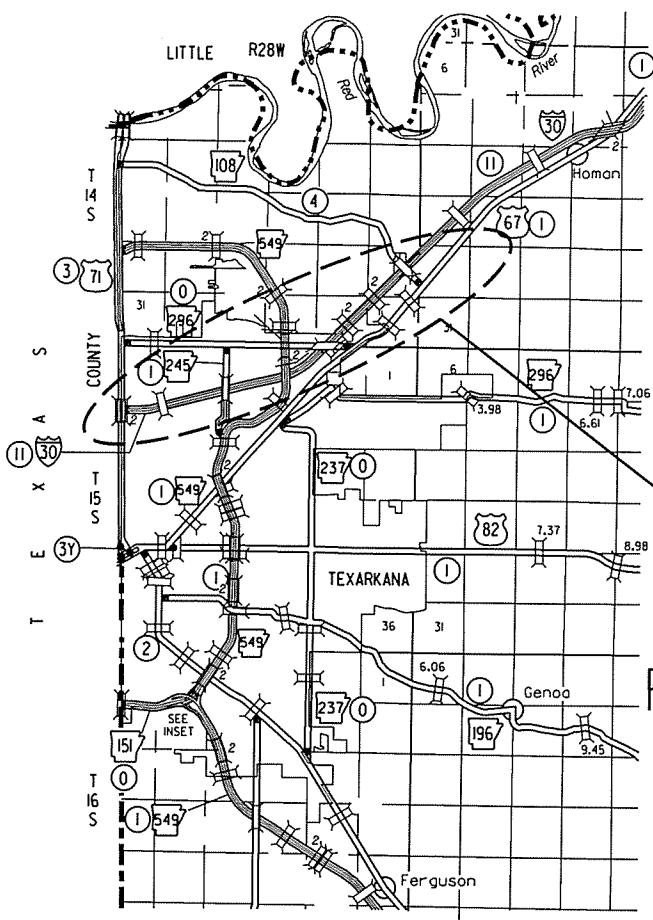


"A FULLY CONTROLLED ACCESS FACILITY"
 ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT
 CONSTRUCTION PLANS FOR STATE HIGHWAY

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0301		1	66

TEXAS STATE LINE-HWY. 108 (S)



VICINITY MAP

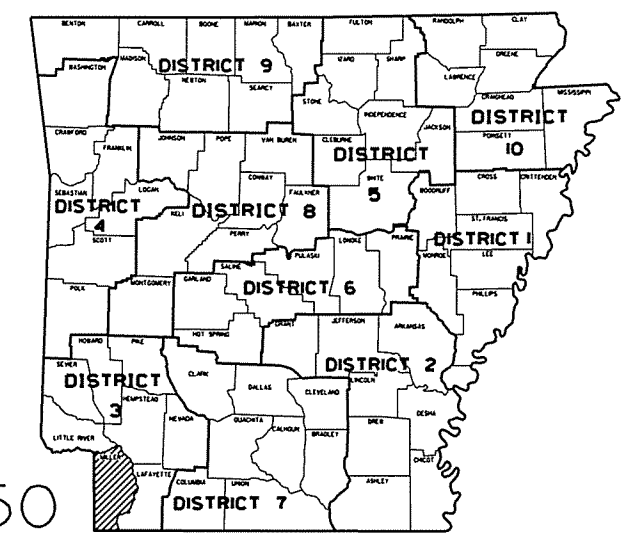
TEXAS STATE LINE-HWY. 108 (S)

MILLER COUNTY

ROUTE 30 SECTION II

JOB NO. BB0301

FED. AID PROJ. BIM-B30-0(201) & 9050

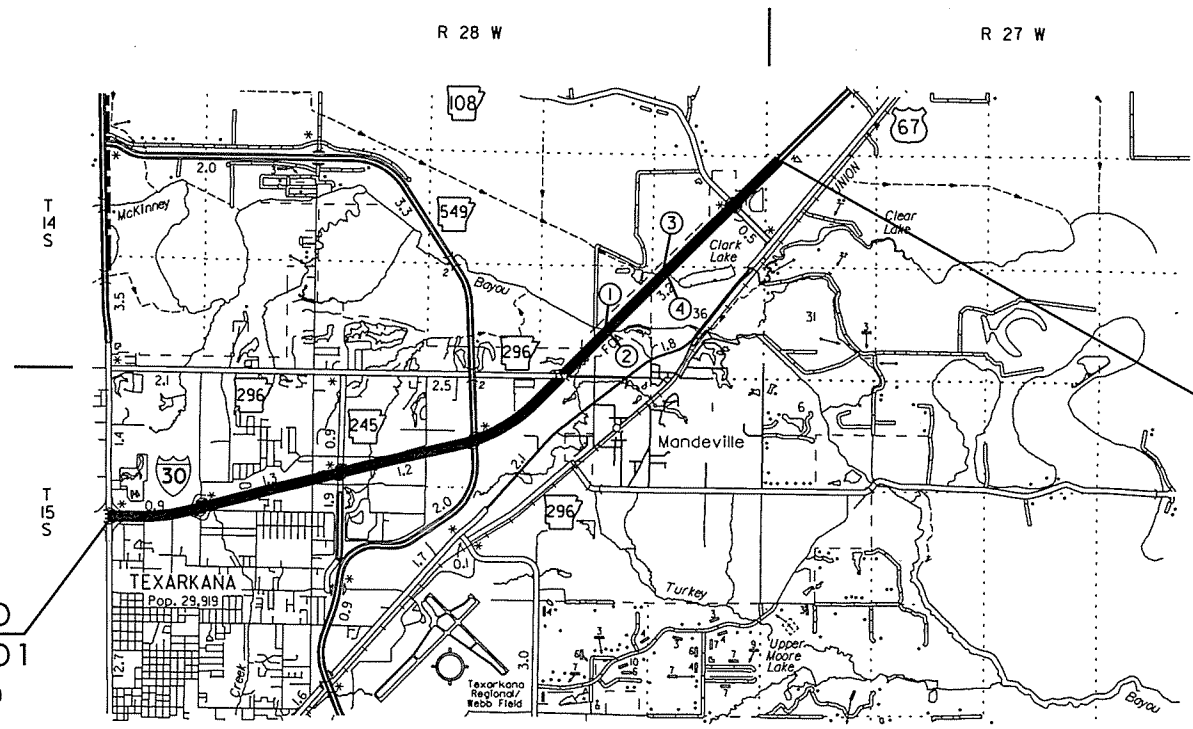


ARK. HWY. DIST. NO. 3

NOT TO SCALE

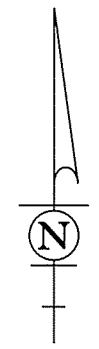
BRIDGE DATA

① STA. 2758+95.23 BR. END EXISTING 120.00' BRIDGE A3569 40'-0" CLEAR ROADWAY STA. 2760+15.23 BR. END POLYMER OVERLAY	② STA. 2759+46.90 BR. END EXISTING 120.00' BRIDGE B3569 40'-0" CLEAR ROADWAY STA. 2760+66.90 BR. END POLYMER OVERLAY
③ STA. 2797+78.65 BR. END EXISTING 120.00' BRIDGE A3570 40'-0" CLEAR ROADWAY STA. 2798+98.65 BR. END POLYMER OVERLAY	④ STA. 2798+28.37 BR. END EXISTING 120.00' BRIDGE B3570 40'-0" CLEAR ROADWAY STA. 2799+48.37 BR. END POLYMER OVERLAY



STA. 2875+63.82
END JOB BB0301
(LOG MILE 7.11)

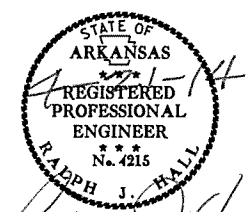
STA. 2500+00.00
BEGIN JOB BB0301
(LOG MILE 0.00)



DESIGN TRAFFIC DATA

DESIGN YEAR.....	2034
2014 ADT.....	37,000
2034 ADT.....	55,500
2034 DHV.....	6,105
DIRECTIONAL DISTRIBUTION.....	0.60
TRUCKS.....	50%
DESIGN SPEED.....	70 MPH

APPROVED



Ralph J. Hall
 DEPUTY DIRECTOR
 AND CHIEF ENGINEER

BEGINNING OF PROJECT	MID POINT OF PROJECT	END OF PROJECT
LATITUDE = N 33°28'13"	LATITUDE = N 33°28'52"	LATITUDE = N 33°30'59"
LONGITUDE = W 94°02'36"	LONGITUDE = W 93°59'00"	LONGITUDE = W 93°56'19"

GROSS LENGTH OF PROJECT	37563.82	FEET	OR	7.14	MILES
NET " " ROADWAY	37323.82	"	"	7.069	"
NET " " BRIDGES	240.00	"	"	0.045	"
NET " " PROJECT	37563.82	"	"	7.14	"

4/17/2014

RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
5-19-14				6	ARK.			
						JOB NO. BB0301	2	66

② INDEX OF SHEETS, GOV. SPECS., & GEN. NOTES



INDEX OF SHEETS

SHEET NO.	TITLE	BRIDGE NO.	DRWG. NO.	DATE
1	TITLE SHEET			
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES			
3	TYPICAL SECTIONS OF IMPROVEMENT			
4 - 10	SPECIAL DETAILS			
11 - 23	TEMPORARY EROSION CONTROL DETAILS			
24 - 28	MAINTENANCE OF TRAFFIC			
29 - 33	QUANTITIES			
34	SUMMARY OF QUANTITIES AND REVISIONS			
35 - 47	PLAN SHEETS			
48	LAYOUT OF BRIDGES OVER MCKINNEY BAYOU (FOR INFORMATION ONLY)	A&B3569	55535	
49	LAYOUT OF BRIDGES OVER PAUPS SPUR BAYOU (FOR INFORMATION ONLY)	A&B3570	55536	
50	DETAILS OF MODIFICATIONS - 30' R.C. SLAB SPANS (FOR INFORMATION ONLY)	A&B3569,A&B3570	55537	
51	CONCRETE DITCH PAVING		CDP-1	11-17-10
52	GUARD RAIL DETAILS		GR-8	7-14-10
53	GUARD RAIL DETAILS		GR-9	4-17-08
54	GUARD RAIL DETAILS		GR-9A	4-17-08
55	GUARD RAIL DETAILS		GR-10	7-14-10
56	GUARD RAIL DETAILS		GR-10A	7-14-10
57	GUARD RAIL DETAILS		GRT-1	7-14-10
58	PAVEMENT MARKING DETAILS		PM-1	9-12-13
59	PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS		PM-2	9-12-13
60	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-1	12-15-11
61	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-2	9-12-13
62	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-3	10-15-09
63	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER		TC-4	2-27-14
64	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER		TC-5	10-15-09
65	TEMPORARY EROSION CONTROL DEVICES		TEC-1	12-15-11
66	DETAILS OF STANDARD TURNOUT FOR ENTRANCE & EXIT RAMP (NON-REINFORCED)		TR-1A	8-22-02

