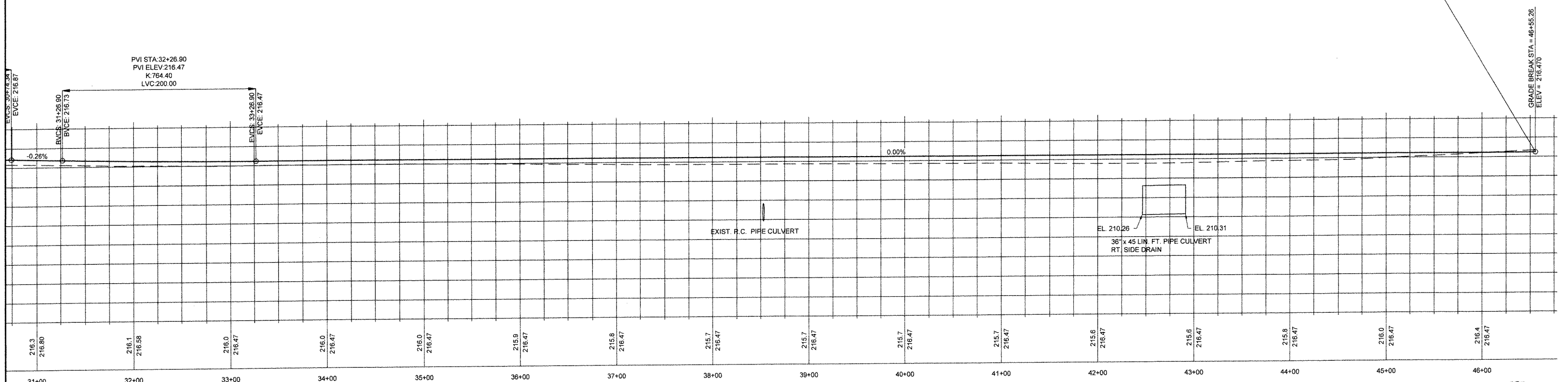
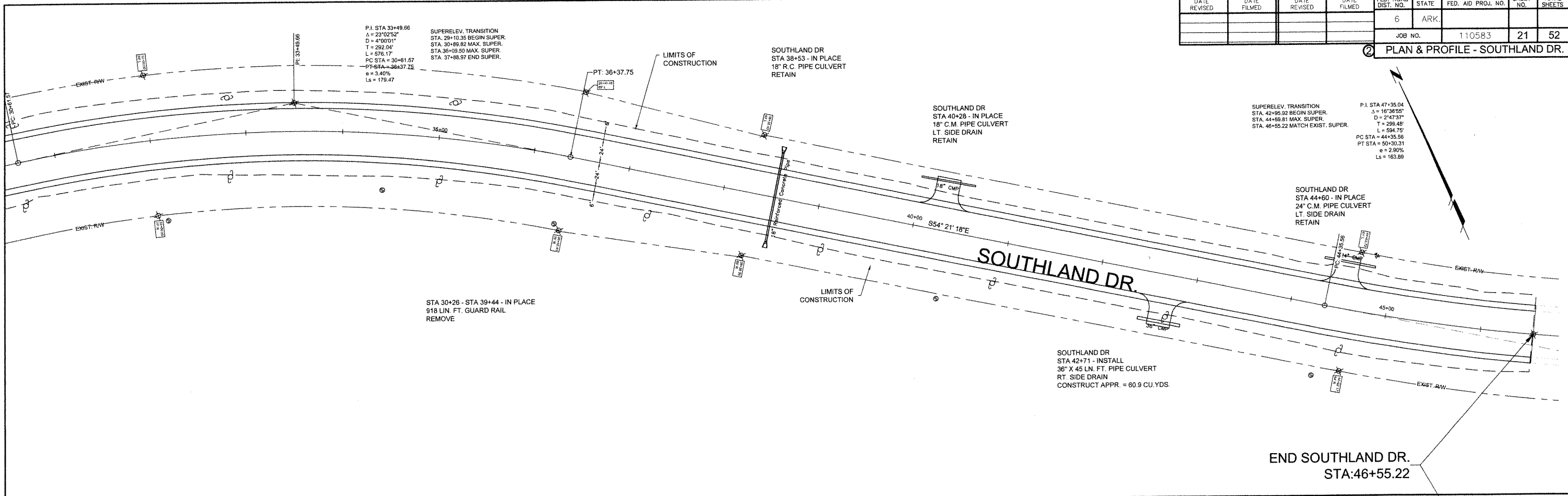


PLAN & PROFILE - SOUTHLAND DR. STA 15+00 - STA 31+00

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				6	ARK.			
JOB NO. 110583							21	52

PLAN & PROFILE - SOUTHLAND DR.



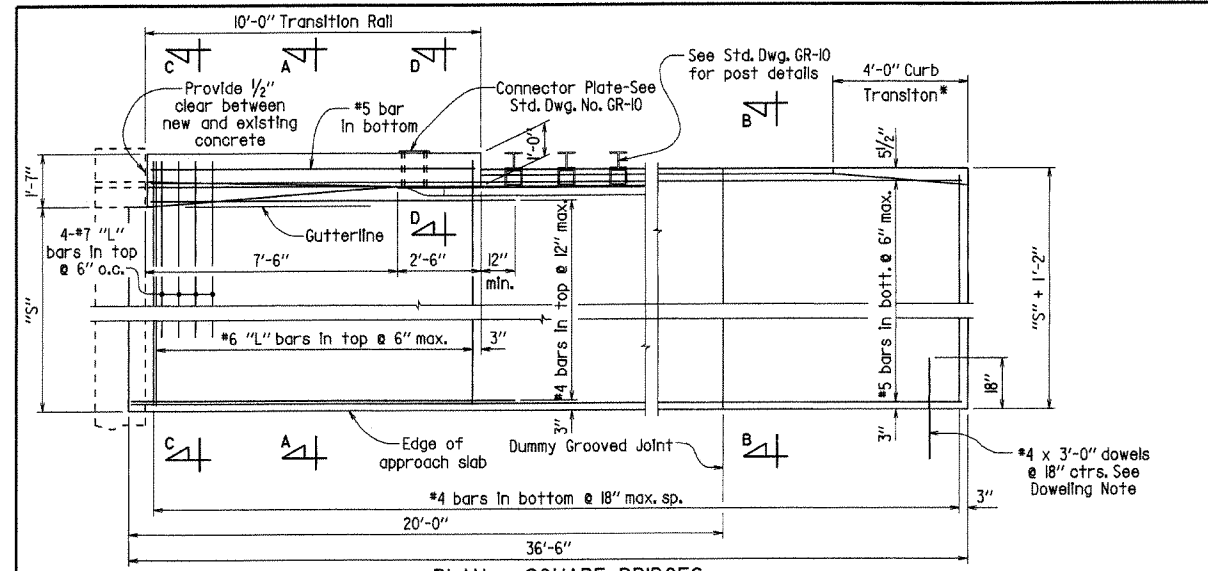
STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
EDDIE E. BRAWLEY
No 3334

PLAN & PROFILE - SOUTHLAND DR. STA 31+00 - STA 46+55

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07-14-2010								

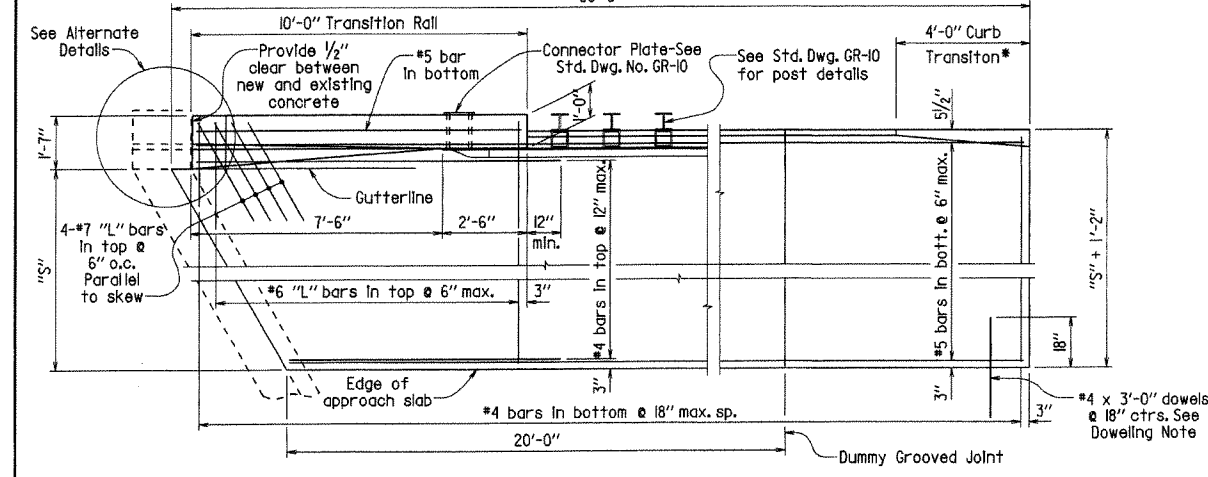
TYPE PT APPR. GUTTER - 2091



PLAN - SQUARE BRIDGES

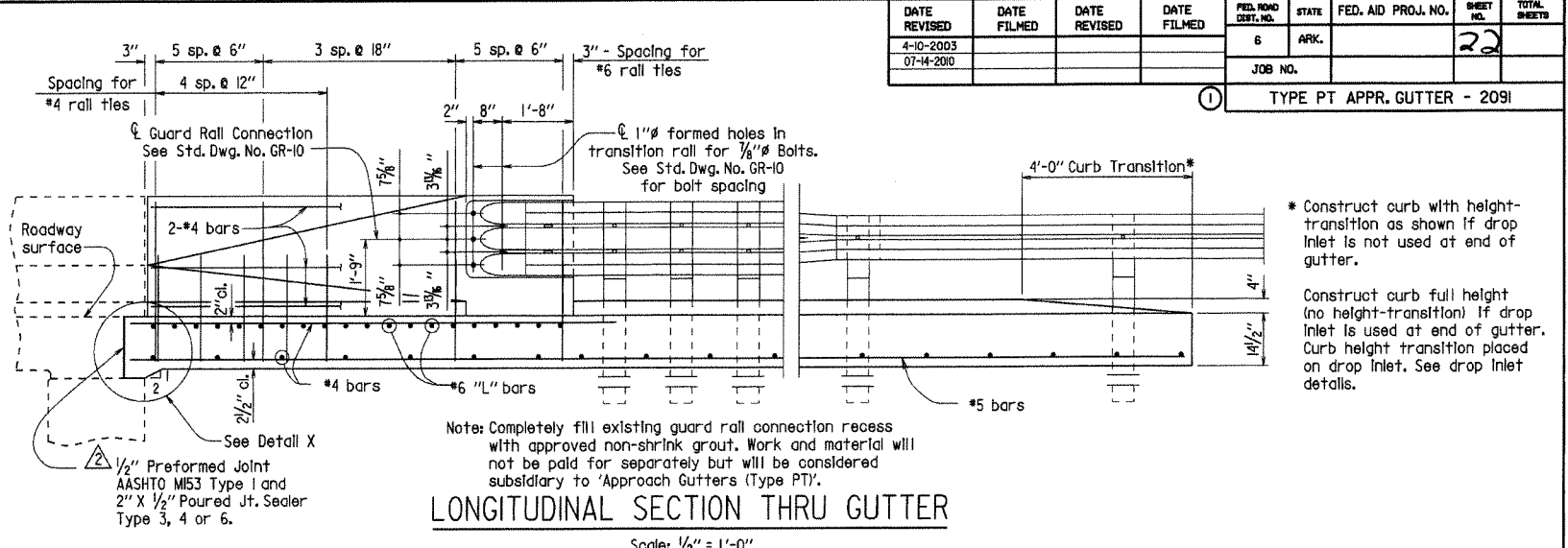
Scale: 3/8" = 1'-0"

'S' = Distance from gutterline to edge of approach slab.



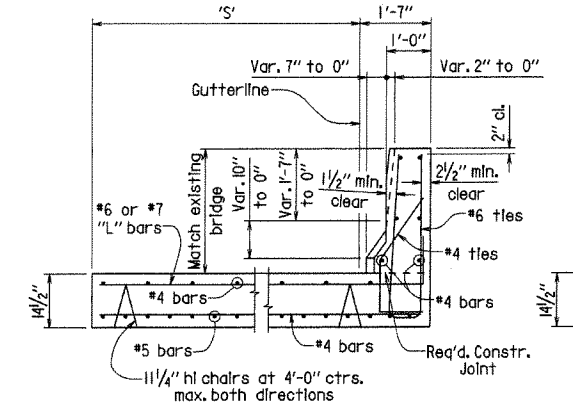
PLAN - SKEWED BRIDGES

Scale: 3/8" = 1'-0"



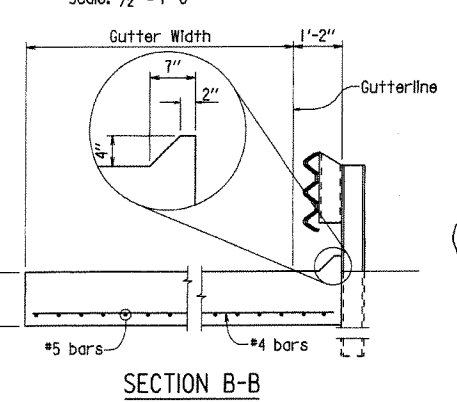
LONGITUDINAL SECTION THRU GUTTER

Scale: 1/2" = 1'-0"



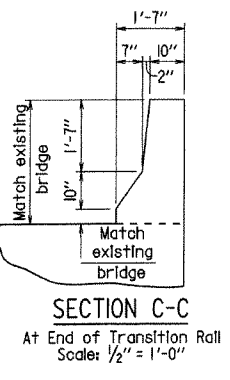
SECTION A-A

Scale: 1/2" = 1'-0"



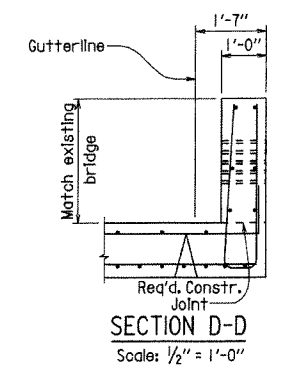
SECTION B-B

Scale: 1/2" = 1'-0"



SECTION C-C

Scale: 1/2" = 1'-0"



SECTION D-D

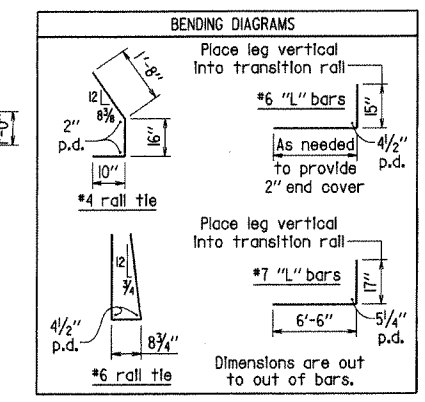
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GENERAL NOTES

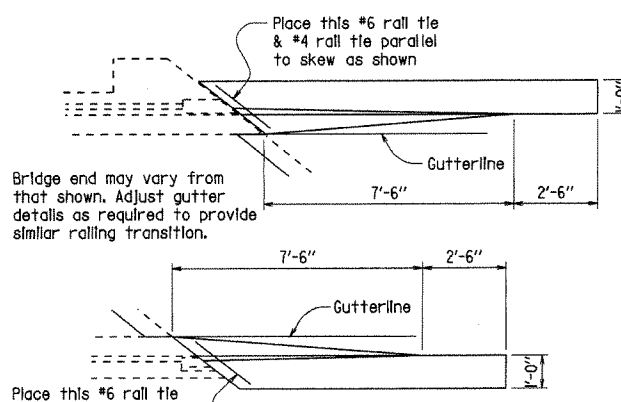
Concrete shall be Class S or (S/AE) or mixture used for Portland Cement Concrete Pavement.
Reinforcing steel shall conform to AASHTO M31 or M53, Grade 60 (fy = 60,000 psi.) Fabricate bar lengths to provide 2" minimum cover at each end.
Approach gutters will be measured and paid for in accordance with Section 504 of the Standard Specifications.

QUANTITIES FOR ONE SQUARE APPROACH GUTTER

'S'	Concrete	Reinforcing Steel
5'-6"	12.49 cu.yd.	1227 lb.
5'-9"	12.91 cu.yd.	1276 lb.
6'-0"	13.34 cu.yd.	1296 lb.
9'-6"	19.23 cu.yd.	1746 lb.
9'-9"	19.65 cu.yd.	1795 lb.
10'-0"	20.07 cu.yd.	1815 lb.



Revised and redrawn 4-10-2003. By KDH Ck. By: CJF 4-10-2003
Added Joint sealer type 07-14-2010 by MJT Checked by: CJF 7-14-2010

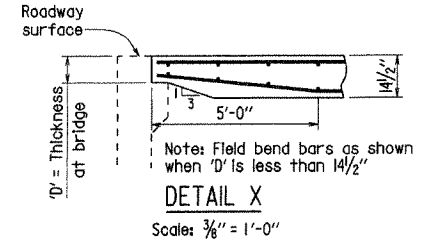


ALTERNATE DETAILS

Scale: 3/8" = 1'-0"

DOWELING NOTE

If new approach slab is used, place dowels into approach slab using 18" embedment.
If existing approach slab is retained, dowels shall be drilled and grouted 18" into existing slab. At the Contractor's option, existing dowels may be retained, cleaned and incorporated into new gutters. Work for drilling and grouting, or retaining and cleaning will not be paid for separately but will be considered subsidiary to 'Approach Gutters (Type PT)'.



DETAIL X

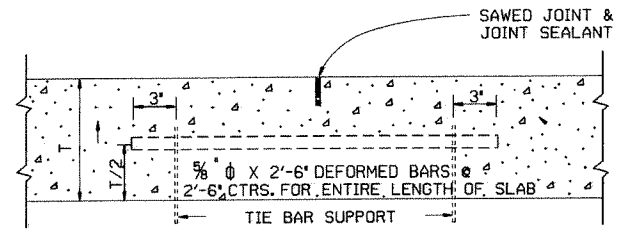
Scale: 3/8" = 1'-0"



DETAILS OF STANDARD TYPE 'PT' APPROACH GUTTERS (BRIDGES WITH CONCRETE PARAPET RAILING)

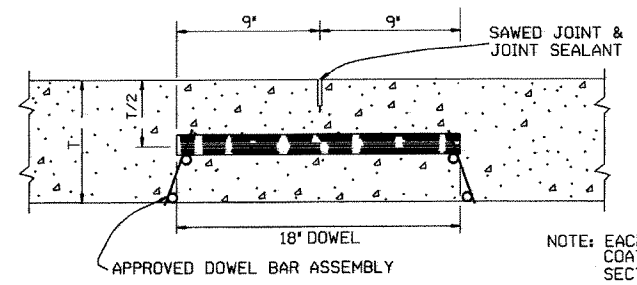
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: KDH DATE: 4-10-2003 FILENAME: B2091.STD
CHECKED BY: CJF DATE: 4-10-2003 SCALE: AS NOTED
DESIGNED BY: STD DATE:
BRIDGE NO. DRAWING NO. 2091

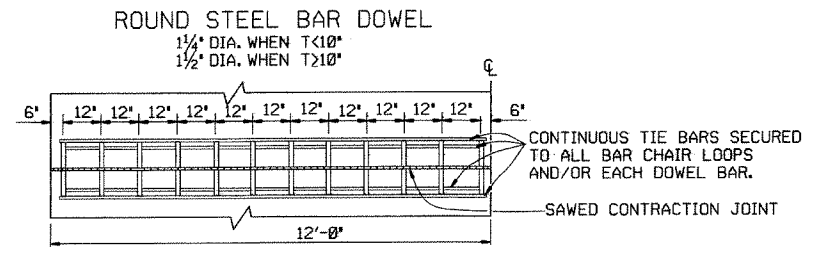


LONGITUDINAL JOINT

NOTE: THE TIE BAR SUPPORT SHOWN ABOVE MAY BE ELIMINATED IF OTHER APPROVED METHODS FOR PLACING AND SUPPORTING THE TIE BARS ARE PROVIDED.
TIE BARS SHALL BE 15' FROM TRANSVERSE JOINTS.



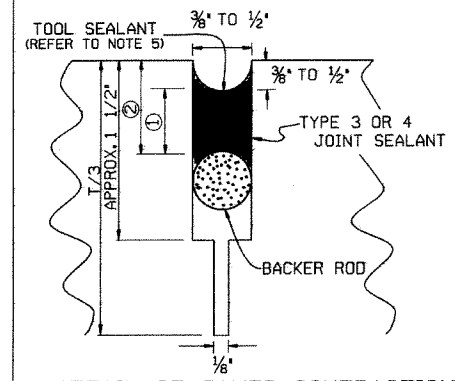
NOTE: EACH DOWEL TO BE COATED ACCORDING TO SECTION 502 OF THE STANDARD SPECIFICATIONS.



ONE-HALF 24' PAVEMENT
12 DOWELS
PLAN

NOTE: FOR 20' PAVEMENT USE 20 DOWELS @ 12' CTRS. WITH 6' SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR 15' PAVEMENT USE 15 DOWELS @ 12' CTRS. WITH 6' SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR 26' PAVEMENT USE 26 DOWELS @ 12' CTRS. WITH 6' SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR PAVEMENT WIDTHS OTHER THAN THOSE SHOWN ABOVE, USE DOWELS AT 12' CTRS. WITH 6' MAX. SPACING FROM C.L. TO FIRST BAR. DISTANCE FROM EDGE OF SLAB TO FIRST BAR SHALL BE ADJUSTED TO MAINTAIN 12' DOWEL BAR SPACING

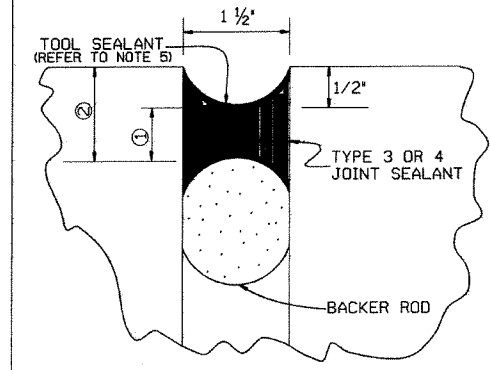
CONTRACTION JOINT DETAILS



DETAIL OF SAWED CONTRACTION JOINT

JOINT CONFIGURATION FOR TYPE 3 OR 4 JOINT SEALANT

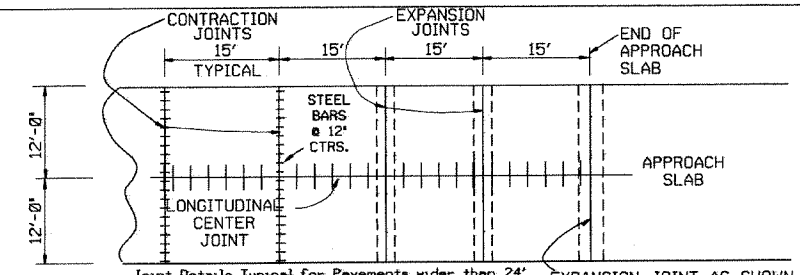
JOINT WIDTH	SEALANT THICKNESS ①	BACKER ROD DIAMETER	BACKER ROD PLACEMENT DEPTH ②
INCHES			
1/4	1/4	3/8	1/2
3/8	1/4	1/2	1/2
1/2	3/8	3/4	1/2
3/4	3/8	3/4	3/4
1	3/4	1	3/4
1 1/2	3/4	2	1 1/4



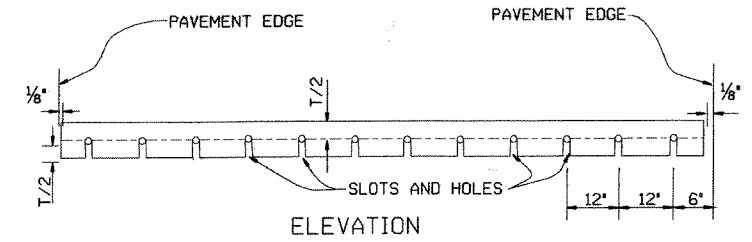
DETAIL OF EXPANSION JOINT

JOINT CONFIGURATION FOR TYPE 5 JOINT SEALANT

JOINT WIDTH	SEALANT THICKNESS ①	BACKER ROD DIAMETER	BACKER ROD PLACEMENT DEPTH ②
INCHES			
1/4	1/4	3/8	3/4
3/8	3/8	1/2	1

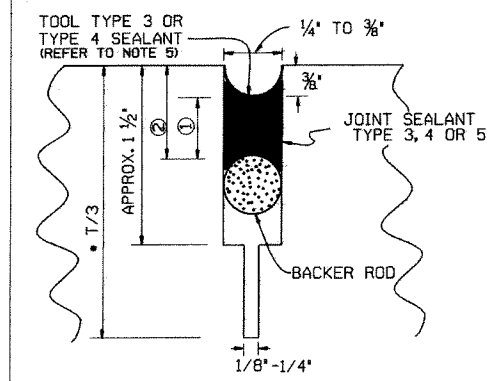


PLAN SHOWING EXPANSION JOINTS AT BRIDGE APPROACH SLABS



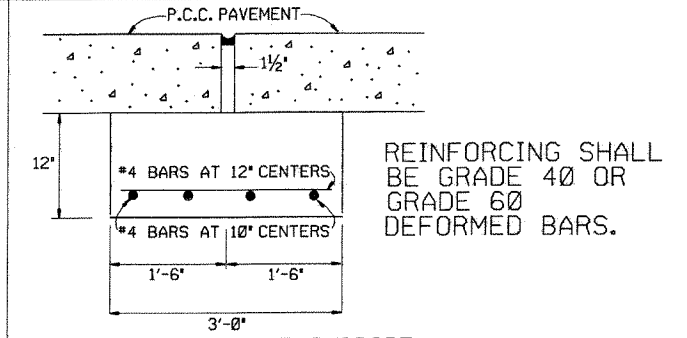
ELEVATION

NOTE: ALL DOWEL BARS SHALL CONFORM TO THE DETAILS FOR CONTRACTION JOINTS.



DETAIL OF SAWED LONGITUDINAL JOINT AND LONGITUDINAL CONSTRUCTION JOINT

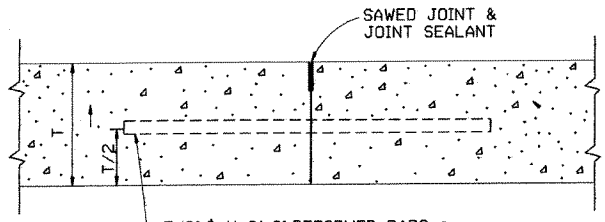
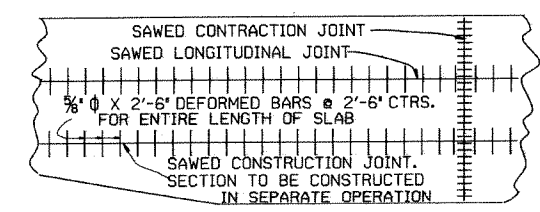
NOTE: T/3 SAW CUT NOT REQUIRED FOR LONGITUDINAL CONSTRUCTION JOINT.



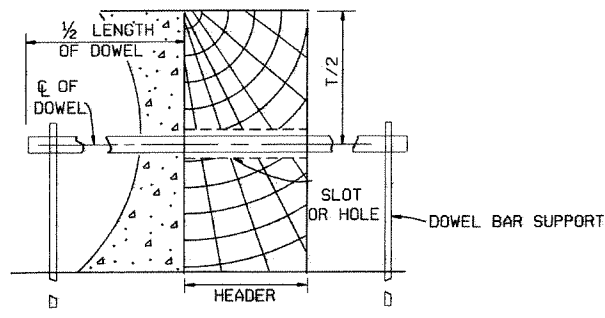
DETAIL OF JOINT SUPPORT FOR EXPANSION JOINTS

REINFORCING SHALL BE GRADE 40 OR GRADE 60 DEFORMED BARS.

- GENERAL NOTES
- *T* DENOTES THICKNESS OF SLAB.
 - DOWEL BARS SHALL BE PLACED IN ACCORDANCE WITH THE DIMENSIONS SHOWN. A TOLERANCE OF PLUS OR MINUS ONE INCH WILL BE ALLOWED FOR THE VERTICAL AND LATERAL PLACEMENT AND A TOLERANCE OF PLUS OR MINUS 1/4" WILL BE ALLOWED FOR THE TILT AND SKEW. DOWEL BARS SHALL BE FIELD COATED FOR A MINIMUM DISTANCE OF 2' GREATER THAN HALF THE LENGTH OF THE BAR WITH AN APPROVED GREASE AS A BOND BREAKER JUST PRIOR TO PLACEMENT OF CONCRETE.
 - THE EXPANSION JOINT SUPPORT MAY BE CONSTRUCTED WITH CLASS 'A', 'S' OR PAVING CONCRETE. PAYMENT FOR THE JOINT SUPPORT SHALL BE FOR THE CONTRACT UNIT PRICE BID FOR THE CLASS OF CONCRETE SPECIFIED IN THE PLANS. PAYMENT FOR ALL OTHER WORK AND MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE JOINT SUPPORT SHALL BE INCLUDED IN THE PRICE BID FOR THE ABOVE ITEMS.
 - CONTRACTION JOINTS SHALL BE CONSTRUCTED ON 15' CENTERS.
 - TOOLING NOT REQUIRED FOR SELF-LEVELING SILICONE.
 - UNLESS OTHERWISE SPECIFIED IN THE PLANS, CONCRETE SHOULDERS SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN HEREON. CONTRACTION JOINTS SHALL MATCH CONTRACTION JOINTS IN THE LANES.
 - TIE WIRES IN DOWEL BAR ASSEMBLIES SHALL NOT BE CUT PRIOR TO PLACEMENT OF PAVING CONCRETE.



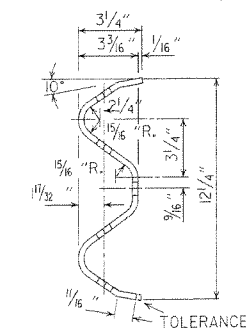
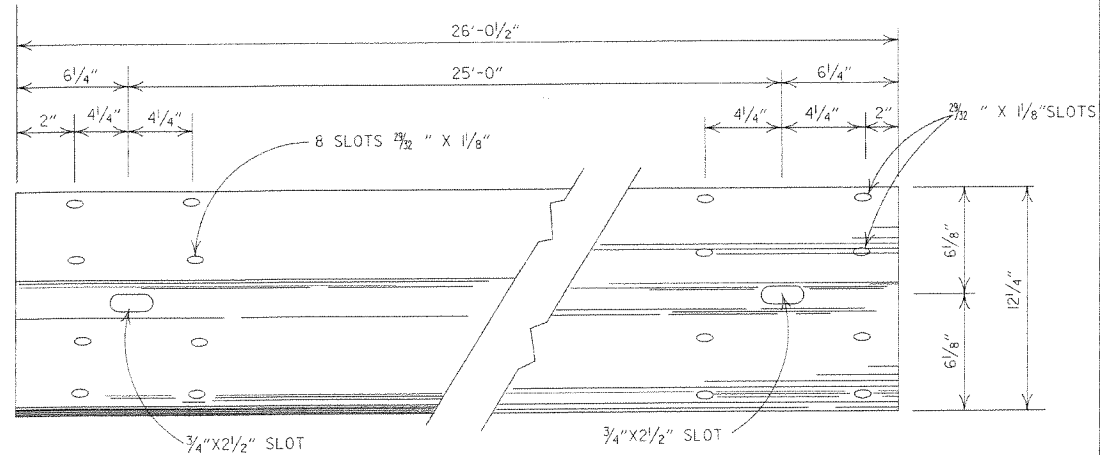
NOTE: TIE BARS SHALL BE 15' FROM TRANSVERSE JOINTS.
LONGITUDINAL CONSTRUCTION JOINT



SECTION
TRANSVERSE CONSTRUCTION JOINT

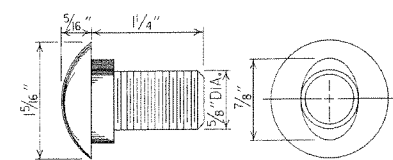
DATE	REVISION	DATE FILMED
5-25-06	ADDED GENERAL NOTE 7	
10-9-03	REMOVED TIE BAR COATING & REVISED GENERAL NOTES	
11-16-01	ADDED TOOL SEALANT AND NOTE 5; REVISED NOTE 3	
4-26-96	REVISED CONTRACTION JOINT NOTE	
11-3-94	ADDED NOTE RE: REINF. BARS	
4-1-93	REVISED DOWEL BARS & GEN. NOTES	4-1-93
10-1-92	REVISED DOWEL SPACING	10-1-92
8-15-91	ADDED SPAC FOR CONTR JTS & DEL KEYWAY	
05-24-90	REVISED TIE BAR, DOWEL & JOINT SIZE	
01-25-90	ADDED EXPANSION JOINT	01-25-90
11-30-89	CHANGED T/4+1 TO T/3+1	11-30-89
03-23-89	ALTERED SAWED JOINT & ADDED NOTE	512-03-23-89
07-15-88	REVISED AND REDRAWN	632-07-15-88

ARKANSAS STATE HIGHWAY COMMISSION
TRANSVERSE & LONGITUDINAL JOINTS FOR CONCRETE PAVEMENT (NON-REINFORCED)
STANDARD DRAWING CPTJ - 6A

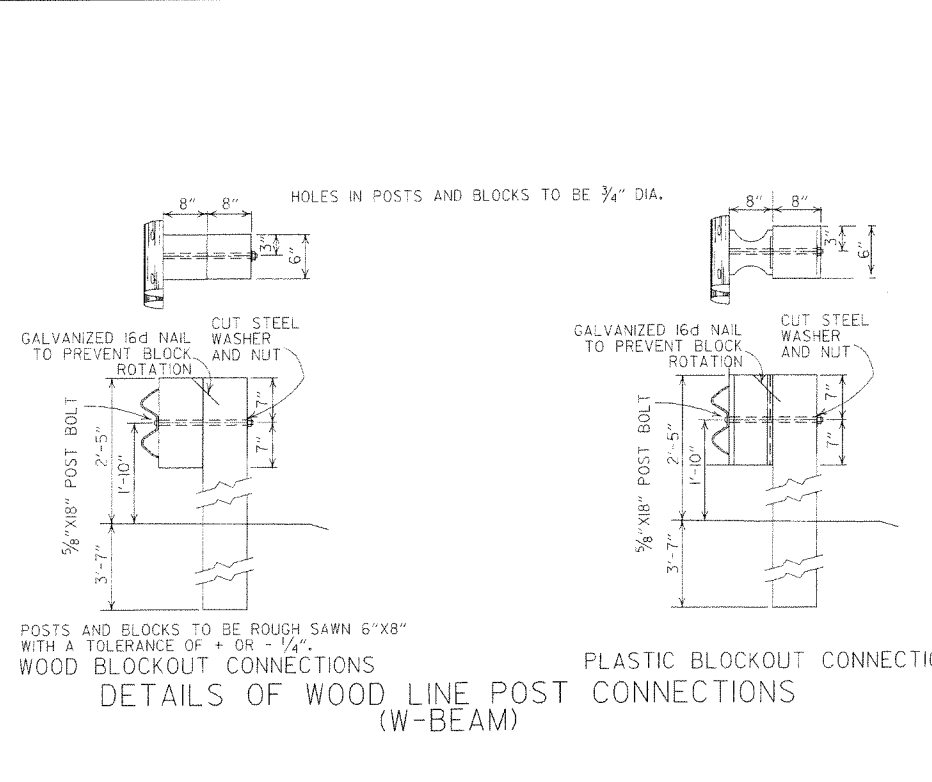
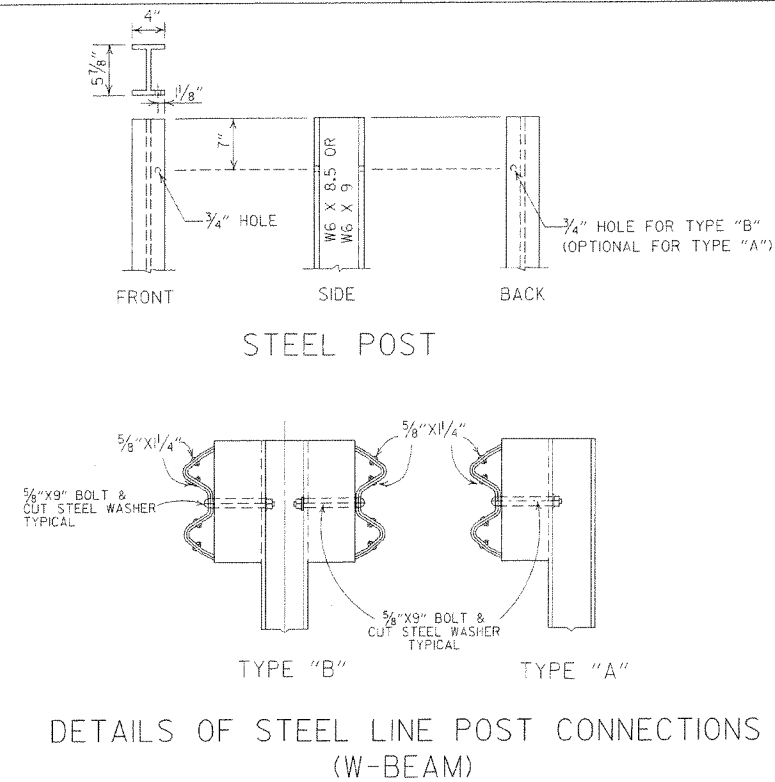
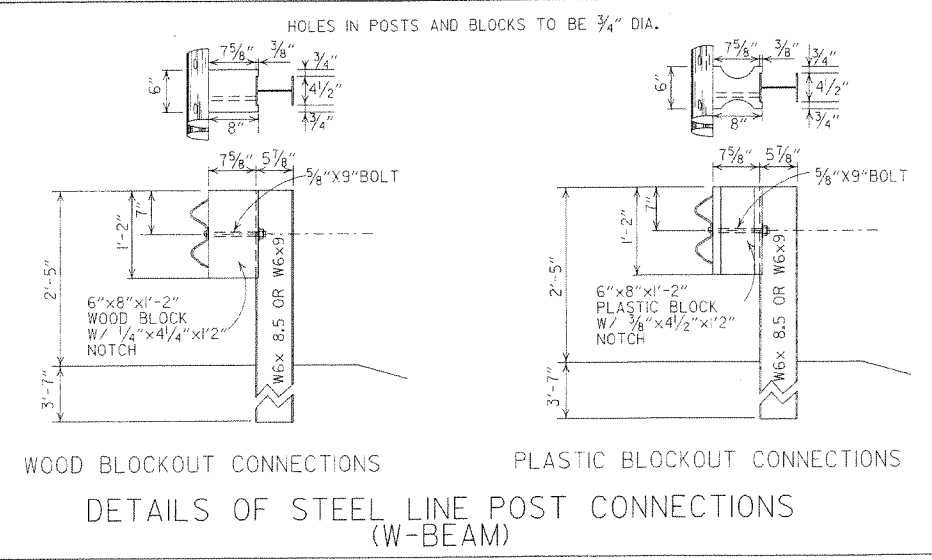
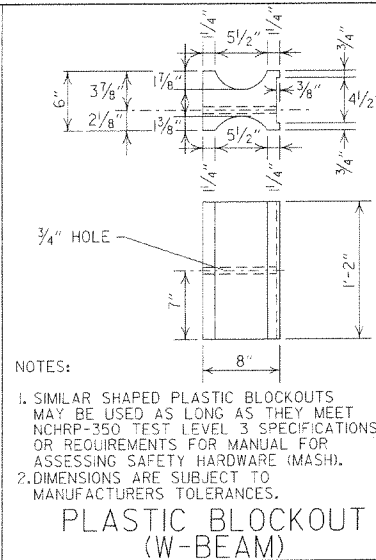
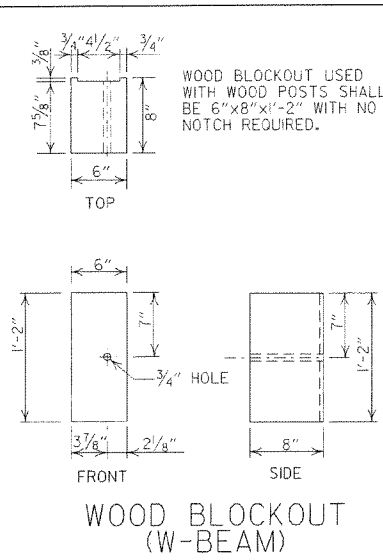
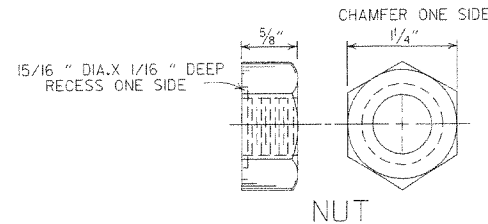
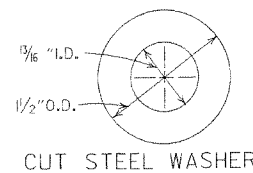


DETAILS OF W-BEAM GUARD RAIL

RAIL SECTION OF CLOSELY SIMILAR DIMENSIONS AND COMPARABLE STRENGTH MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.



SPLICE BOLT
POST BOLT - SAME EXCEPT LENGTH



-GENERAL NOTES-

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.

WHERE W-BEAM GUARD RAIL CONTINUES, THE INTERMEDIATE SECTIONS SHALL HAVE A POST SPACING OF 6'-3" UNLESS OTHERWISE NOTED.

W-BEAM GUARD RAIL REPRESENTING INTERMEDIATE SECTIONS WILL BE MEASURED ALONG THE ROADWAY FACE FROM CENTERLINE OF POST TO CENTERLINE OF POST.

USE W-BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. FOR EXTENSIONS OR MODIFICATION OF EXISTING GUARD RAIL, W-BEAM GUARD RAIL COMPONENTS OF THE SAME TYPE AS THOSE EXISTING SHALL BE USED.

ANY BACKFILLING UNDER OR AROUND POST SHALL BE DAMP SAND THOROUGHLY TAMPED IN PLACE.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (1400 F) OR NO. 1.1350 F SOUTHERN PINE.

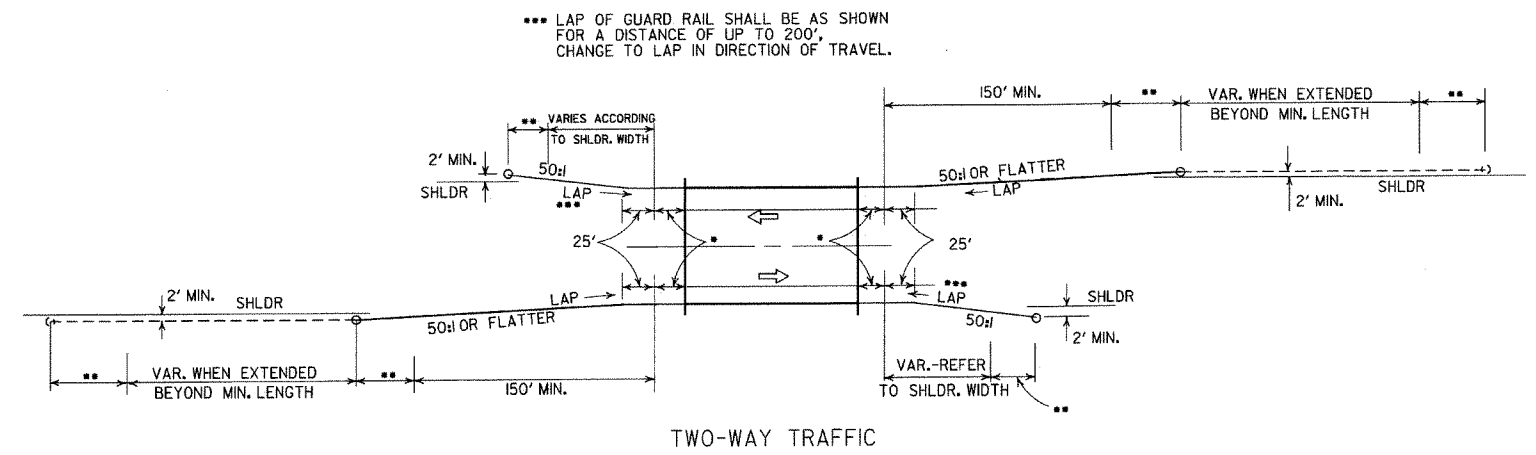
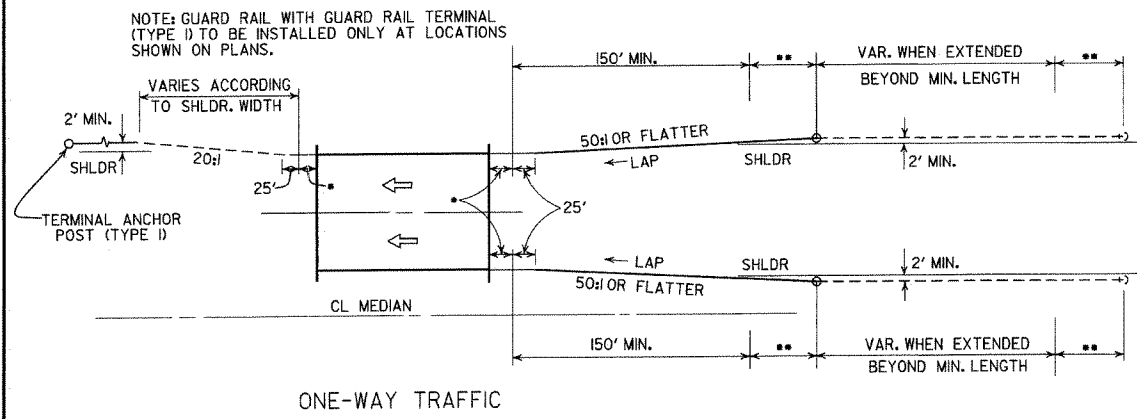
CONTRACTOR SHALL HAVE THE OPTION OF USING WOOD BLOCKOUTS FOR W-BEAM GUARD RAIL OR PLASTIC BLOCKOUTS, AS LONG AS BLOCKOUT USED MEETS NCHRP-350 TEST LEVEL 3 SPECIFICATIONS OR REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) FOR W-BEAM GUARD RAIL.

7-4-10	RAISED HEIGHT OF GUARD RAIL 1"	
10-15-09	ADDED REFERENCE TO MASH	
4-10-03	REVISED GENERAL NOTES	
8-22-02	REVISED DIMENSION ON WOOD & PLASTIC BLOCKOUT CONNECTIONS & ON STEEL POST	
11-16-01	REVISED WOOD BLOCKOUT & DETAILS OF WOOD LINE POST CONNECTIONS	
3-30-00	REMOVED GUARD RAIL AT BRIDGE ENDS	
1-12-00	ADDED PLASTIC BLOCKOUT	
8-12-98	REV. BLOCKOUTS TO WOOD, DELETED CONC. POST & REV. GENERAL NOTE, DELETED DET. OF GUARD RAIL REPLACE. BEHIND CURB & DET. OF POST PLACE. IN SOLID ROCK, & ADDED DETAILS OF STEEL LINE POST CONN. REMOVED BACK-UP PLATE, REVISED HOLES IN STEEL POLES	
4-3-97	REMOVED "LAP IN DIRECTION OF TRAFFIC" NOTE & PLACED ARROWS ON WASHERS	
10-18-96	REVISED WOOD POST NOTE	
6-2-94	ADDED ALT. STEEL POST SIZE	
8-5-93	REVISED STEEL POST SIZE	8-5-93
10-1-92	REDRAWN & REVISED	10-1-92
8-15-91	REVISED WASHER NOTE	8-5-91
8-2-90	REV. GEN. NOTE & DET. OF ANC. POST IN ROCK	8-2-90
7-5-88	REVISED SECTION 3 & GENERAL NOTES	
3-4-88	REV. ANCHOR POST ELEV. NOTES & POST IN ROCK	780-3-4-88
10-30-87	REVISED WOOD LINE POST DETAIL	546-10-30-87
10-9-87	REDRAWN & REVISED	802-10-9-87
DATE	REVISION	DATE FILM

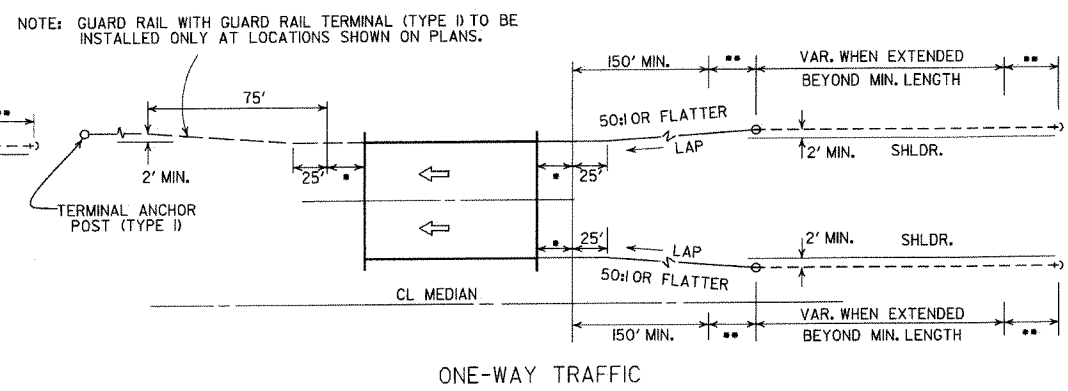
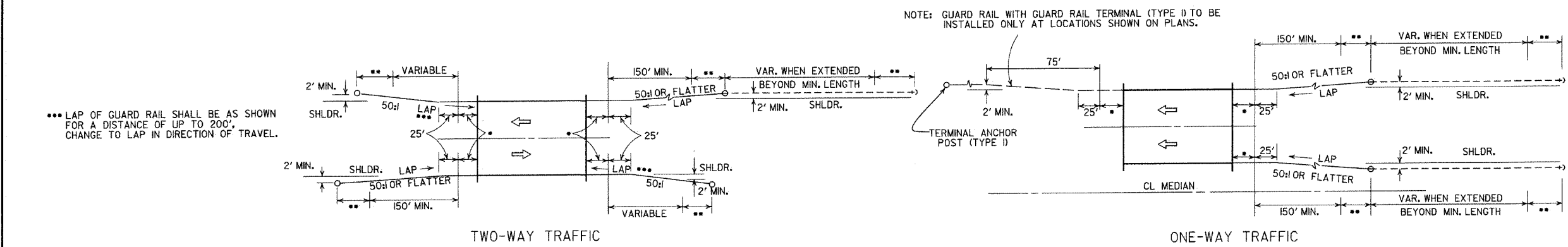
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

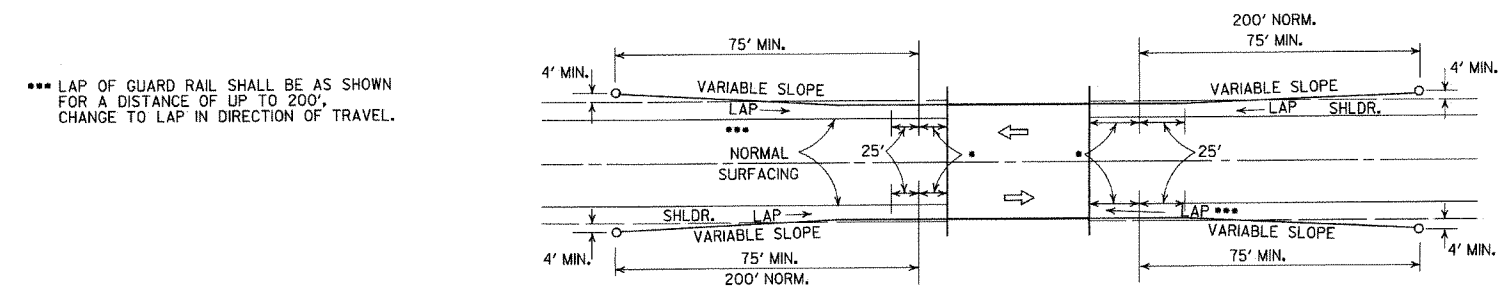
STANDARD DRAWING GR-8



METHODS OF INSTALLATION OF GUARD RAIL AT LESS THAN FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)



METHOD OF INSTALLATION OF GUARD RAIL AT FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)

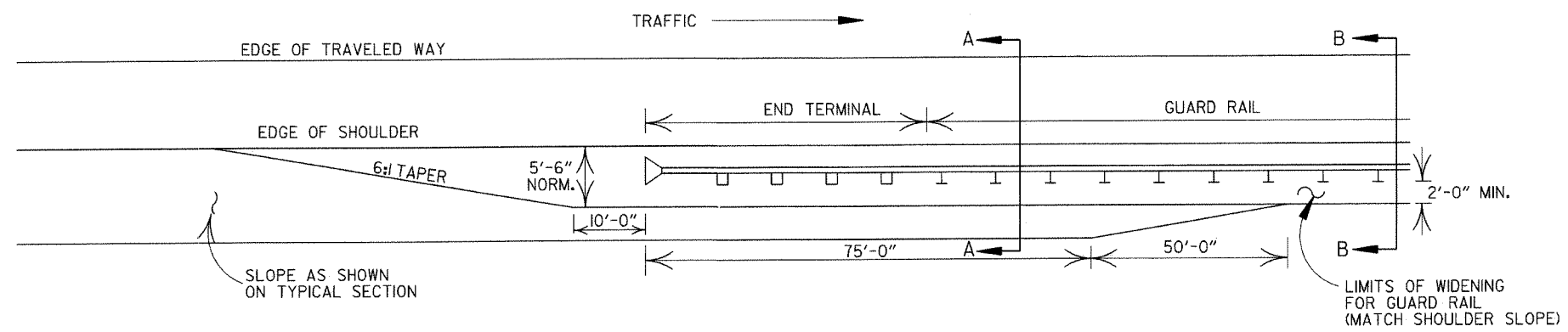


LEGEND

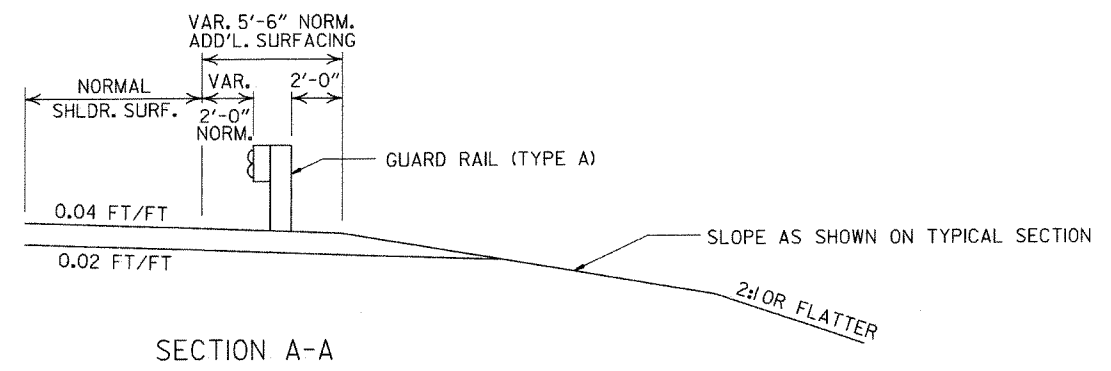
- THREE BEAM GUARD RAIL TERMINAL
- GUARD RAIL TERMINAL (TYPE 2)

METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERMINAL (TYPE I) (FULL SHOULDER WIDTH OR LESS BRIDGES)

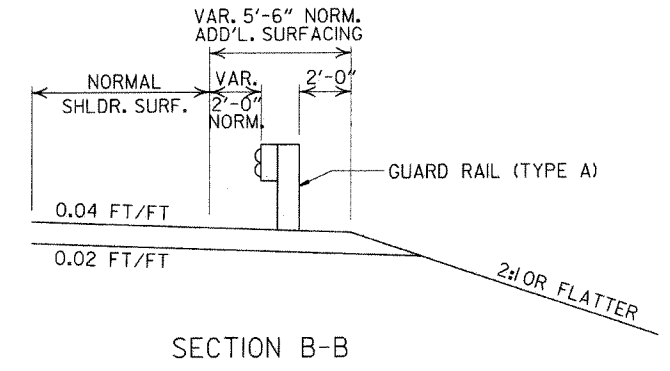
ARKANSAS STATE HIGHWAY COMMISSION		
GUARD RAIL DETAILS		
STANDARD DRAWING GR-9		
4-17-08	REVISED LAYOUTS	
11-10-05	REMOVED GUARD RAIL NOTES AND DETAILS	
11-16-01	DELETED NOTE-METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERM. (TY. I)	
1-12-00	ADDED CONSTRUCTION NOTE	1-12-00
6-26-97	REVISED LAYOUT	
10-1-92	REDRAWN & REVISED	10-1-92
10-9-87	ADDED NOTE	
10-9-87	REDRAWN & REVISED	
DATE	REVISION	DATE FILM



NOTE: NORMAL SECTION TO BE WIDENED APPROX. 5'-6" EACH SIDE TO SUPPORT GUARD RAIL.

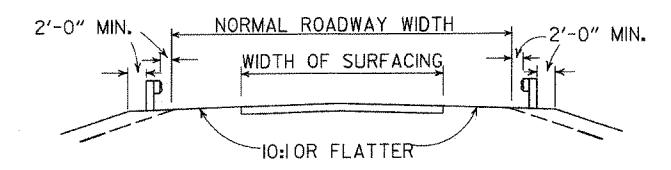


SECTION A-A

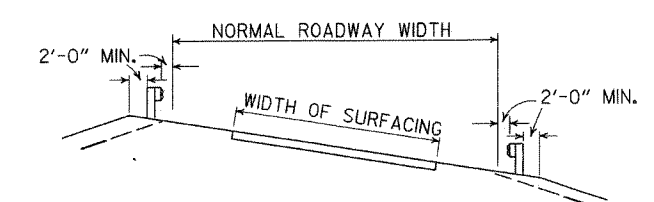


SECTION B-B

DETAILS OF WIDENING FOR GUARD RAIL

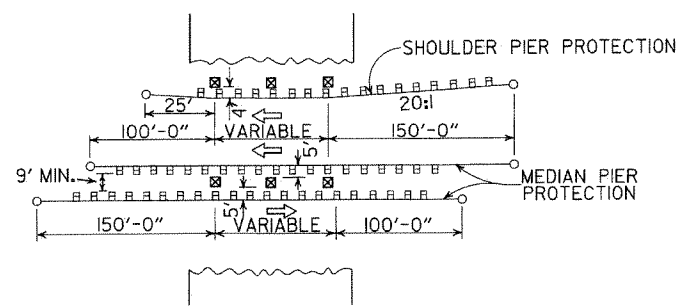


SECTION ON TANGENT



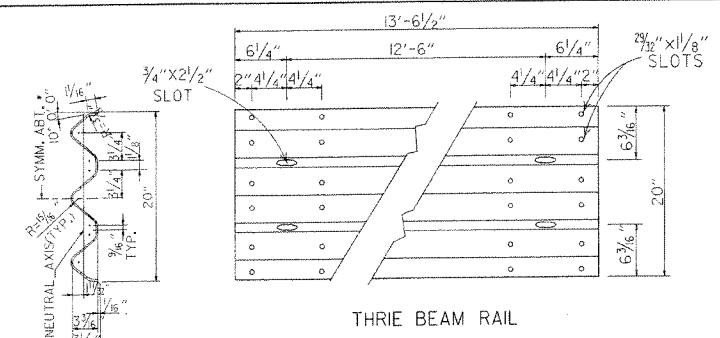
SECTION ON CURVE

DETAILS SHOWING POSITION OF GUARD RAIL ON HIGHWAY

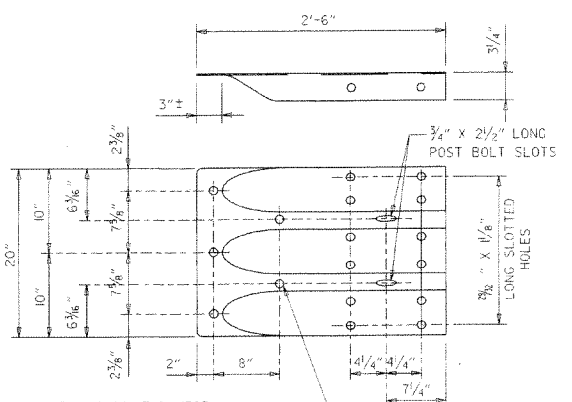


METHOD OF INSTALLATION OF GUARD RAIL AT FIXED OBSTACLE

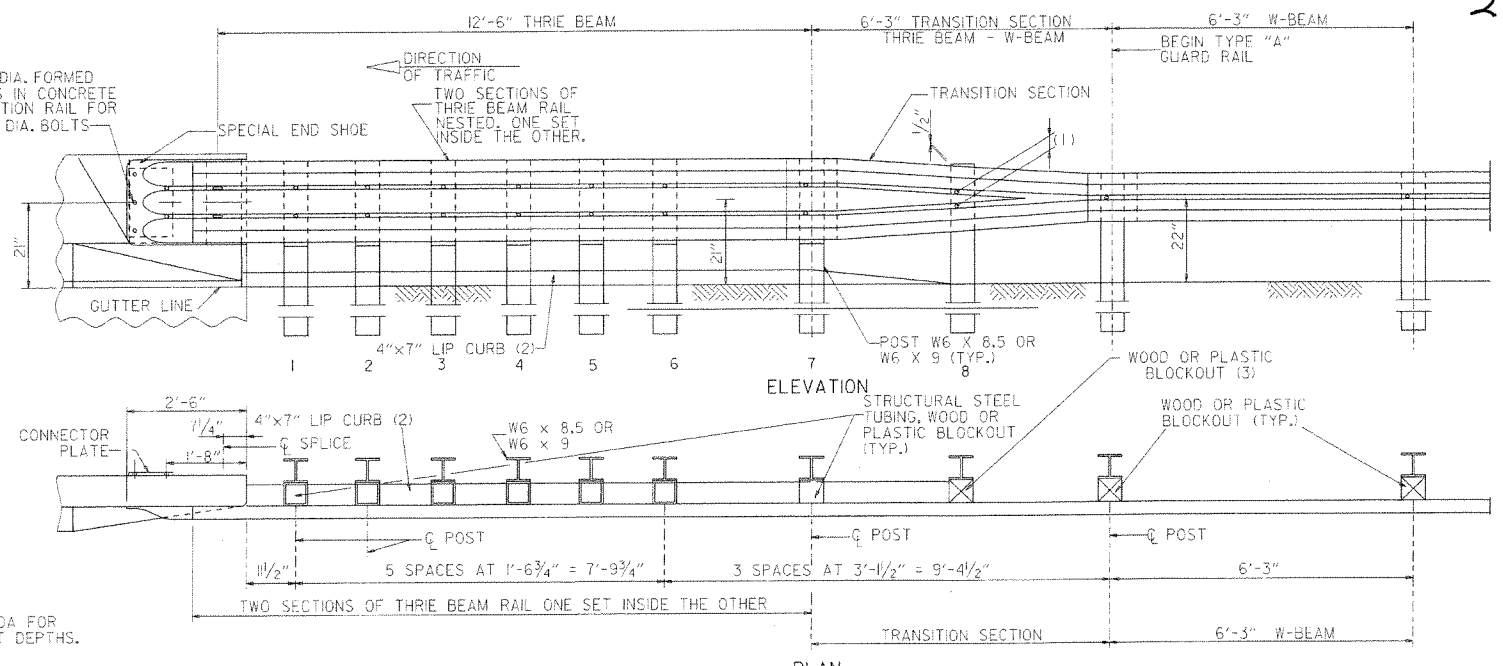
ARKANSAS STATE HIGHWAY COMMISSION			
GUARD RAIL DETAILS			
STANDARD DRAWING GR-9A			
4-17-08	MINOR REVISION		
11-10-05	DRAWN		
DATE	REVISION	DATE	FILM



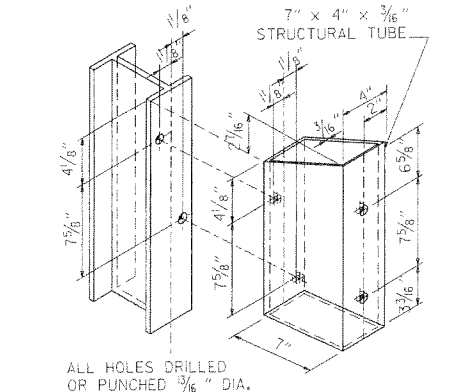
SECTION THRU THRIE BEAM RAIL



SPECIAL END SHOE

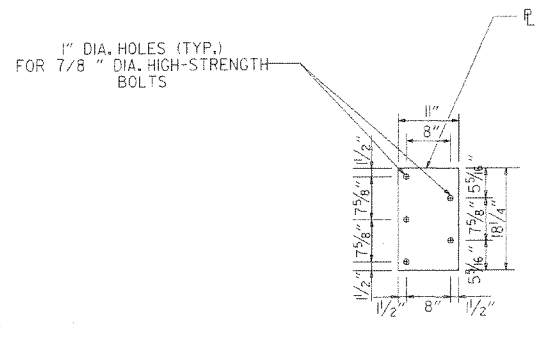


ELEVATION



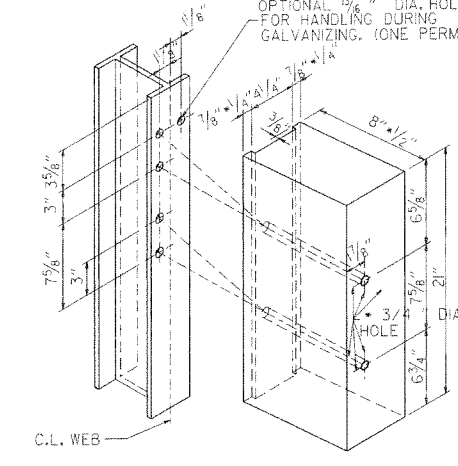
STRUCTURAL STEEL TUBING BLOCKOUT DETAIL

ATTACH BLOCKOUT TO POST USING 5/8" DIA. HEX HEAD BOLTS WITH 1/2" O.D. CUT STEEL WASHERS AND NUT.



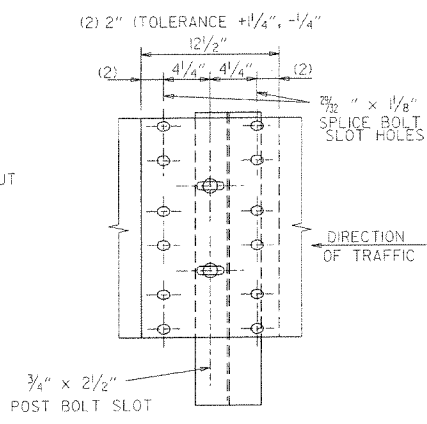
CONNECTOR PLATE

CONNECTOR PLATE SHALL BE AASHTO M270, GR. 36 AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING SHALL CONFORM TO SUBSECTION 807.19 OF THE STANDARD SPECIFICATIONS. CONNECTOR PLATE TO BE BOLTED TO SPECIAL END SHOE USING 5/8" DIA. HIGH STRENGTH BOLTS, WITH THE HEADS PLACED ON THE TRAFFIC FACE. WASHERS SHALL BE USED UNDER THE HEAD AND NUT. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED AND SHALL CONFORM TO SUBSECTION 807.06.

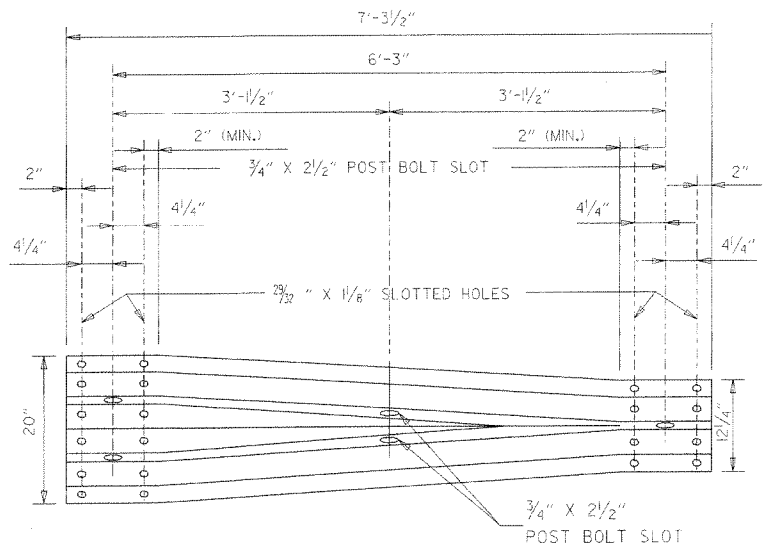


HOLE PUNCHING DETAIL FOR STEEL POST & WOOD OR PLASTIC BLOCKOUTS

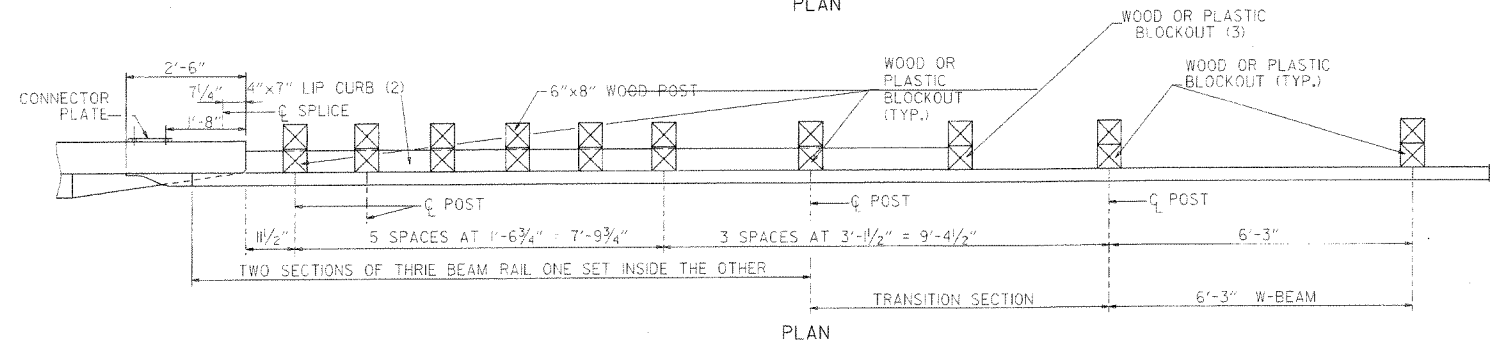
NOTE: BLOCKS SHALL BE THE SAME TYPE THROUGHOUT THE PROJECT LIMITS.



THRIE BEAM RAIL SPLICE AT POST



TRANSITION SECTION



PLAN

PLAN

- (1) VERIFY BOLT SPACING FROM RAIL TRANSITION PRODUCER.
- (2) REFER TO APPROACH GUTTER DETAILS.
- (3) LENGTH OF BLOCKOUT ON POST 8 TO BE MODIFIED TO FIT RAIL WIDTH.

THRIE BEAM GUARD RAIL CONNECTION AT BRIDGE ENDS

GENERAL NOTES:

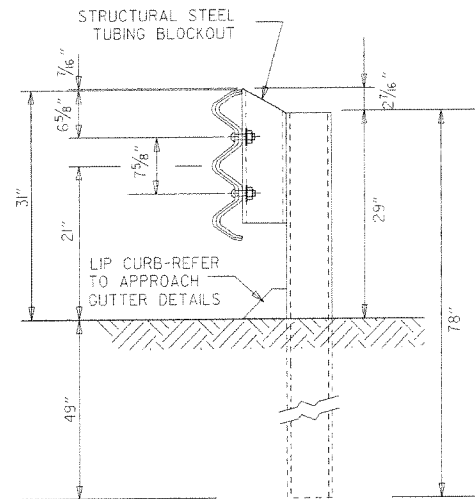
THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE I. RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION. ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT. ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-9 & GR-11. WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (1400 F) OR NO. 1 1350 F SOUTHERN PINE. REFER TO STD. DRWG. GR-10A FOR POST DETAILS. USE THRIE BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W-BEAM POSTS FOR ENTIRE JOB.

DATE	REVISION	DATE FILM
7-14-10	RAISED HEIGHT OF W-BEAM 1"	
11-29-07	ADDED PLASTIC BLOCKOUTS	
11-10-05	ADDED NOTE FOR ATTACHING STEEL BLOCKOUT	
11-18-04	REVISED GENERAL NOTES	
10-9-03	REVISED GENERAL NOTES	
4-10-03	REVISED GENERAL NOTES	
8-22-02	REVISED NOTE (2)	
6-29-00	MOVED DIMENSION LINES	
5-18-00	ADDED NOTE	
3-30-00	DRAWN & ISSUED	

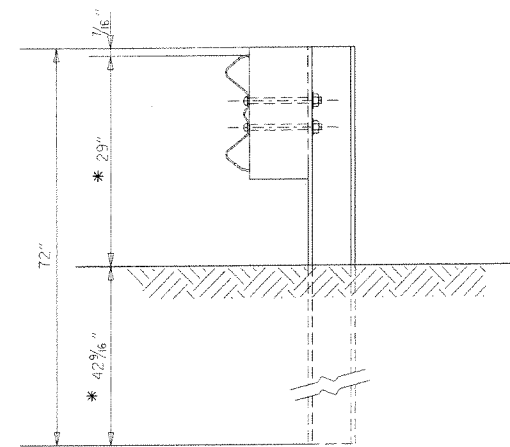
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

STANDARD DRAWING GR-10

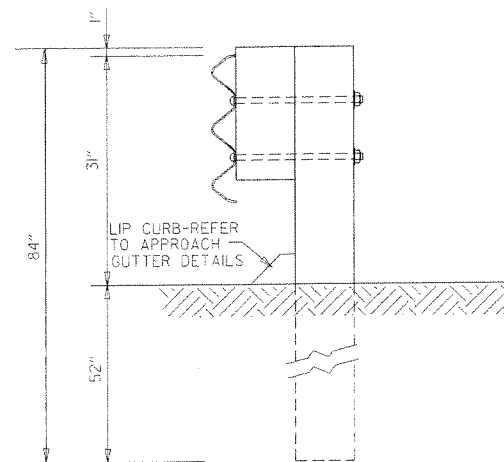


THRIE BEAM RAIL WITH STEEL TUBING BLOCKOUT AND STEEL POST POSTS 1-7

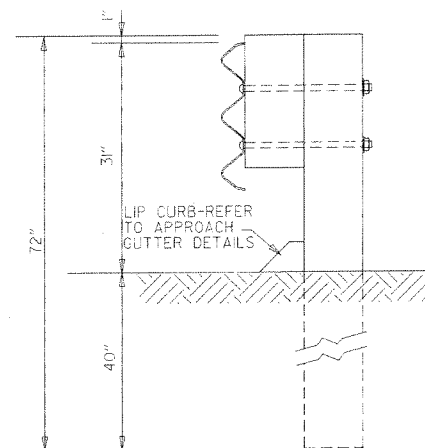


W-BEAM TO THRIE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT AND STEEL POST POST 8

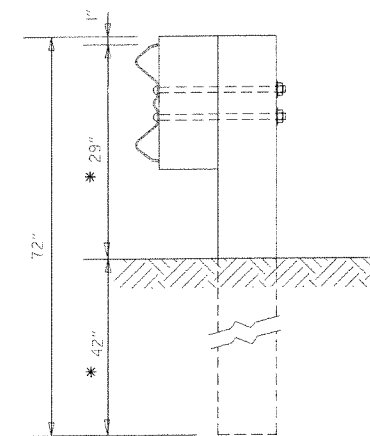
* NOTE:
THESE DIMENSIONS WILL NEED TO BE ADJUSTED IN THE FIELD TO MAKE THE TRANSITION FROM 21" MID POINT OF THRIE BEAM TO 22" MID POINT OF W-BEAM.



THRIE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUTS & WOOD POSTS POSTS 1-6



THRIE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST POST 7



W-BEAM TO THRIE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST POST 8

GENERAL NOTES:
RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.

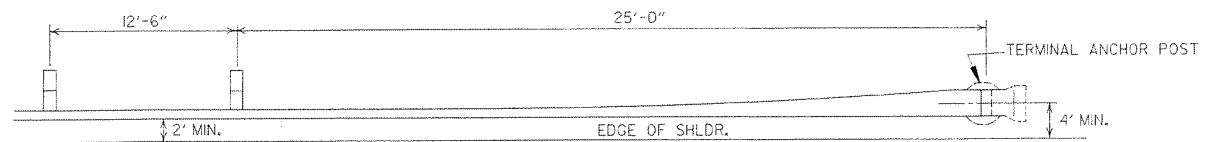
WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 2.7 f (400 f) OR NO. 1 (350 f) SOUTHERN PINE.

DATE	REVISION	DATE FILM
7-14-10	REVISED POST 8 DIMENSIONS	
11-29-07	ADDED PLASTIC BLOCKOUTS	
8-22-02	REVISED LIP CURB NOTE	
3-30-00	DRAWN & ISSUED	

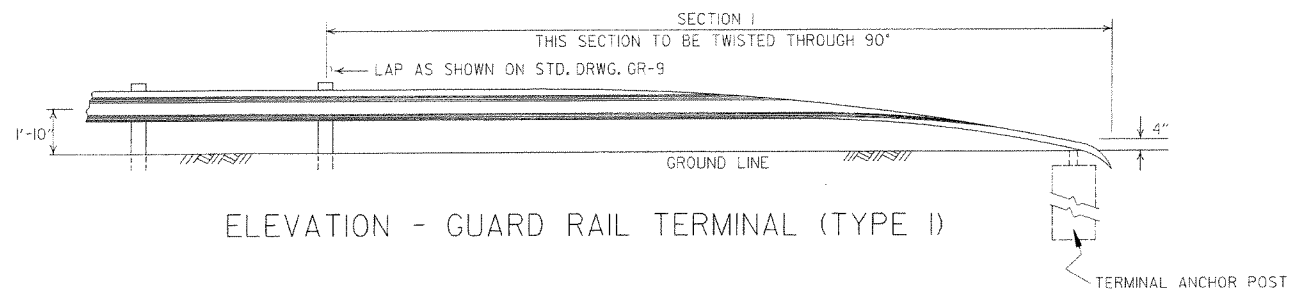
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

STANDARD DRAWING GR-10A

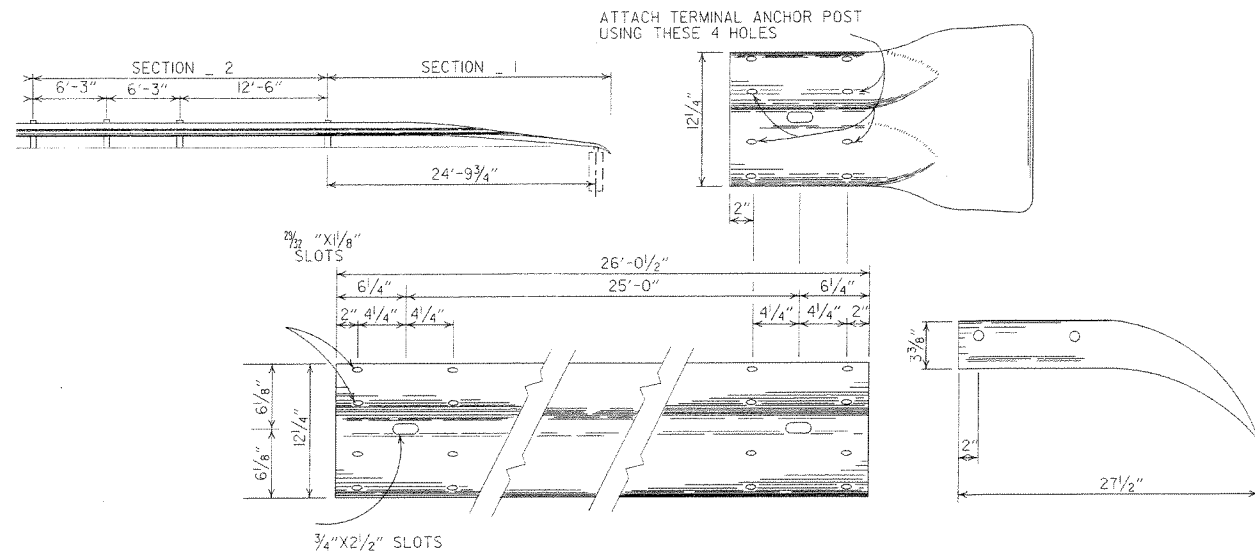


PLAN - GUARD RAIL TERMINAL (TYPE I)



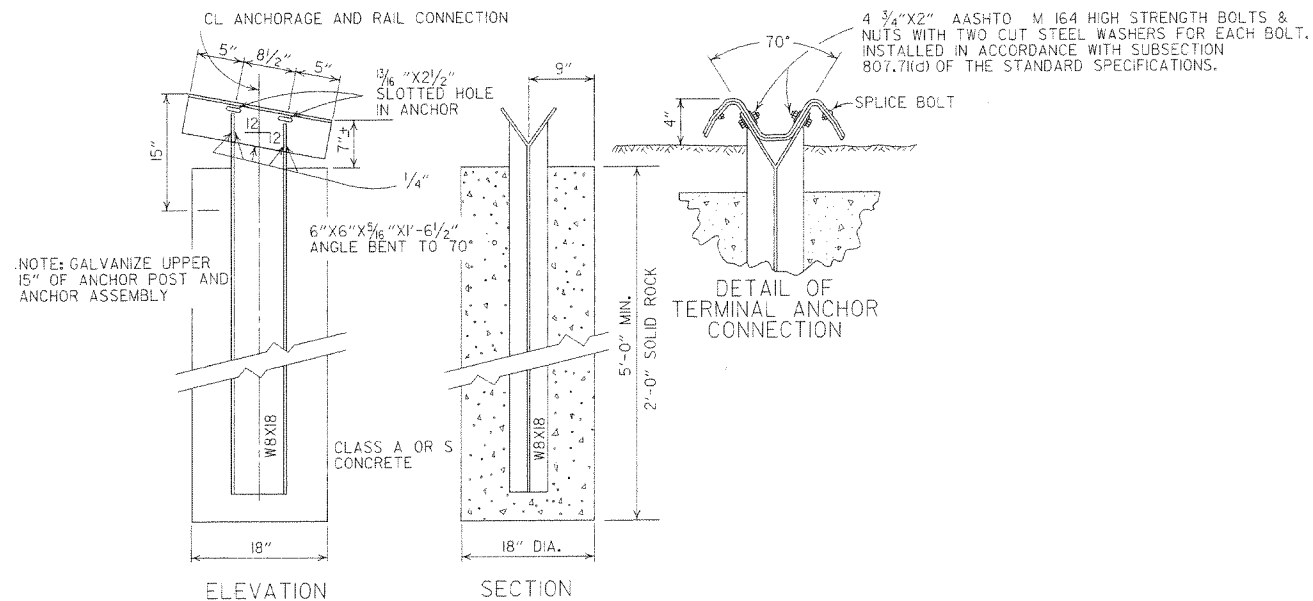
ELEVATION - GUARD RAIL TERMINAL (TYPE I)

NOTE:
SECTIONS 1 AND 2 OF GUARD RAIL TERMINAL SHALL BE PAID FOR AT THE PRICE BID PER LINEAR FOOT OF THE TYPE OF GUARD RAIL SPECIFIED.