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

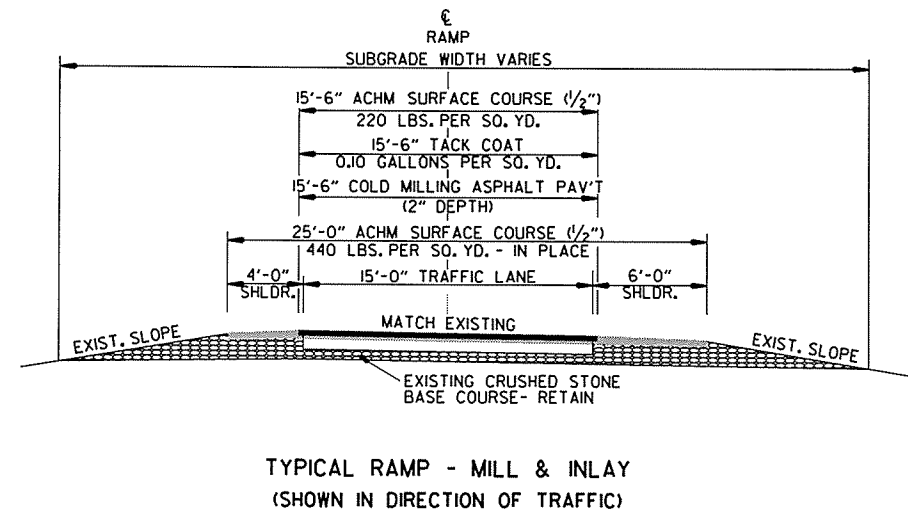
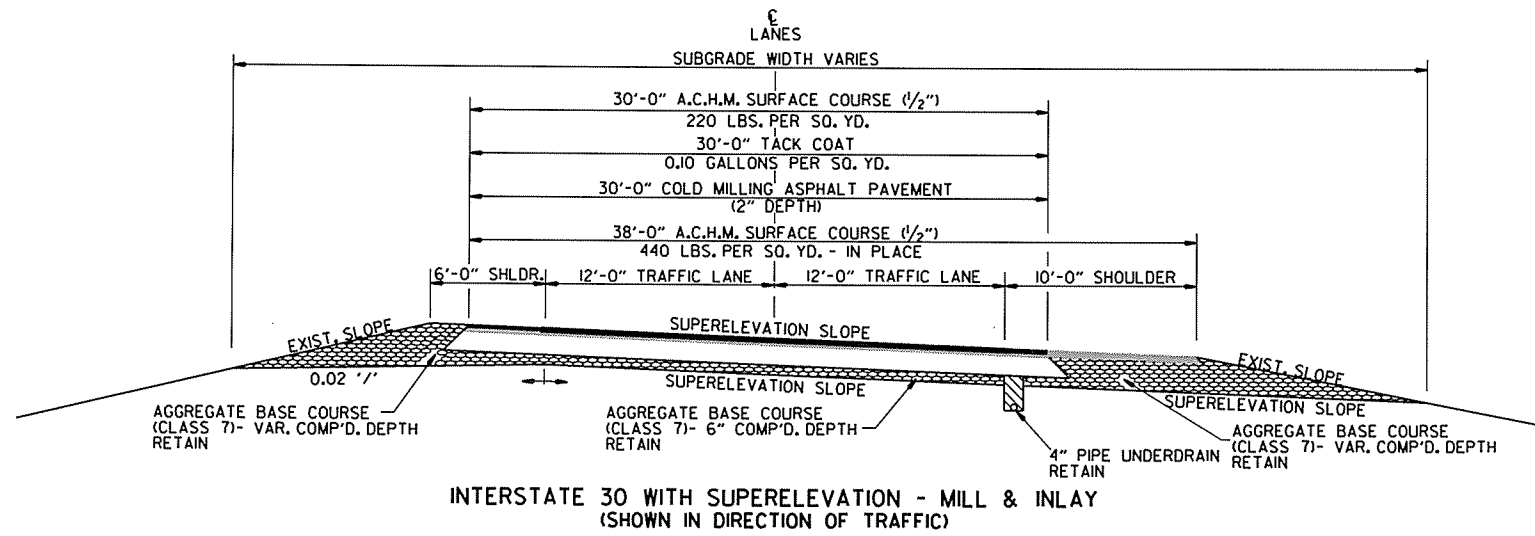
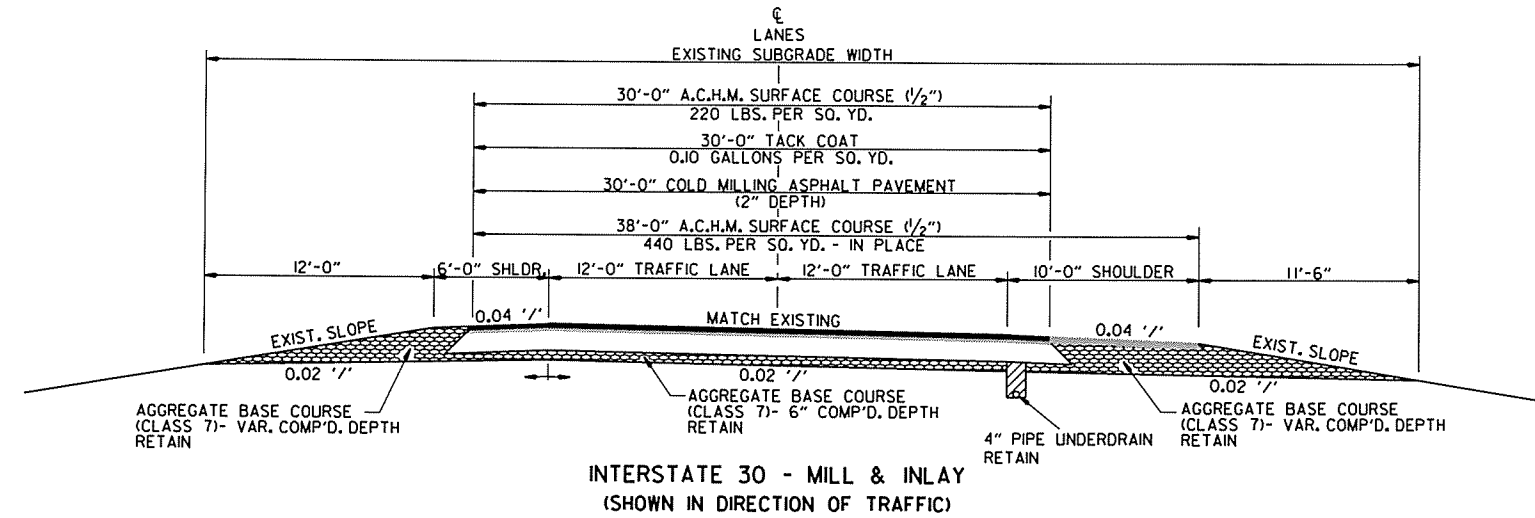
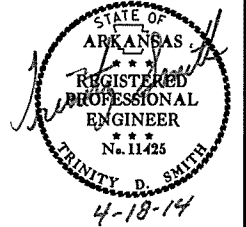
NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - TRAINING PROGRAM - JOB BB0301
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
108-1	LIQUIDATED DAMAGES
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
620-1	MULCH COVER
JOB BB0301	BRIDGE DECK REPAIR FOR POLYMER OVERLAYS
JOB BB0301	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB BB0301	CONCRETE DITCH PAVING
JOB BB0301	ELECTRONIC SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB BB0301	EMPLOYMENT REPORTING
JOB BB0301	FOG SEAL
JOB BB0301	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB BB0301	FURNISH AND OPERATION OF MOBILE SPEED NOTIFICATION SYSTEM
JOB BB0301	HIGH PERFORMANCE PAVEMENT MARKING
JOB BB0301	JOINT REHABILITATION FOR BRIDGE DECKS
JOB BB0301	MAINTENANCE OF TRAFFIC
JOB BB0301	MANDATORY USE OF INTERNET BIDDING
JOB BB0301	PARTNERING REQUIREMENTS
JOB BB0301	PERCENT WITHIN LIMITS
JOB BB0301	POLYMER OVERLAY
JOB BB0301	PORTABLE CHANGEABLE MESSAGE SIGNS (TRUCK MOUNTED)
JOB BB0301	REMOVAL AND DISPOSAL OF GUARDRAIL
JOB BB0301	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIERS
JOB BB0301	REMOVAL AND DISPOSAL OF PLOWABLE PAVEMENT MARKER
JOB BB0301	SEQUENCE OF CONSTRUCTION
JOB BB0301	SITE USE (A + C METHOD)
JOB BB0301	STORM WATER POLLUTION PREVENTION PLAN
JOB BB0301	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB BB0301	UTILITY ADJUSTMENTS
JOB BB0301	VALUE ENGINEERING
JOB BB0301	WARM MIX ASPHALT
JOB BB0301	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS
JOB BB0301	WIRE ROPE SAFETY FENCE (POST REPAIR)
JOB BB0301	WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS
JOB BB0301	WRSF TRAINING WORKSHOP

GENERAL NOTES

- ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210 - UNCLASSIFIED EXCAVATION.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- ANY REQUIRED EROSION CONTROL MEASURES FROM WASTING MATERIALS SHALL BE AT THE CONTRACTOR'S EXPENSE.

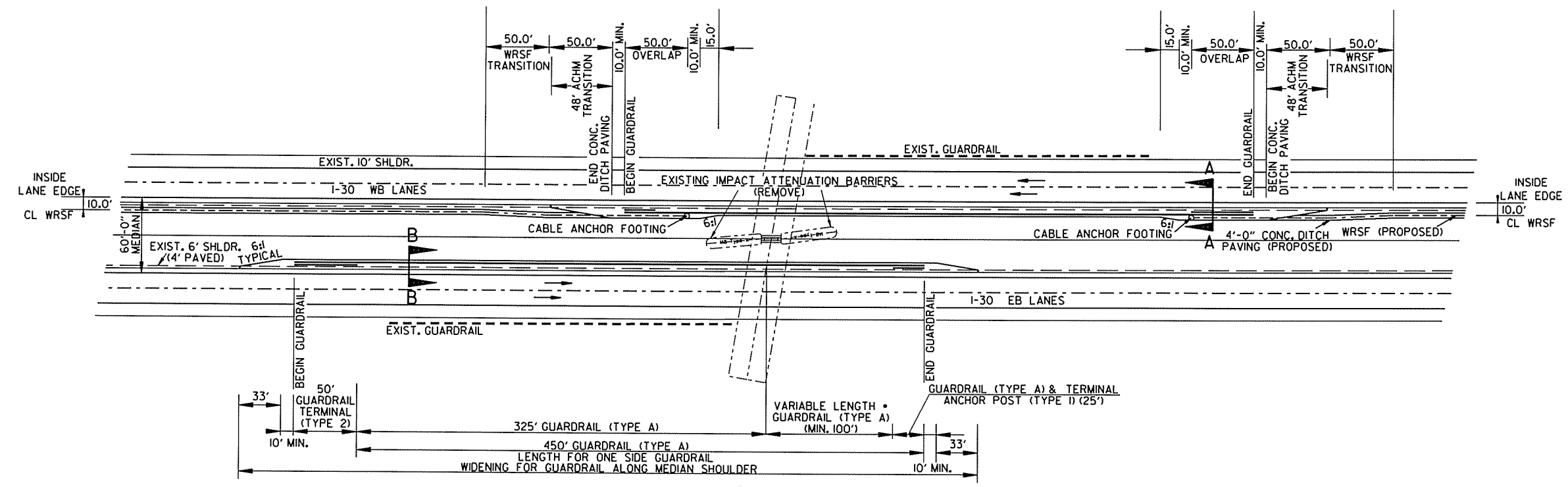
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							3	66

2 TYPICAL SECTIONS OF IMPROVEMENT



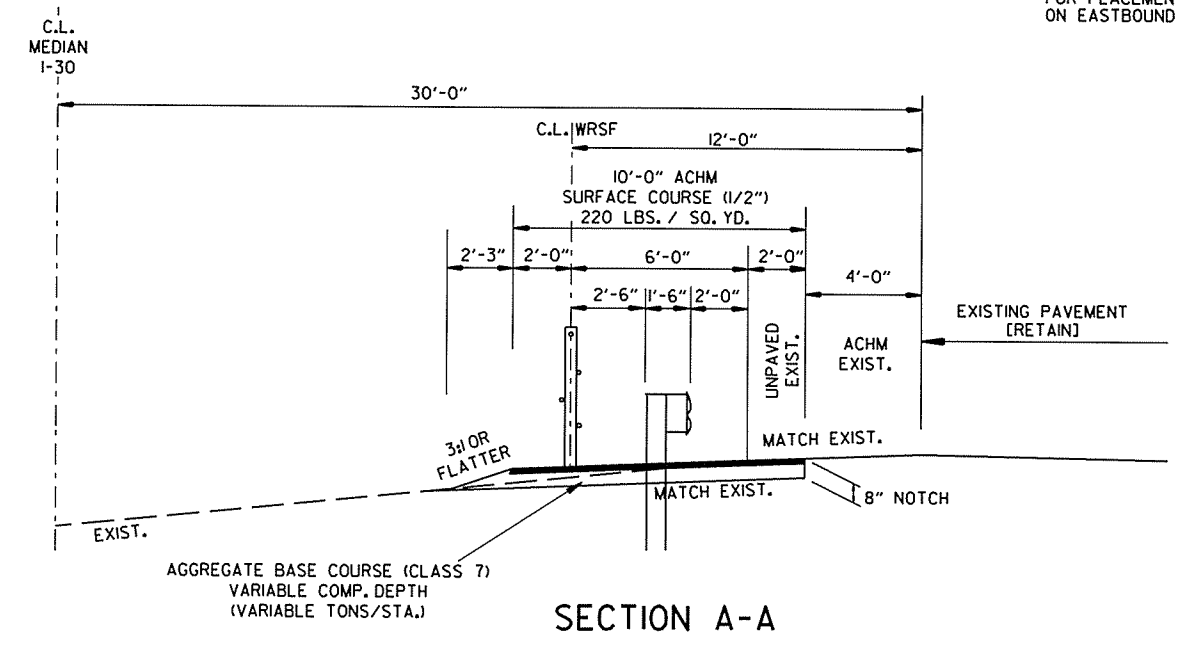
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				6	ARK.			
JOB NO. BB0301							4	66

2 SPECIAL DETAILS

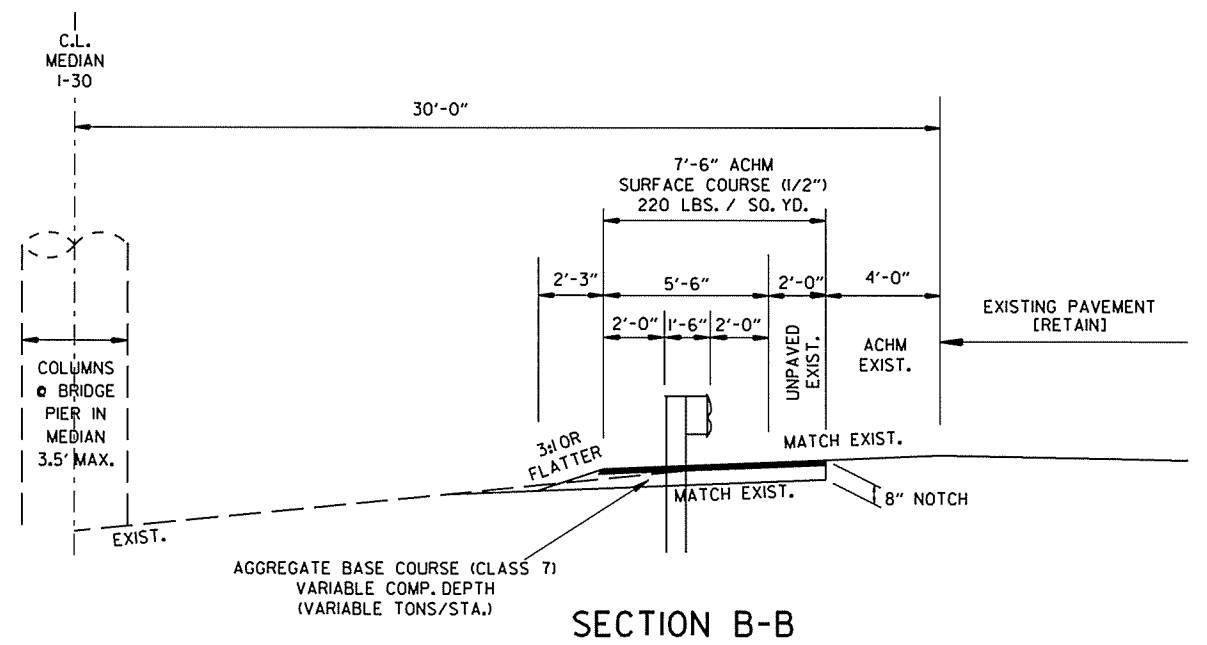


DETAIL AT OVERPASSES

NOTE: REFER TO PLAN SHEETS FOR PLACEMENT OF WIRE ROPE SAFETY FENCE ON EASTBOUND OR WESTBOUND FORESLOPES.