SECTION 1

TERMINAL SECTION



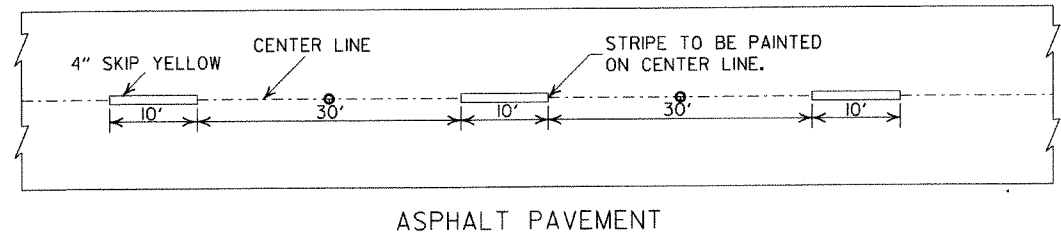
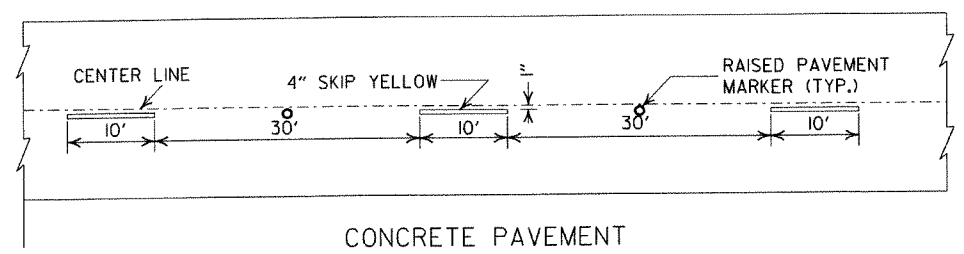
DETAIL OF TERMINAL ANCHOR POST (TYPE I)

NOTE: RAIL MEMBERS MAY BE BOLTED TO ANGLE AT TERMINAL ANCHOR AND THE TWO ASSEMBLIES POSITIONED TO PROPER ALIGNMENT PRIOR TO PLACING CONCRETE AROUND 8 W 17 POST IF CONTRACTOR SO DESIRES.

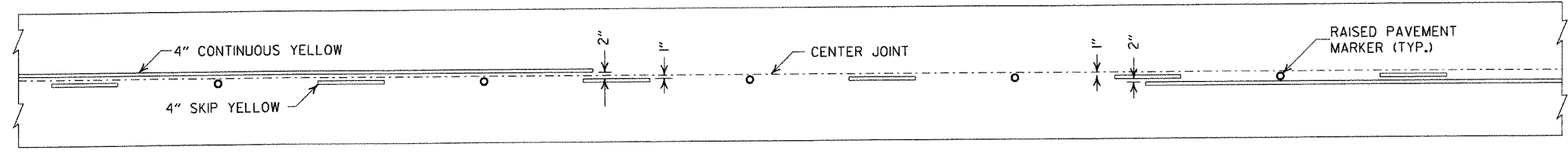
			ARKANSAS STATE HIGHWAY COMMISSION
			GUARD RAIL DETAILS
			STANDARD DRAWING GRT-1
7-14-10	RAISED HEIGHT OF GUARD RAIL		
6-26-97	REVISED LAP NOTE		
10-18-96	REVISED ASTM REF. TO AASHTO		
11-3-94	DIMENSION TERMINAL DETAIL		
11-11-92	ADDED NOTE FOR PAYMENT	11-11-92	
10-1-92	DRAWN & ISSUED	10-1-92	
DATE	REVISION	DATE	FILM

NOTES:

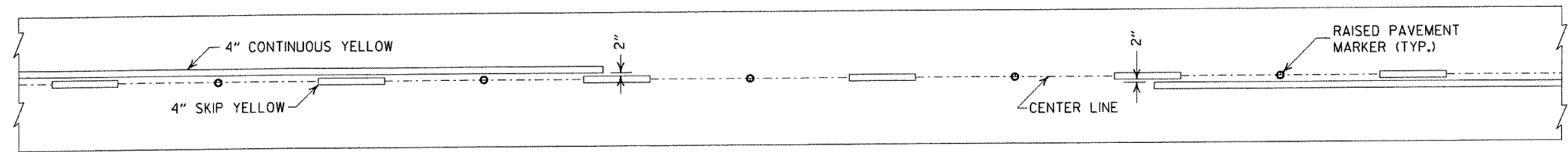
1. ALL LINES SHALL HAVE A WIDTH OF 4 INCHES.
2. THE THICKNESS AND RATE OF PAINT APPLICATION SHALL BE AS SPECIFIED IN SECTION 718 OF THE STANDARD SPECIFICATIONS.
3. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. RAISED PAVEMENT MARKERS SHALL BE CENTERED BETWEEN SKIP LINES ON 40 FEET SPACING UNLESS OTHERWISE SHOWN ON THE PLANS.



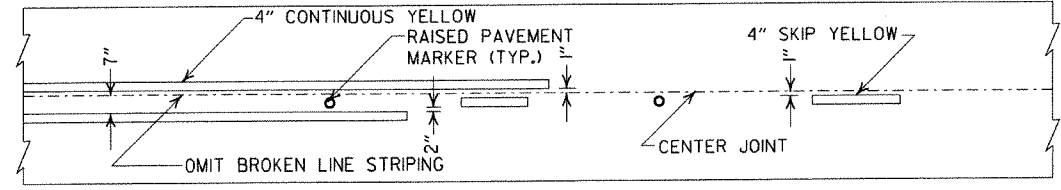
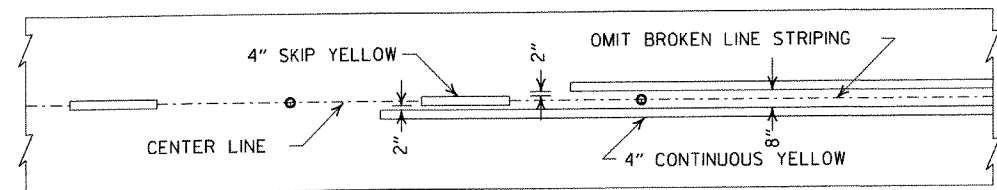
BROKEN LINE STRIPING



SOLID LINE STRIPING ON CONCRETE PAVEMENT



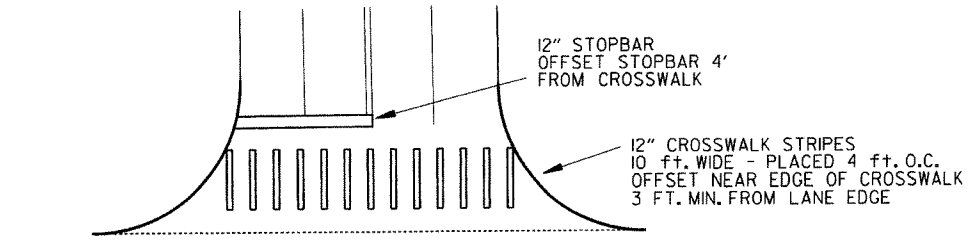
SOLID LINE STRIPING ON ASPHALT PAVEMENT



ASPHALT PAVEMENT

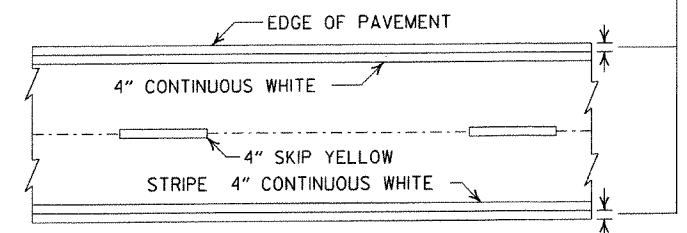
CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

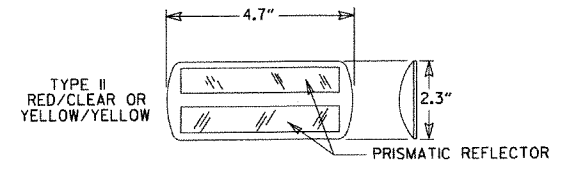


CROSSWALK AND STOPBAR DETAILS

2" FOR ASPHALT OR CONCRETE PAVEMENT
6" FOR BITUMINOUS SURFACE TREATMENT



PAVEMENT EDGE LINE MARKING



NOTE:
THE RED LENS OF THE
TYPE II R.P.M. SHALL
FACE THE INCORRECT
TRAFFIC MOVEMENT.

DETAIL OF
STANDARD
RAISED PAVEMENT MARKERS

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND RAISED PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION	
PAVEMENT MARKING DETAILS	
STANDARD DRAWING PM-1	

SUPERELEVATION TABLE FOR TWO - WAY TRAFFIC

DEGREE OF CURVE	30 MPH		40 MPH		50 MPH		55 MPH		60 MPH		70 MPH	
	Ls (FT)		Ls (FT)		Ls (FT)		Ls (FT)		Ls (FT)		Ls (FT)	
	MINIMUM	DESIRABLE	MINIMUM	DESIRABLE	MINIMUM	DESIRABLE	MINIMUM	DESIRABLE	MINIMUM	DESIRABLE	MINIMUM	DESIRABLE
0° 15'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.	
0° 30'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.	
0° 45'	N.C.		N.C.		N.C.		0.022		0.023		0.028	
1° 00'	N.C.		N.C.		0.021		0.026		0.030		0.037	
1° 15'	N.C.		N.C.		0.026		0.032		0.037		0.046	
1° 30'	N.C.		0.021		0.031	200	0.036	225	0.043	250	0.054	300
1° 45'	N.C.		0.025	175	0.036	200	0.043	300	0.049	300	0.062	
2° 00'	R.C.		0.028		0.040		0.048		0.055		0.070	
2° 15'	R.C.		0.031		0.045	250	0.053		0.061		0.078	300
2° 30'	R.C.		0.034		0.049		0.058		0.067		0.085	350
2° 45'	0.023		0.037		0.053		0.063		0.072		0.091	350
3° 00'	0.025	150	0.040		0.057		0.067	230	0.077	260	0.096	350
3° 15'	0.027		0.043	200	0.061		0.072	245	0.082	275	0.100	400
3° 30'	0.029		0.046		0.065	205	0.076	255	0.086	285		
3° 45'	0.031	200	0.049		0.069	215	0.080	265	0.090	295		
4° 00'	0.033		0.051		0.072	225	0.083	270	0.093	305		
4° 30'	0.037		0.056		0.078	240	0.087	280	0.096	315		
5° 00'	0.040		0.061		0.083	250	0.091	295	0.098	320		
5° 30'	0.043		0.066	185	0.088	260	0.094	300				
6° 00'	0.046		0.070	190	0.092	270	0.096	305				
6° 30'	0.050		0.074	200	0.095	280	0.100	315				
7° 00'	0.053		0.078	210	0.098	285						
7° 30'	0.056		0.081	215	0.099	290						
8° 00'	0.058		0.084	220	1.00	290						
8° 30'	0.061		0.087	225								
9° 00'	0.063		0.089	230								
10° 00'	0.068	160	0.094	235								
11° 00'	0.072	170	0.097	250								
12° 00'	0.076	175	0.099	250								
13° 00'	0.080	180	0.100	250								
14° 00'	0.083	190										
15° 00'	0.086	195										
16° 00'	0.089	200										
17° 00'	0.091	200										
18° 00'	0.093	205										
19° 00'	0.095	210										
20° 00'	0.097	215										
21° 00'	0.098	215										
22° 00'	0.099	215										
23° 00'	0.099	215										
24° 00'	0.100	220										

D MAX = 24° 45'

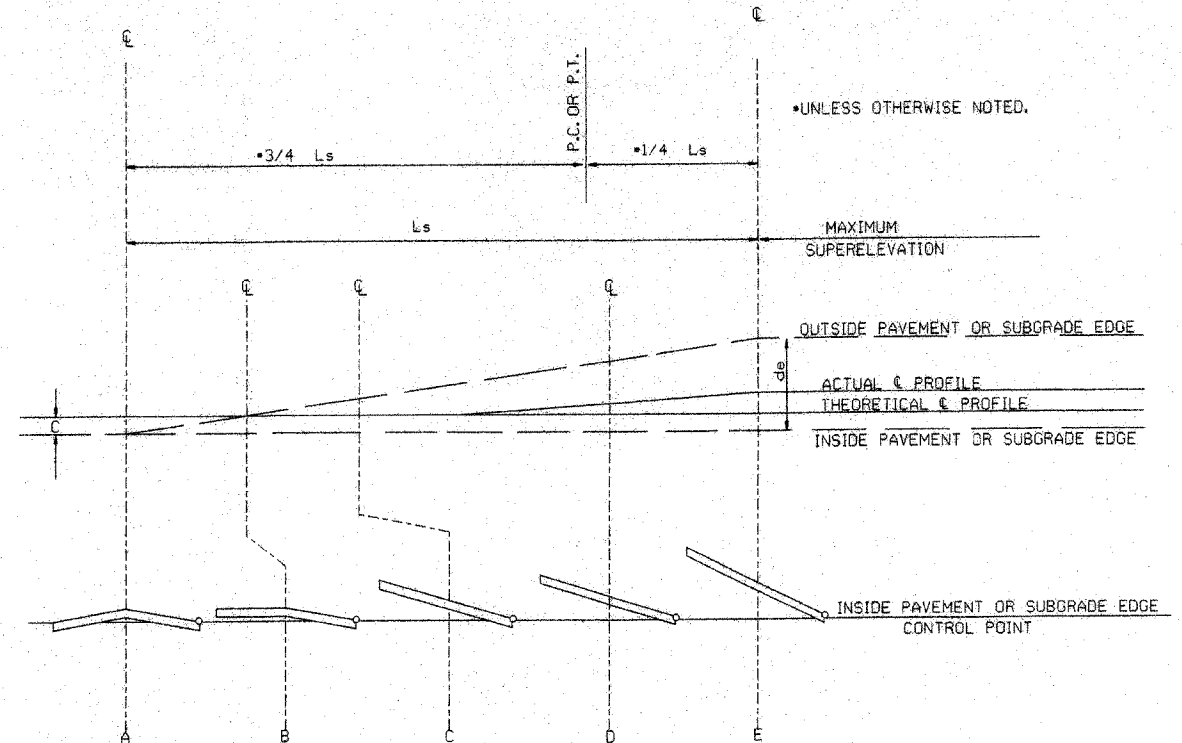
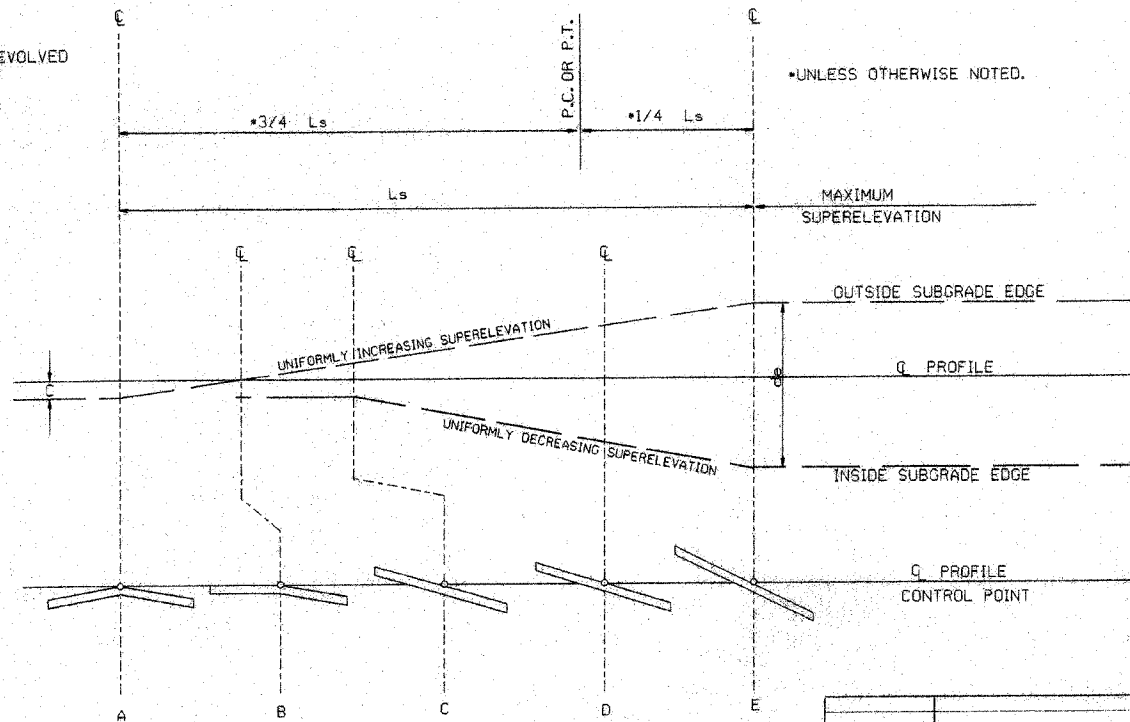
ABBREVIATIONS

- NC - NORMAL CROWN
- RC - REVERSE CROWN, SUPERELEVATION AT NORMAL CROWN SLOPE
- e - RATE OF SUPERELEVATION (FT. PER FT.)
- Ls - LENGTH OF SUPERELEVATION TRANSITION (FT.)
- L - DISTANCE FROM BEGINNING OF SUPERELEVATION TRANSITION TO ANY POINT (FT.)
- d - WIDTH OF PAVEMENT (FT.) OR WIDTH OF SUBGRADE (FT.)
- C - NORMAL CROWN (FT.)

GENERAL NOTES

1. ON PAVEMENT WITH TWO-WAY TRAFFIC, THE SUPERELEVATION SHALL BE REVOLVED ON THE INSIDE PAVEMENT EDGE UNLESS OTHERWISE NOTED ON THE PLANS
2. SUPERELEVATION VALUES SHOWN ON THE CROSS SECTIONS ARE VALUES (+) OR (-) TO BE ADDED TO OR SUBTRACTED FROM THE POINT OF CONTROL.
3. LENGTHS FOR L MAY BE ROUNDED IN MULTIPLES OF 25 FT. OR 50 FT. TO PERMIT SIMPLER CALCULATIONS.
4. PAVEMENTS WIDER THAN 2 LANES SHALL HAVE ADDITIONAL TRANSITION LENGTHS AS FOLLOWS:
 - 3 LANE UNDIVIDED ----- +20%
 - 4 LANE UNDIVIDED ----- +50%
 - 3 LANE UNDIVIDED ----- +80%
 - 6 LANE UNDIVIDED ----- +100%

NOTE: MAINTAIN NORMAL CROWN ON INSIDE UNTIL SUPERELEVATION EXCEEDS 2C.
RATE OF SUPERELEVATION SHALL BE COMPUTED ON STRAIGHT LINE METHOD USING APPLICABLE Ls.



NOTE: MAINTAIN NORMAL CROWN ON INSIDE UNTIL SUPERELEVATION EXCEEDS 2C.

SUPERELEVATION FORMULA = $\frac{Lde}{Ls}$

ARKANSAS STATE HIGHWAY COMMISSION	
TABLES AND METHOD OF SUPERELEVATION FOR TWO-WAY TRAFFIC	
STANDARD DRAWING SE-2	

10-18-96	ADDED FORMULA	10-18-96
01-09-87	ISSUED	534-1-9-87
DATE	REVISION	DATE FILMED

500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD


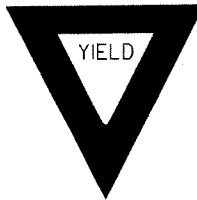
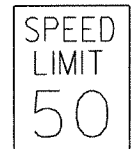
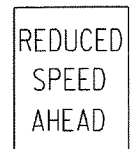

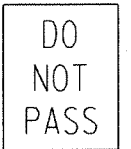

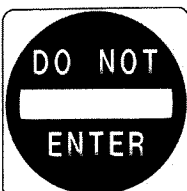

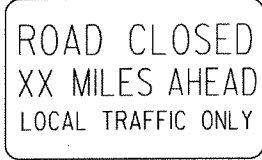
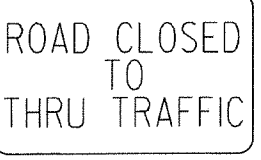

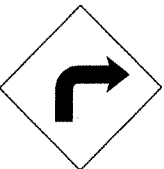
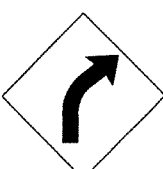


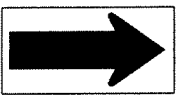

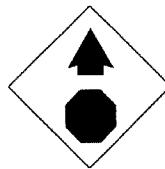
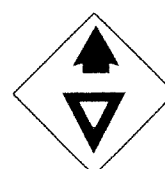
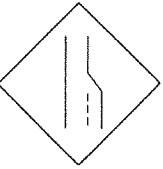

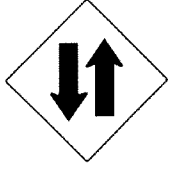

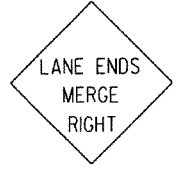


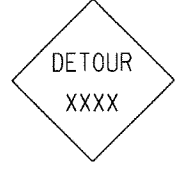


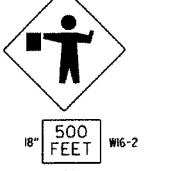


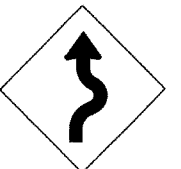


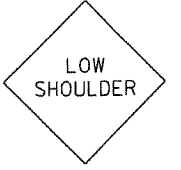
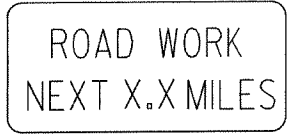
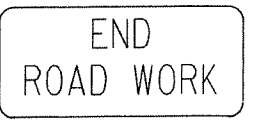
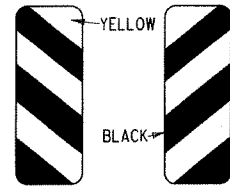
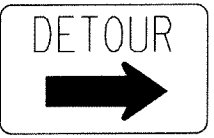

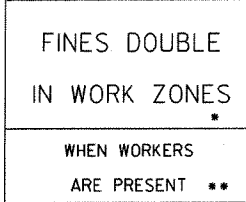
GENERAL NOTES:

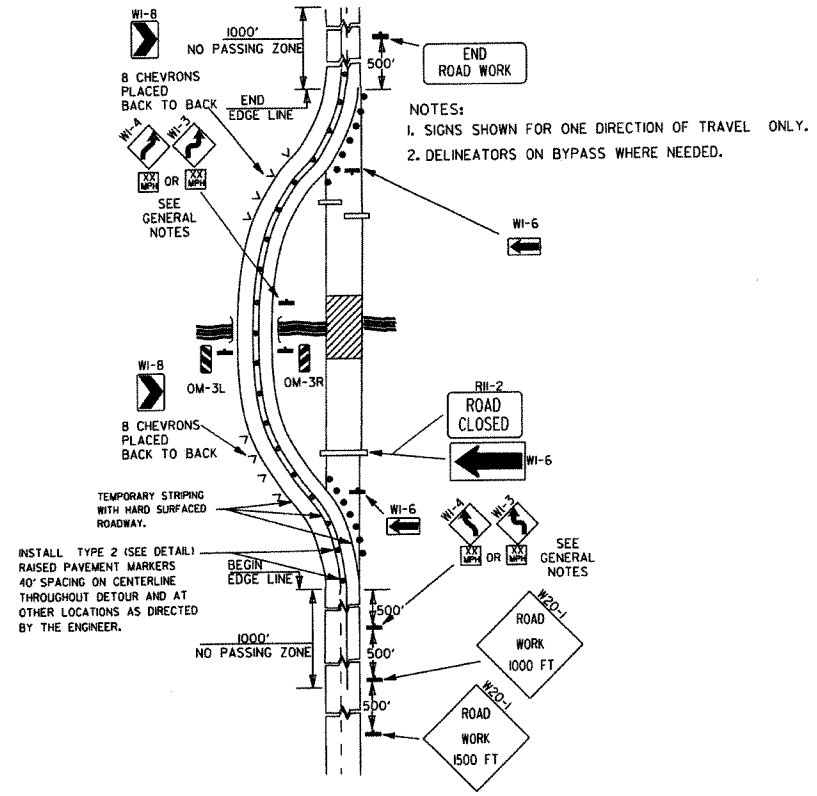
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

* NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

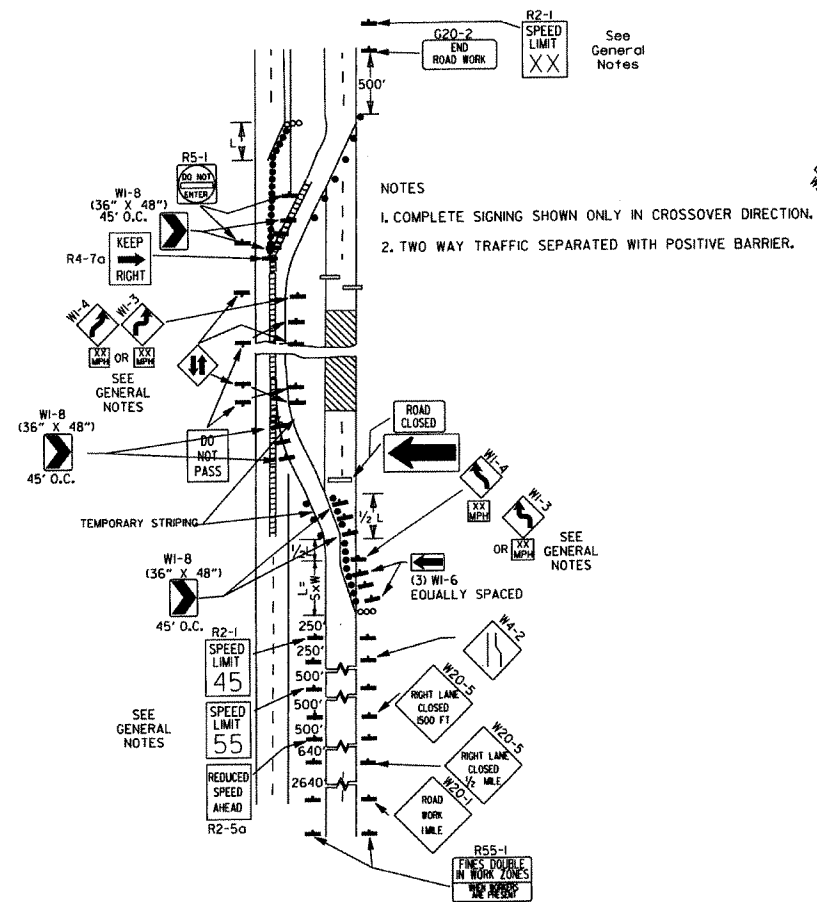
12-5-8	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-5-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-1

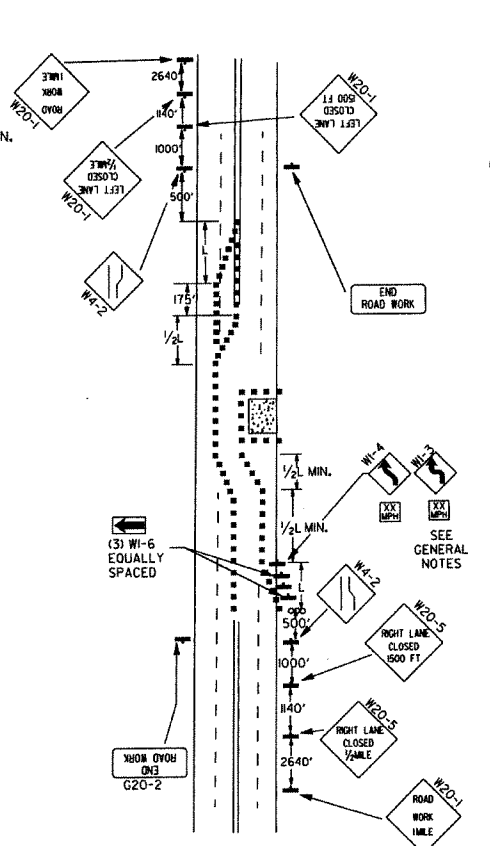
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>
						<p>R55-1</p>  <p>36"x60"</p> <p>WHEN WORKERS ARE PRESENT **</p> <p>* USE 6" C LETTERS ** USE 4" D LETTERS</p>



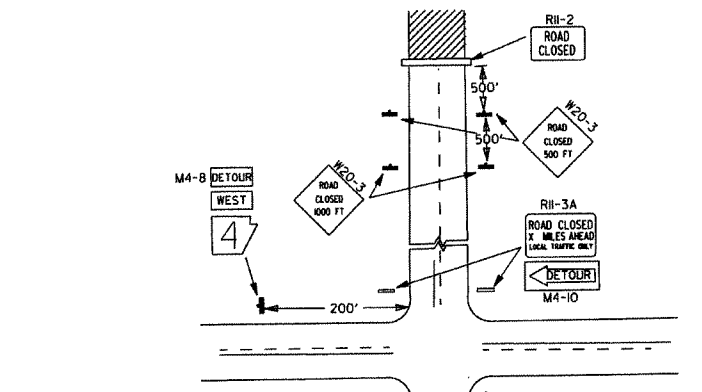
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



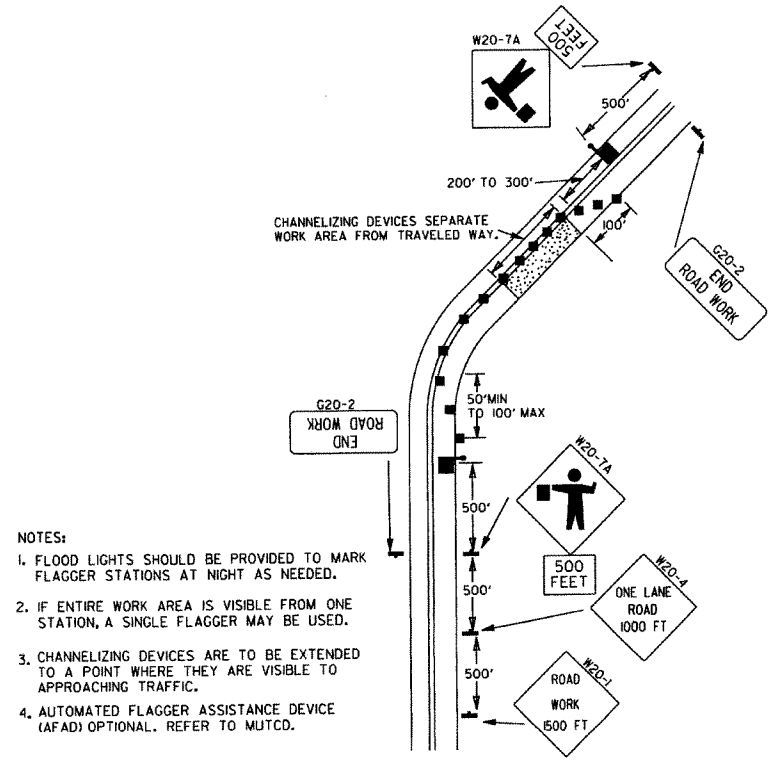
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



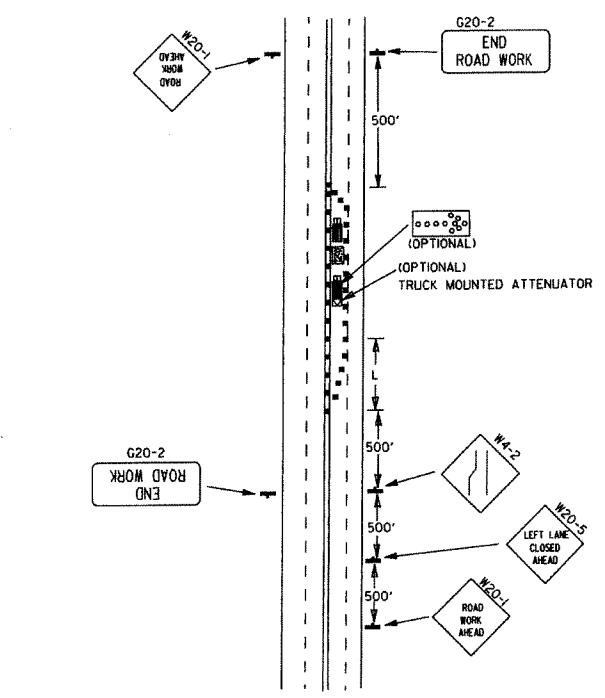
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



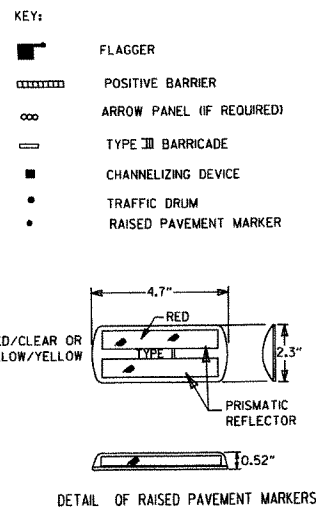
(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.



TYPICAL ADVANCE WARNING SIGN PLACEMENT

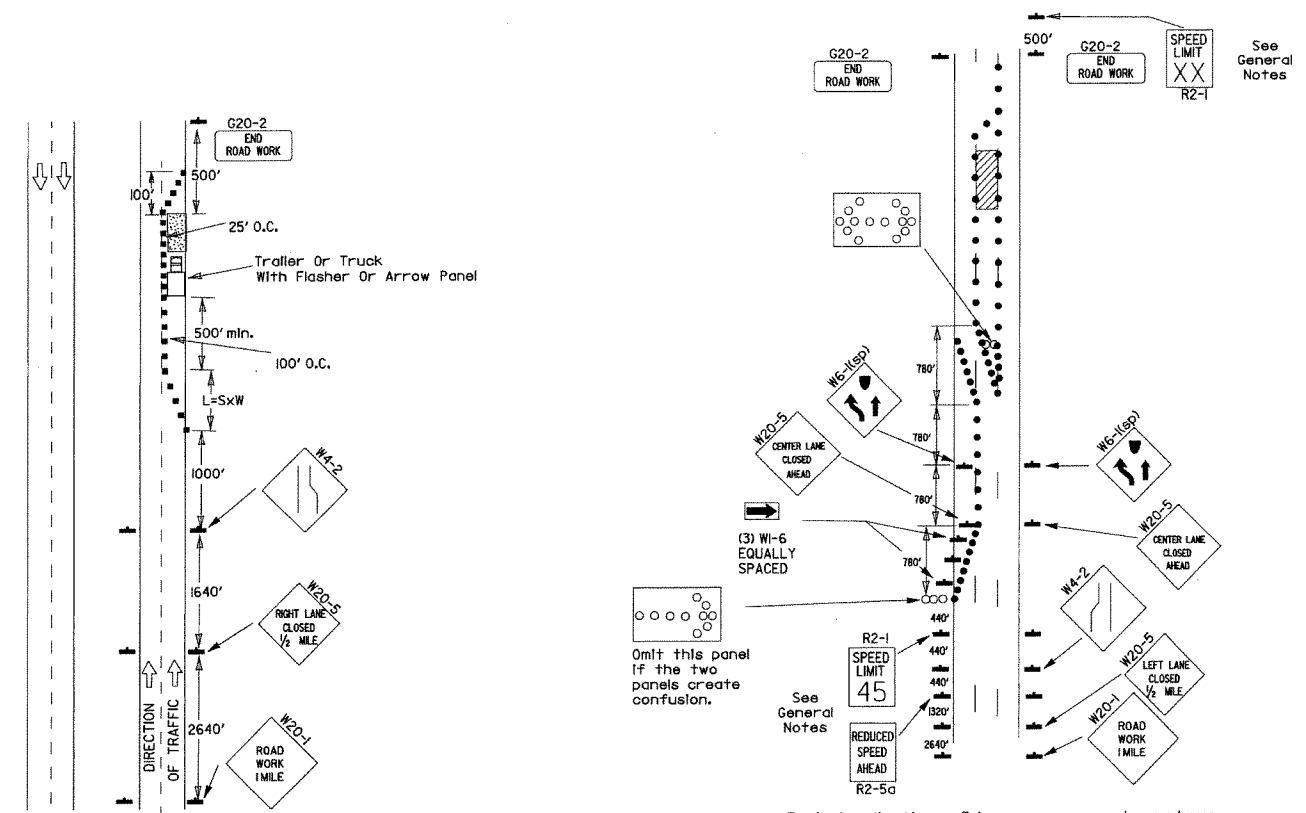
TAPER FORMULAE:

L = SXW FOR SPEEDS OF 45MPH OR MORE.
 L = $\frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
 WHERE:
 L = MINIMUM LENGTH OF TAPER.
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
 W = WIDTH OF OFFSET.

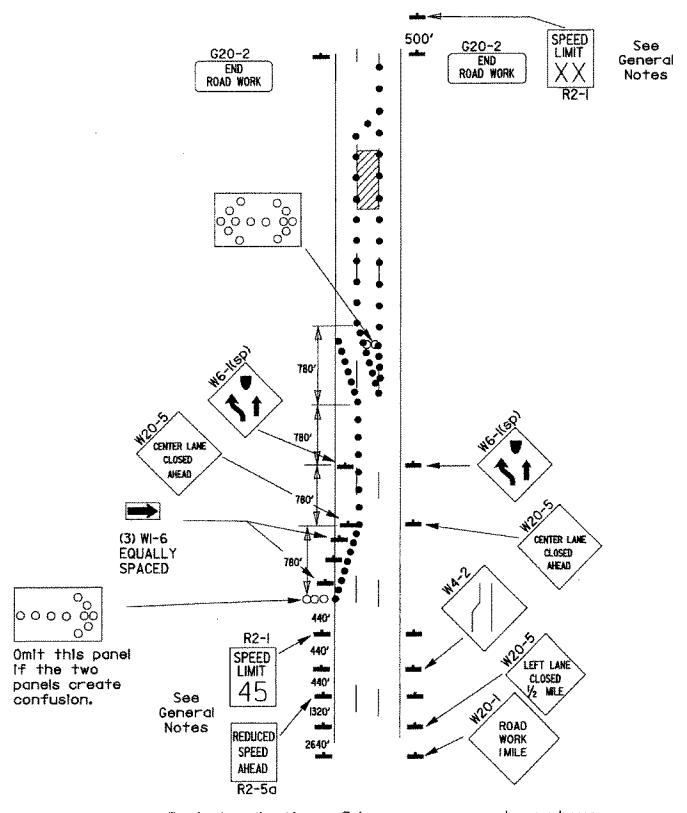
- GENERAL NOTES:
 1. ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE R2-5A SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.

DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-8-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VL MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

Channelizing devices



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.



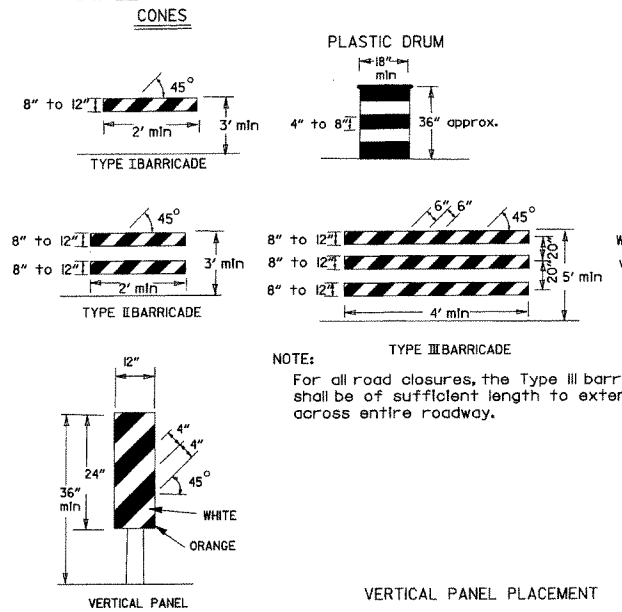
(B) Typical application - 3-lane oneway roadway where center lane is closed.

- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

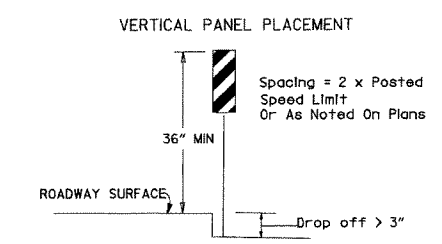
GENERAL NOTES:

1. A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
2. When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-1 45mph speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-1 55mph speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
4. The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
5. Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
7. The G20-sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-sign shall be erected 125' in advance of the job limit. Additional W20-1 (1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
8. Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
9. All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual for Assessing Safety Hardware (MASH).
10. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

When cones are used on freeways and multi-lane highways, they shall be 28" min. During hours of darkness, 28" cones shall be used on all roadways, and shall be reflectorized in accordance with the M.U.T.C.D.



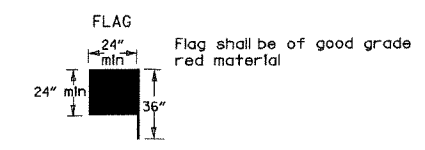
NOTE: For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



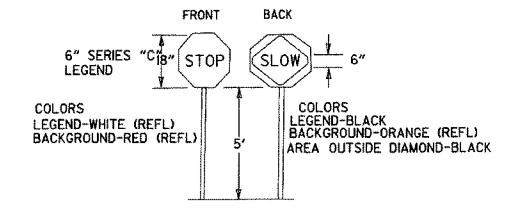
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-1 and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

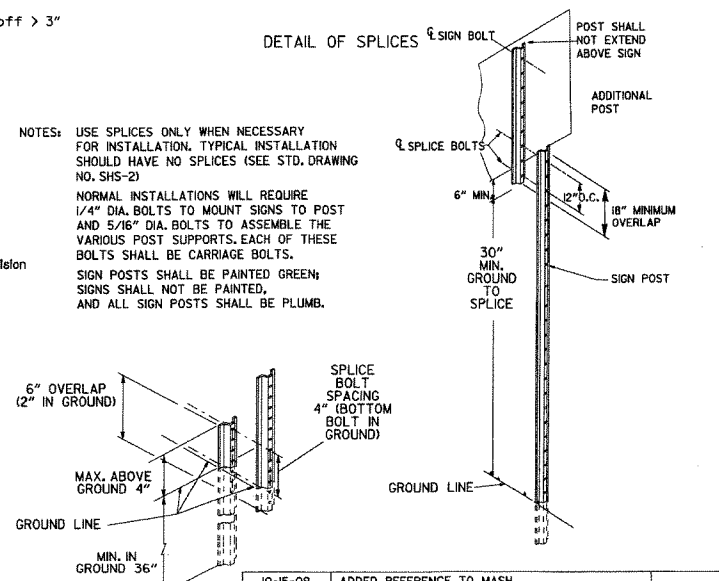
When shown on the plans concrete barrier will be used. When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



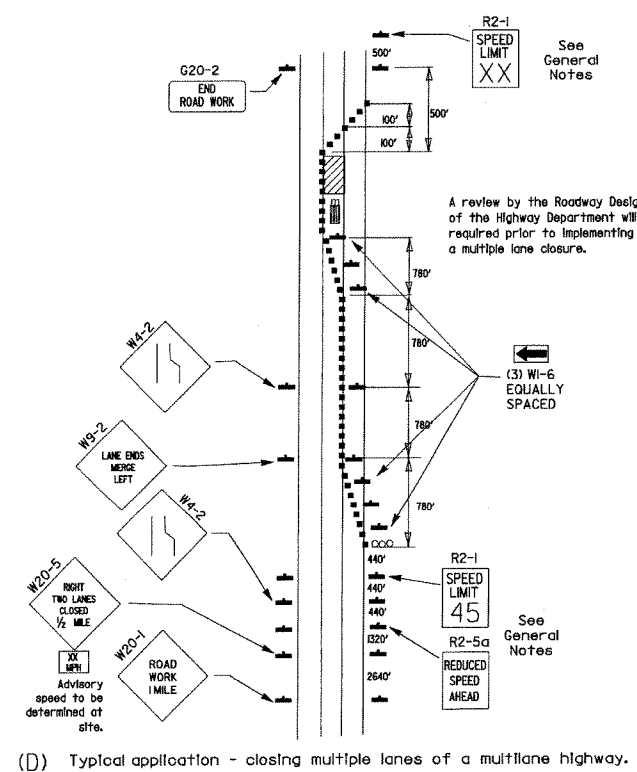
STOP SLOW PADDLE



DETAIL OF SPLICES



NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2). NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.

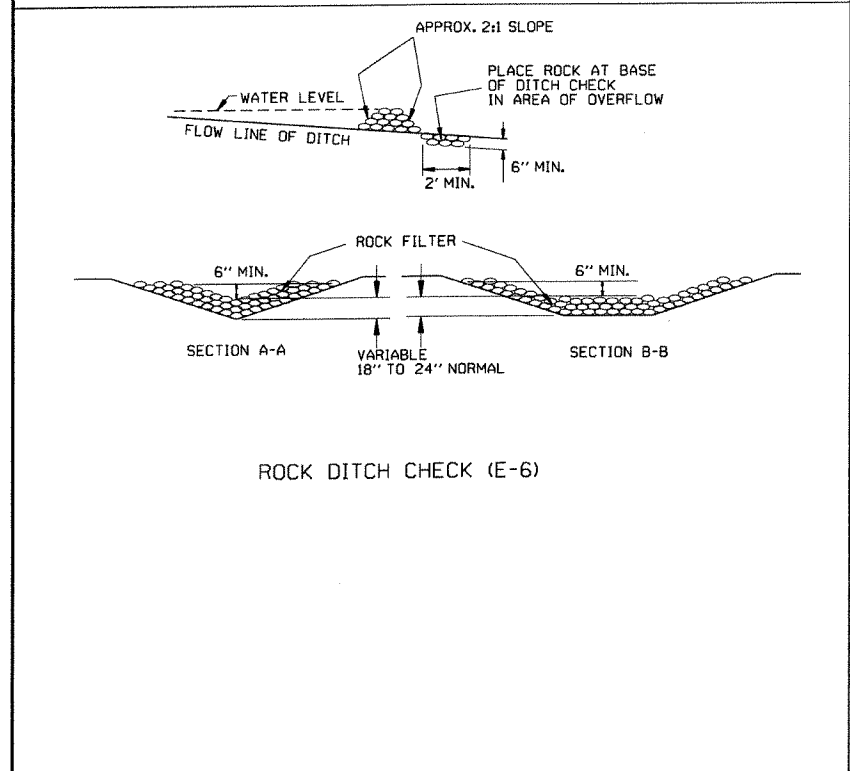
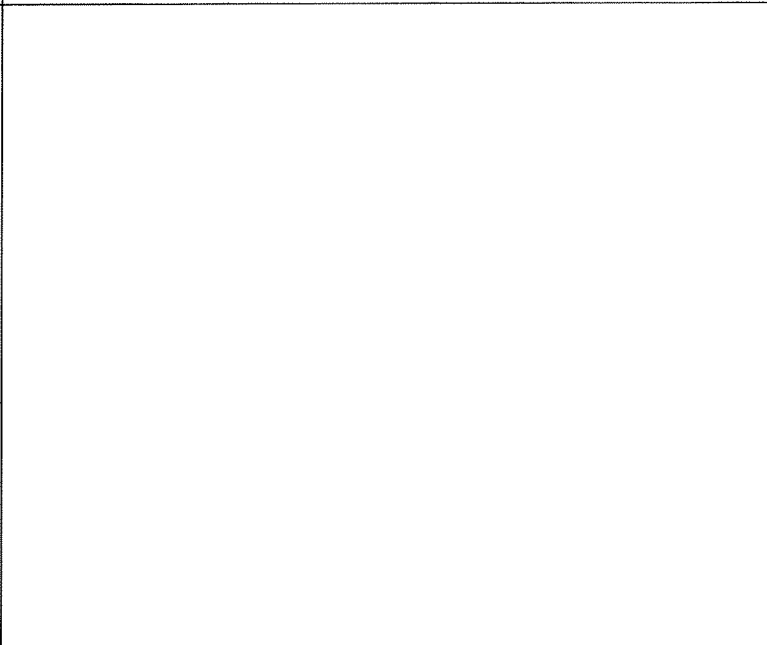
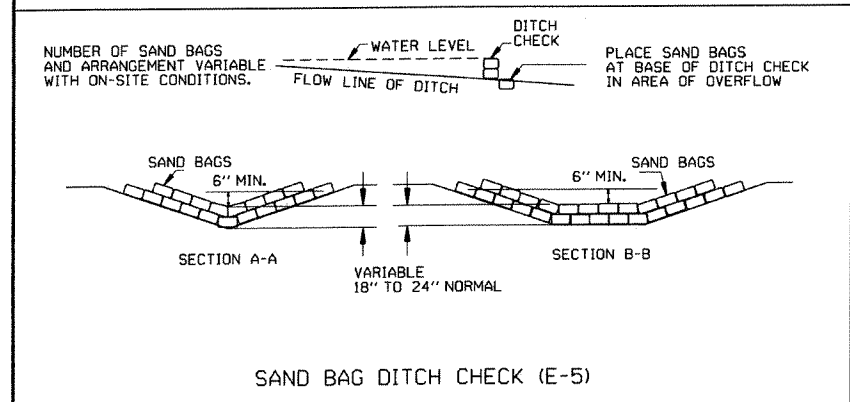
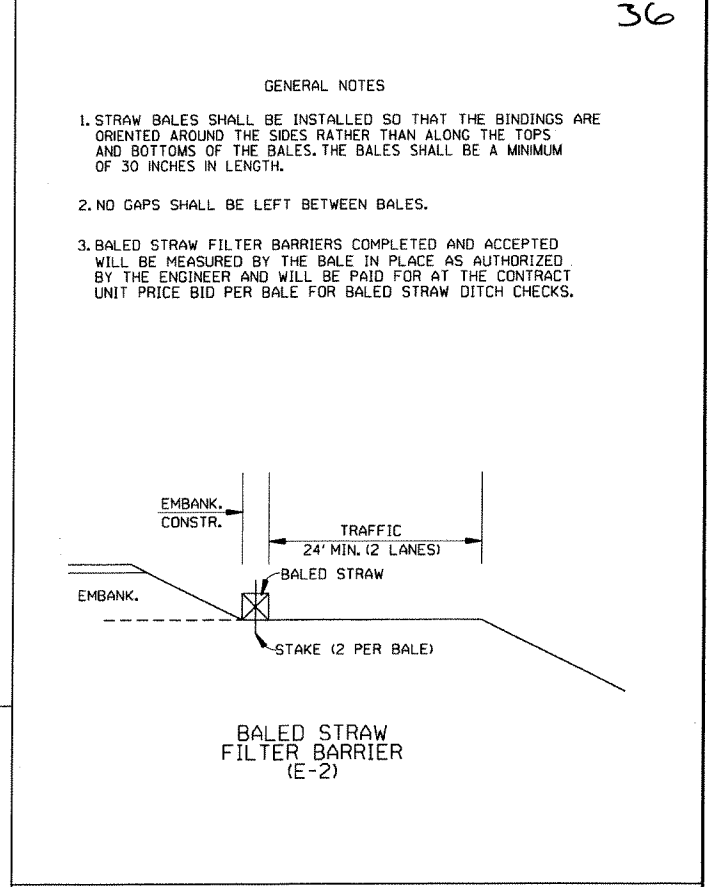
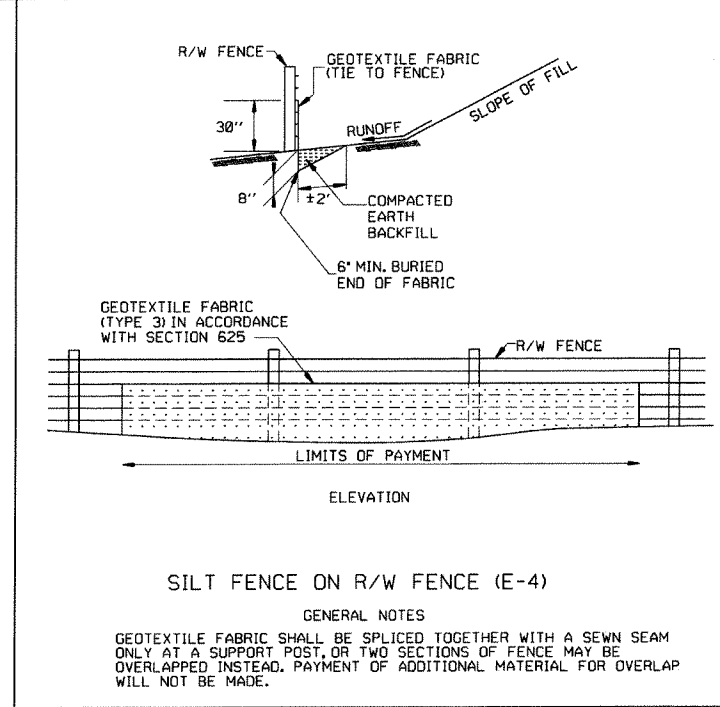
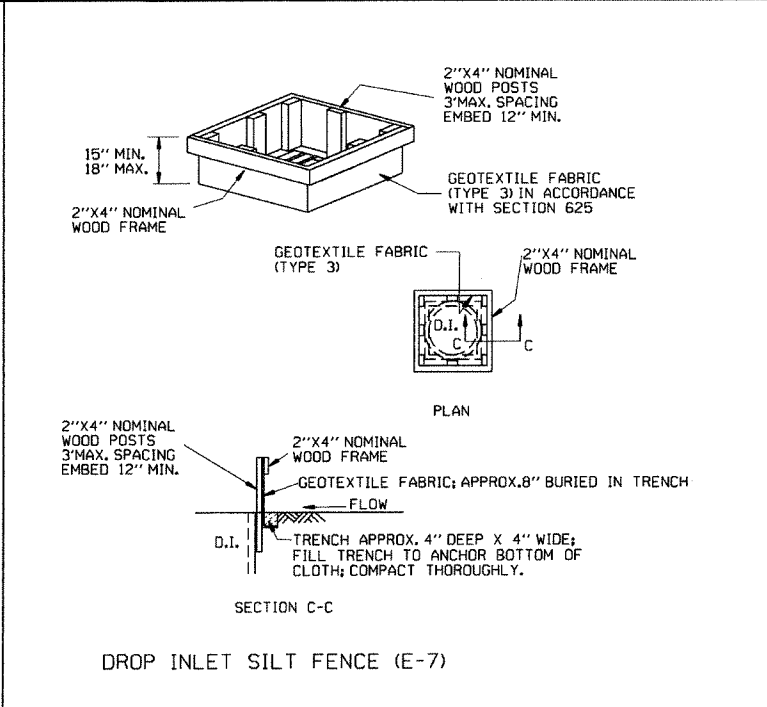
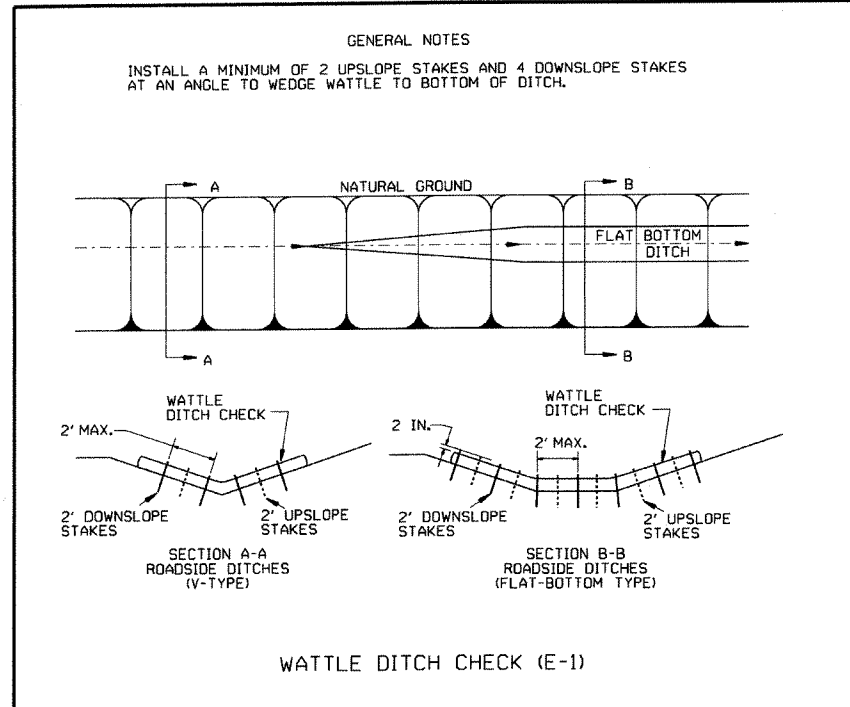


(D) Typical application - closing multiple lanes of a multi-lane highway.

(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

DATE	REVISION	FILMED
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-3

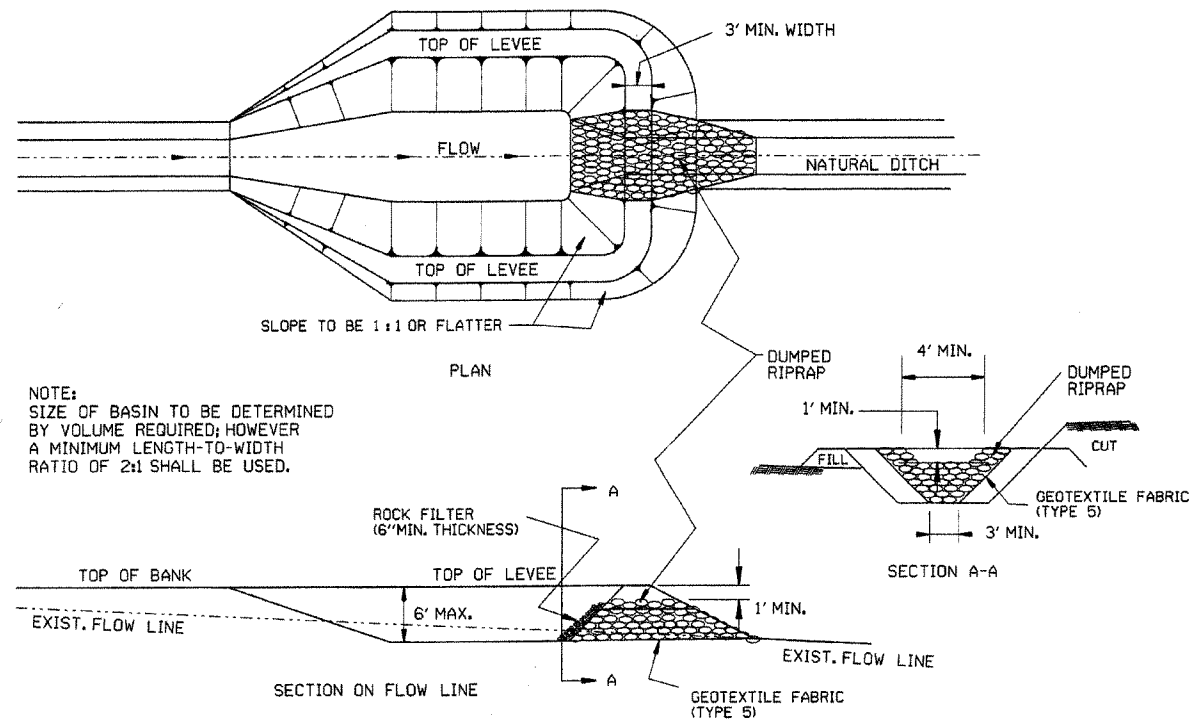


12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
11-18-98	ADDED NOTES	
7-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
7-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
7-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC	
6-2-94	REVISED E-1, 4, 7 & 11; DELETED E-2 & 3	6-2-94
4-1-93	REDRAWN	
10-1-92	REDRAWN	
8-2-76	ISSUED R.D.M.	298-7-28-76
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

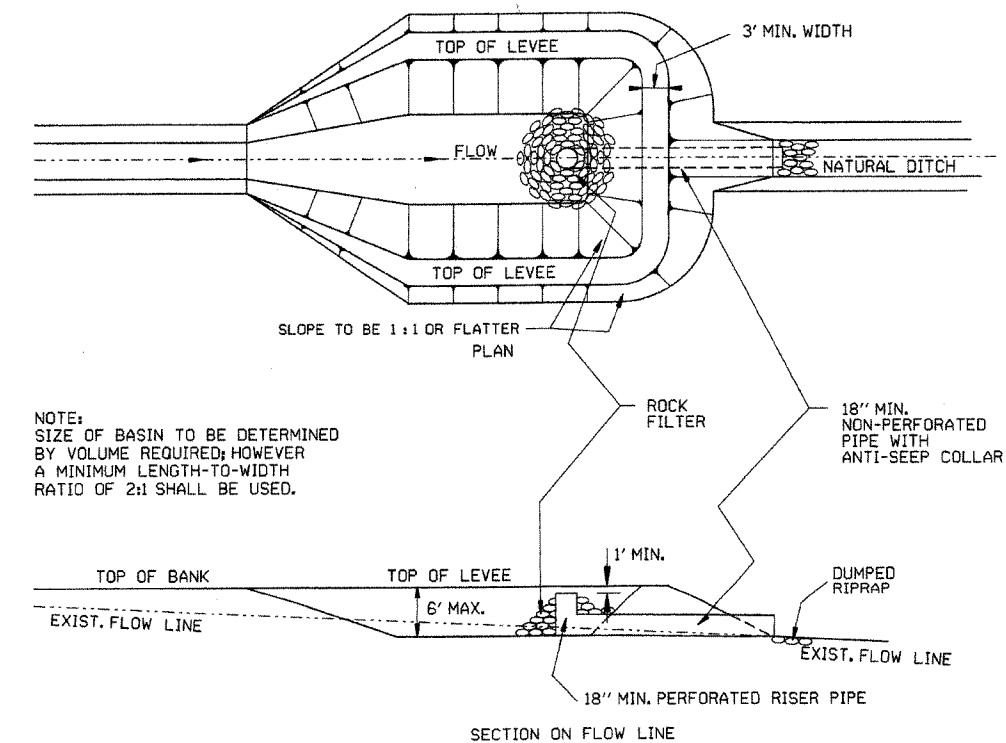
TEMPORARY EROSION CONTROL DEVICES

STANDARD DRAWING TEC-1



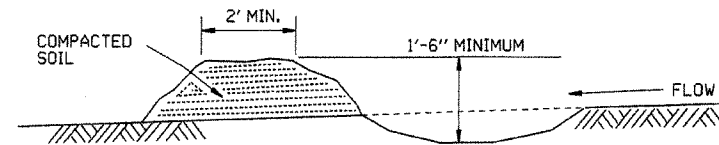
NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.

SEDIMENT BASIN WITH RIPRAP OUTLET (E-9)

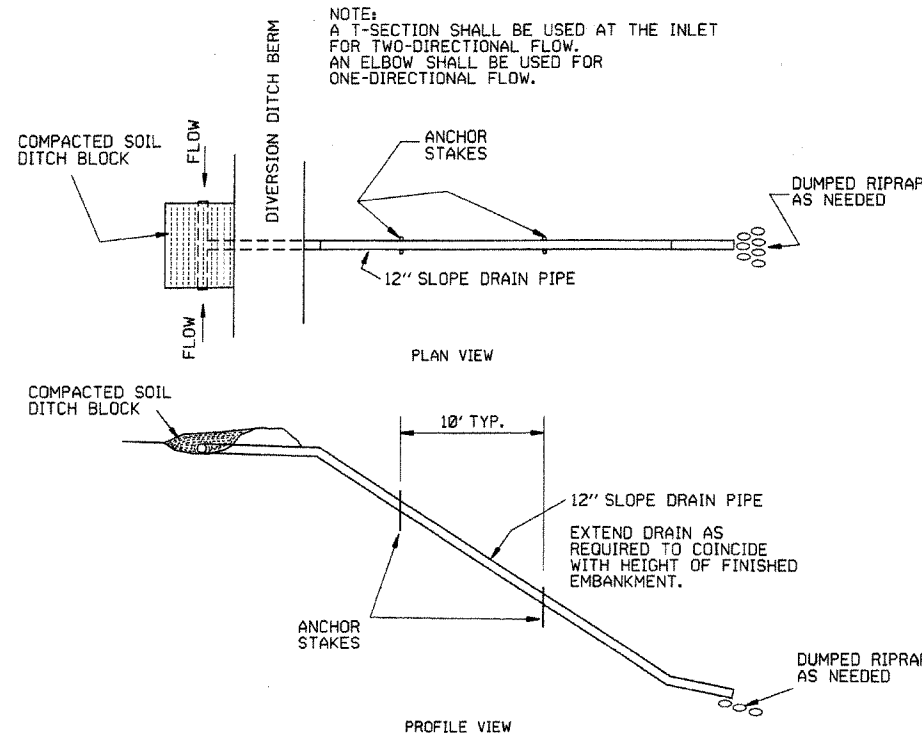


NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.

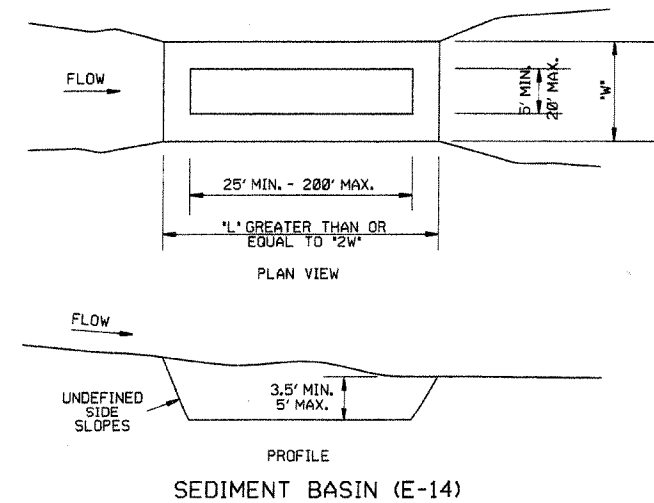
SEDIMENT BASIN WITH PIPE OUTLET (E-10)



DIVERSION DITCH (E-8)



SLOPE DRAIN (E-12)



SEDIMENT BASIN (E-14)

6-2-94	Revised E-8 & E-12; Added E-14 & Deleted E-13		
4-1-93	ISSUED		
DATE	REVISION		FILMED

ARKANSAS STATE HIGHWAY COMMISSION

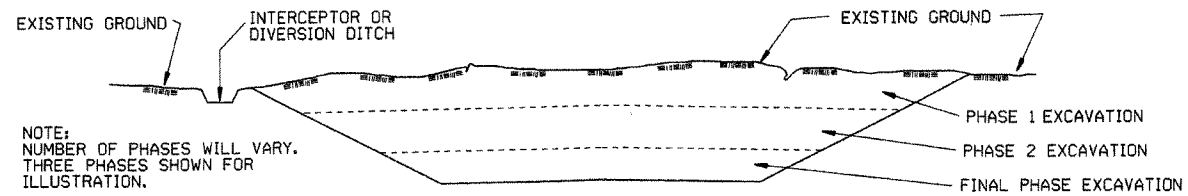
TEMPORARY EROSION
CONTROL DEVICES

STANDARD DRAWING TEC-2

CLEARING AND GRUBBING

- CONSTRUCTION SEQUENCE
1. PLACE PERIMETER CONTROLS (I.E. SILT FENCES , DIVERSION DITCHES, SEDIMENT BASINS, ETC.)
 2. PERFORM CLEARING AND GRUBBING OPERATION.

EXCAVATION



NOTE:
NUMBER OF PHASES WILL VARY.
THREE PHASES SHOWN FOR
ILLUSTRATION.

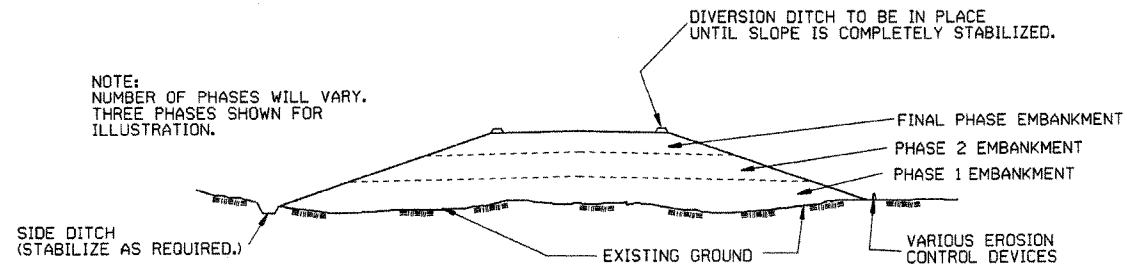
GENERAL NOTE

ALL CUT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE EXCAVATED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

1. EXCAVATE AND STABILIZE INTERCEPTOR AND/OR DIVERSION DITCHES.
2. PERFORM PHASE 1 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
3. PERFORM PHASE 2 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
4. PERFORM FINAL PHASE OF EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING. STABILIZE DITCHES. CONSTRUCT DITCH CHECKS, DIVERSION DITCHES, SEDIMENT BASINS, OR OTHER EROSION CONTROL DEVICES AS REQUIRED.

EMBANKMENT



NOTE:
NUMBER OF PHASES WILL VARY.
THREE PHASES SHOWN FOR
ILLUSTRATION.

GENERAL NOTE

ALL EMBANKMENT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE CONSTRUCTED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

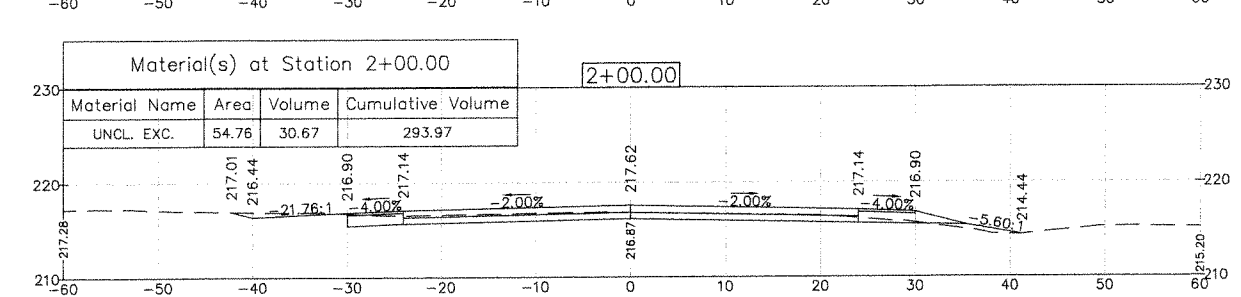
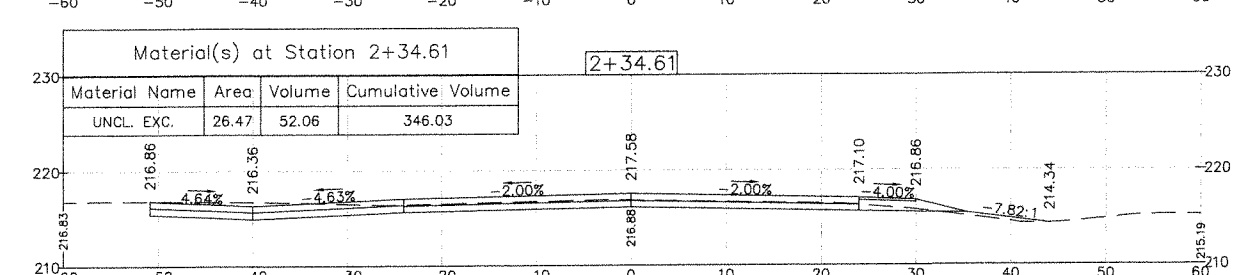
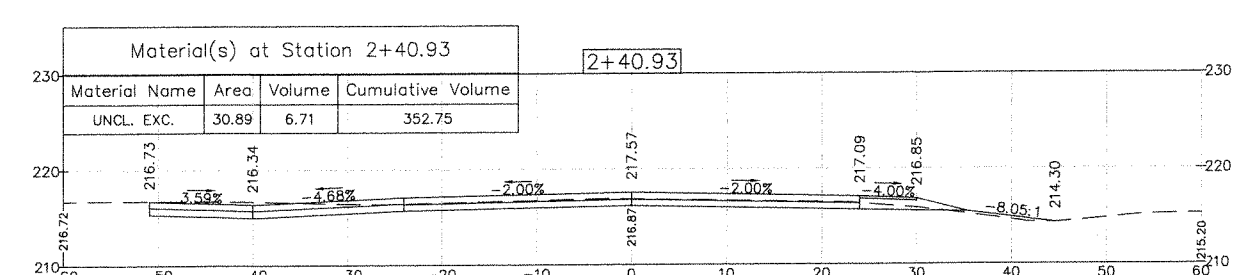
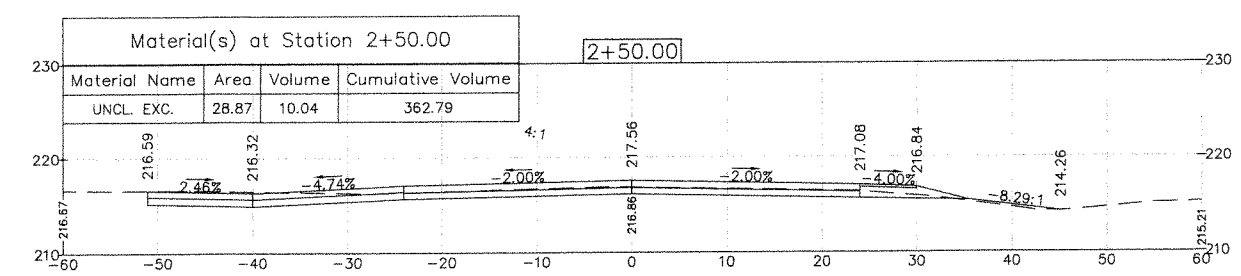
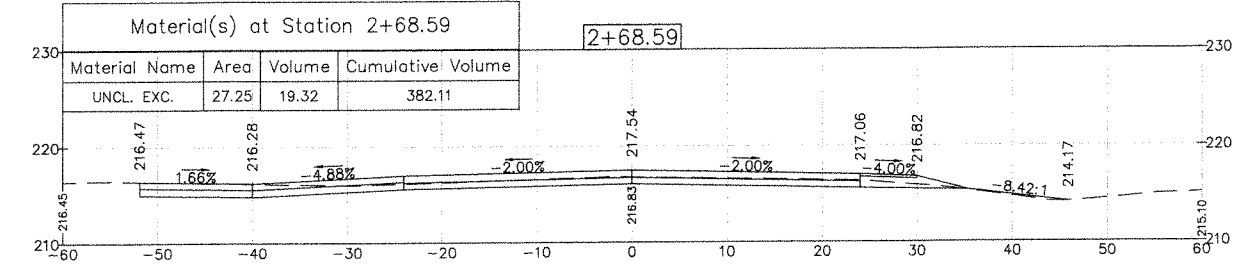
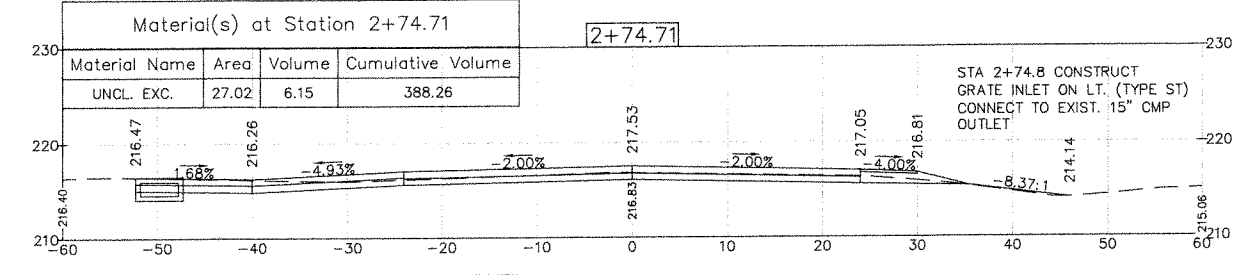
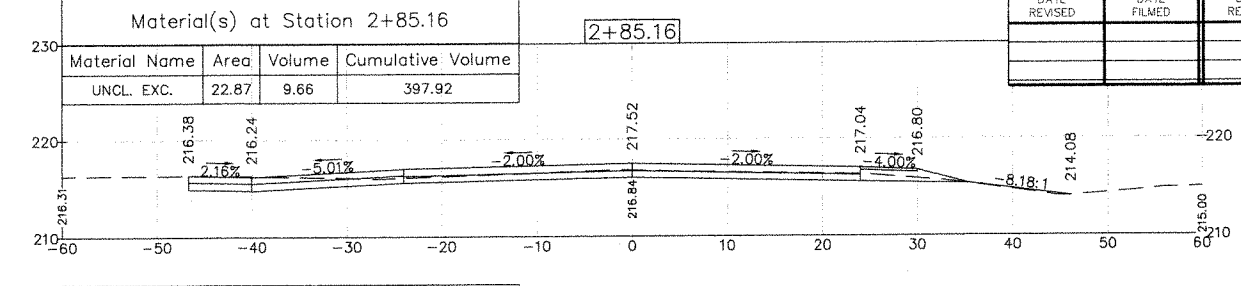
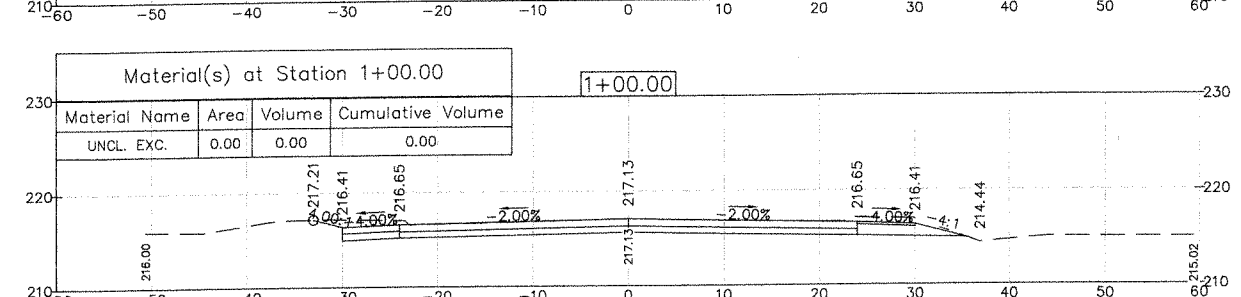
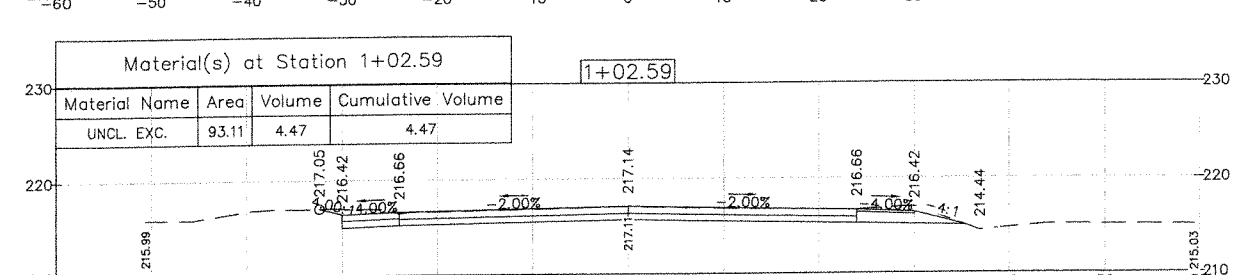
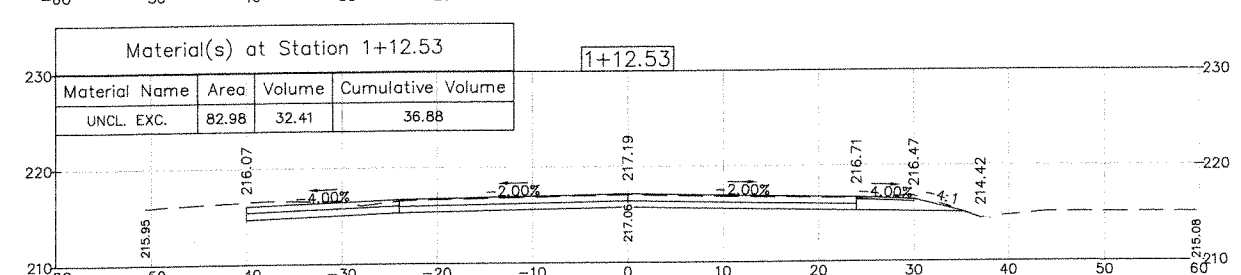
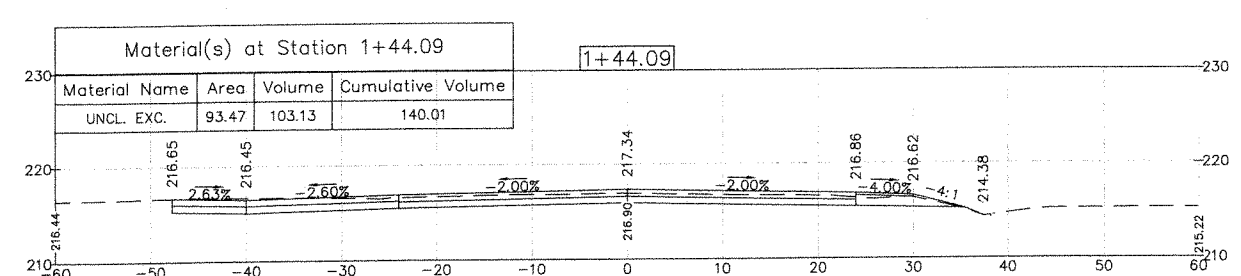
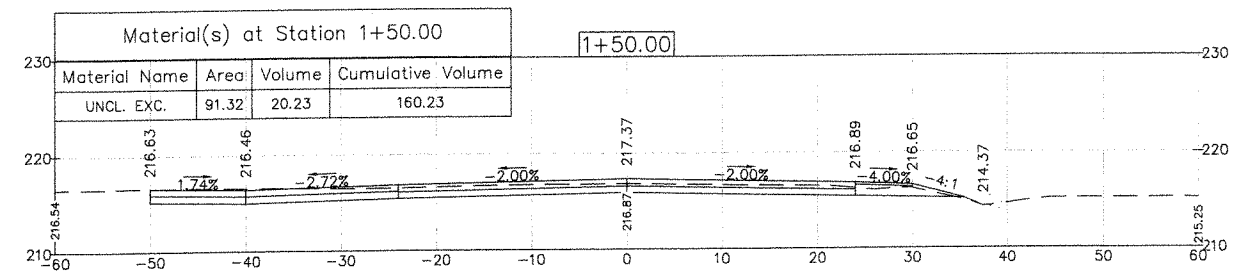
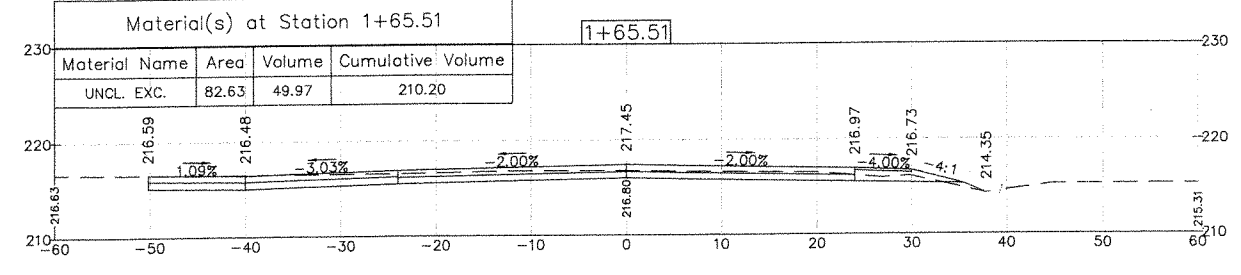
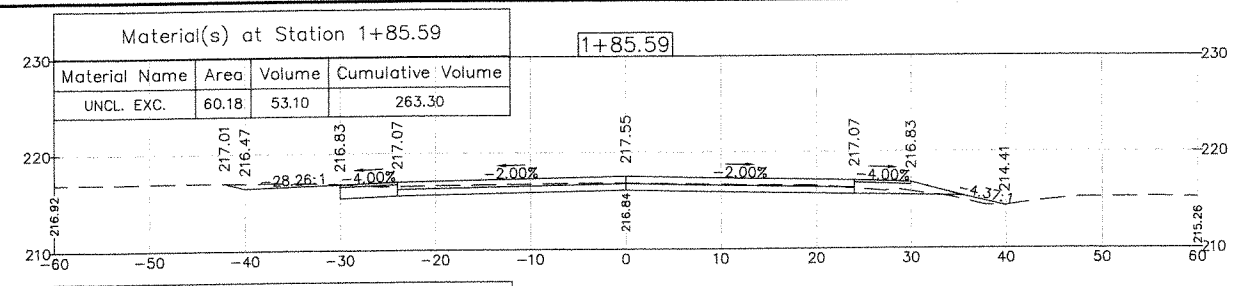
CONSTRUCTION SEQUENCE

1. CONSTRUCT DIVERSION DITCHES, DITCH CHECKS, SEDIMENT BASINS, SILT FENCES, OR OTHER EROSION CONTROL DEVICES AS SPECIFIED.
2. PLACE PHASE 1 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
3. PLACE PHASE 2 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
4. PLACE FINAL PHASE OF EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PLACE DIVERSION DITCHES AND SLOPE DRAINS AND MAINTAIN UNTIL ENTIRE SLOPE IS STABILIZED.