SECTION A-A



SECTION B-B

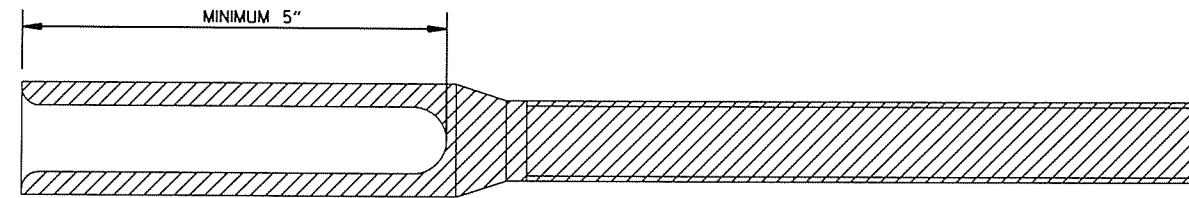
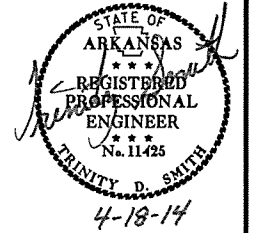
DETAILS OF SHOULDER WIDENING FOR GUARDRAIL AND OVERLAPS WITH ENDS OF WIRE ROPE SAFETY FENCE

4/16/2014

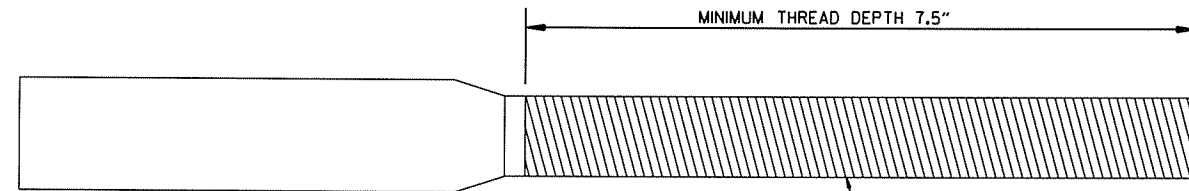
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							5	66

2 SPECIAL DETAILS



SECTION VIEW



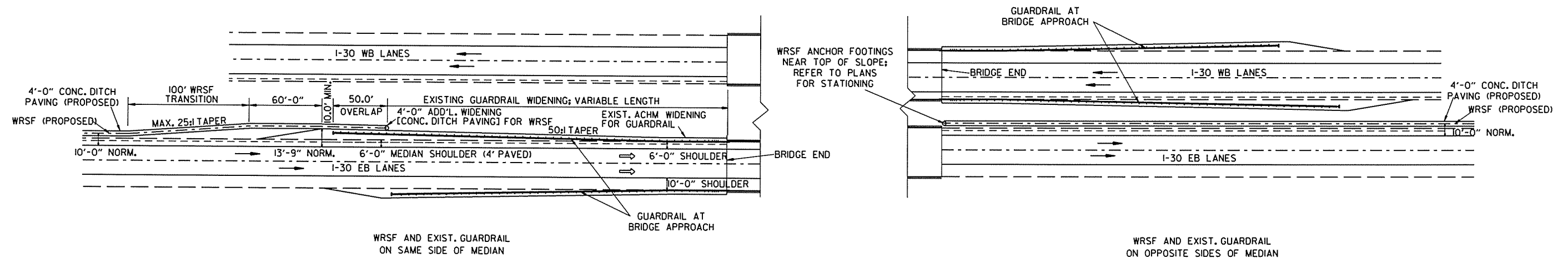
ELEVATION VIEW

SCREW THREADS LEFT HAND / RIGHT HAND

NOTE:

REFER TO "WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS" SPECIAL PROVISION FOR ADDITIONAL REQUIREMENTS.

THREADED TERMINAL DETAIL



DETAIL OF WIRE ROPE SAFETY FENCE AT EXISTING BRIDGE ENDS

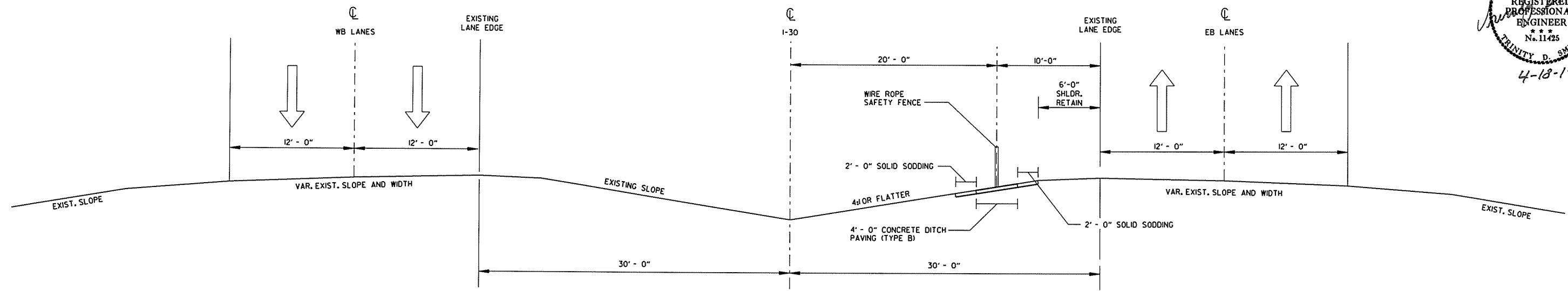
REFER TO PLANS FOR RELATIVE PLACEMENT OF GUARDRAIL AND WIRE ROPE SAFETY FENCE AT EACH BRIDGE END

4/16/2014

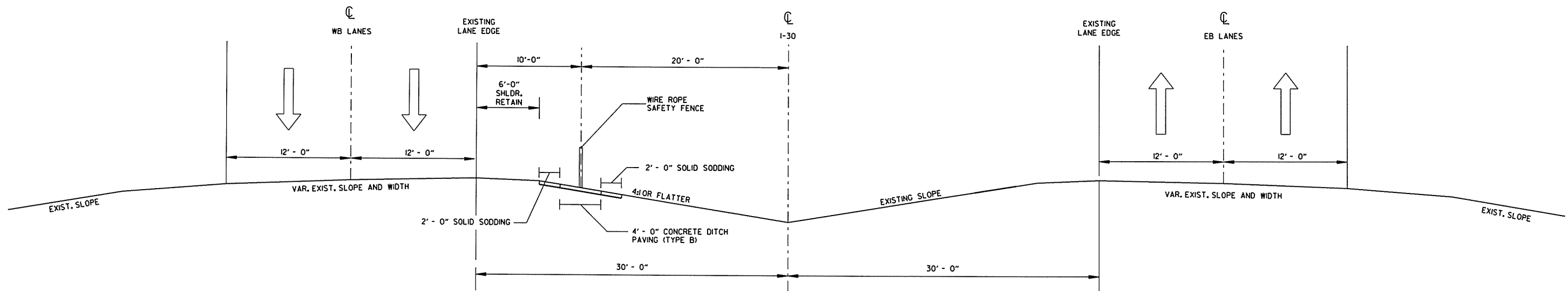
RB0301.DGN

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				6	ARK.			
JOB NO. BB0301							6	66

2 SPECIAL DETAILS



TYPICAL SECTION OF IMPROVEMENT FOR WIRE ROPE SAFETY FENCE RIGHT OF CENTERLINE



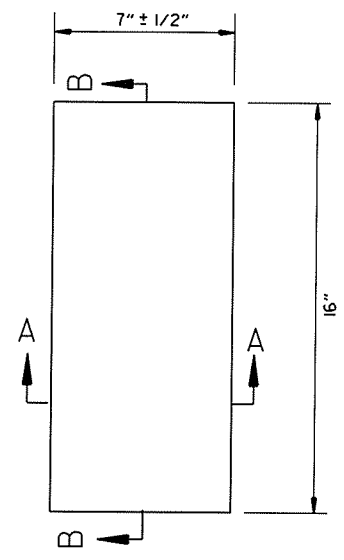
TYPICAL SECTION OF IMPROVEMENT FOR WIRE ROPE SAFETY FENCE LEFT OF CENTERLINE

SPECIAL DETAILS

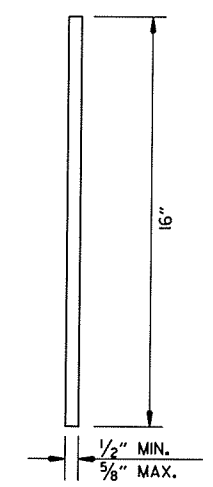
4/16/2014
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
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JOB NO. BB0301							7	66

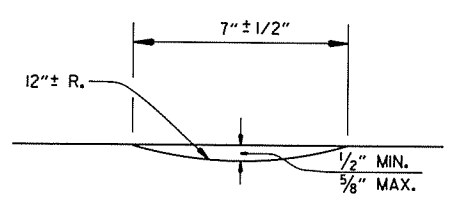
2 SPECIAL DETAILS



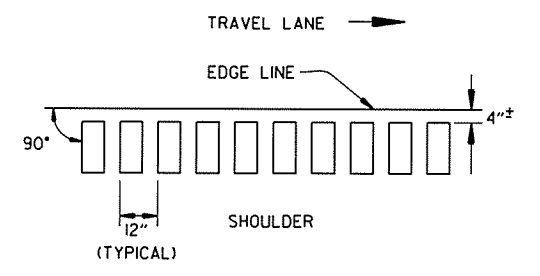
PLAN



SECTION B-B

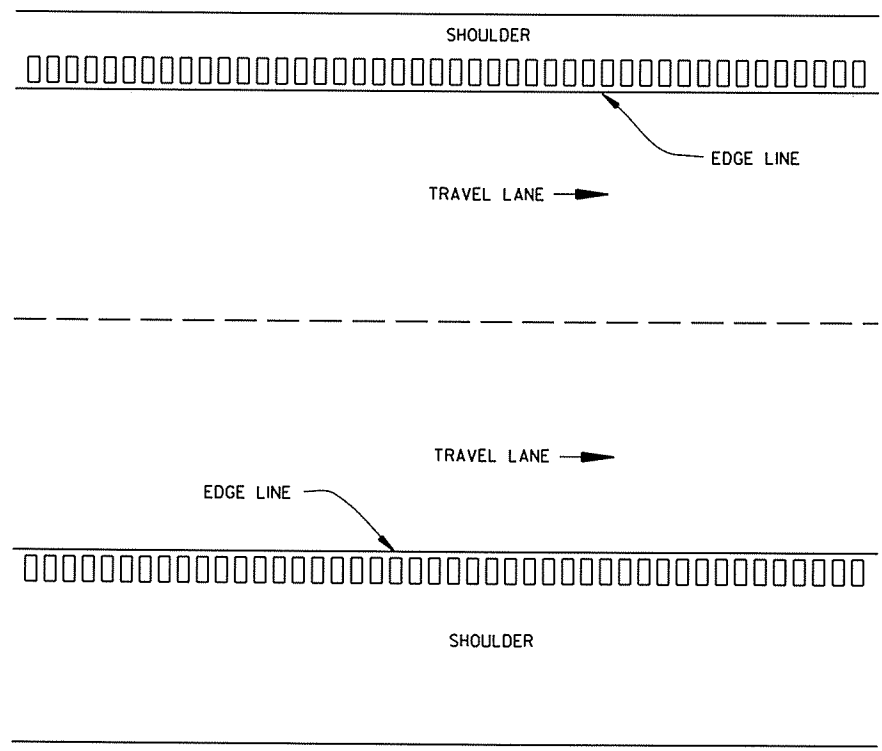


SECTION A-A



LOCATION PLAN OF RUMBLE STRIPS
LEFT OR RIGHT SHOULDER

DETAILS OF RUMBLE STRIPS



PLAN VIEW

NOTES:

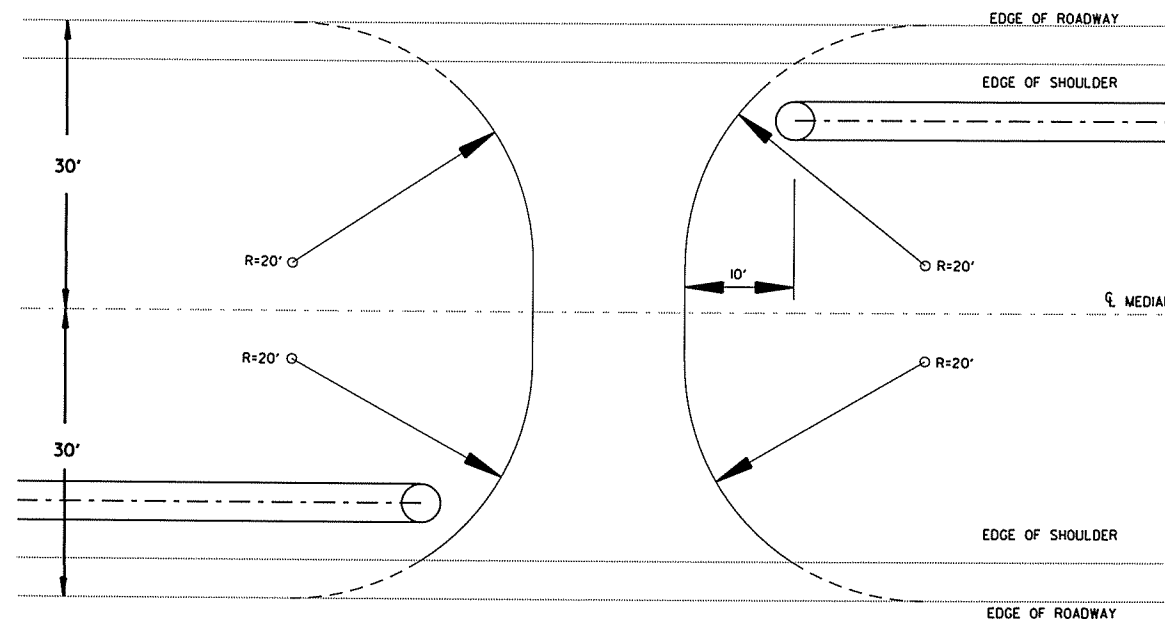
1. ALIGNMENT OF RUMBLE STRIPS SHALL GENERALLY BE STRAIGHT AND OFFSET APPROXIMATELY 4" FROM THE OUTER EDGE OF THE EDGE LINE. THIS OFFSET MAY BE ADJUSTED TO ACCOMMODATE VARIATIONS IN THE EDGE LINE.
2. THE 1/2" DEPTH SHALL GENERALLY APPLY FOR THE ENTIRE 16" LENGTH. SOME VARIATION TO SUIT SHOULDER SLOPE BREAKS MAY BE NECESSARY.
3. RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS, APPROACH SLABS, OR ACROSS TRANSVERSE JOINTS OF CONCRETE SHOULDERS.

4/16/2014

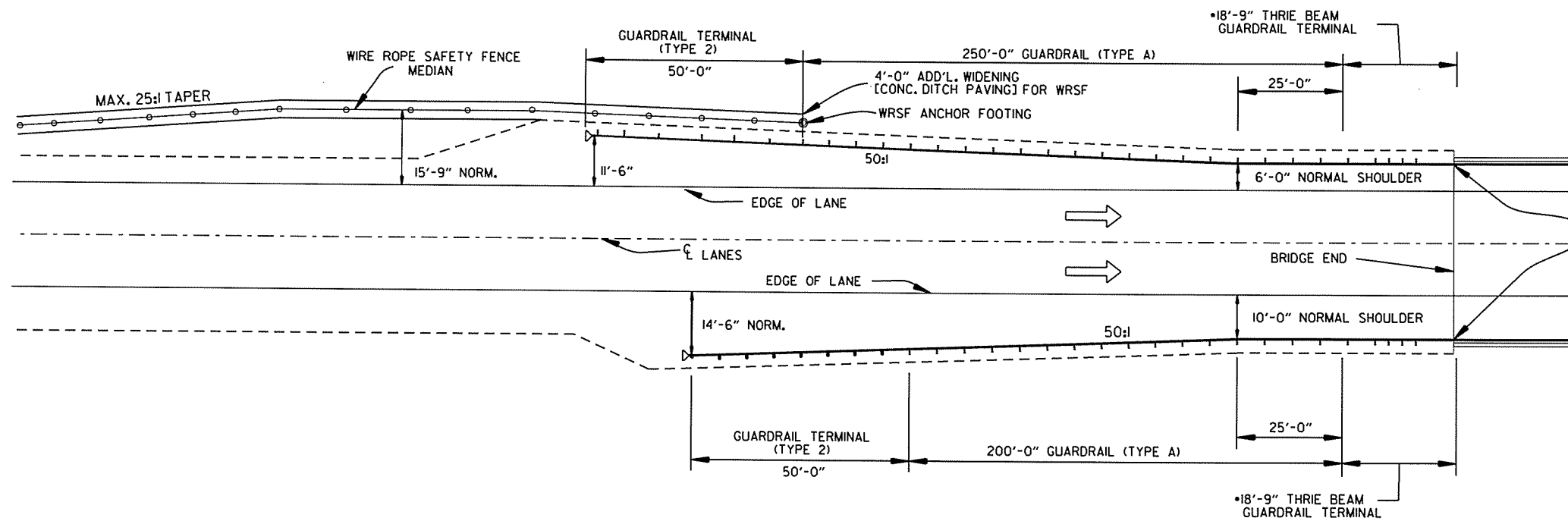
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							8	66

2 SPECIAL DETAILS



DETAIL OF PROPOSED MEDIAN CROSSING



*THE CONTRACTOR SHALL DRILL 1" DIA. HOLES FOR THE NEW THRIE BEAM CONNECTION BOLTS IN THE EXISTING TRANSITION RAIL. CARE SHALL BE EXERCISED TO AVOID THE EXISTING REINFORCING STEEL IN THE RAIL. THIS WORK WILL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS CONTRACT ITEMS. SEE STANDARD DRAWING GR-10 FOR ADDITIONAL DETAILS.

THRIE BEAM GUARDRAIL CONNECTION AT BRIDGE END. SEE STD. DWG. GR-10.

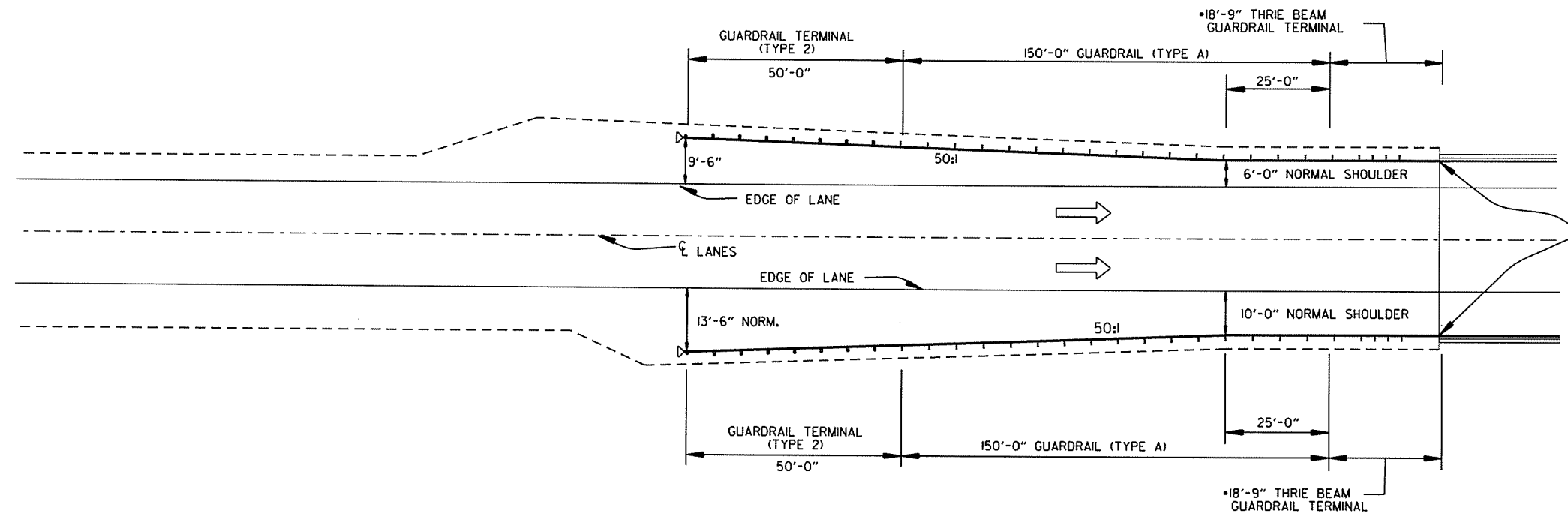
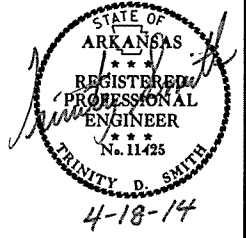
TYPICAL LAYOUT OF GUARDRAIL AT BRIDGE ENDS

4/16/2014

RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							9	66

2 SPECIAL DETAILS



*THE CONTRACTOR SHALL DRILL 1" DIA. HOLES FOR THE NEW THRIE BEAM CONNECTION BOLTS IN THE EXISTING TRANSITION RAIL. CARE SHALL BE EXERCISED TO AVOID THE EXISTING REINFORCING STEEL IN THE RAIL. THIS WORK WILL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS CONTRACT ITEMS. SEE STANDARD DRAWING GR-10 FOR ADDITIONAL DETAILS.

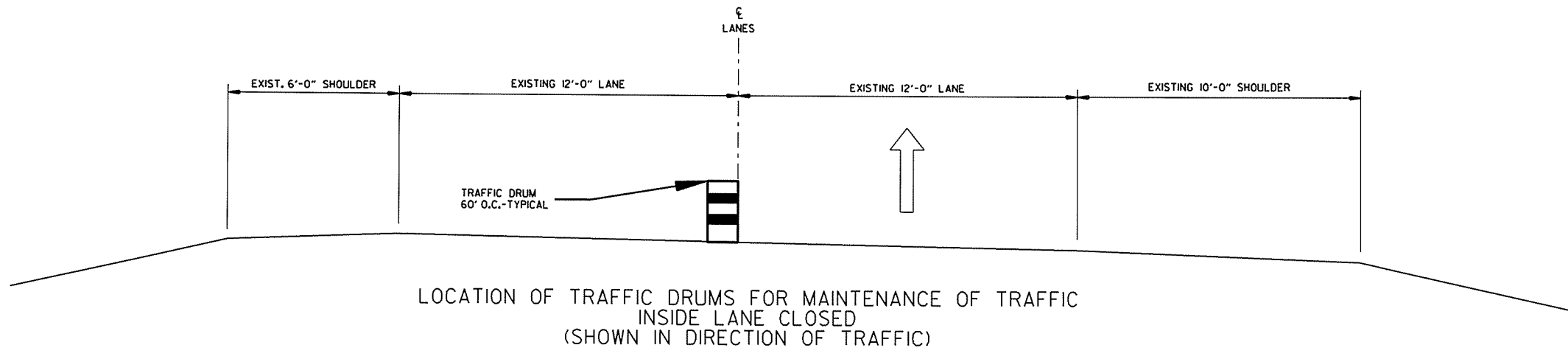
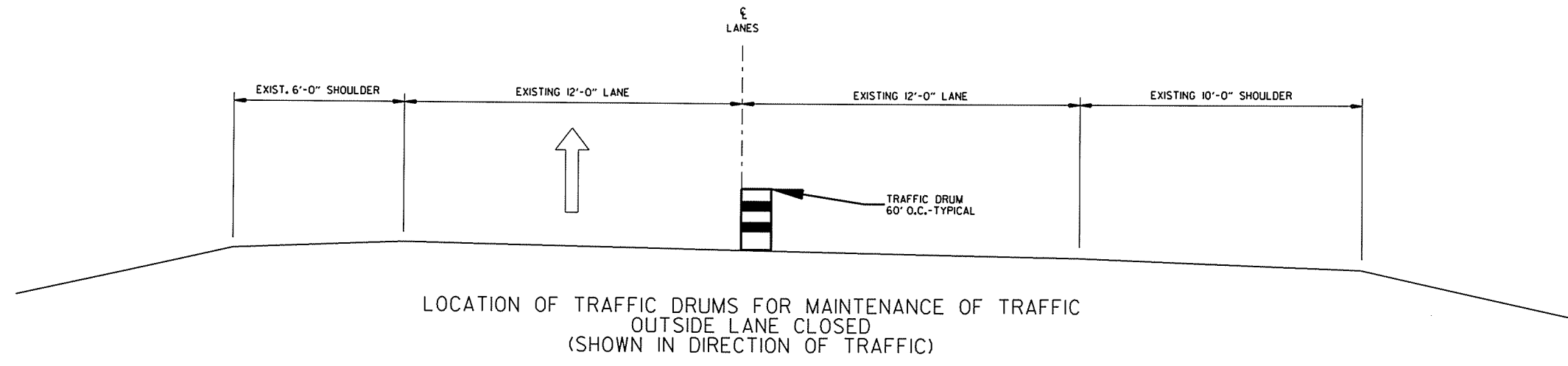
THRIE BEAM GUARDRAIL CONNECTION AT CONCRETE PIER PROTECTION SEE STD. DWG. GR-10.