ARKANSAS STATE HIGHWAY COMMISSION			
TEMPORARY EROSION CONTROL DEVICES			
STANDARD DRAWING TEC-3			
11-03-94	CORRECTED SPELLING		
6-2-94	Drawn & Issued		6-2-94
DATE	REVISION		FILMED

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 110583							39	52
CROSS SECTIONS - INGRAM BLVD.								

SCALE: 1"=10'

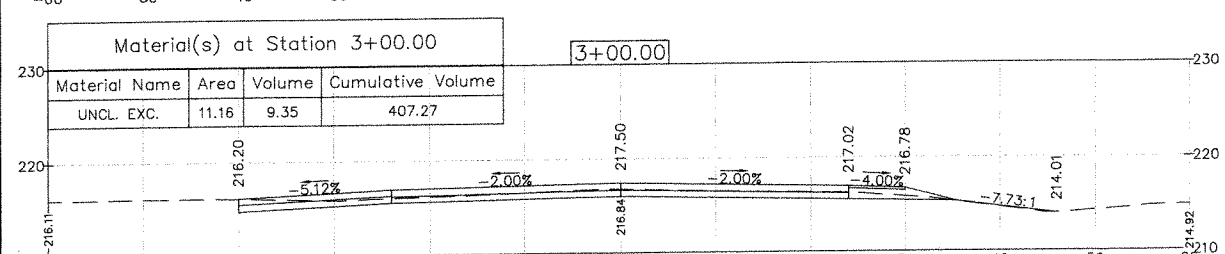
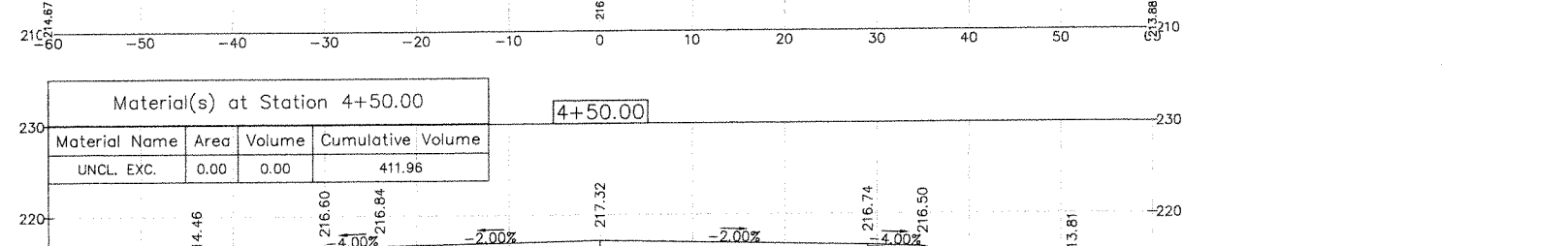
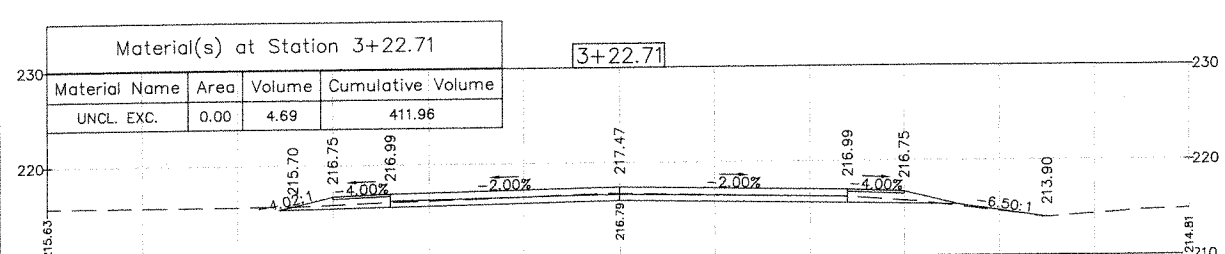
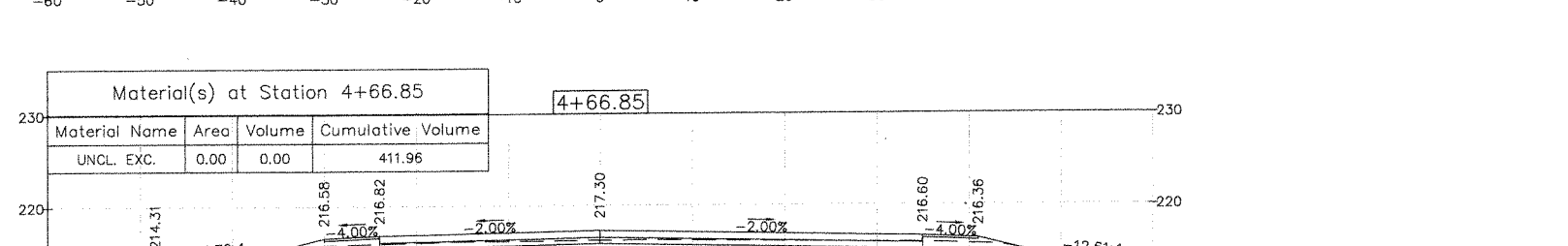
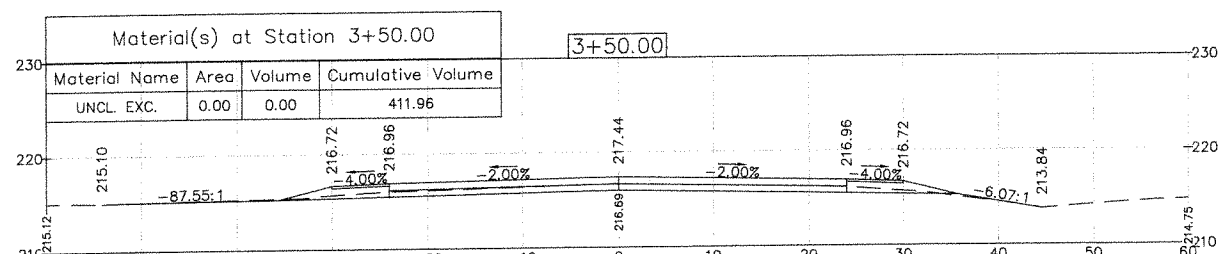
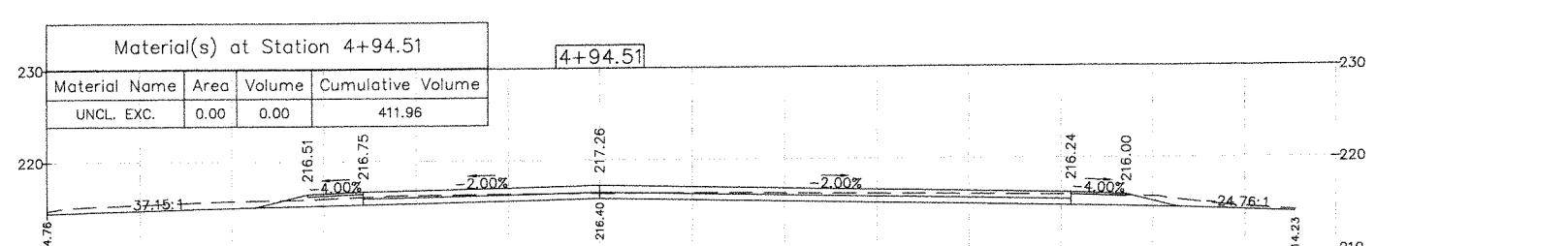
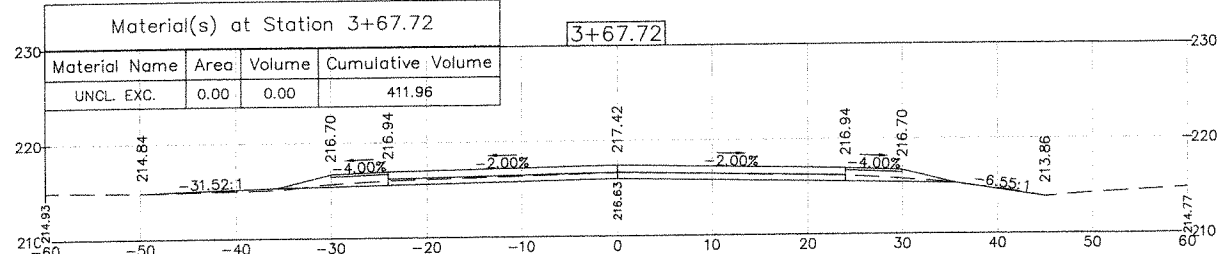
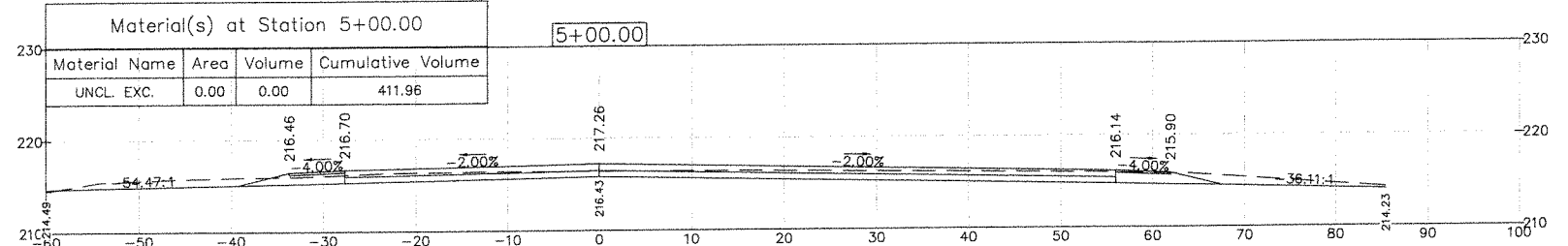
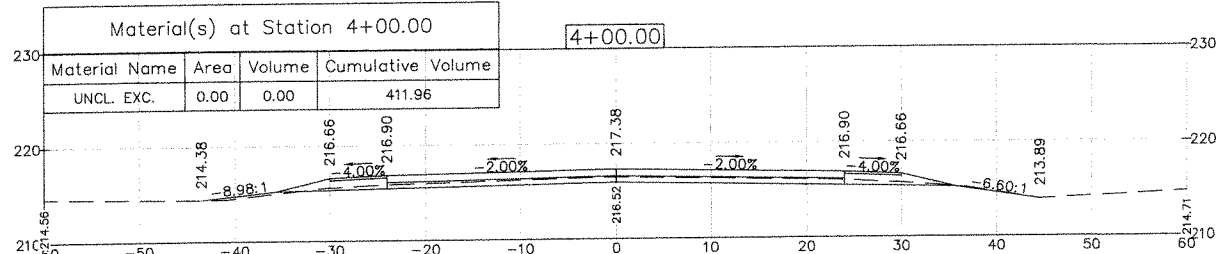
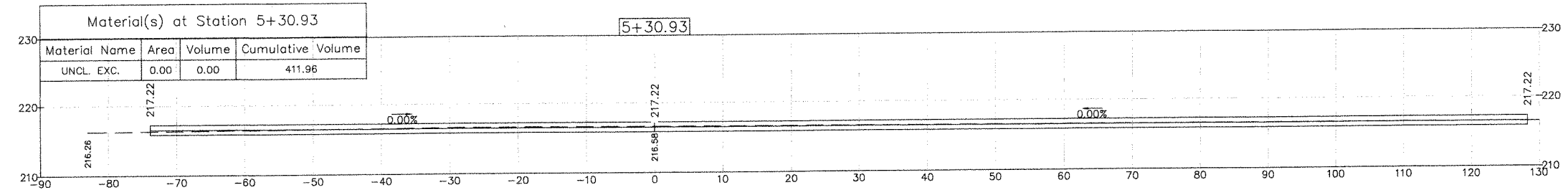


CROSS SECTIONS - INGRAM BLVD.
STA 1+00 - STA 2+85.16

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						110583	40	52

CROSS SECTIONS - INGRAM BLVD

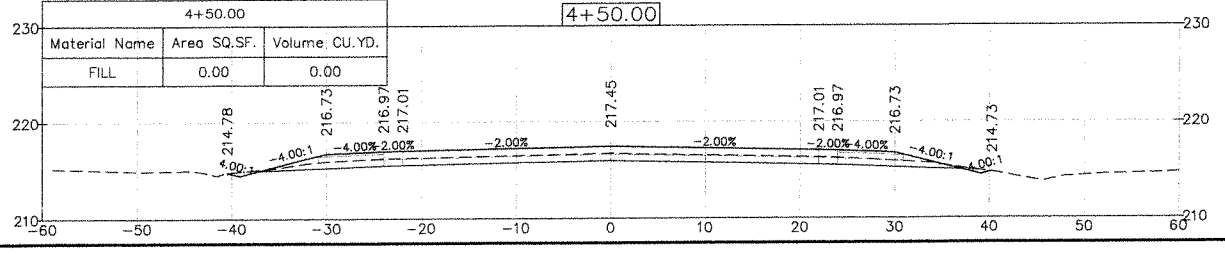
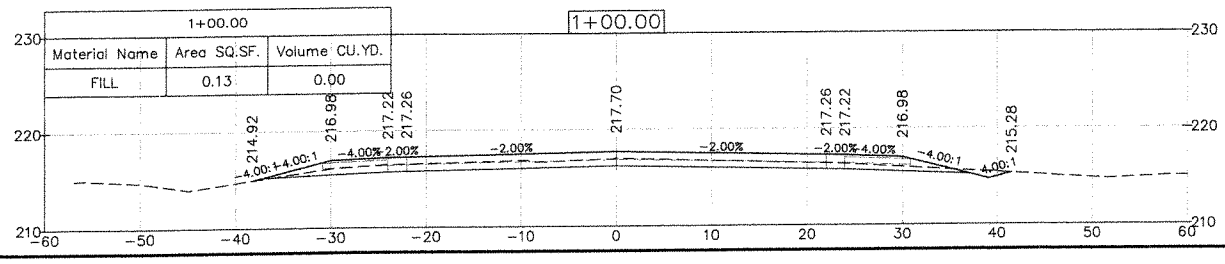
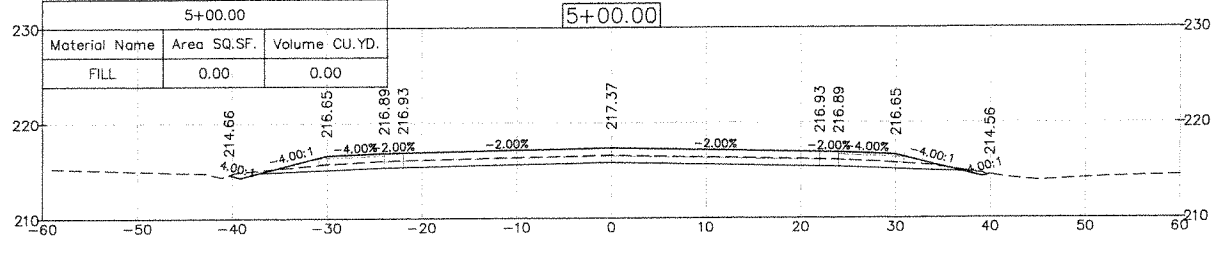
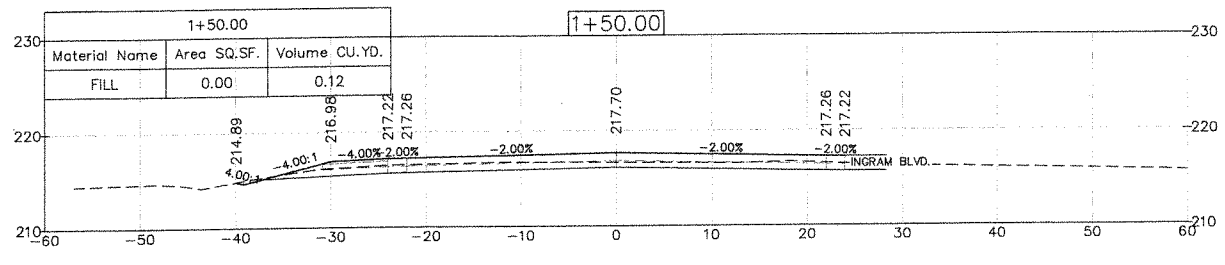
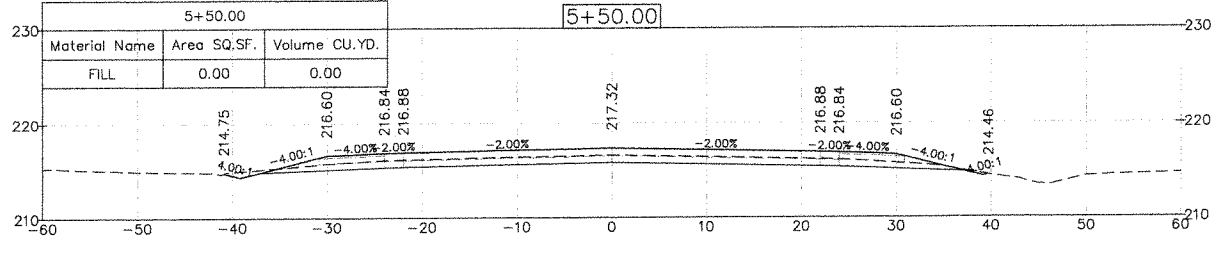
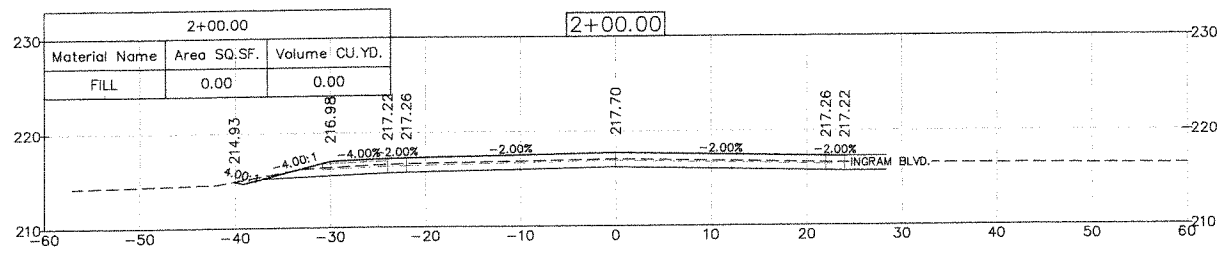
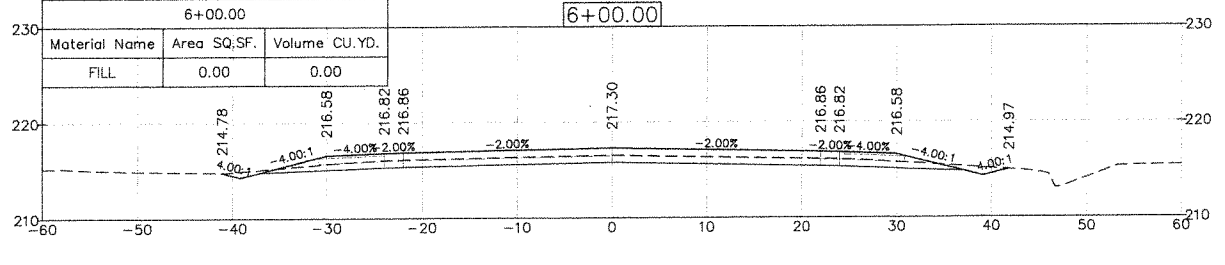
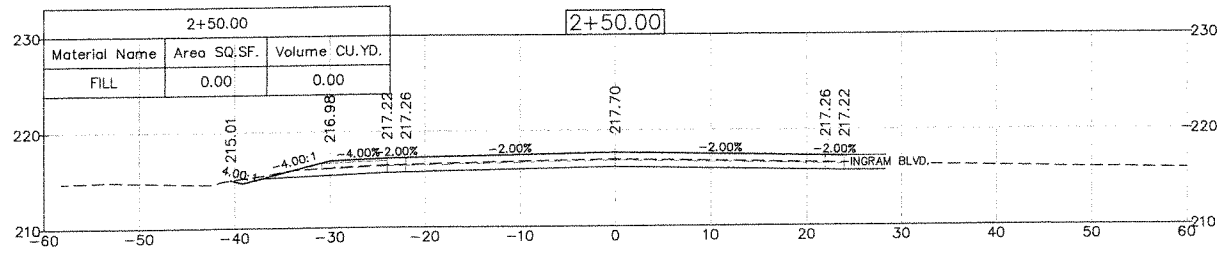
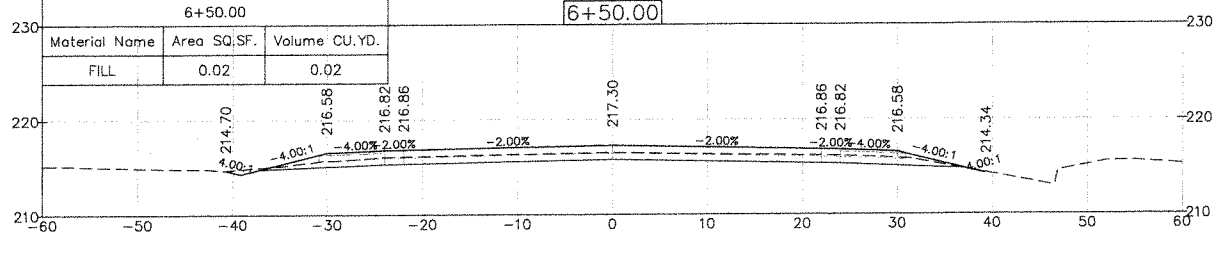
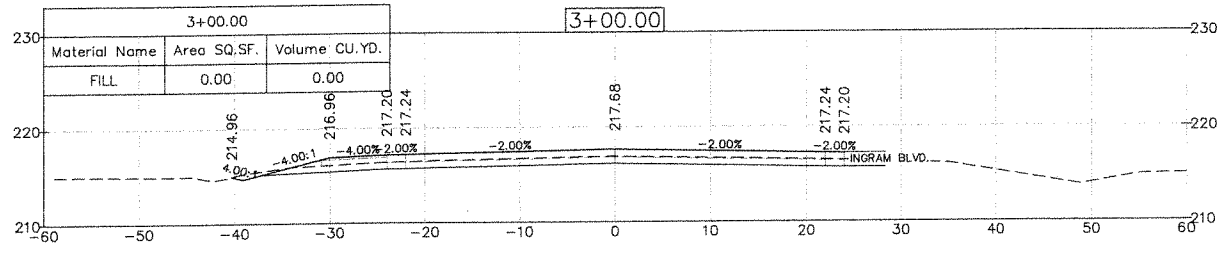
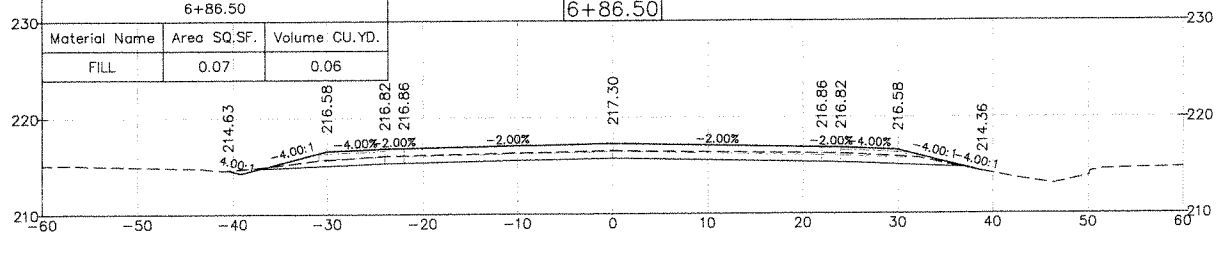
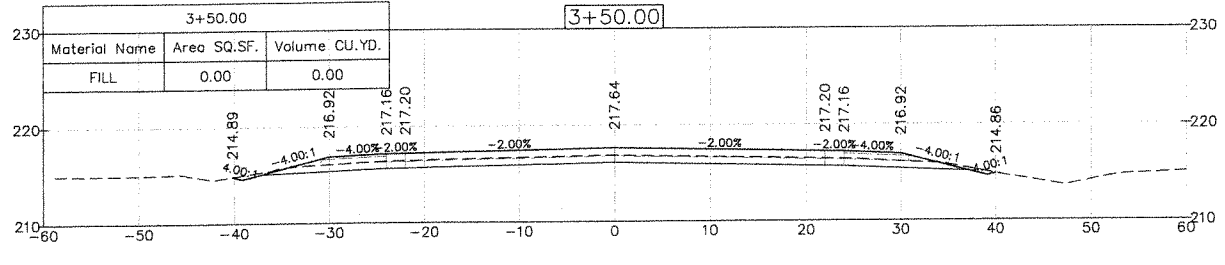
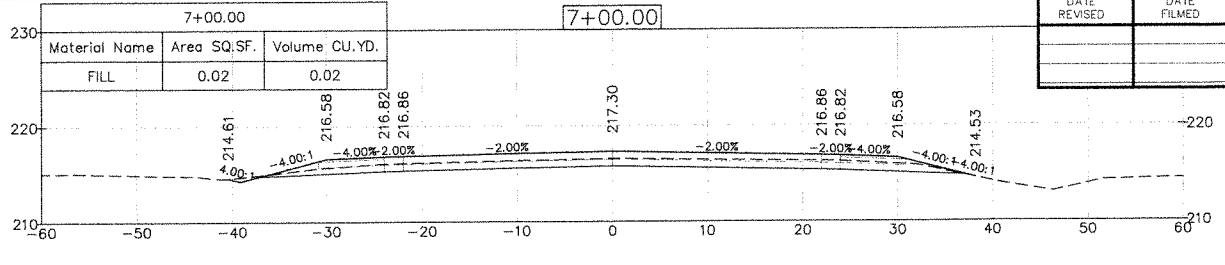
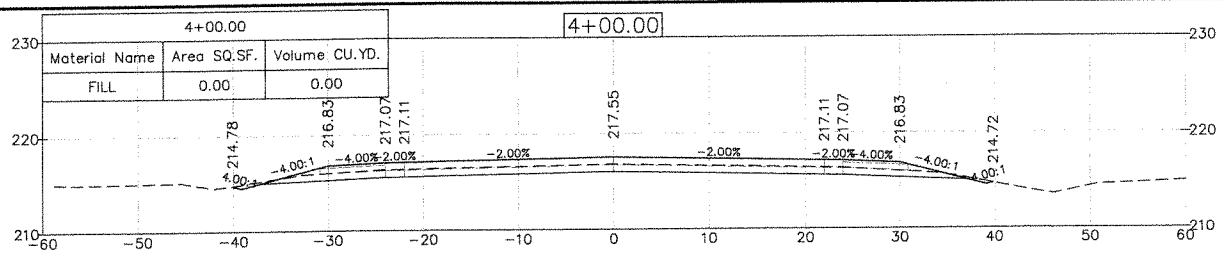
SCALE: 1"=10'



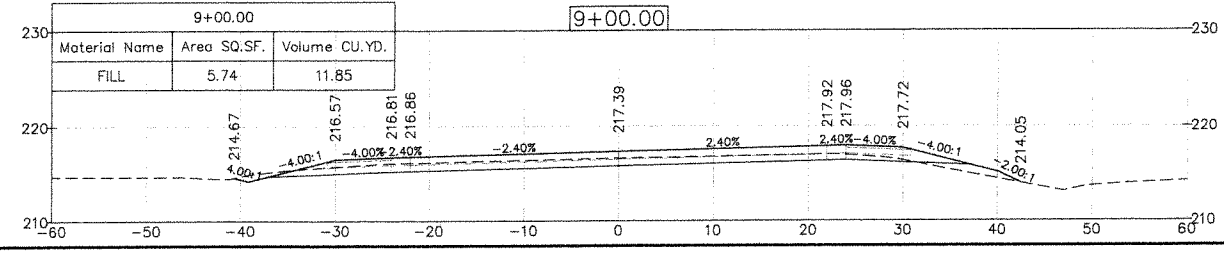
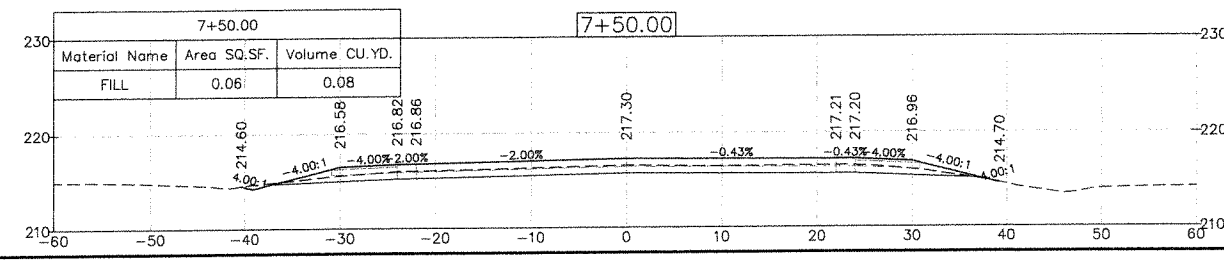
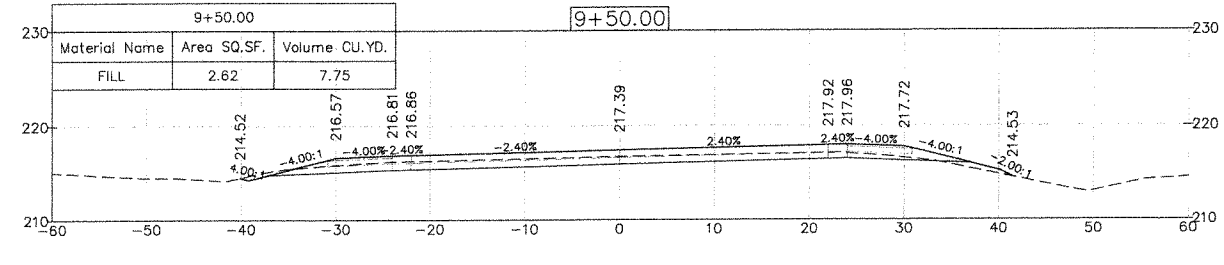
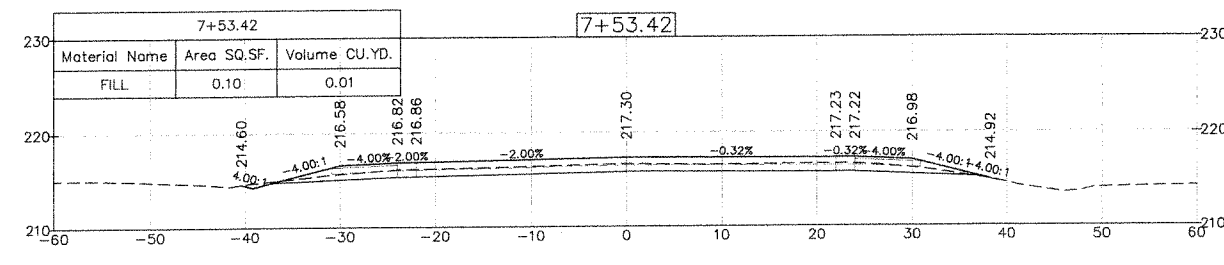
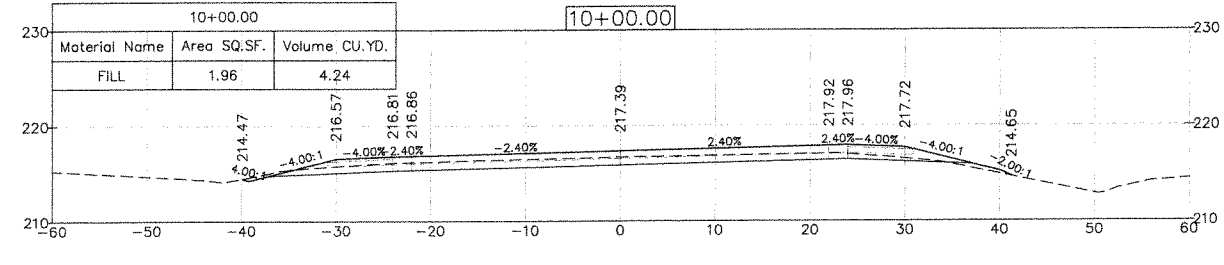
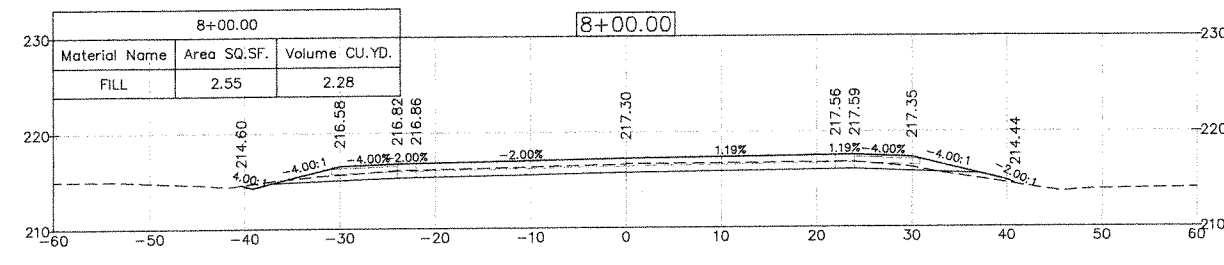
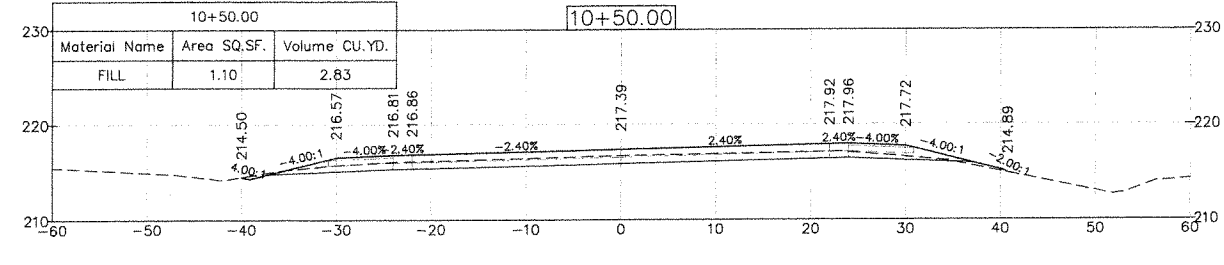
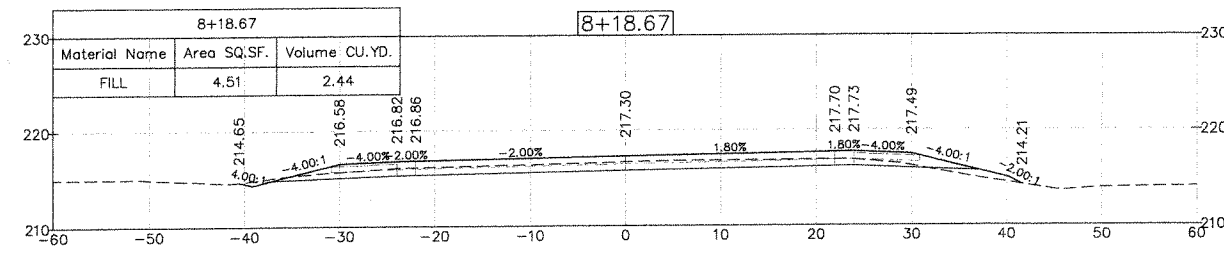
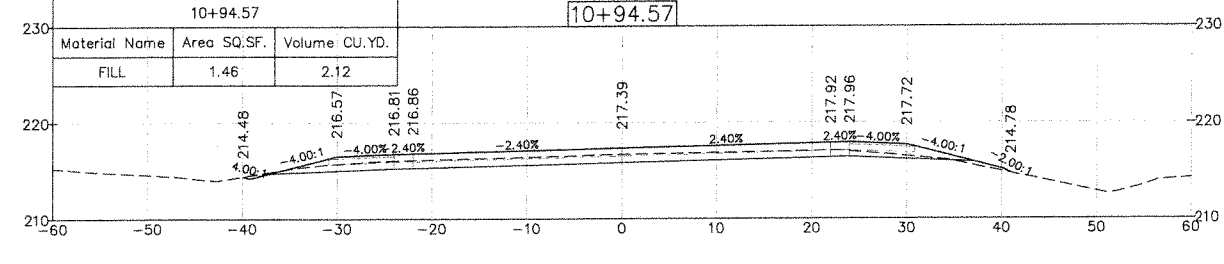
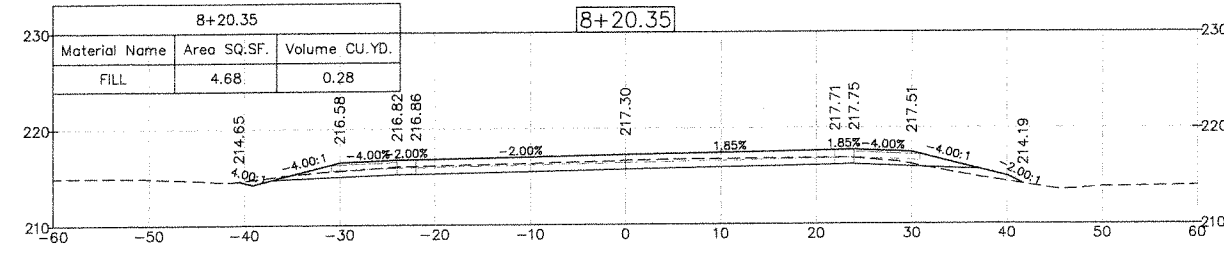
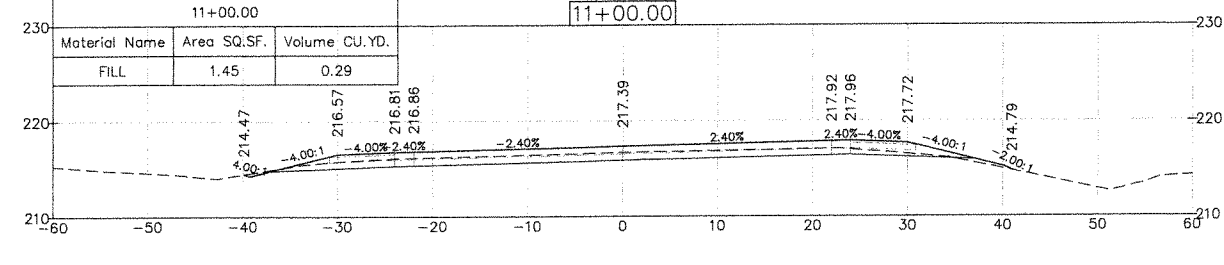
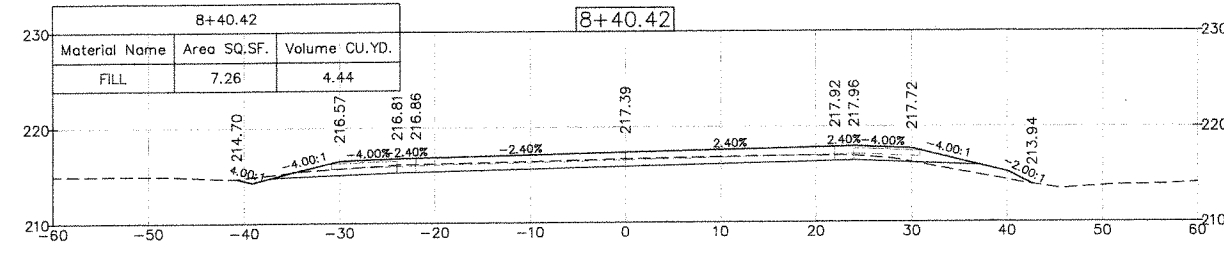
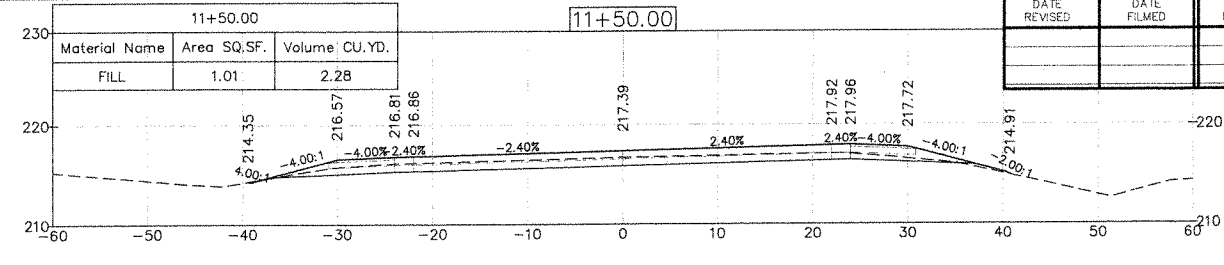
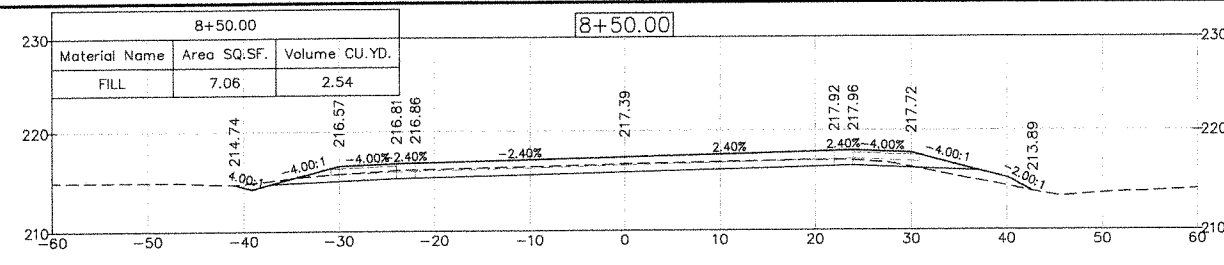
CROSS SECTIONS - INGRAM BLVD.
STA 3+00 - STA 5+30.93