TYPICAL LAYOUT OF GUARDRAIL AT OVERPASSES

4/16/2014
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						BB0301	10	66

2 SPECIAL DETAILS



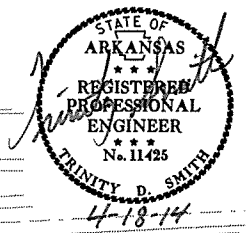
4/16/2014

RB0301.DGN

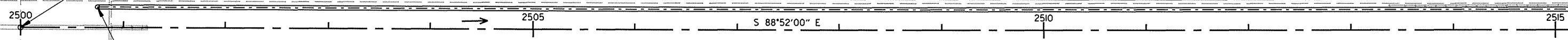
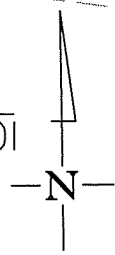
SPECIAL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						BB0301	11	66

② TEMPORARY EROSION CONTROL DETAILS



STA. 2500+00.00
 BEGIN JOB BB0301
 LOG MILE 0.00



STA. 2500+74
 BEGIN WRSF

REVISIONS

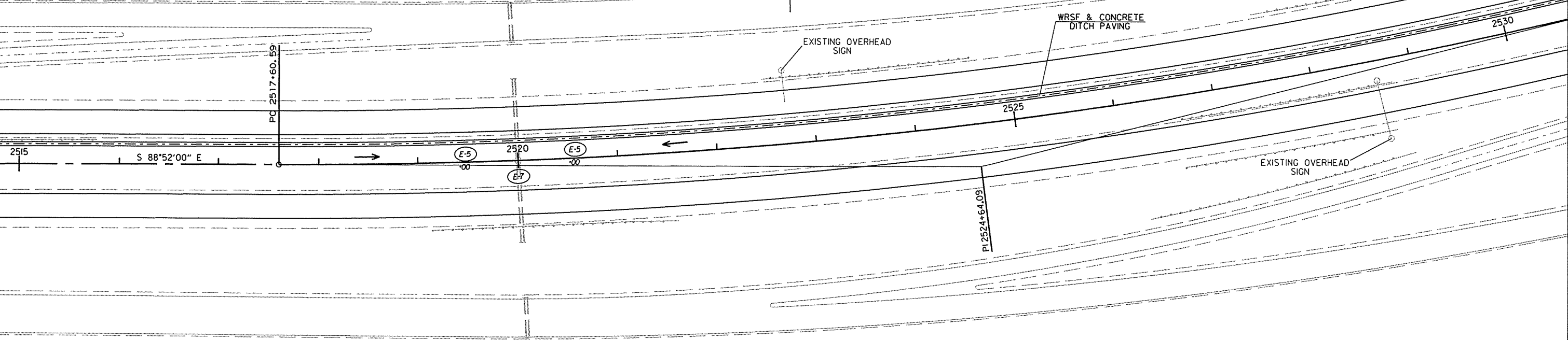
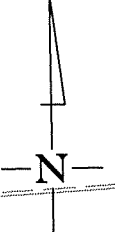
DATE OF REVISION	REVISION

LEGEND

- (E-5) SAND BAG DITCH CHECKS
- (E-7) DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

TEXAS
 ARKANSAS

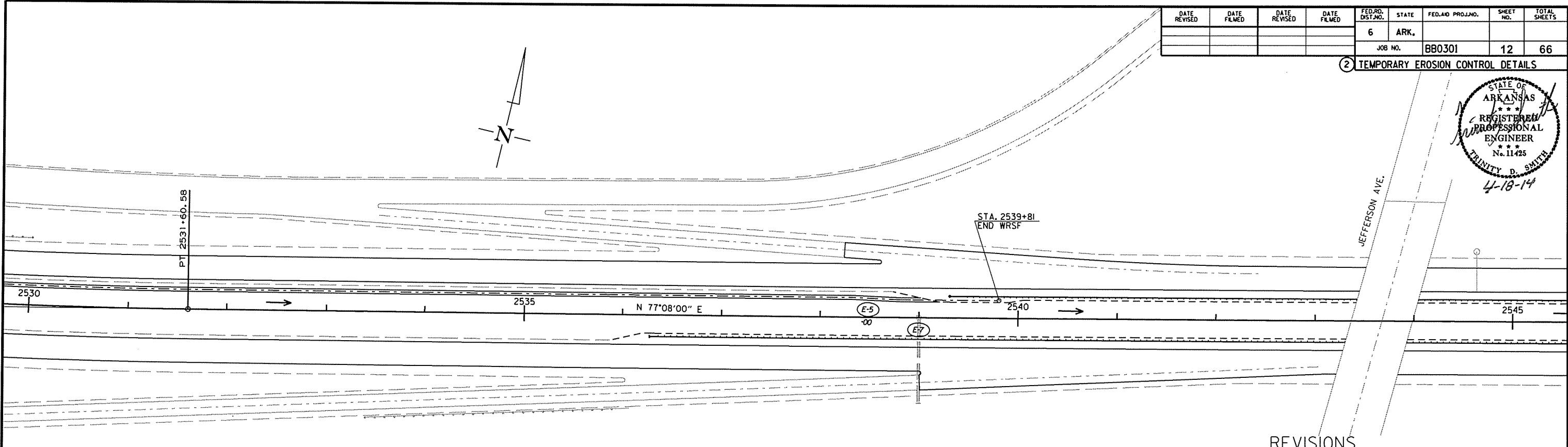
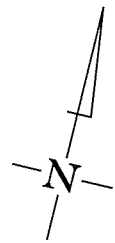
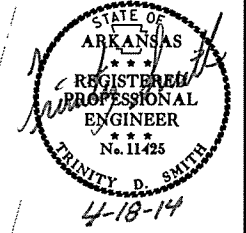


4/16/2014

R880301.DGN



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0301		12	66

② TEMPORARY EROSION CONTROL DETAILS



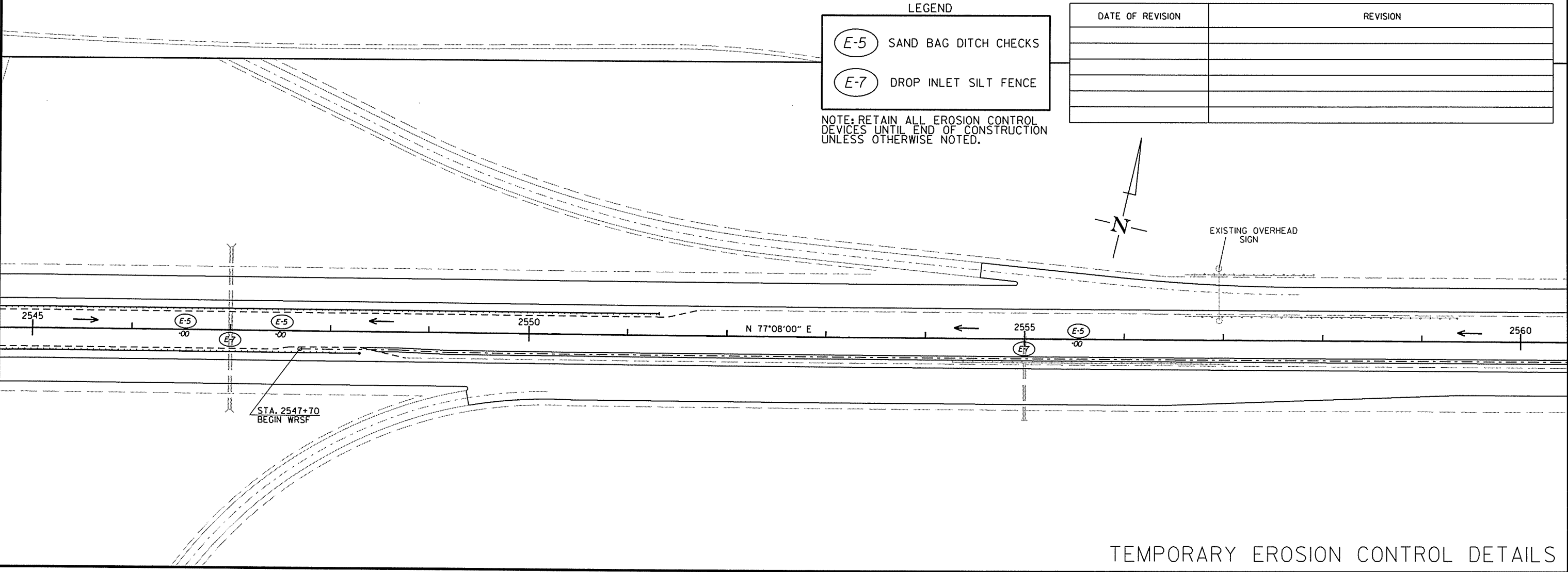
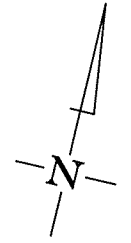
REVISIONS

LEGEND

-  SAND BAG DITCH CHECKS
-  DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

DATE OF REVISION	REVISION



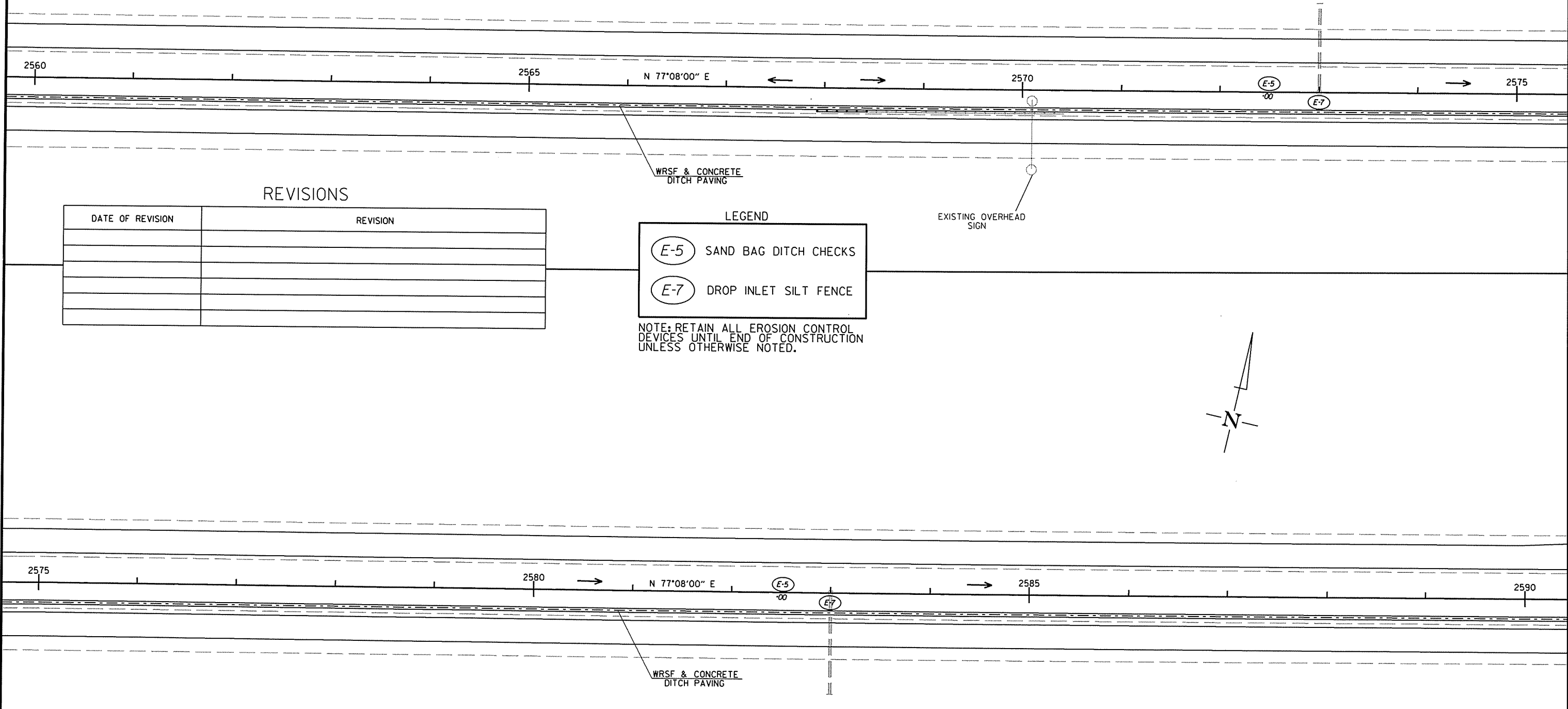
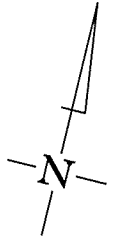
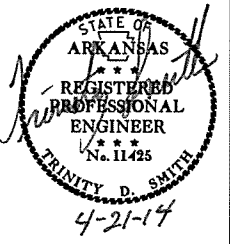
TEMPORARY EROSION CONTROL DETAILS

4/16/2014

RB0301.DCN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							13	66



② TEMPORARY EROSION CONTROL DETAILS



REVISIONS

DATE OF REVISION	REVISION

LEGEND

-  SAND BAG DITCH CHECKS
-  DROP INLET SILT FENCE

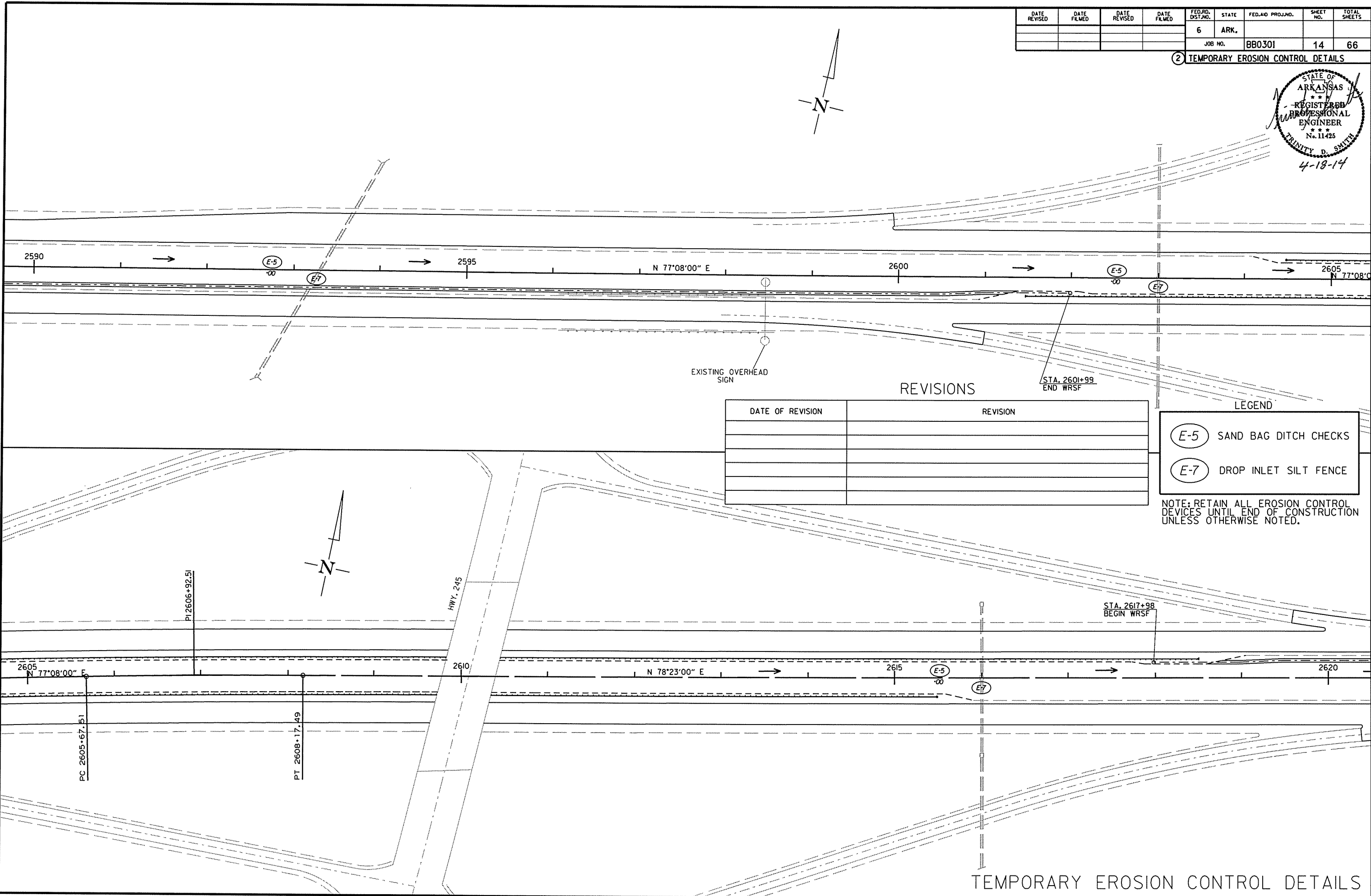
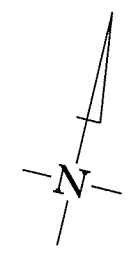
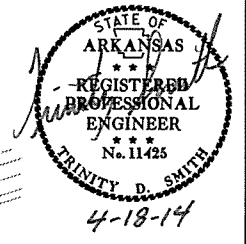
NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

4/21/2014

RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							14	66

② TEMPORARY EROSION CONTROL DETAILS



EXISTING OVERHEAD SIGN

REVISIONS

DATE OF REVISION	REVISION

LEGEND

- E-5 SAND BAG DITCH CHECKS
- E-7 DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

STA. 2601+99
END WRSF

STA. 2617+98
BEGIN WRSF

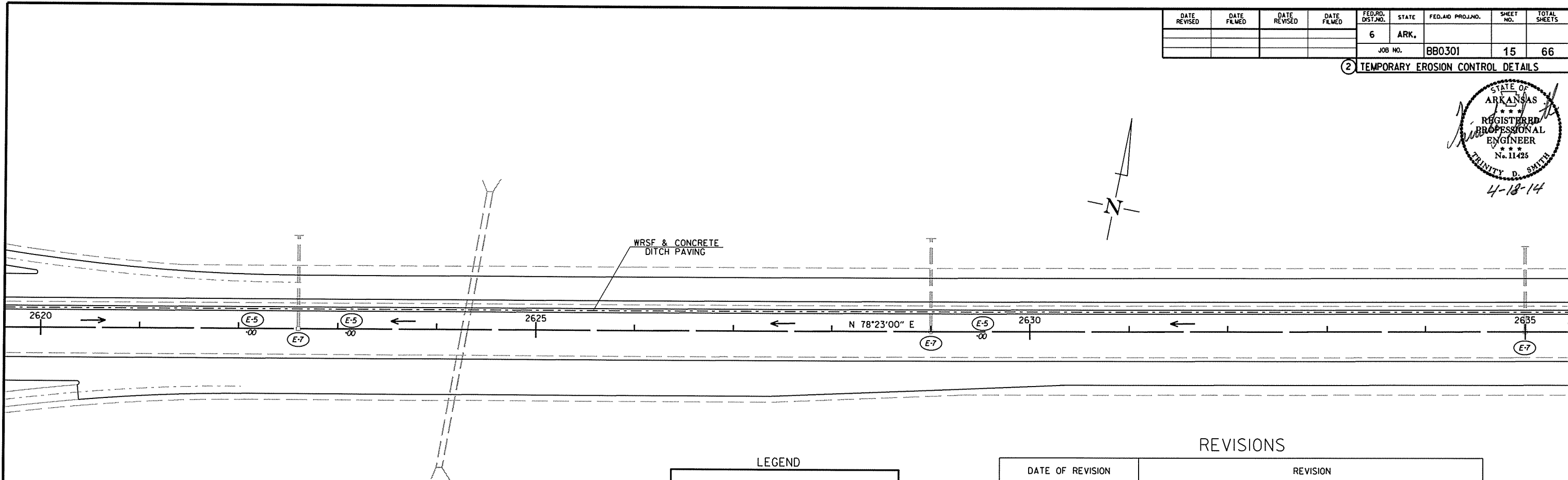
TEMPORARY EROSION CONTROL DETAILS

4/16/2014

RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						BB0301	15	66

② TEMPORARY EROSION CONTROL DETAILS



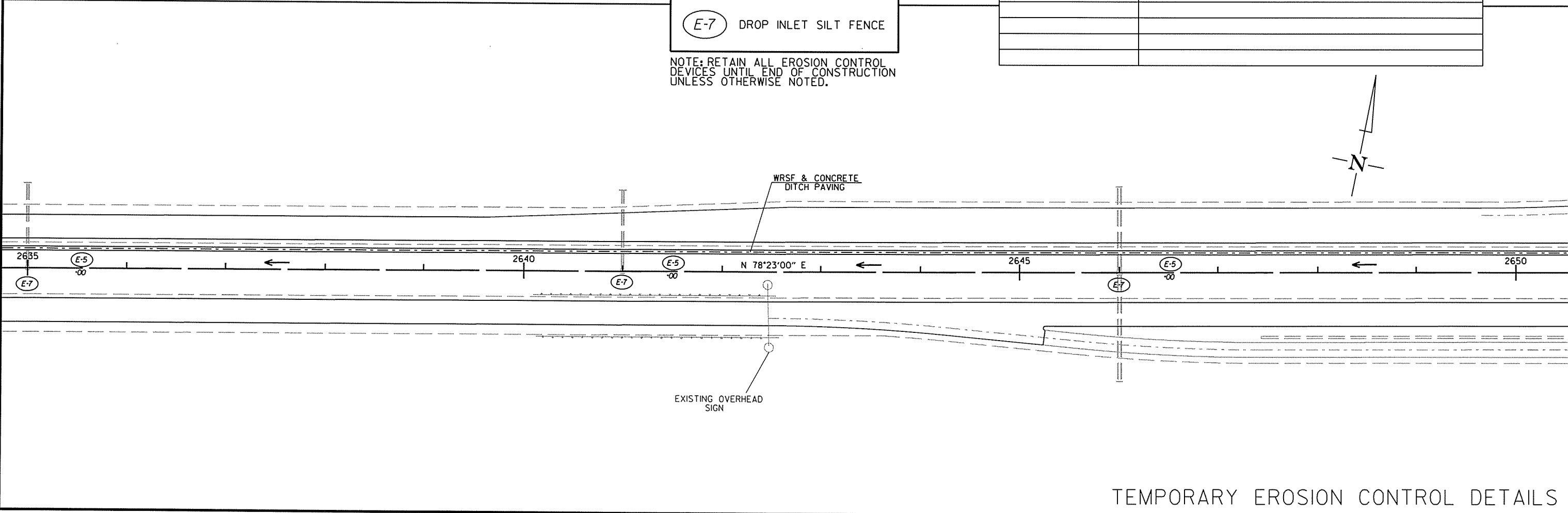
LEGEND

(E-5)	SAND BAG DITCH CHECKS
(E-7)	DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

REVISIONS

DATE OF REVISION	REVISION

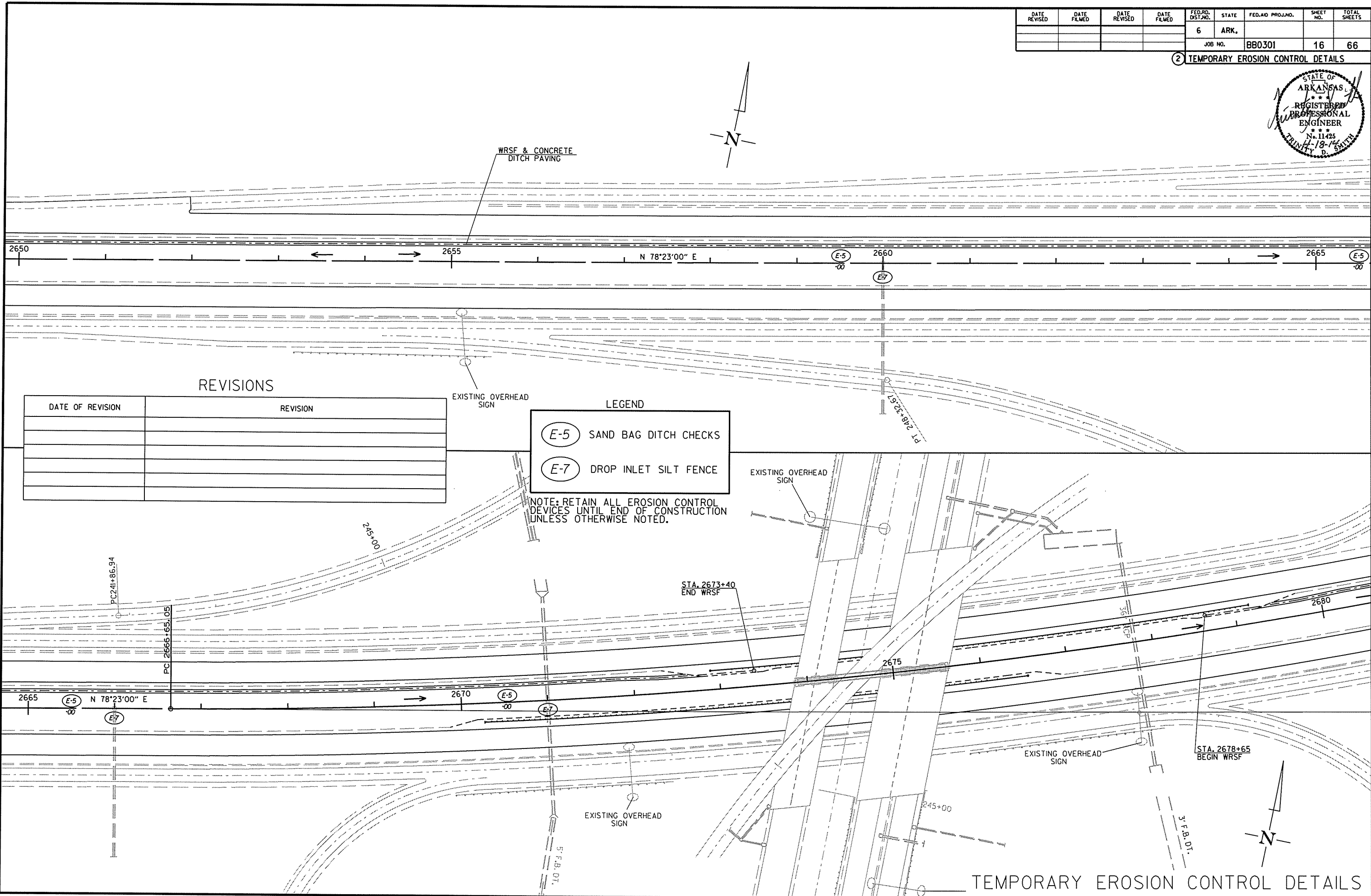


TEMPORARY EROSION CONTROL DETAILS

4/16/2014
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						BB0301	16	66