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK	110583	41	52

2 CROSS SECTIONS - SOUTHLAND DR

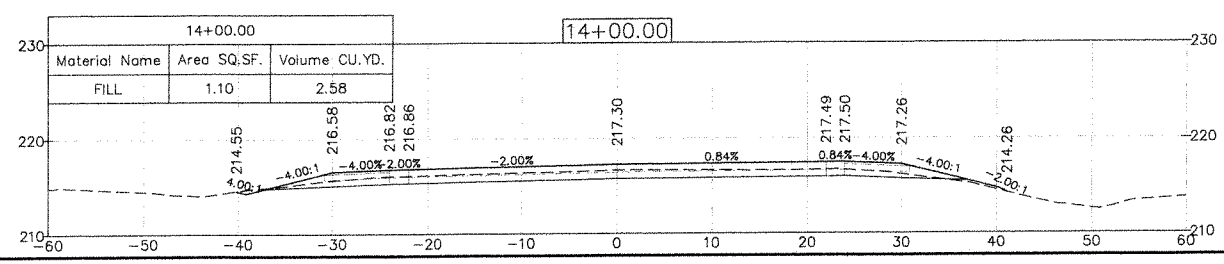
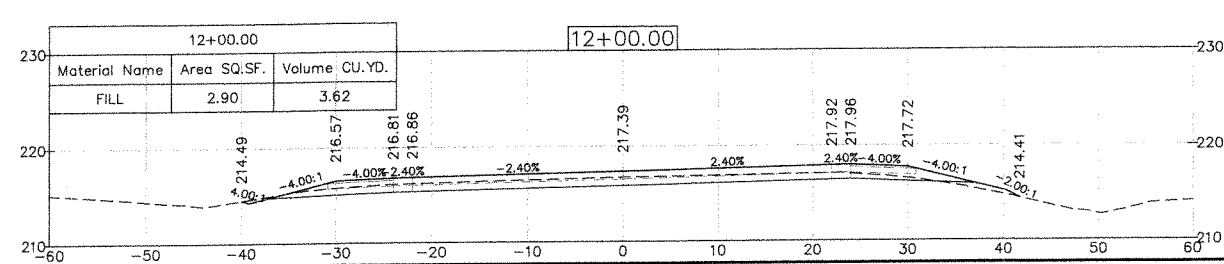
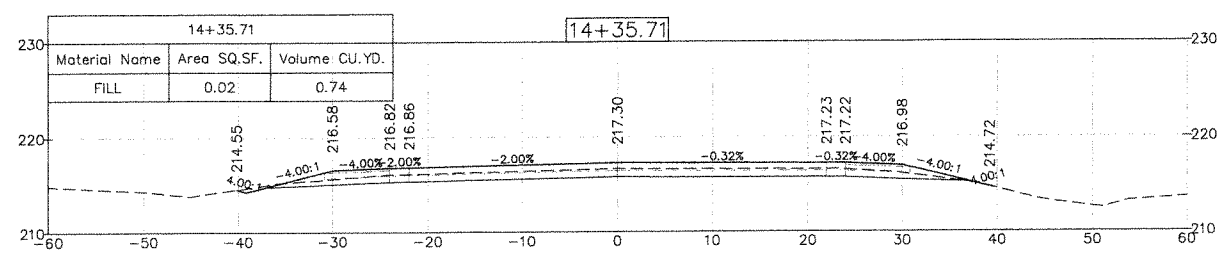
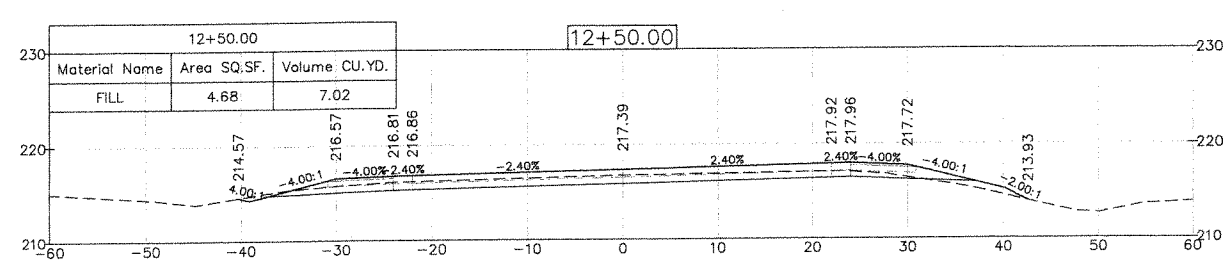
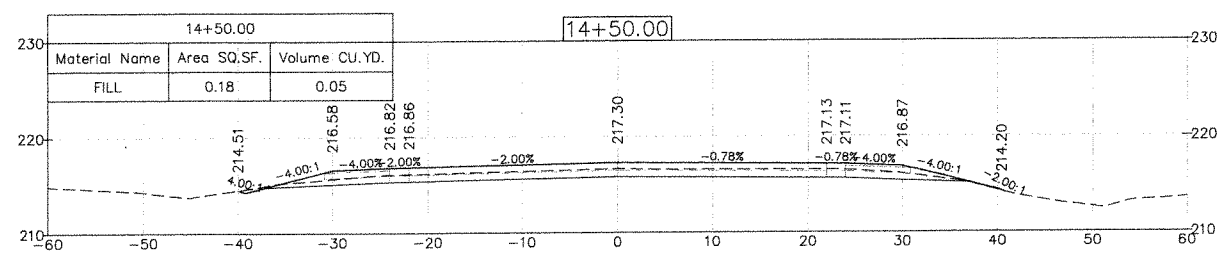
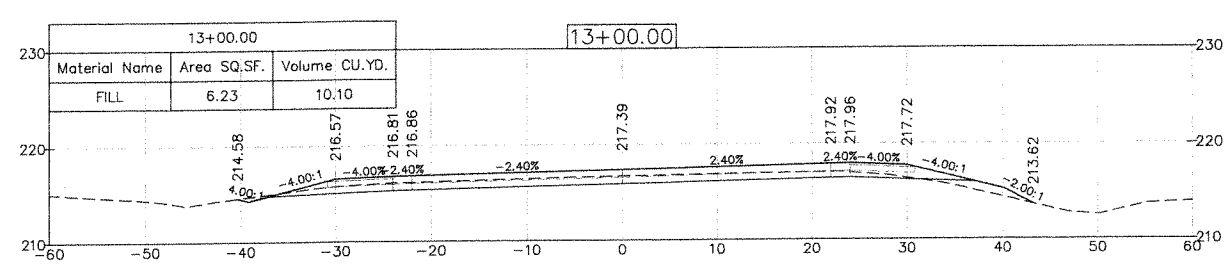
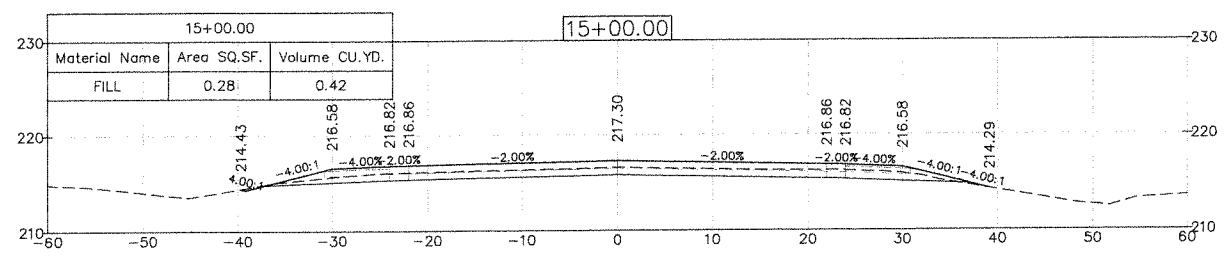
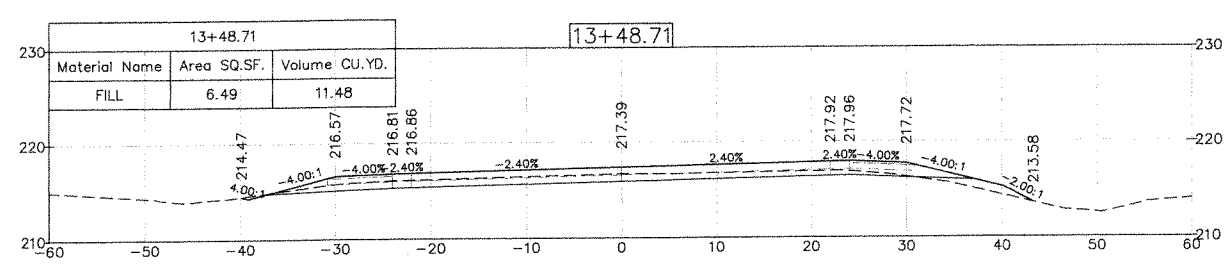
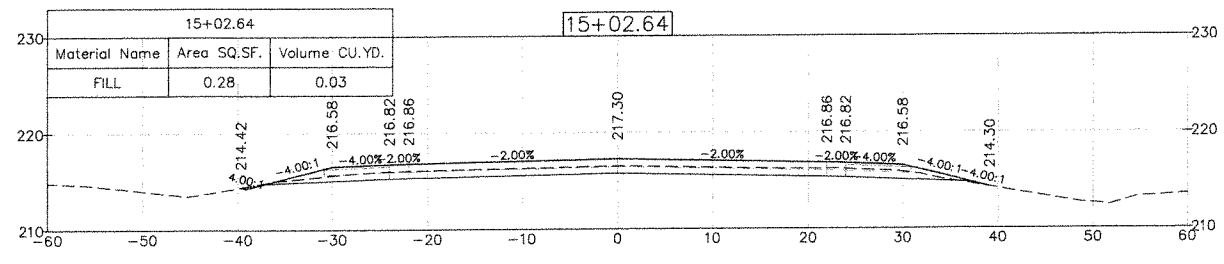
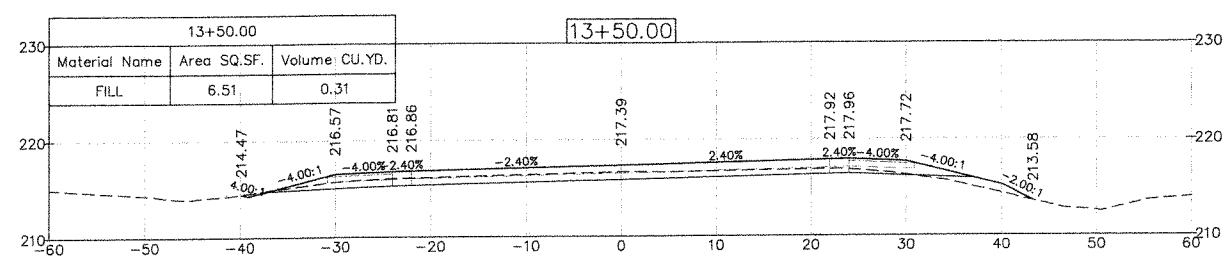
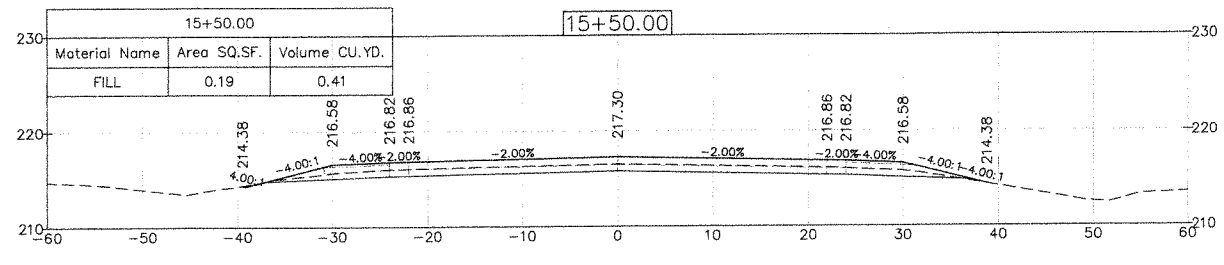
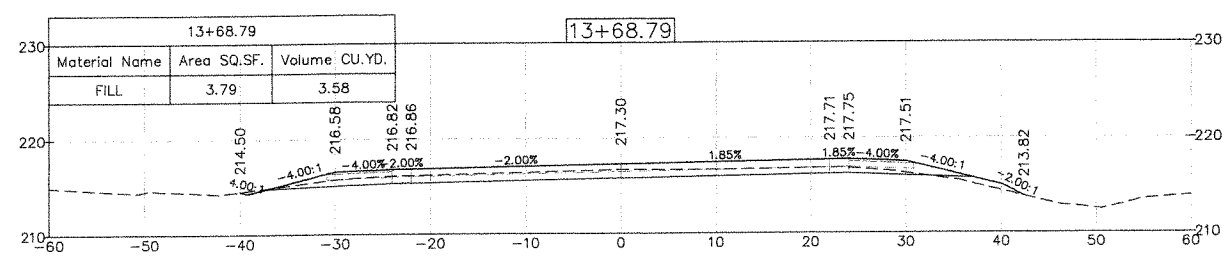
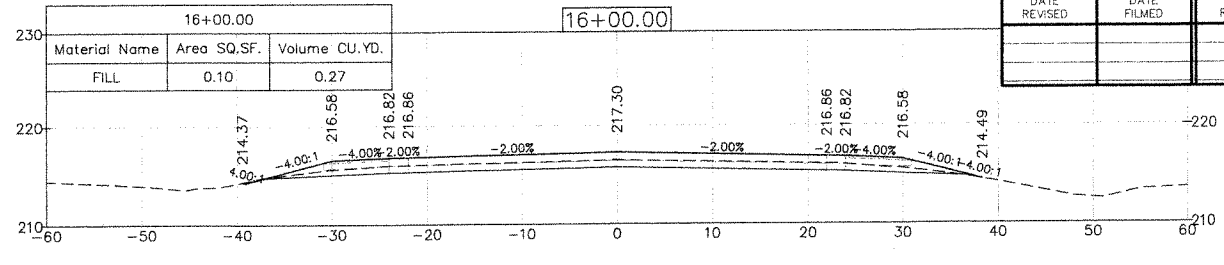
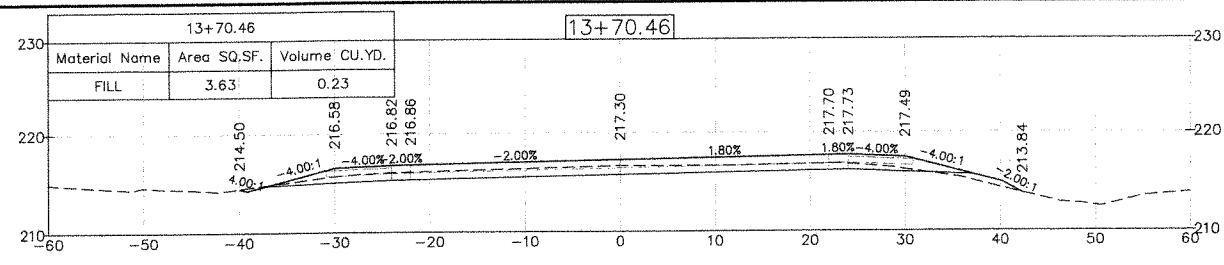


CROSS SECTIONS - SOUTHLAND DR
STA 1+00 - STA 7+00



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 110583							43	52

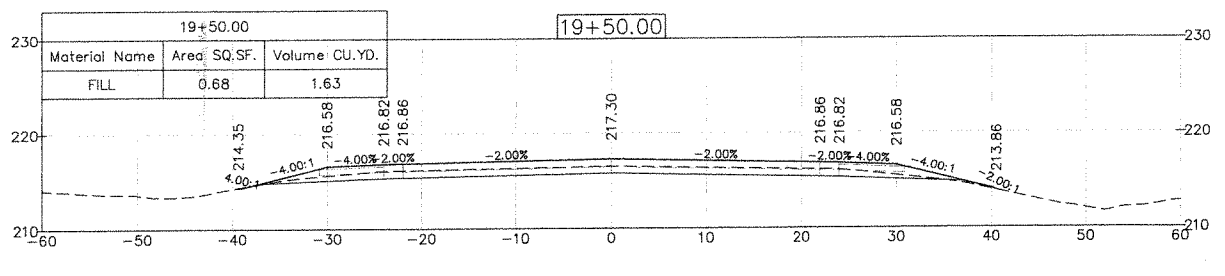
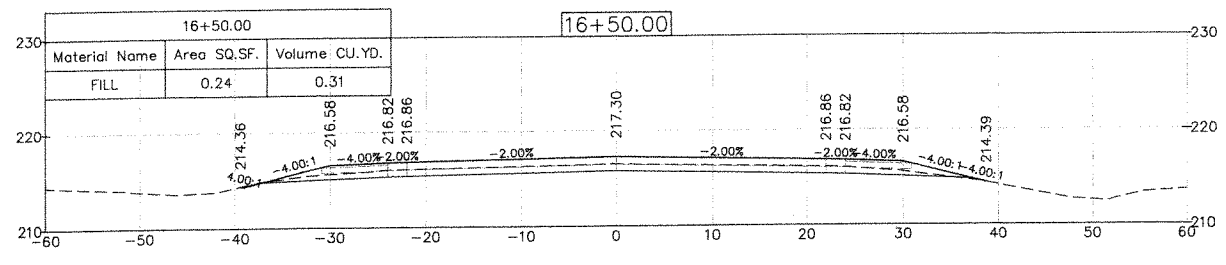
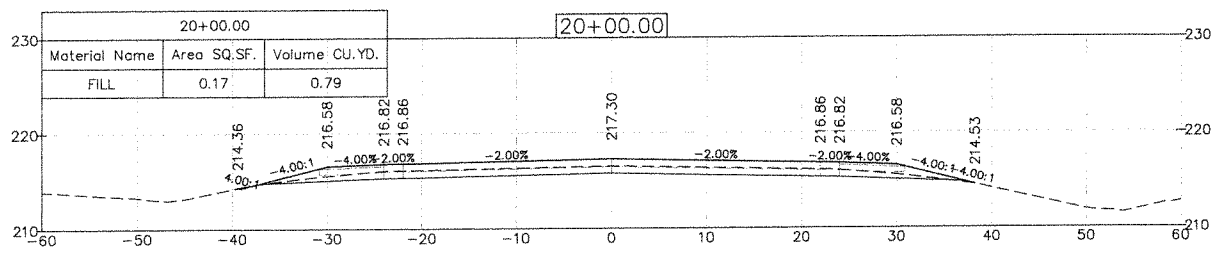
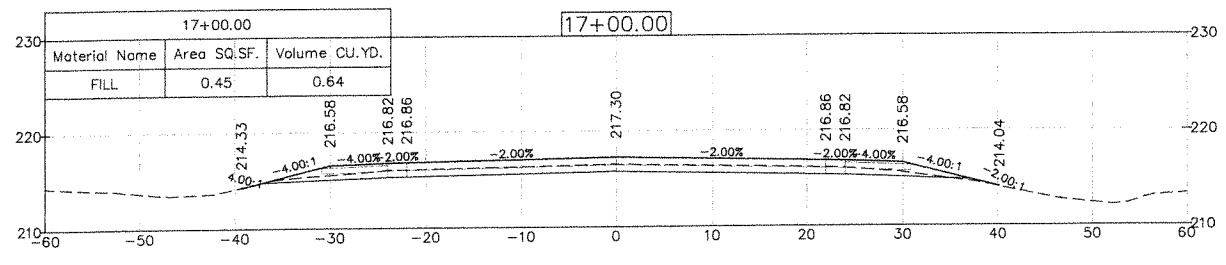
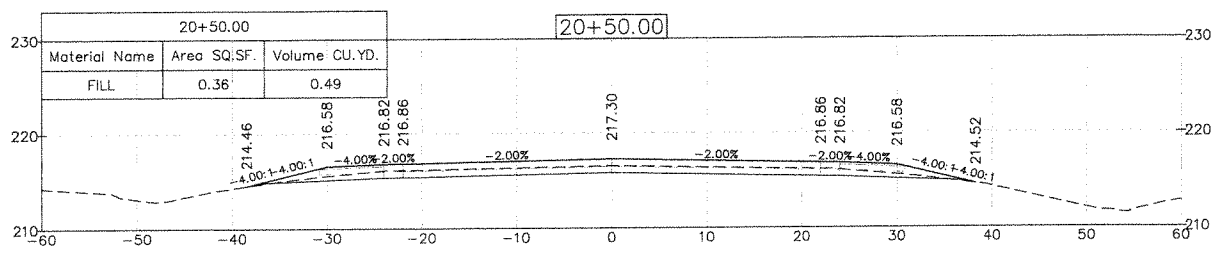
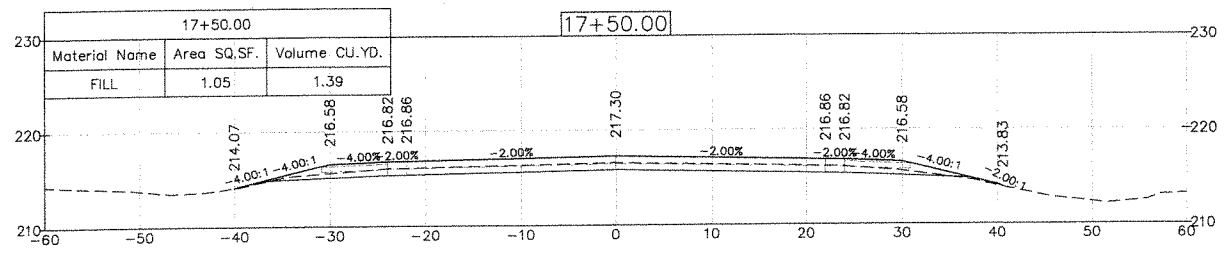
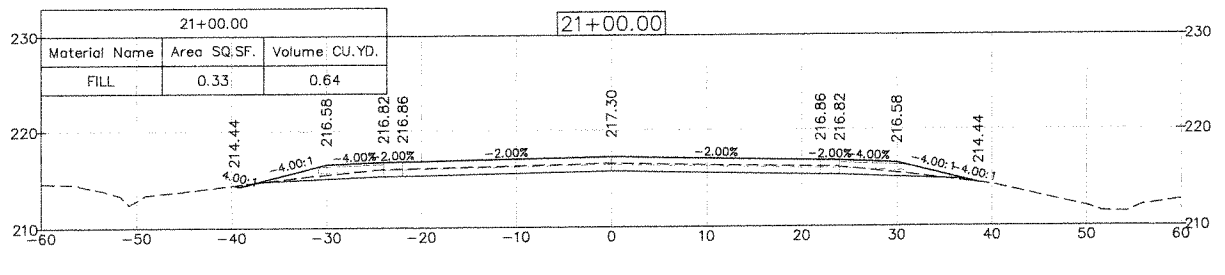
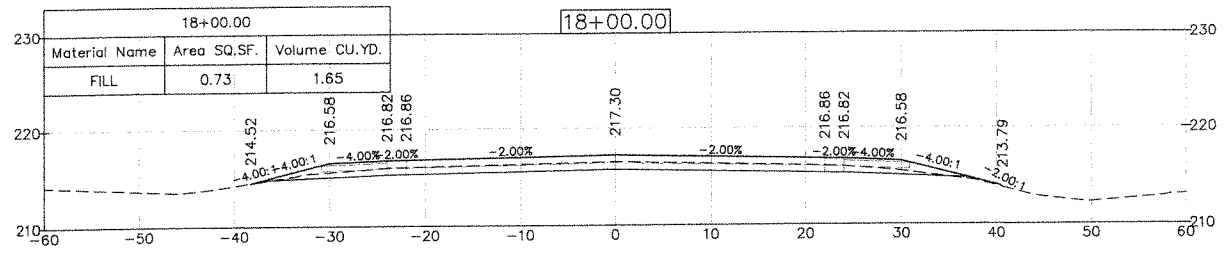
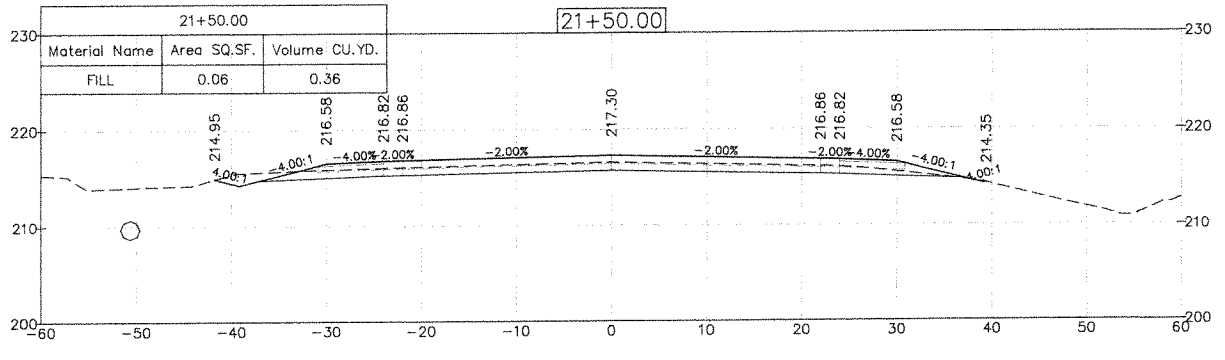
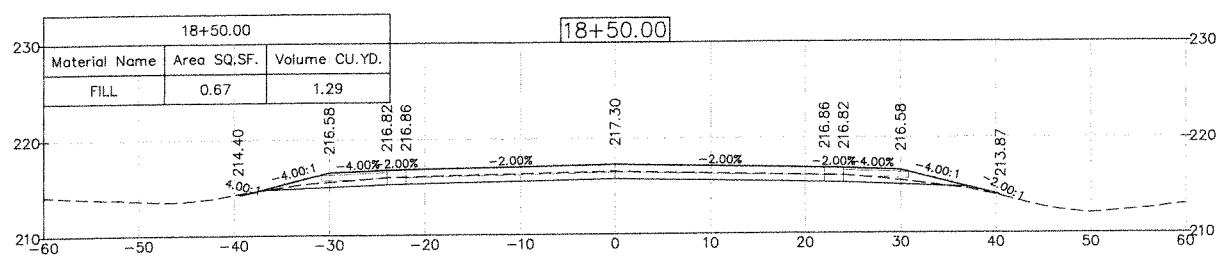
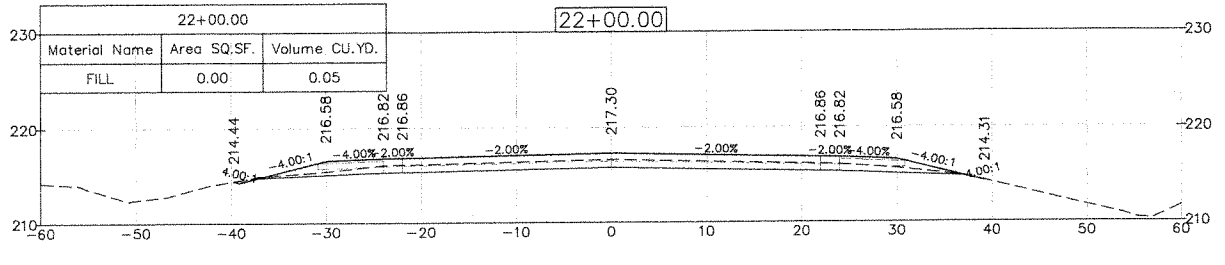
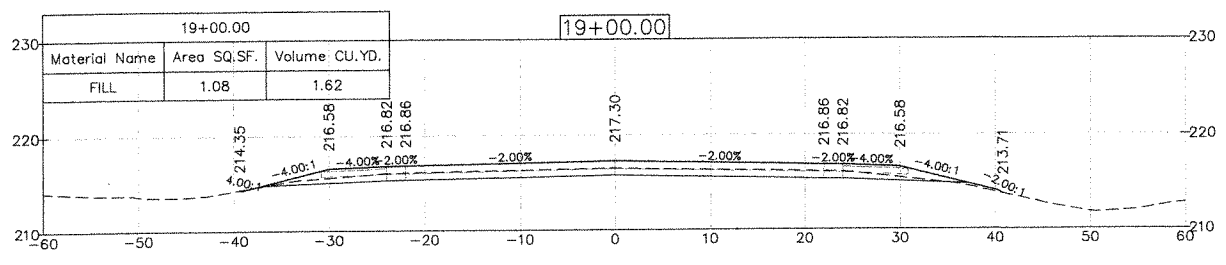
2 CROSS SECTIONS - SOUTHLAND DR



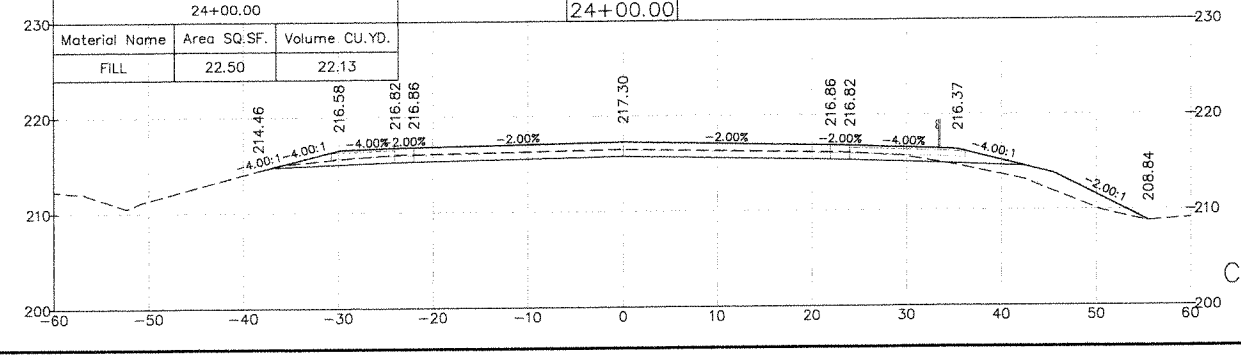
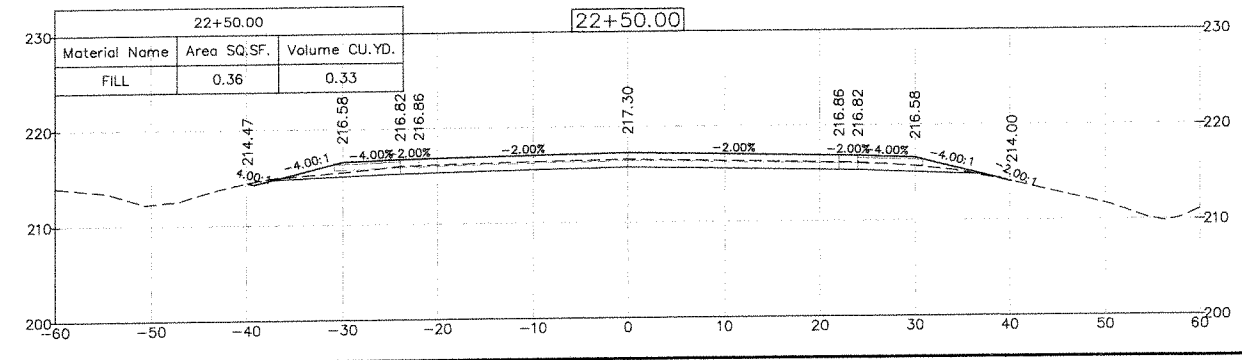
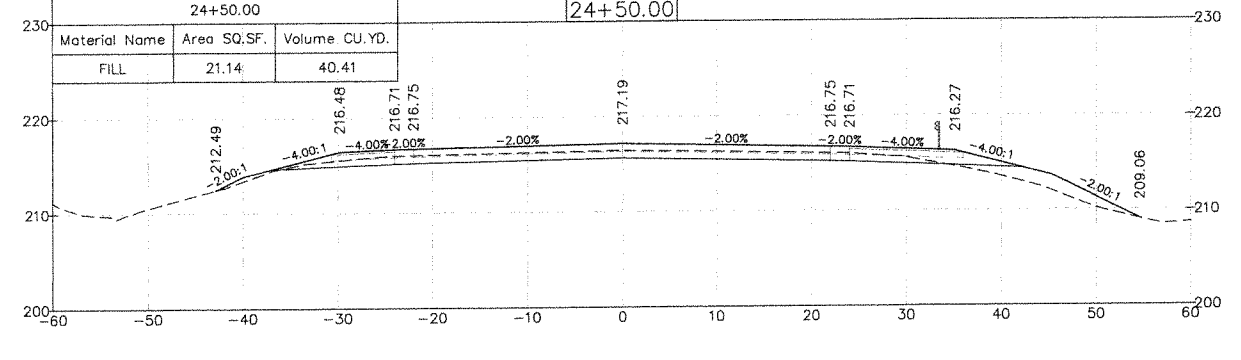
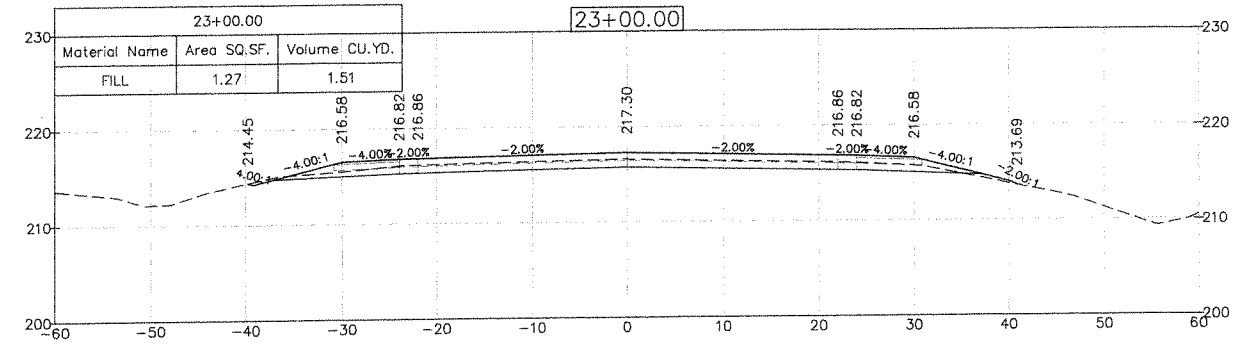
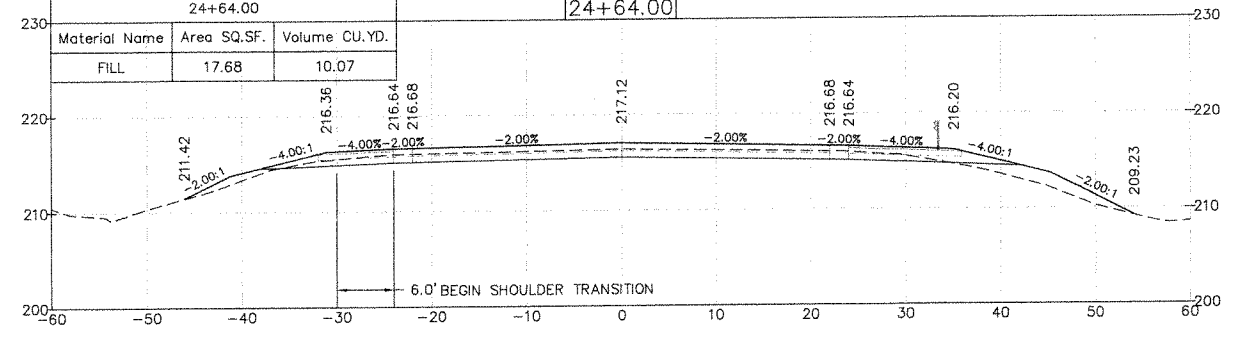
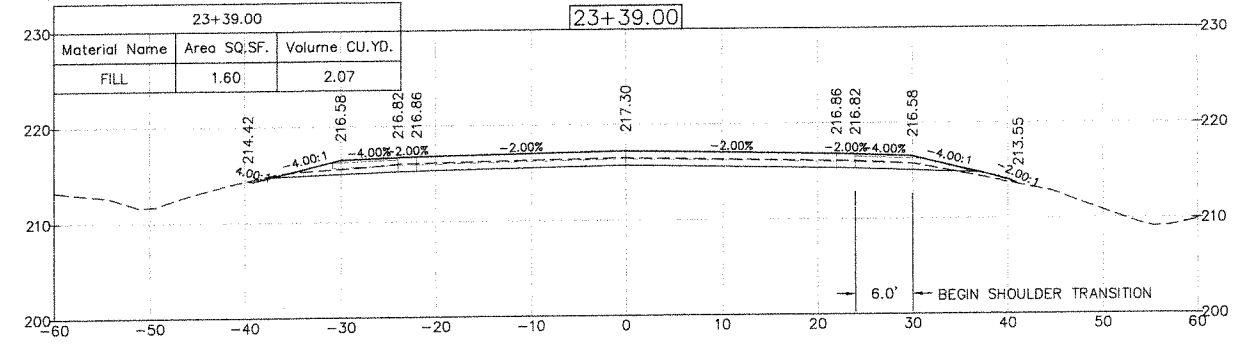
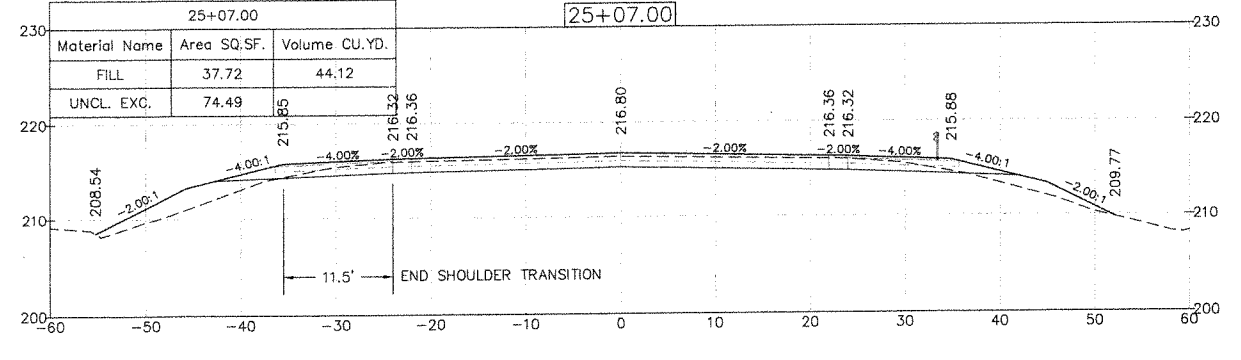
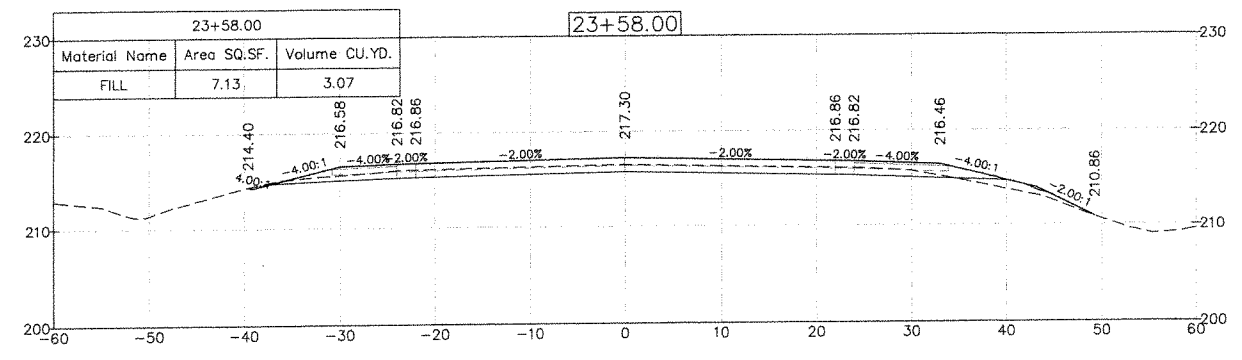
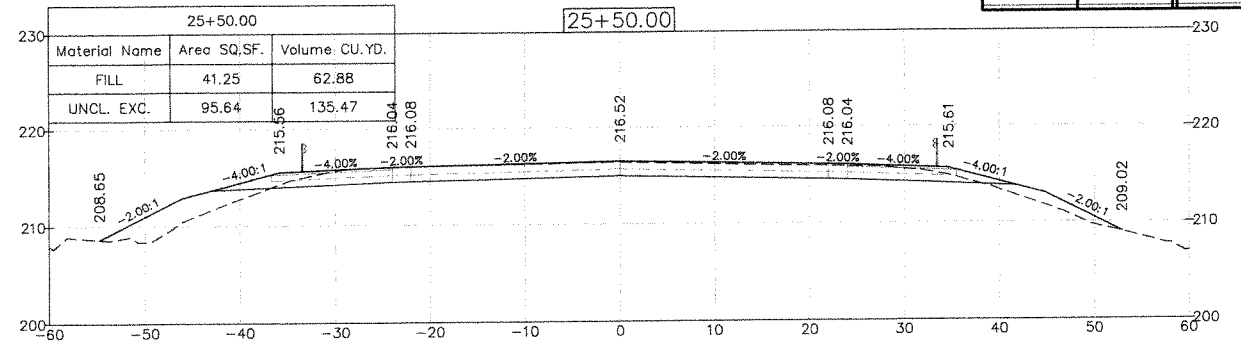
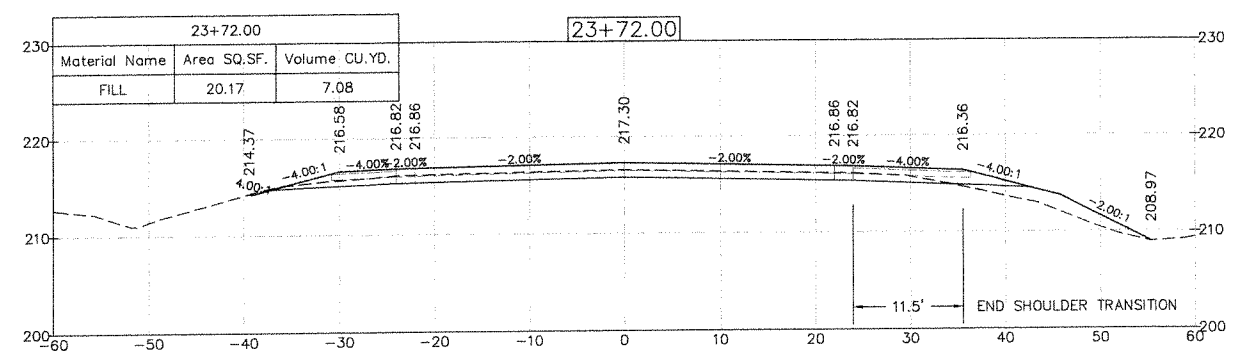
CROSS SECTIONS - SOUTHLAND DR
STA 12+00 - STA 16+00

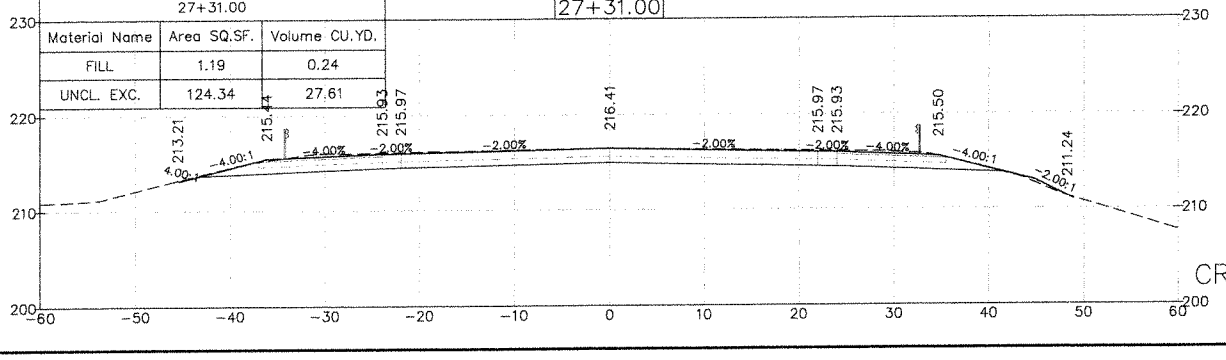
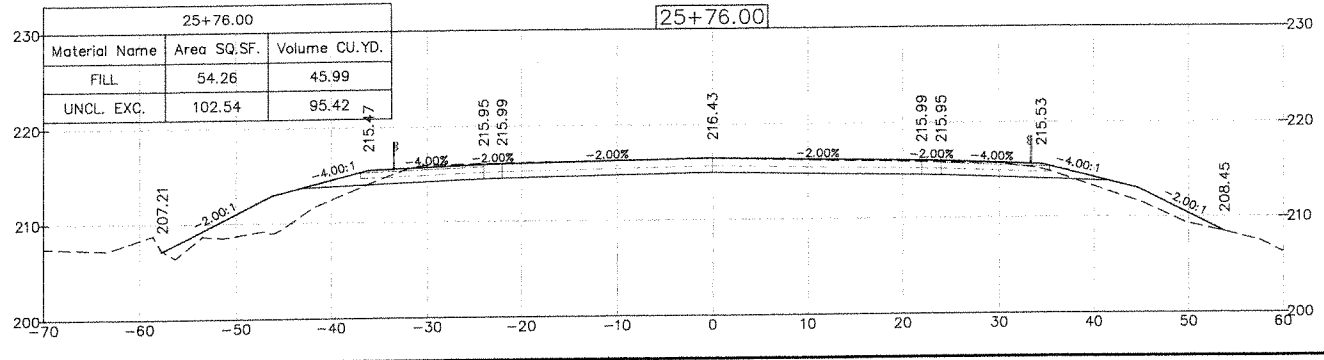
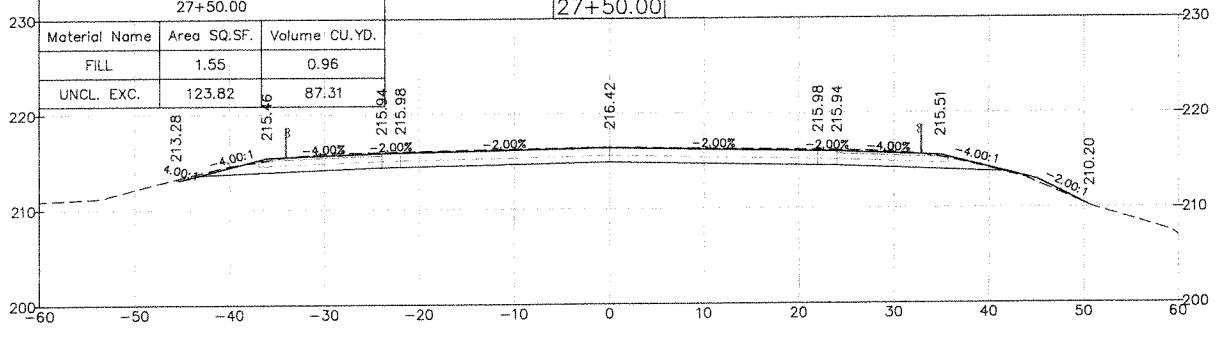
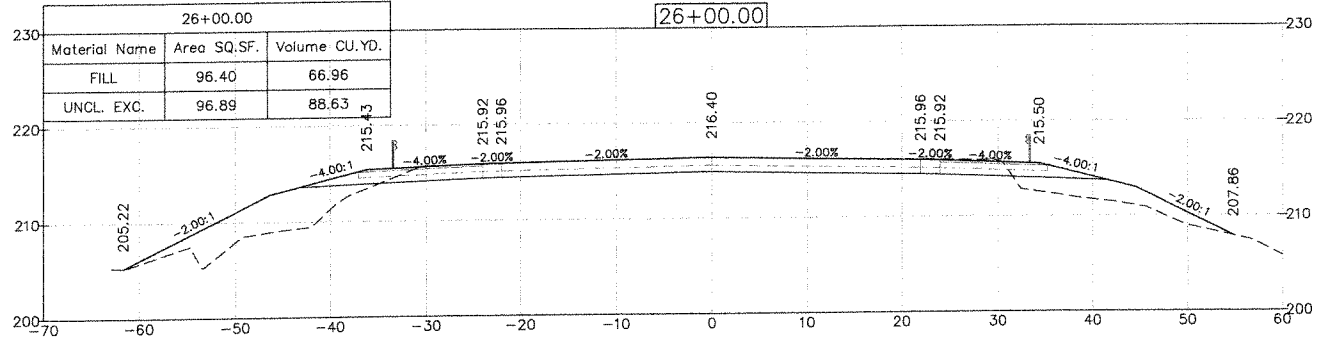
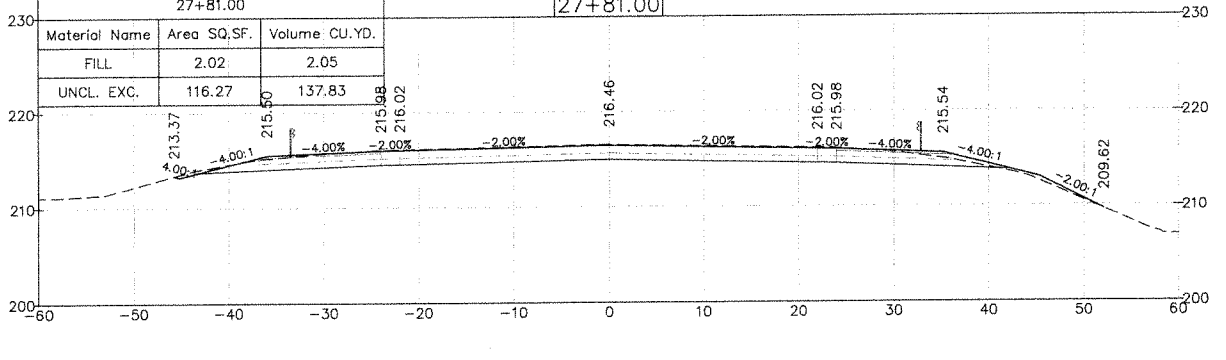
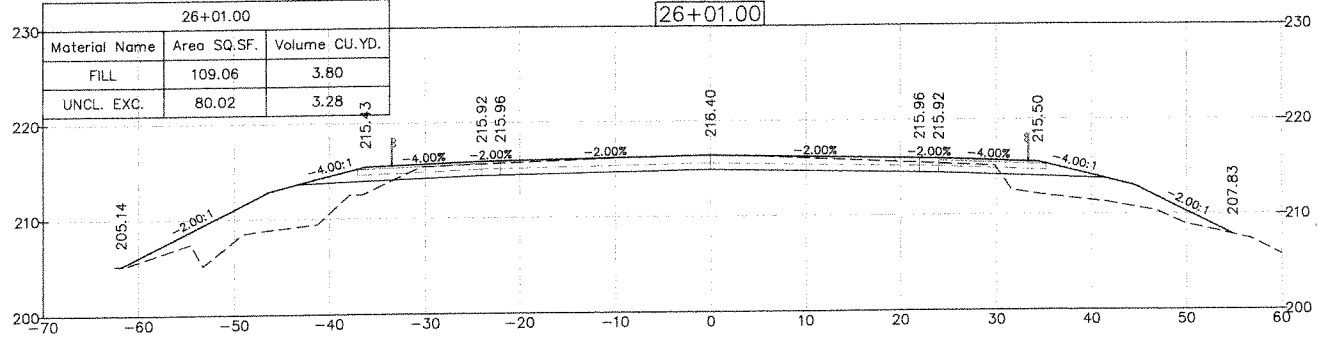
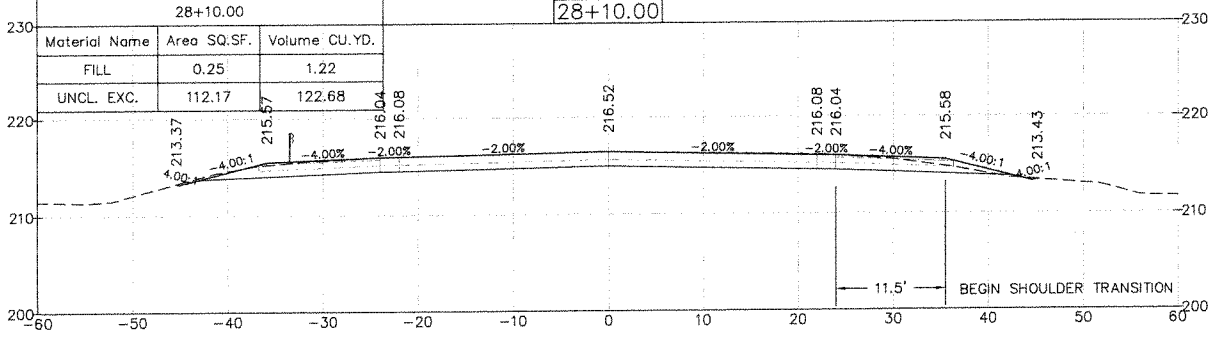
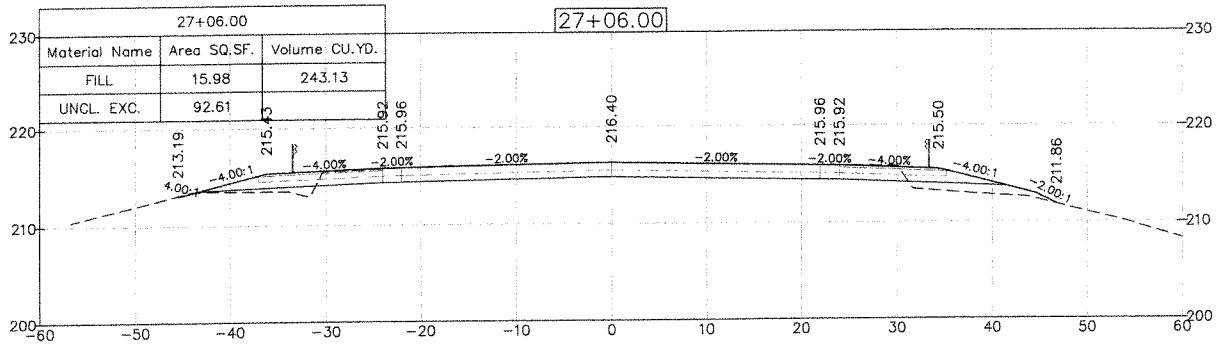
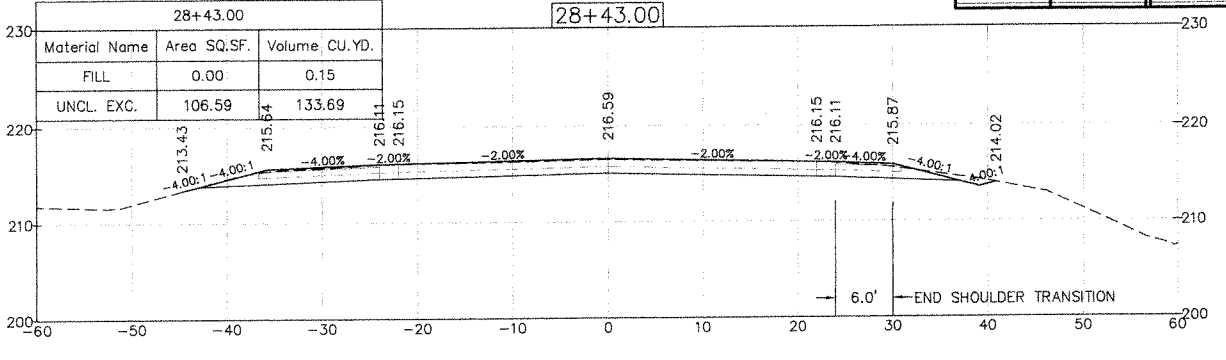
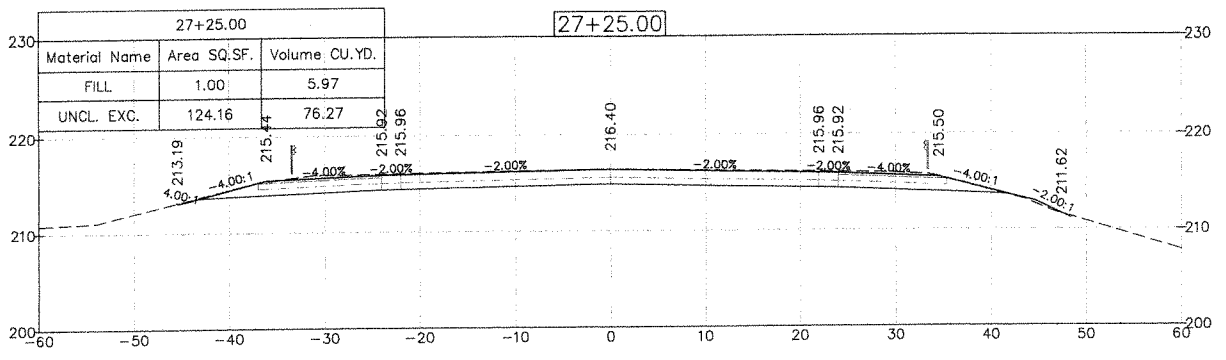
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO. 110583	44 52

2 CROSS SECTIONS - SOUTHLAND DR



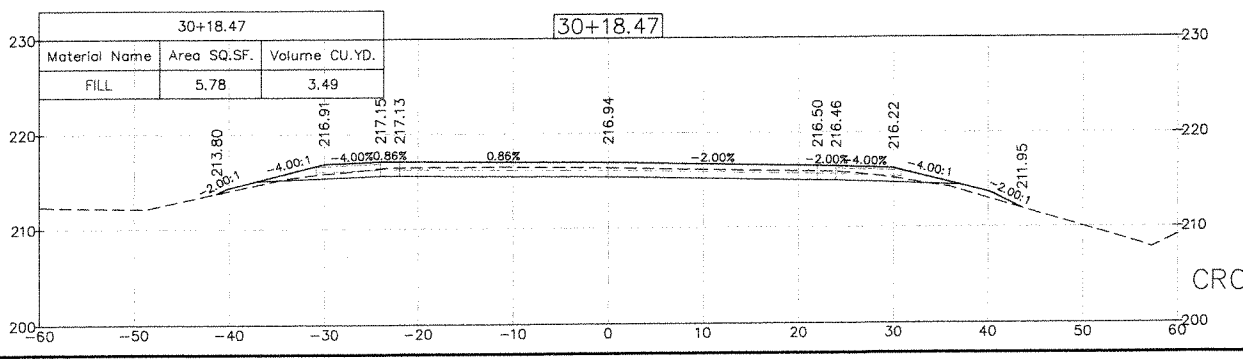
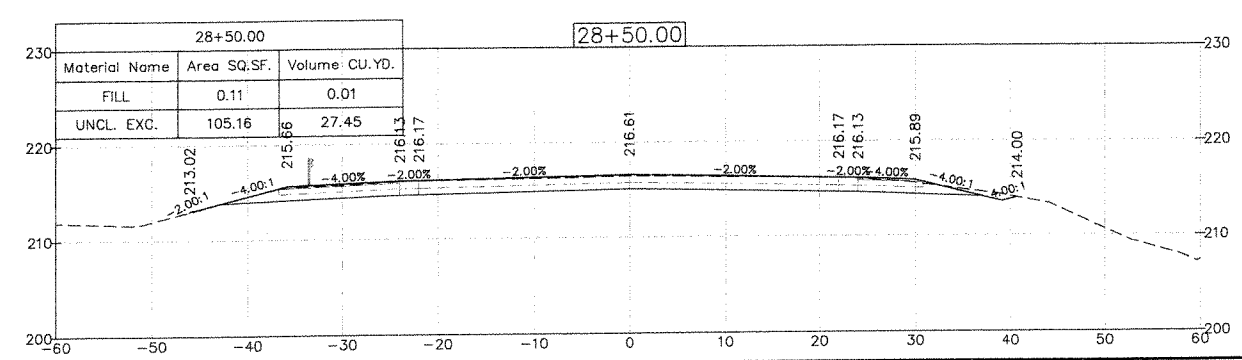
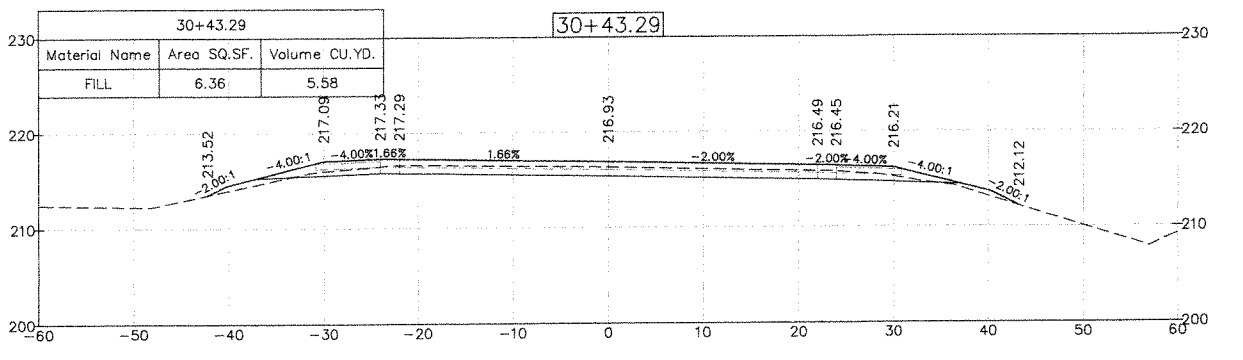
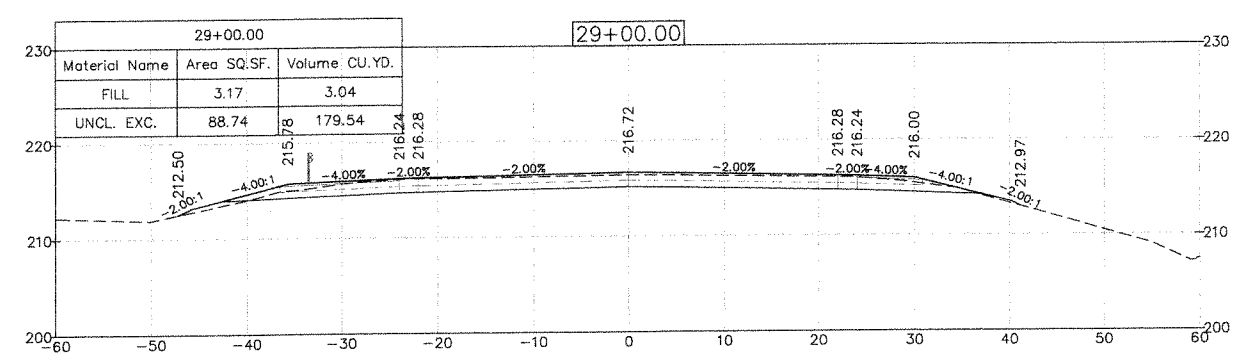
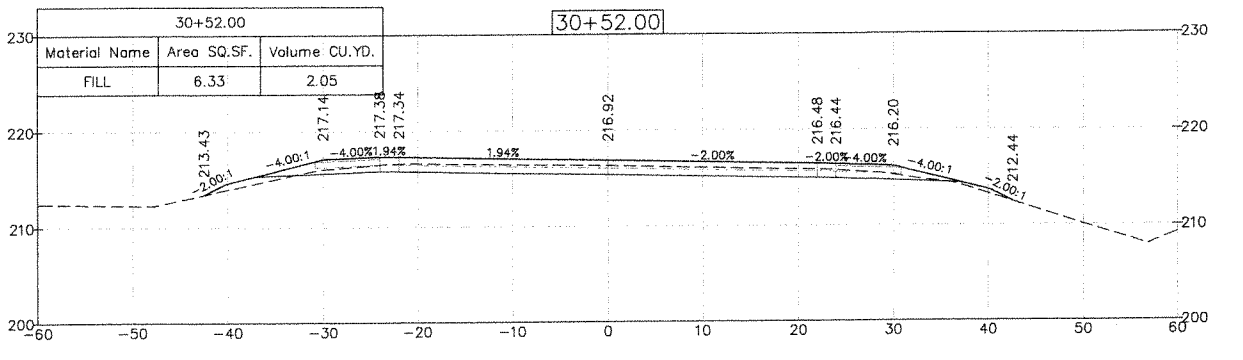
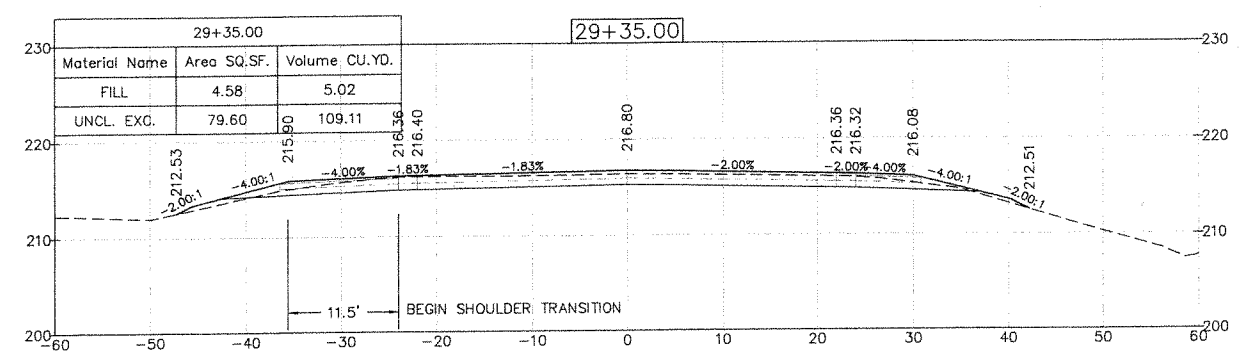
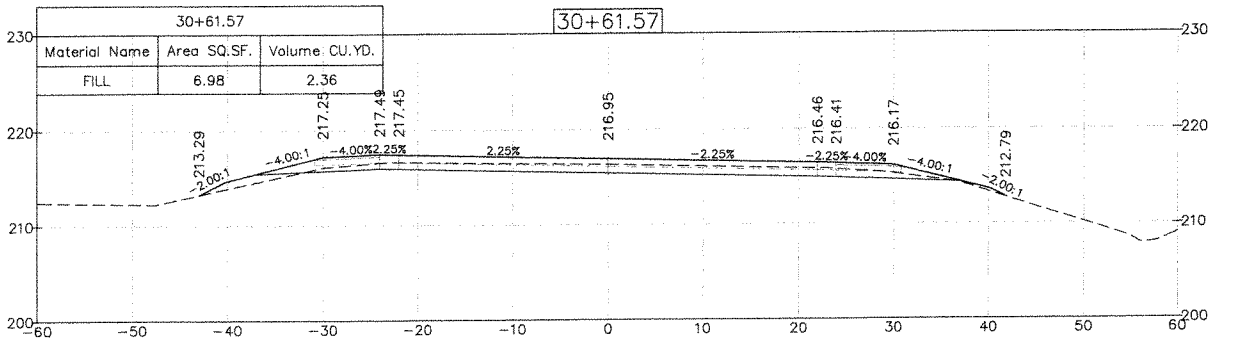
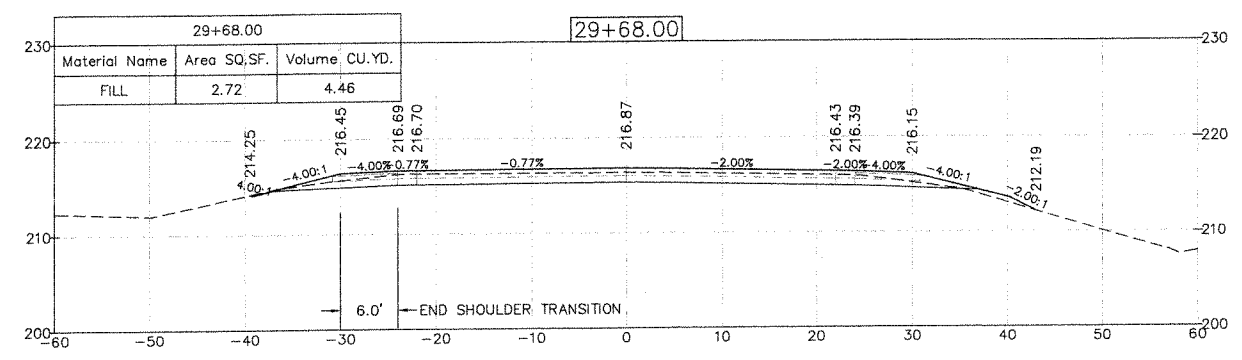
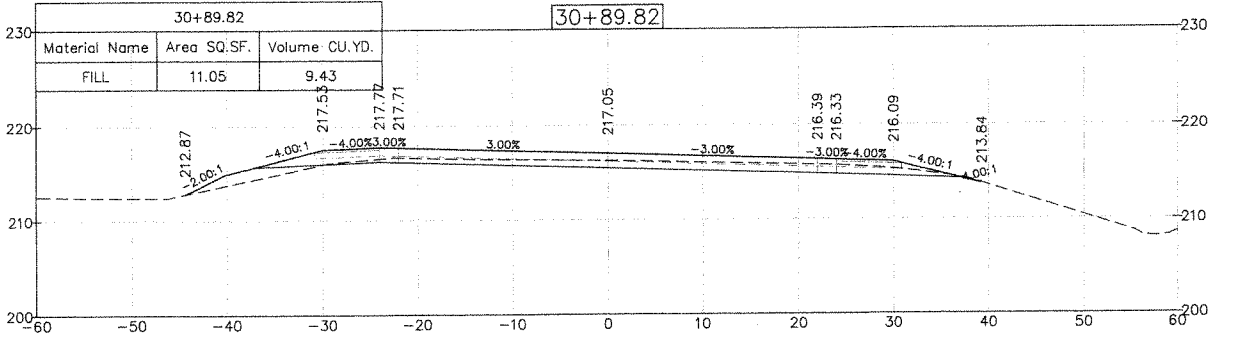
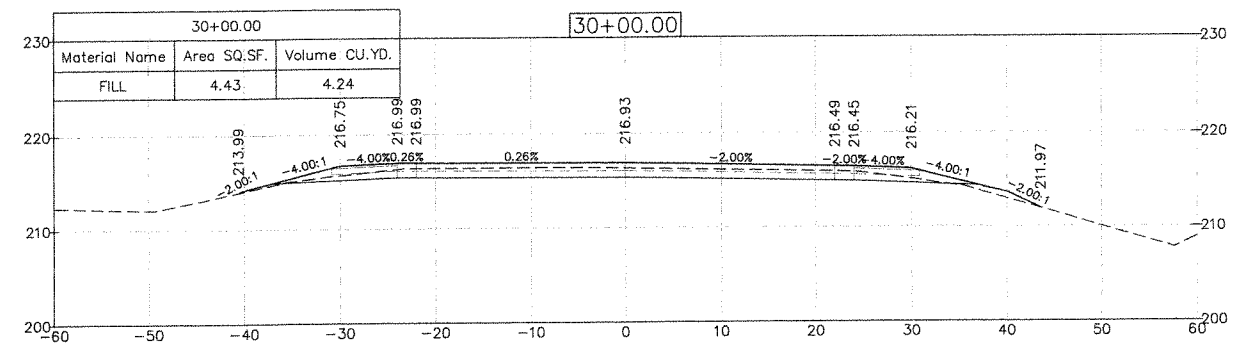
CROSS SECTIONS - SOUTHLAND DR
STA 16+50 - STA 22+00





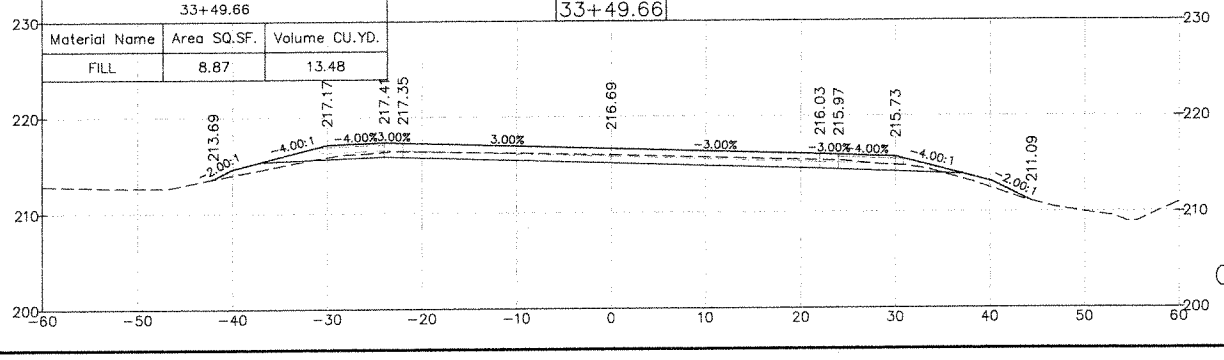
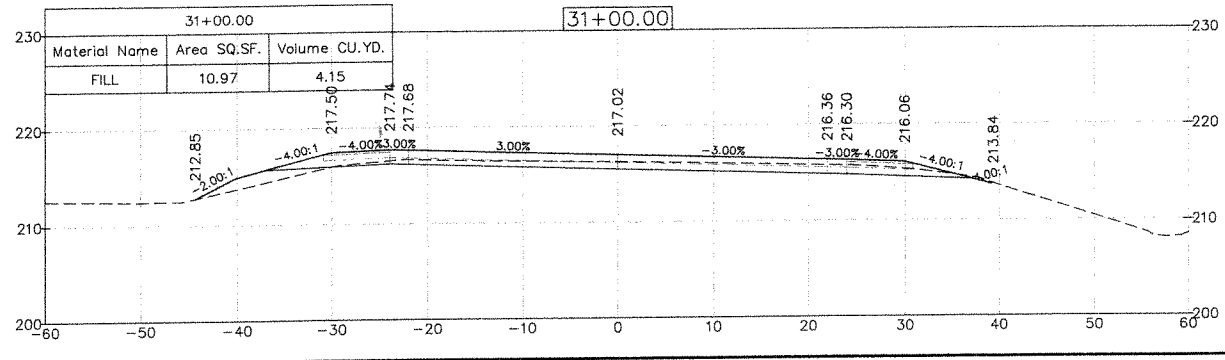
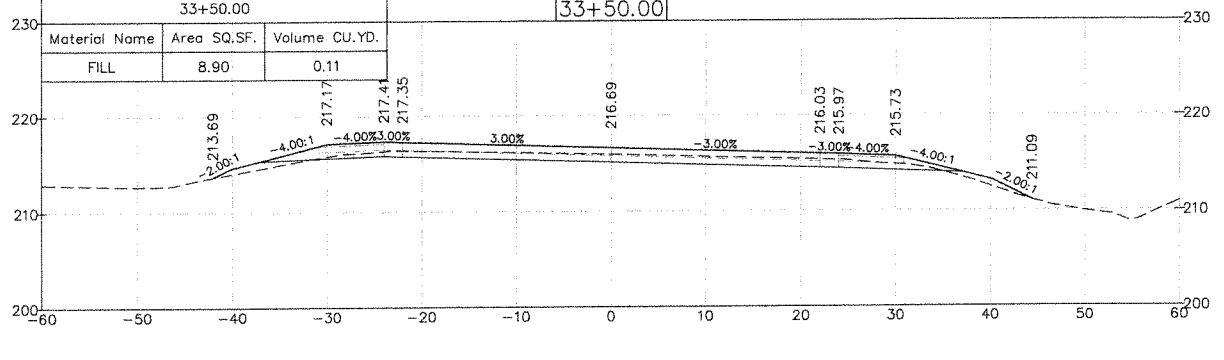
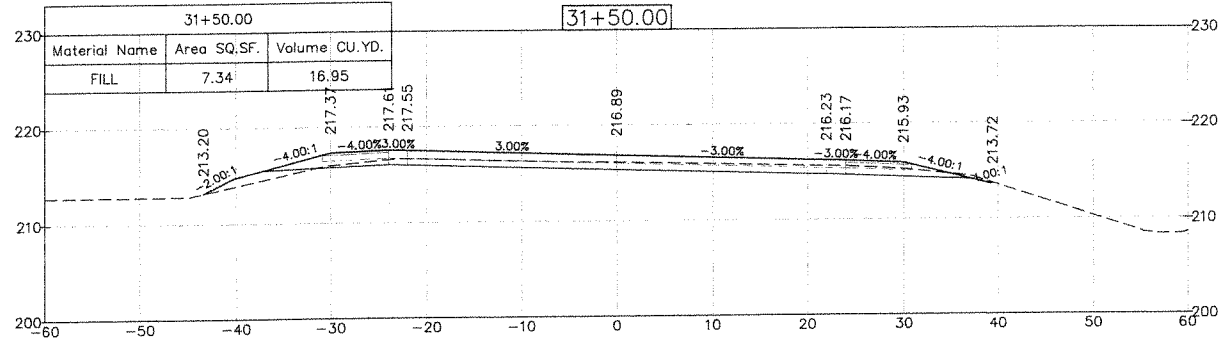
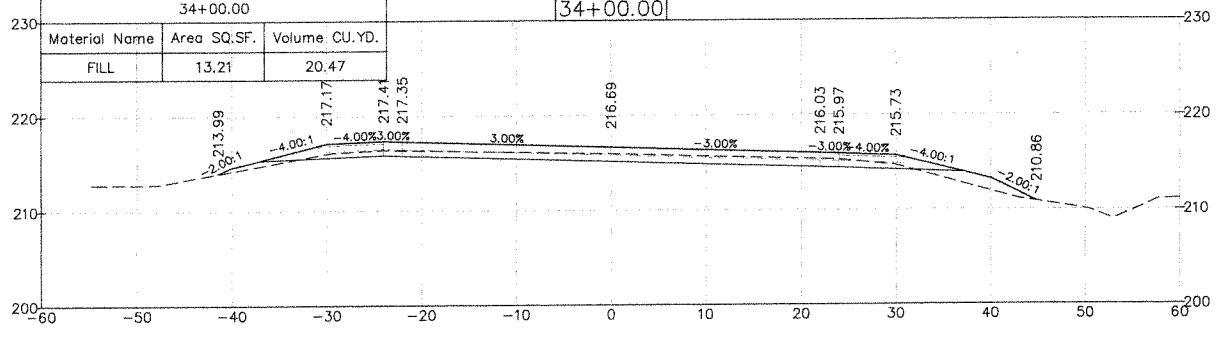
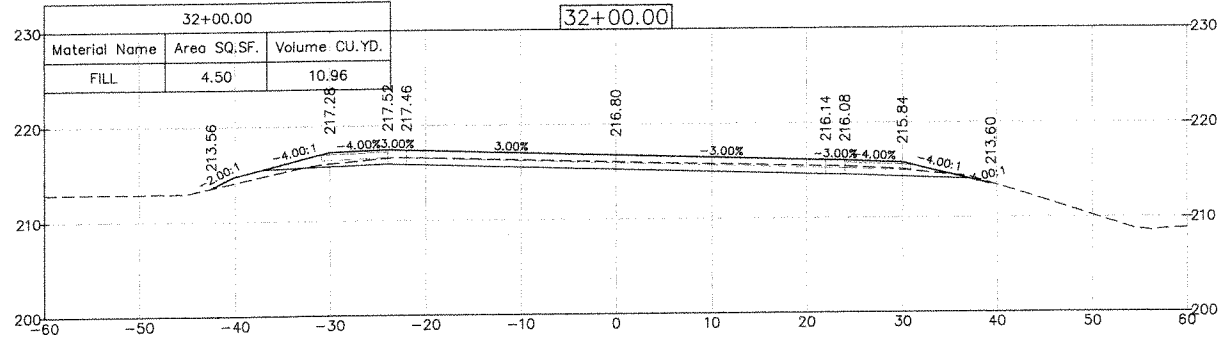
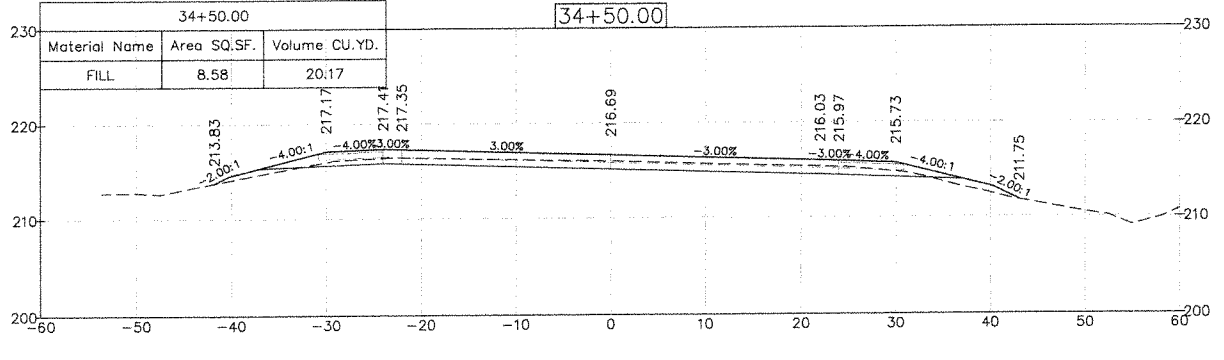
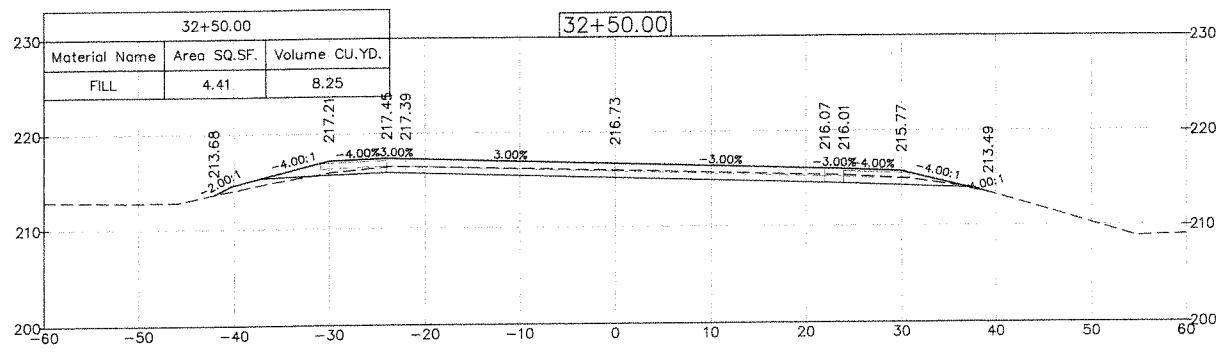
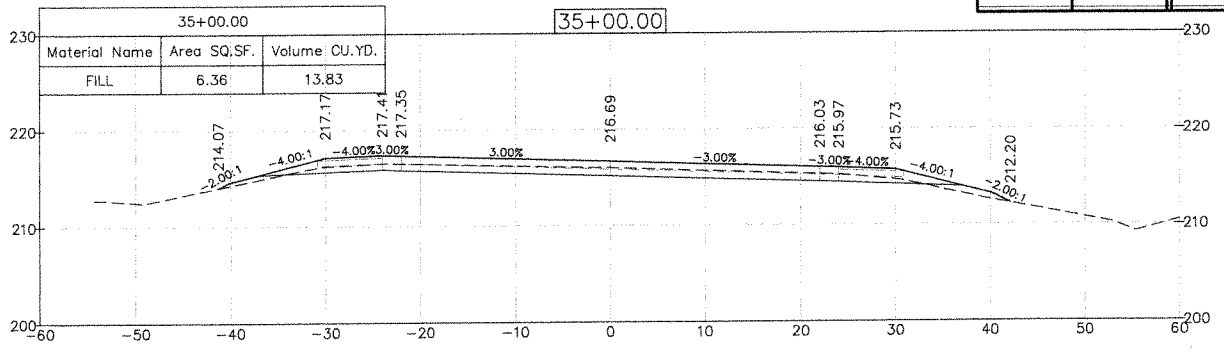
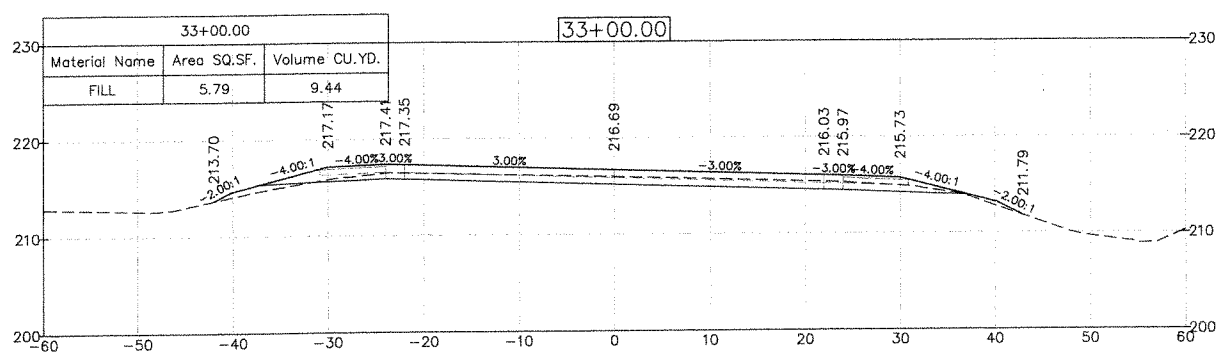
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						110583	47	52