② TEMPORARY EROSION CONTROL DETAILS



REVISIONS

DATE OF REVISION	REVISION

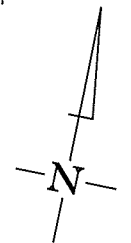
LEGEND

(E-5) SAND BAG DITCH CHECKS

(E-7) DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

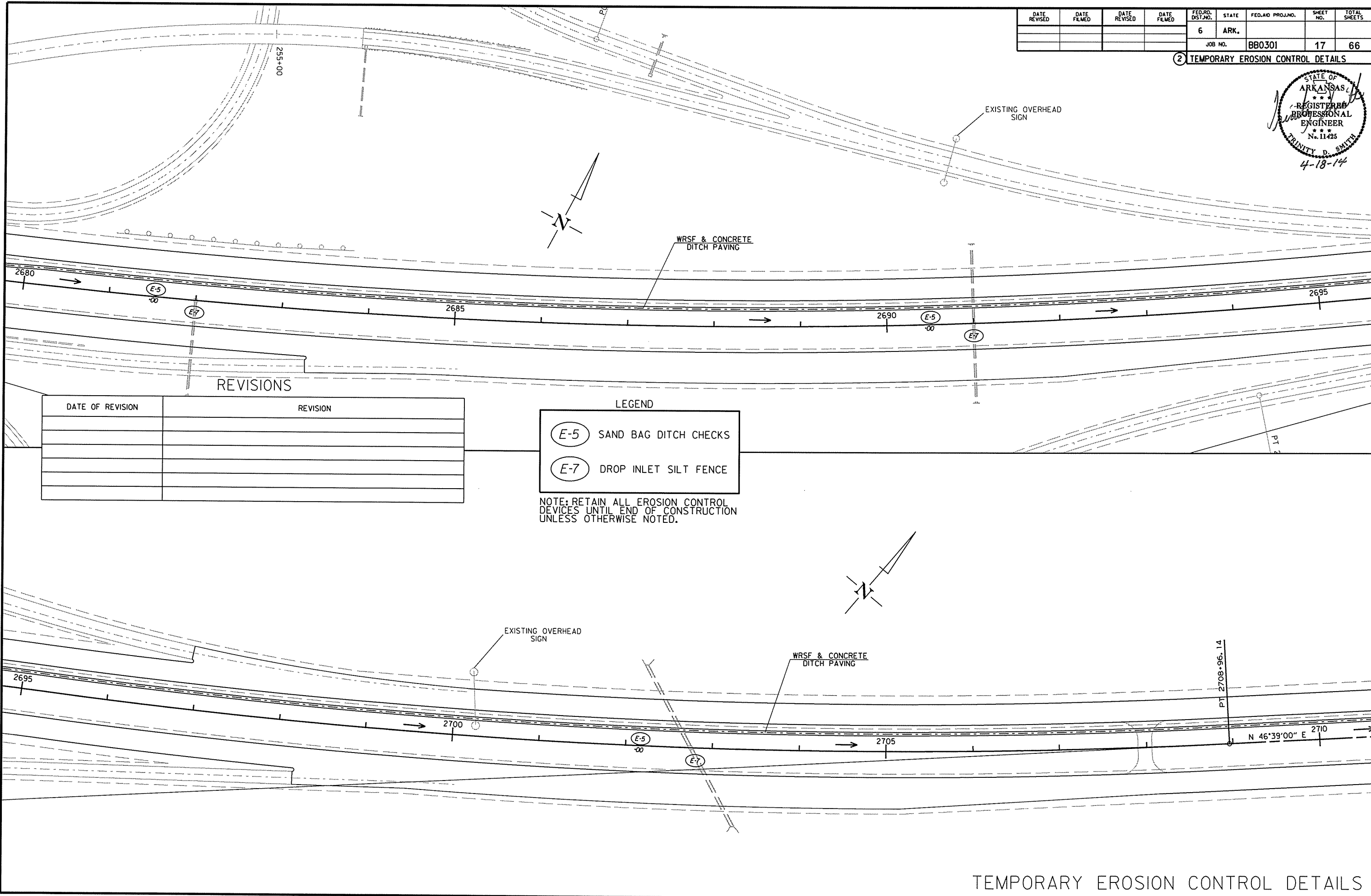
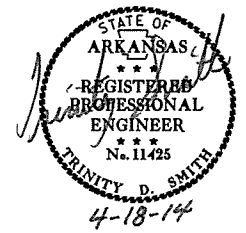
4/16/2014
RB0301.DGN



TEMPORARY EROSION CONTROL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	17	66



② TEMPORARY EROSION CONTROL DETAILS



REVISIONS

DATE OF REVISION	REVISION

LEGEND

-  SAND BAG DITCH CHECKS
-  DROP INLET SILT FENCE

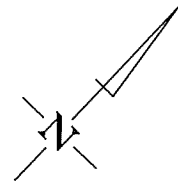
NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

4/16/2014

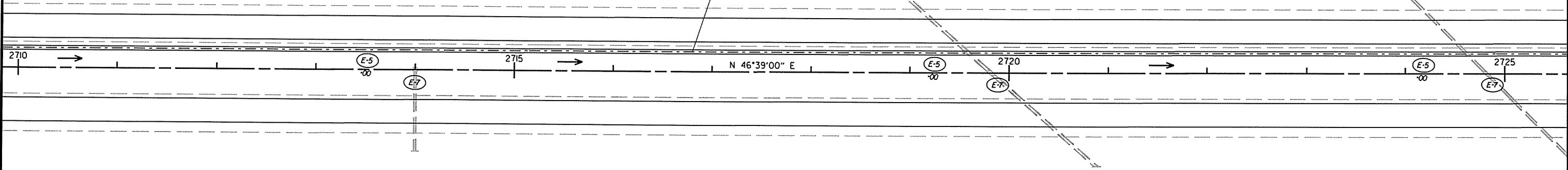
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						BB0301	18	66

② TEMPORARY EROSION CONTROL DETAILS





WRSF & CONCRETE
DITCH PAVING



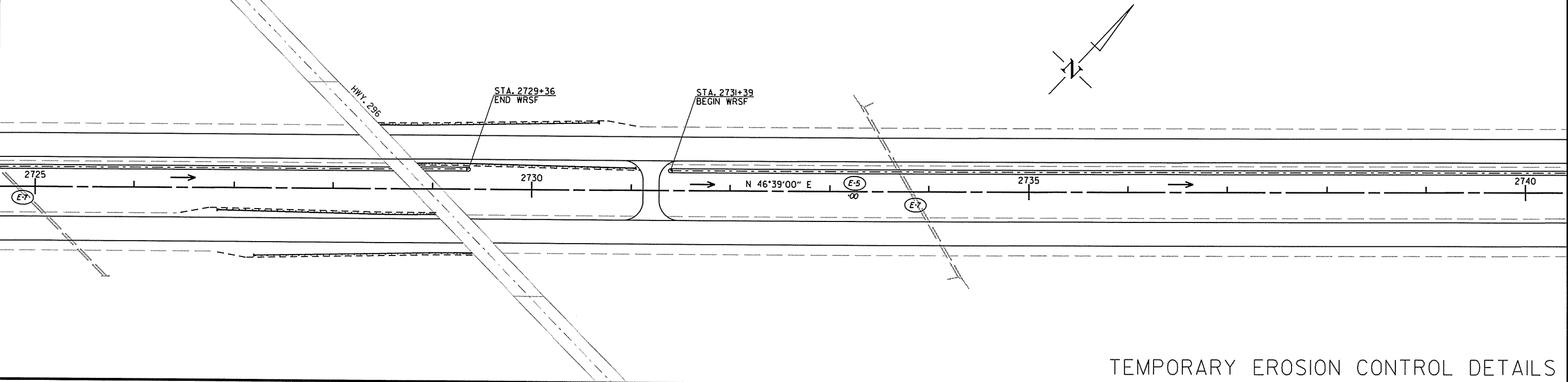
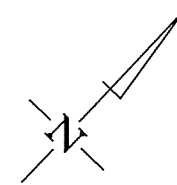
REVISIONS

DATE OF REVISION	REVISION

LEGEND

-  SAND BAG DITCH CHECKS
-  DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

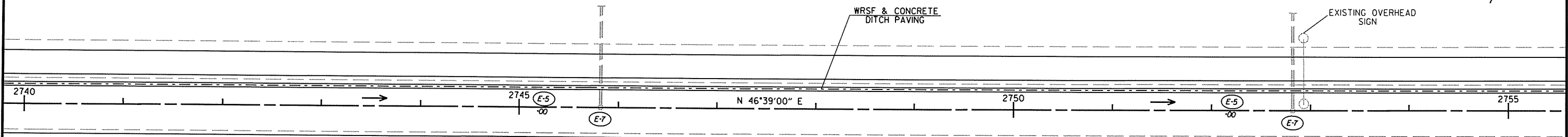


4/16/2014

RBB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							19	66

2 TEMPORARY EROSION CONTROL DETAILS



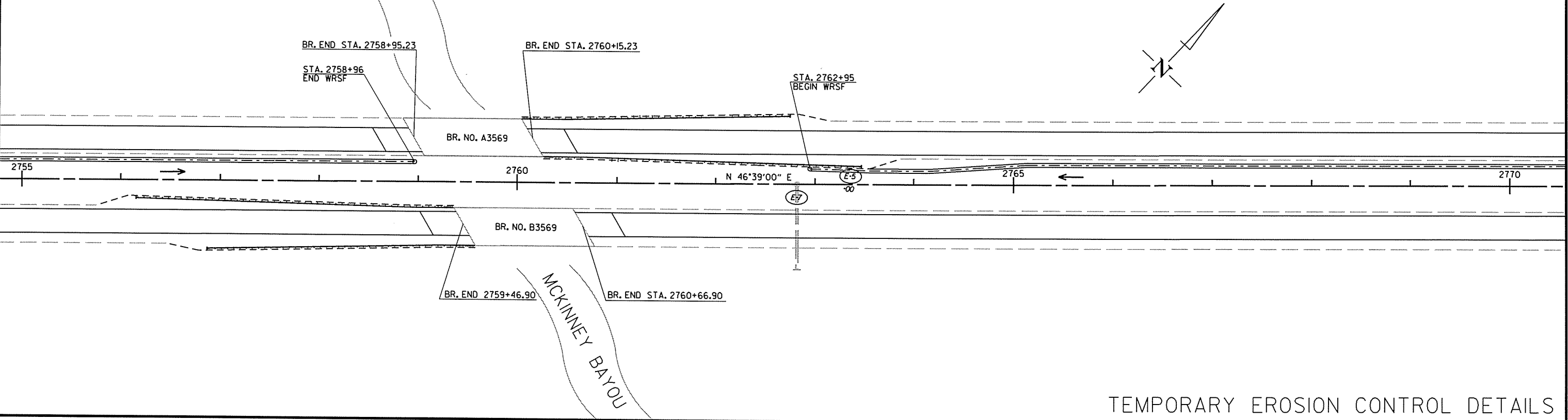
REVISIONS

DATE OF REVISION	REVISION

LEGEND

- SAND BAG DITCH CHECKS
- DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.



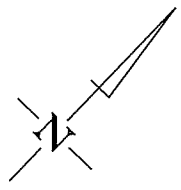
TEMPORARY EROSION CONTROL DETAILS

4/16/2014

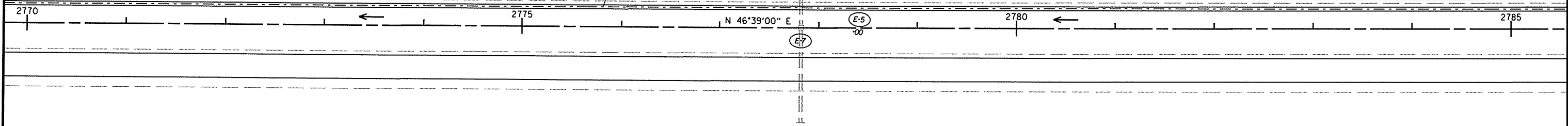
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							20	66

2 TEMPORARY EROSION CONTROL DETAILS





WRSF & CONCRETE DITCH PAVING



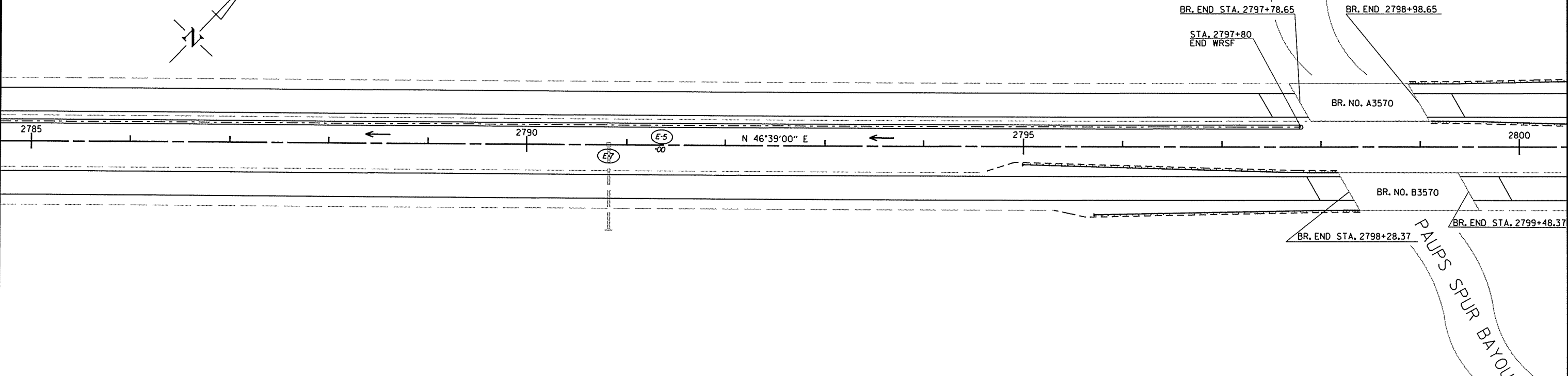
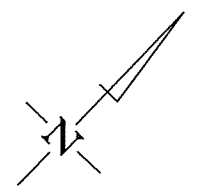
REVISIONS

DATE OF REVISION	REVISION

LEGEND

-  SAND BAG DITCH CHECKS
-  DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.