2 CROSS SECTIONS - SOUTHLAND DR



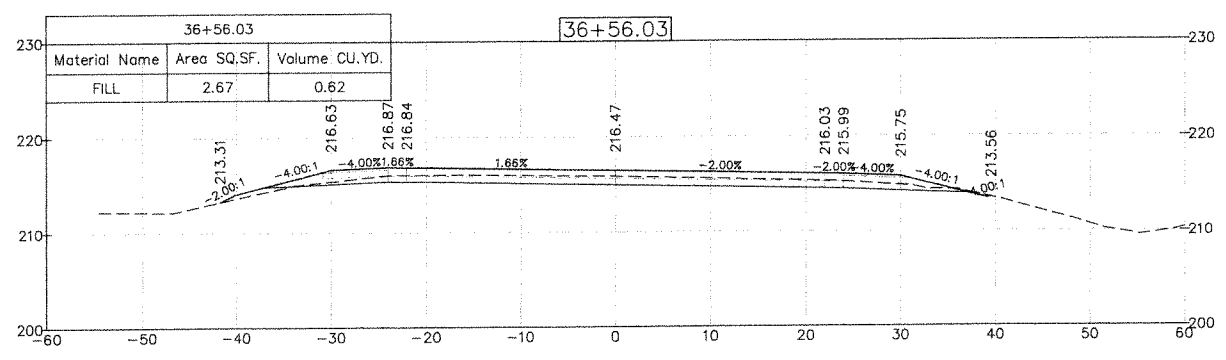
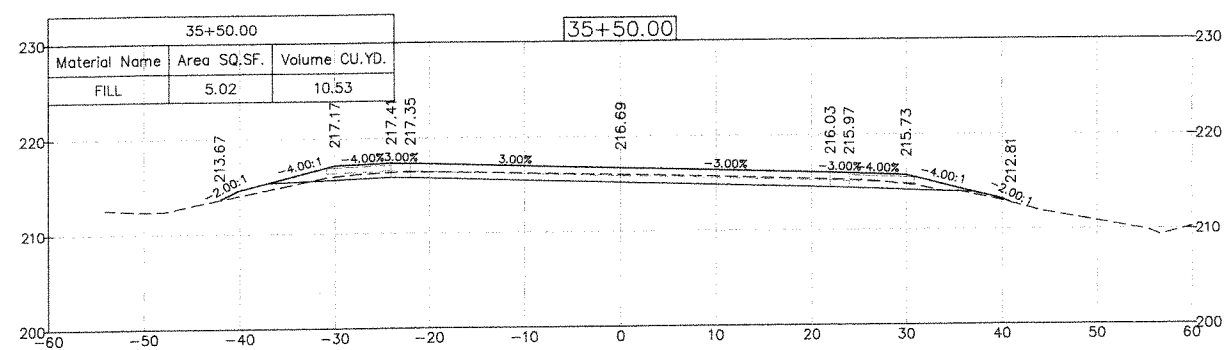
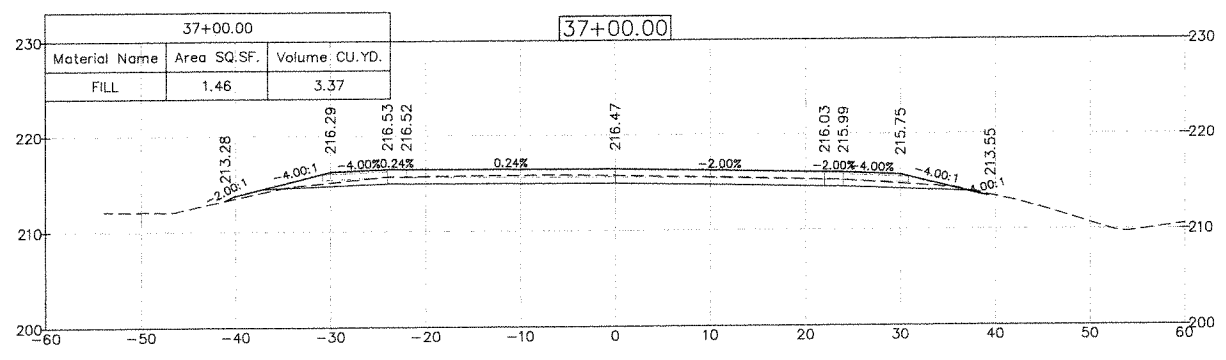
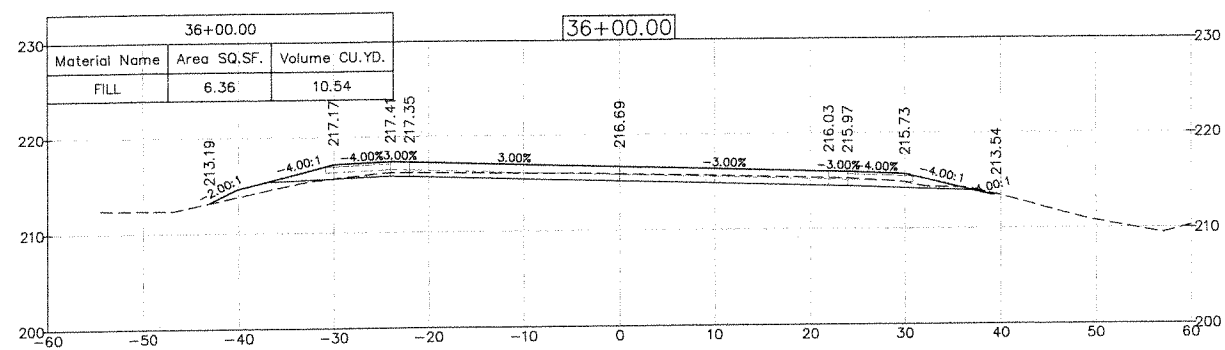
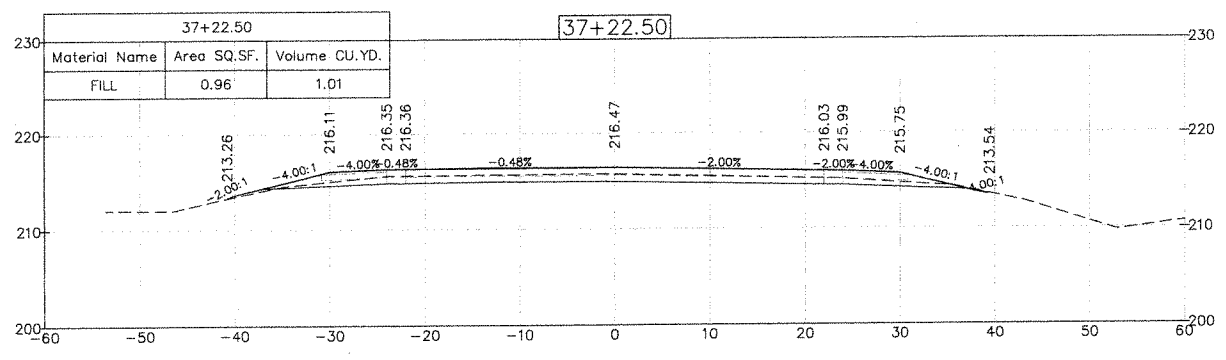
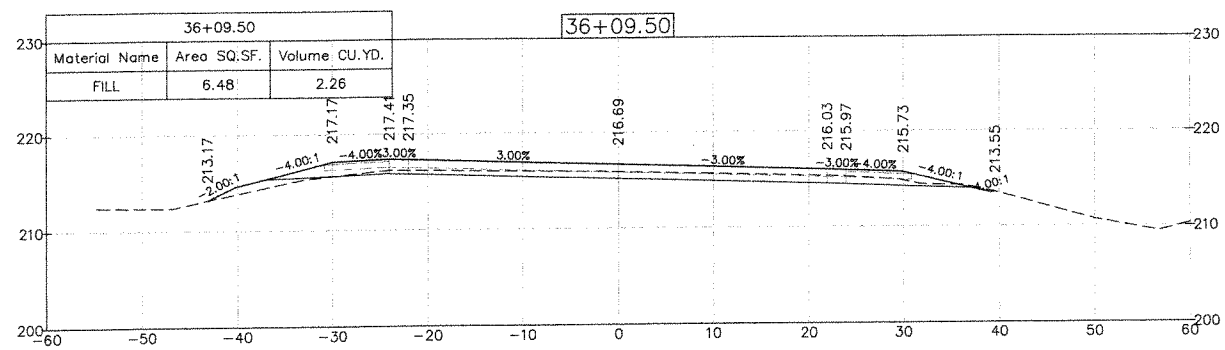
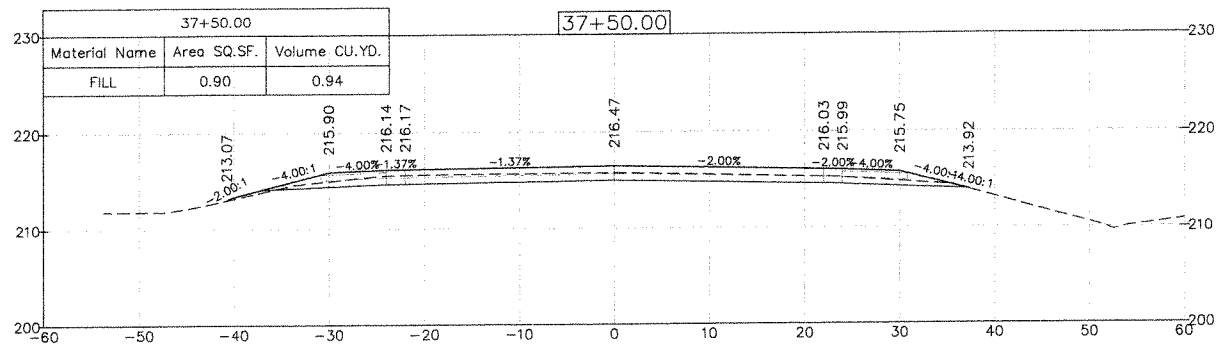
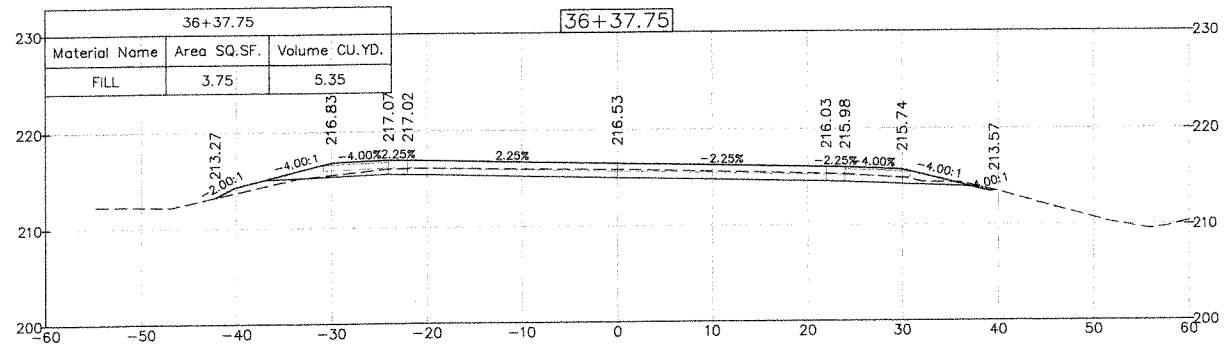
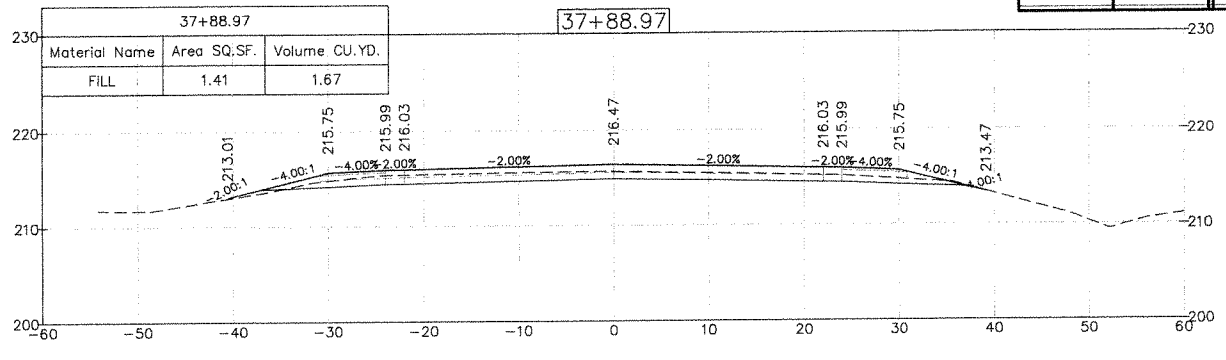
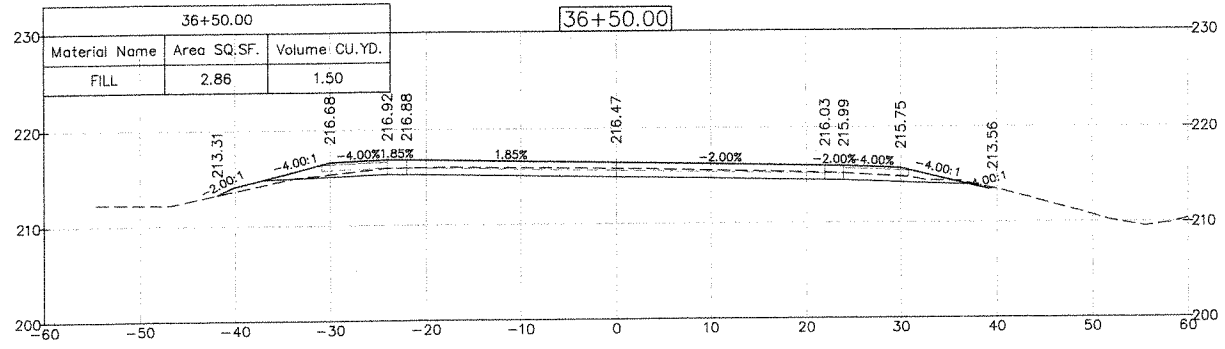
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK		48	52

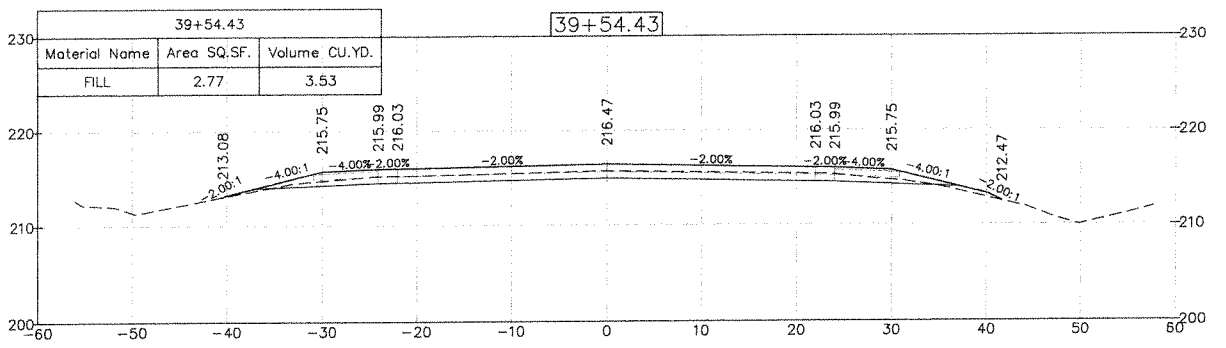
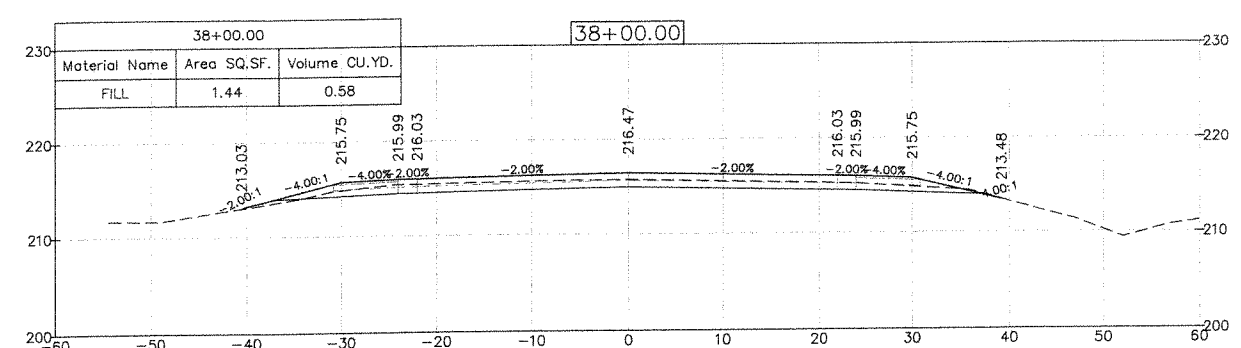
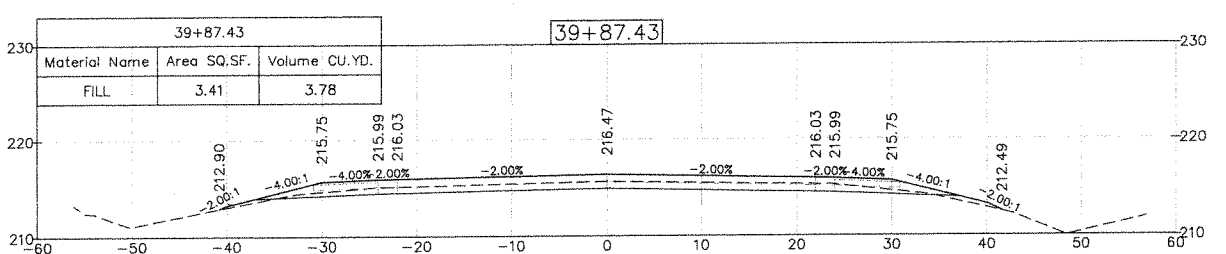
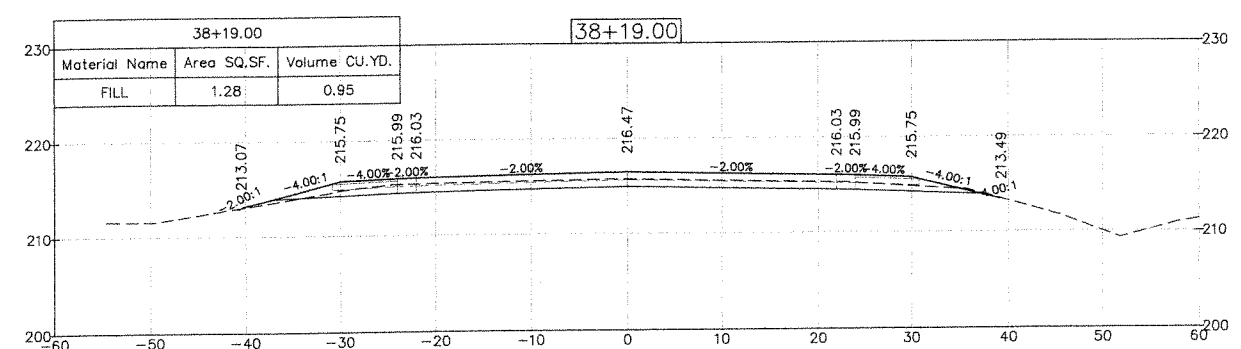
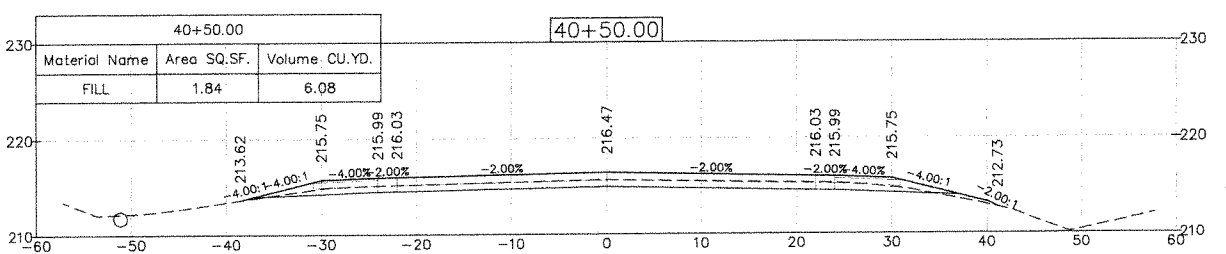
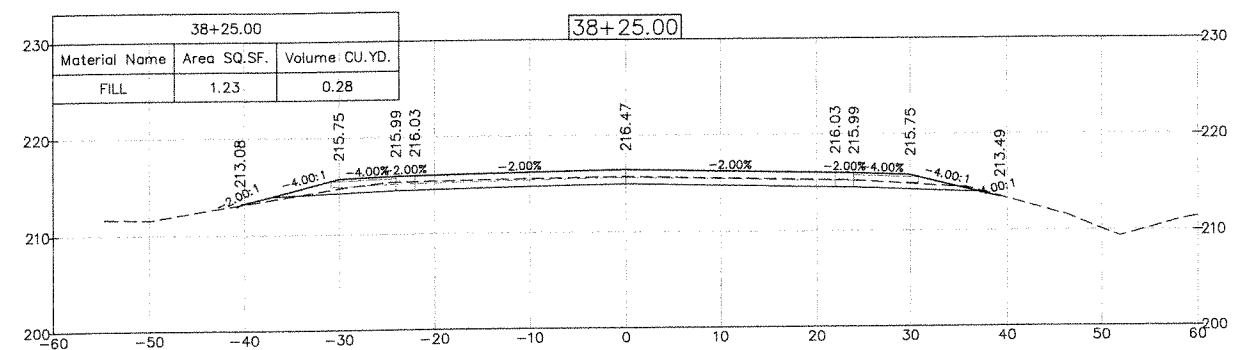
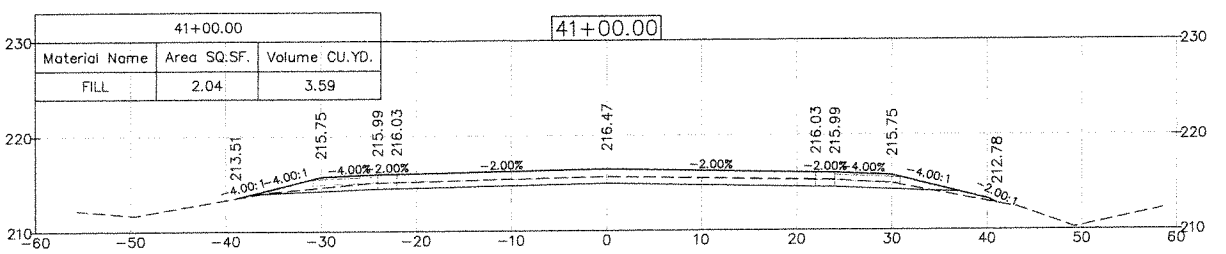
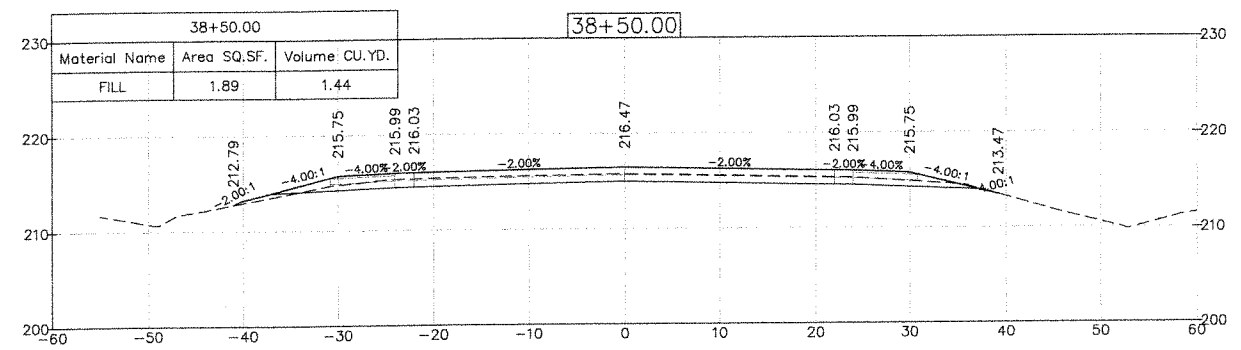
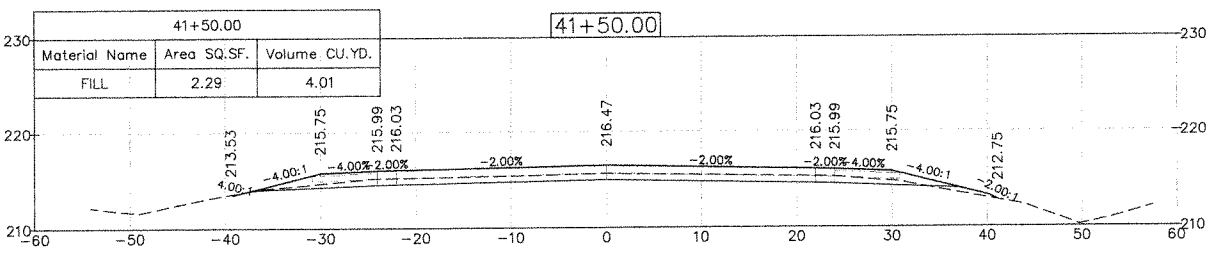
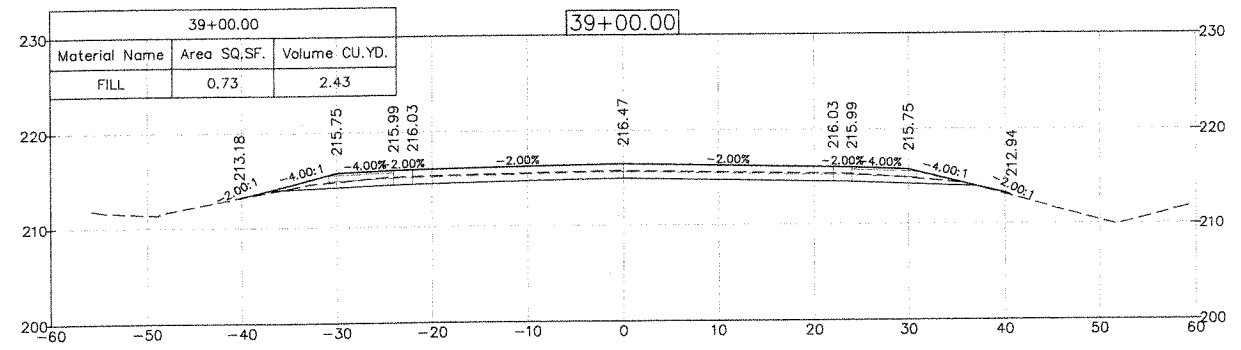
JOB NO. 110583 CROSS SECTIONS - SOUTHLAND DR



CROSS SECTIONS - SOUTHLAND DR
STA 31+00 - STA 35+00

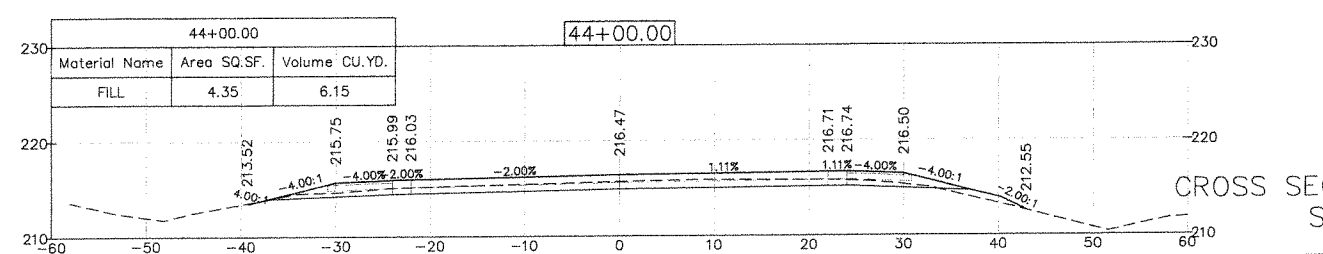
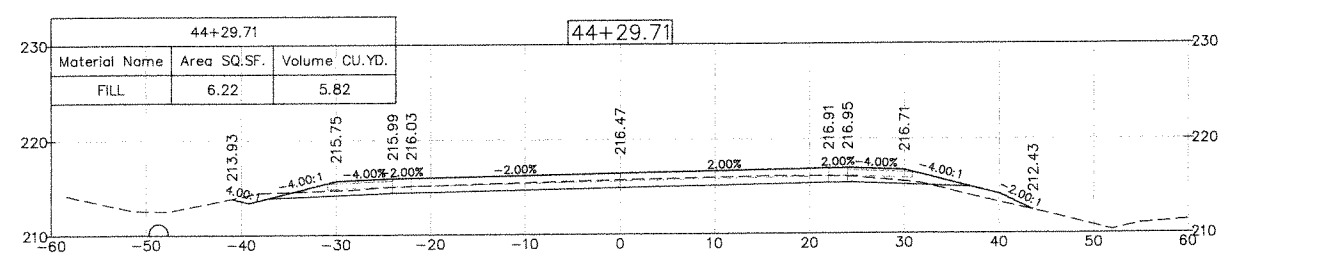
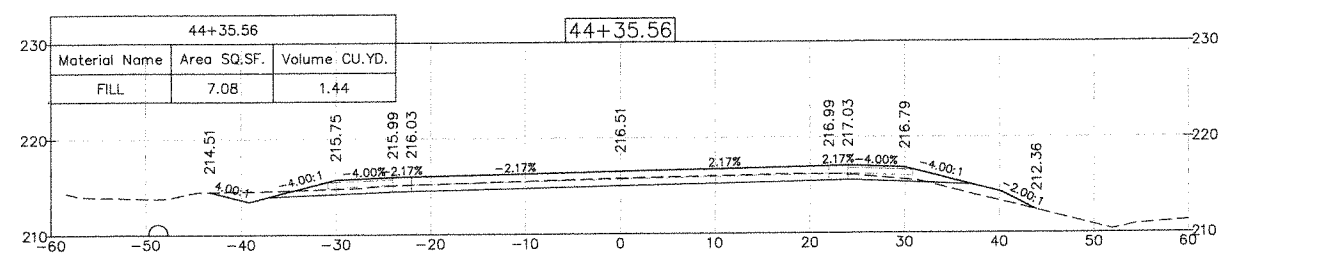
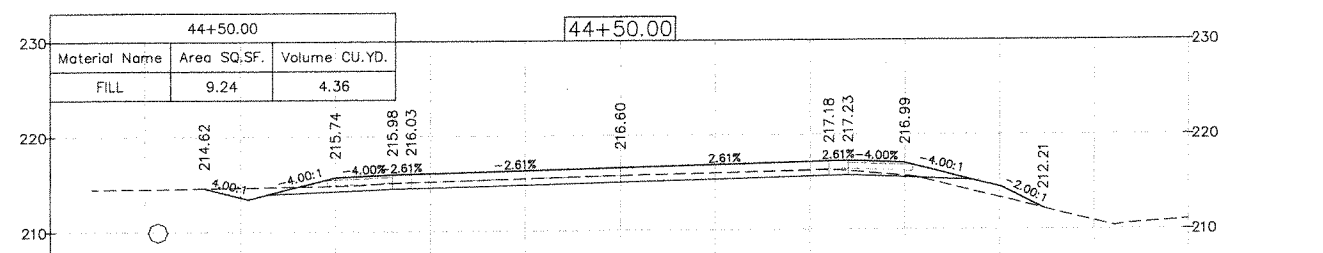
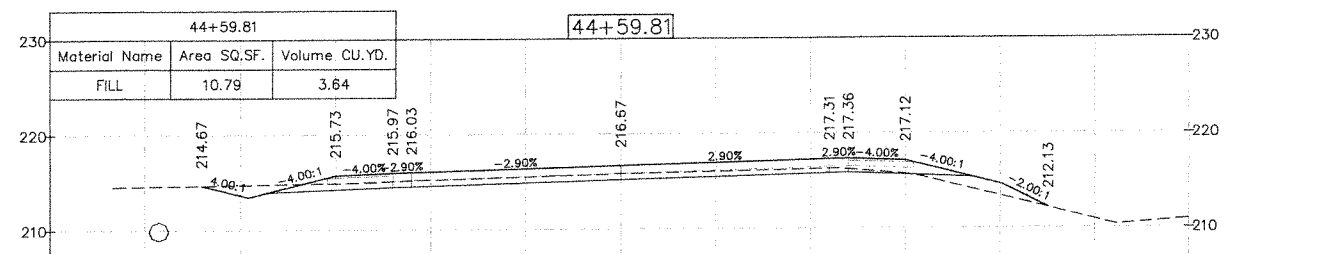
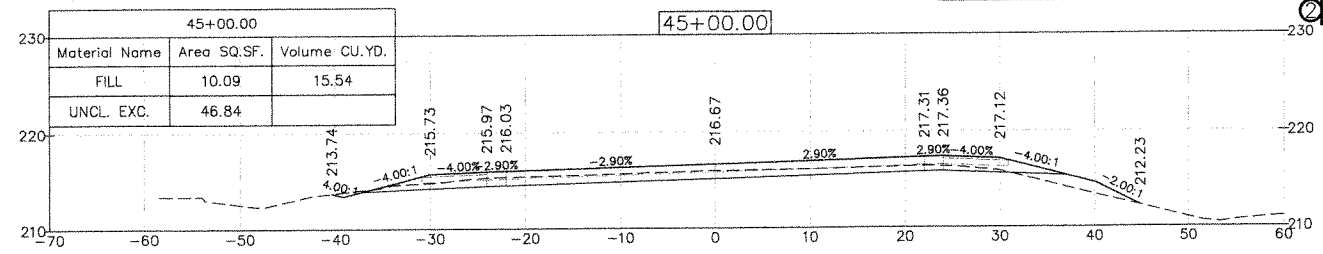
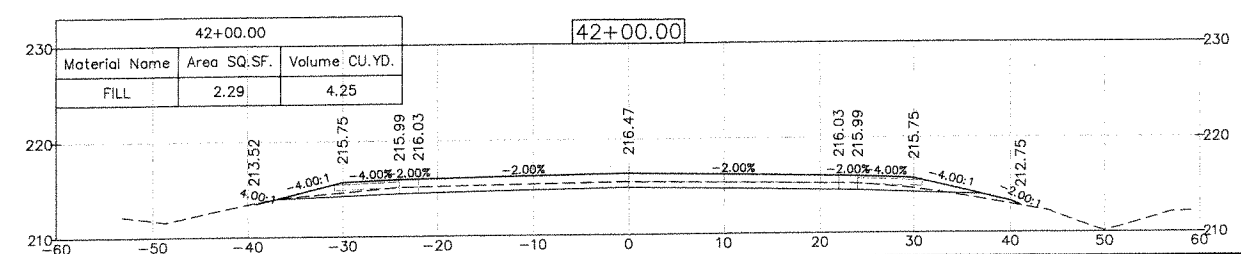
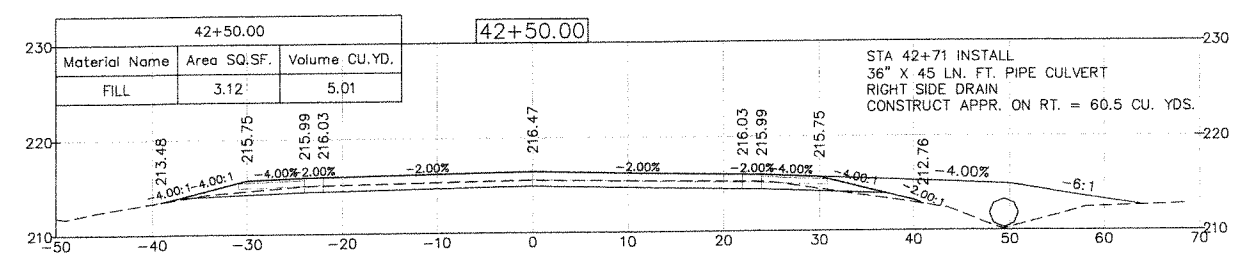
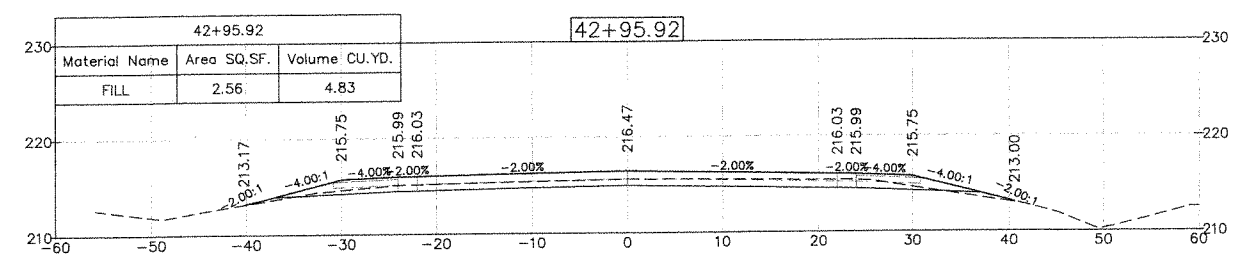
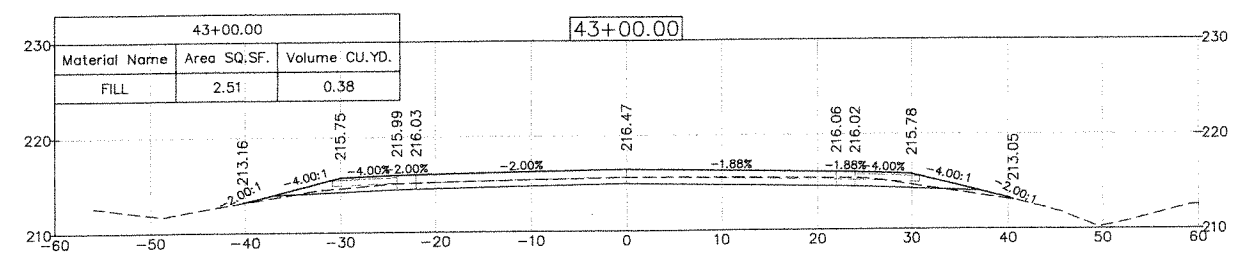
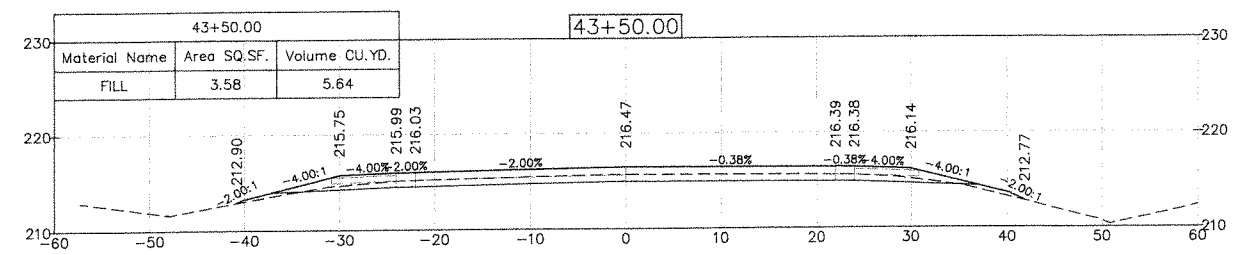
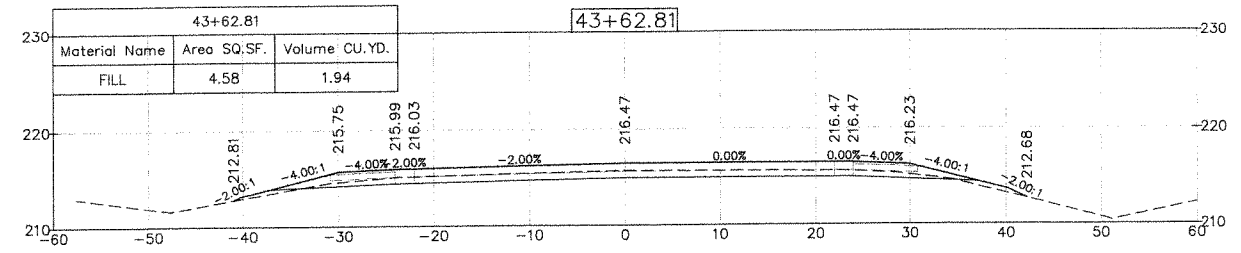
2 CROSS SECTIONS - SOUTHLAND DR





DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK			
							JOB NO. 110583	51 52

CROSS SECTIONS - SOUTHLAND DR



CROSS SECTIONS - SOUTHLAND DR
STA 42+00 - STA 45+00