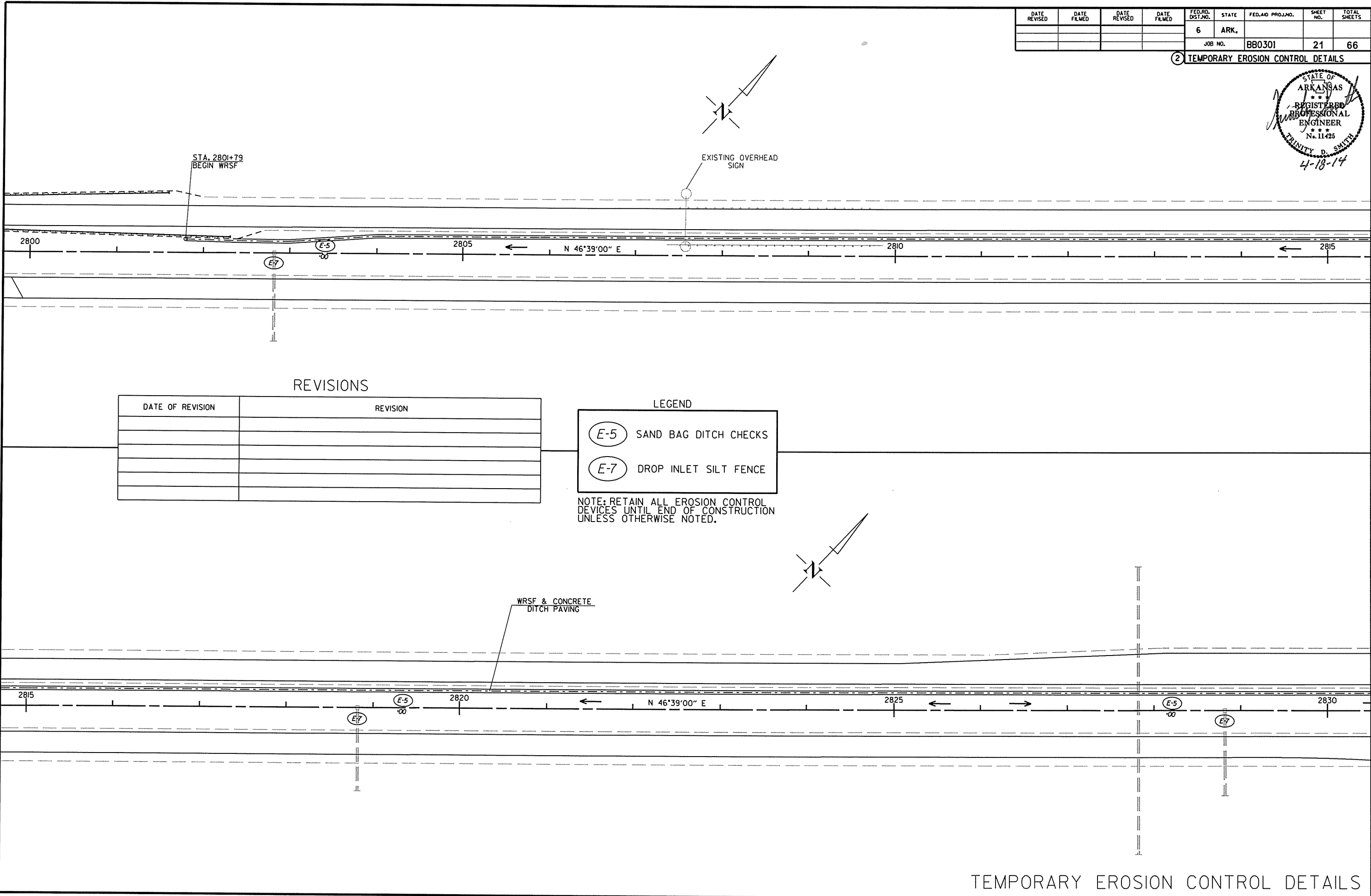
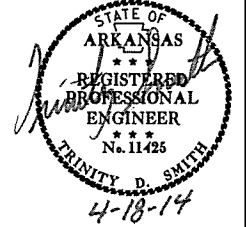
4/16/2014

RB0301.DGN

TEMPORARY EROSION CONTROL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						BB0301	21	66

② TEMPORARY EROSION CONTROL DETAILS



REVISIONS

DATE OF REVISION	REVISION

LEGEND

- (E-5) SAND BAG DITCH CHECKS
- (E-7) DROP INLET SILT FENCE

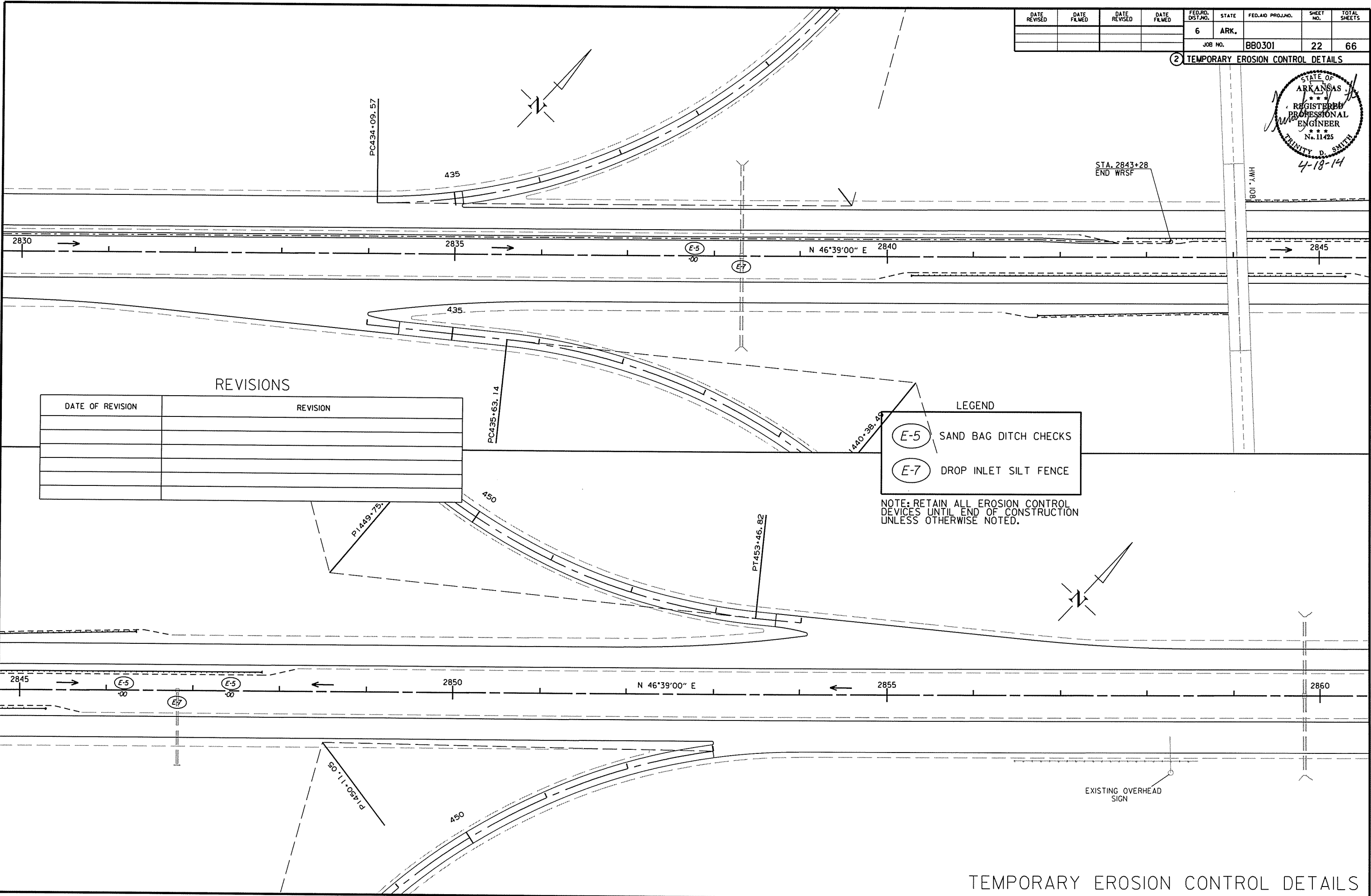
NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

4/16/2014

RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							22	66

2 TEMPORARY EROSION CONTROL DETAILS



REVISIONS

DATE OF REVISION	REVISION

LEGEND

- (E-5) SAND BAG DITCH CHECKS
- (E-7) DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.

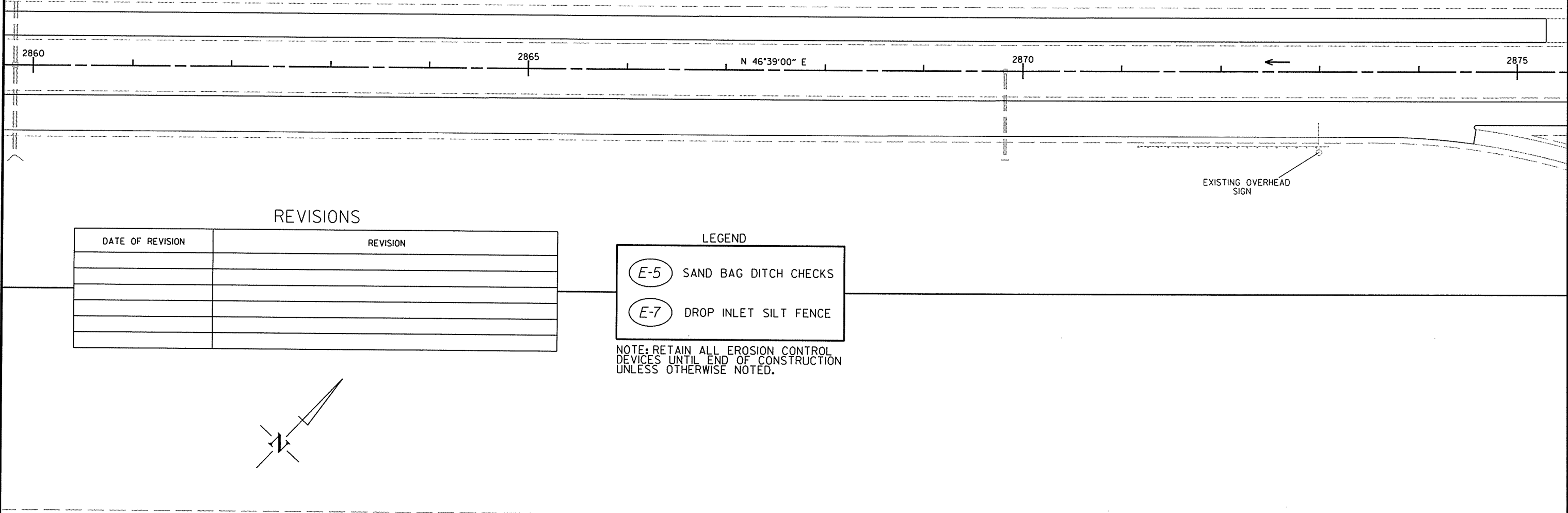
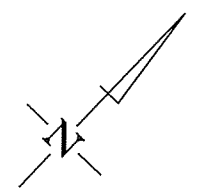
4/16/2014

RB0301.DGN

TEMPORARY EROSION CONTROL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							BB0301	23	66

② TEMPORARY EROSION CONTROL DETAILS



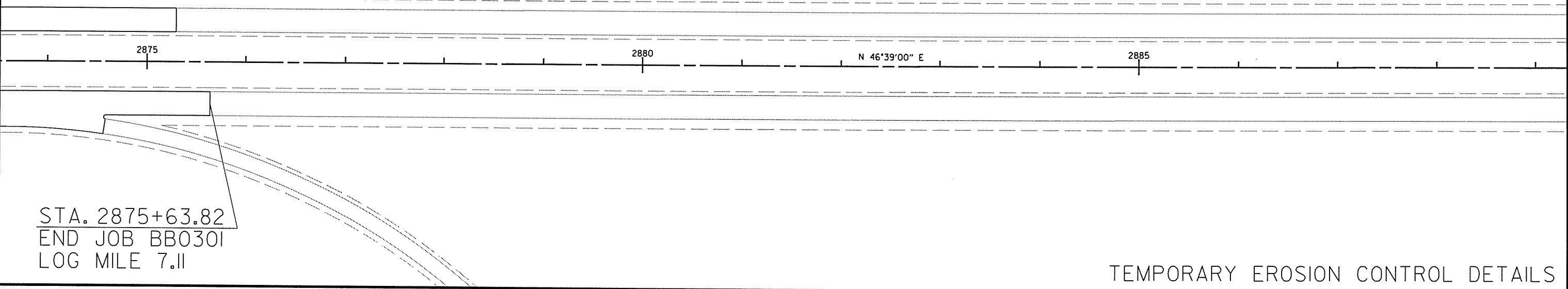
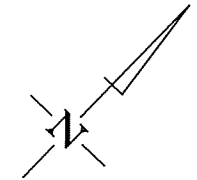
REVISIONS

DATE OF REVISION	REVISION

LEGEND

- (E-5) SAND BAG DITCH CHECKS
- (E-7) DROP INLET SILT FENCE

NOTE: RETAIN ALL EROSION CONTROL DEVICES UNTIL END OF CONSTRUCTION UNLESS OTHERWISE NOTED.



STA. 2875+63.82
 END JOB BB0301
 LOG MILE 7.11

TEMPORARY EROSION CONTROL DETAILS

4/16/2014

RBB0301.DGN

CONSTRUCTION PAVEMENT MARKINGS:
 APPLY CONSTRUCTION PAVEMENT MARKINGS
 ACCORDING TO STD. DWG. PM-2
 4" YELLOW - 84365 LIN. FT.
 4" (SKIP LINE) WHITE - 18780 LIN. FT.
 4" WHITE - 83574 LIN. FT.
 8" WHITE - 6615 LIN. FT.

PERMANENT PAVEMENT MARKINGS:
 APPLY PERMANENT PAVEMENT MARKINGS
 ACCORDING TO STD. DWG. PM-2
 4" YELLOW = 84365 LIN. FT.
 4" (SKIP LINE) WHITE = 18580 LIN. FT.
 4" WHITE CONTRAST = 200 LIN. FT.
 4" WHITE = 83574 LIN. FT.
 8" WHITE = 6615 LIN. FT.
 RAISED PAV'T MARKINGS (TYPE II) = 1950 EACH
 80" SPACING (EXCEPT WHERE SHOWN ON STD. DWG. PM-2)

NOTE:
 CONSTRUCTION PAVEMENT MARKINGS
 QUANTITY BASED ON ONE APPLICATION
 OF EXISTING PAVEMENT MARKINGS.
 FOR ADDITIONAL INFORMATION,
 SEE STD. DRG. PM-2.

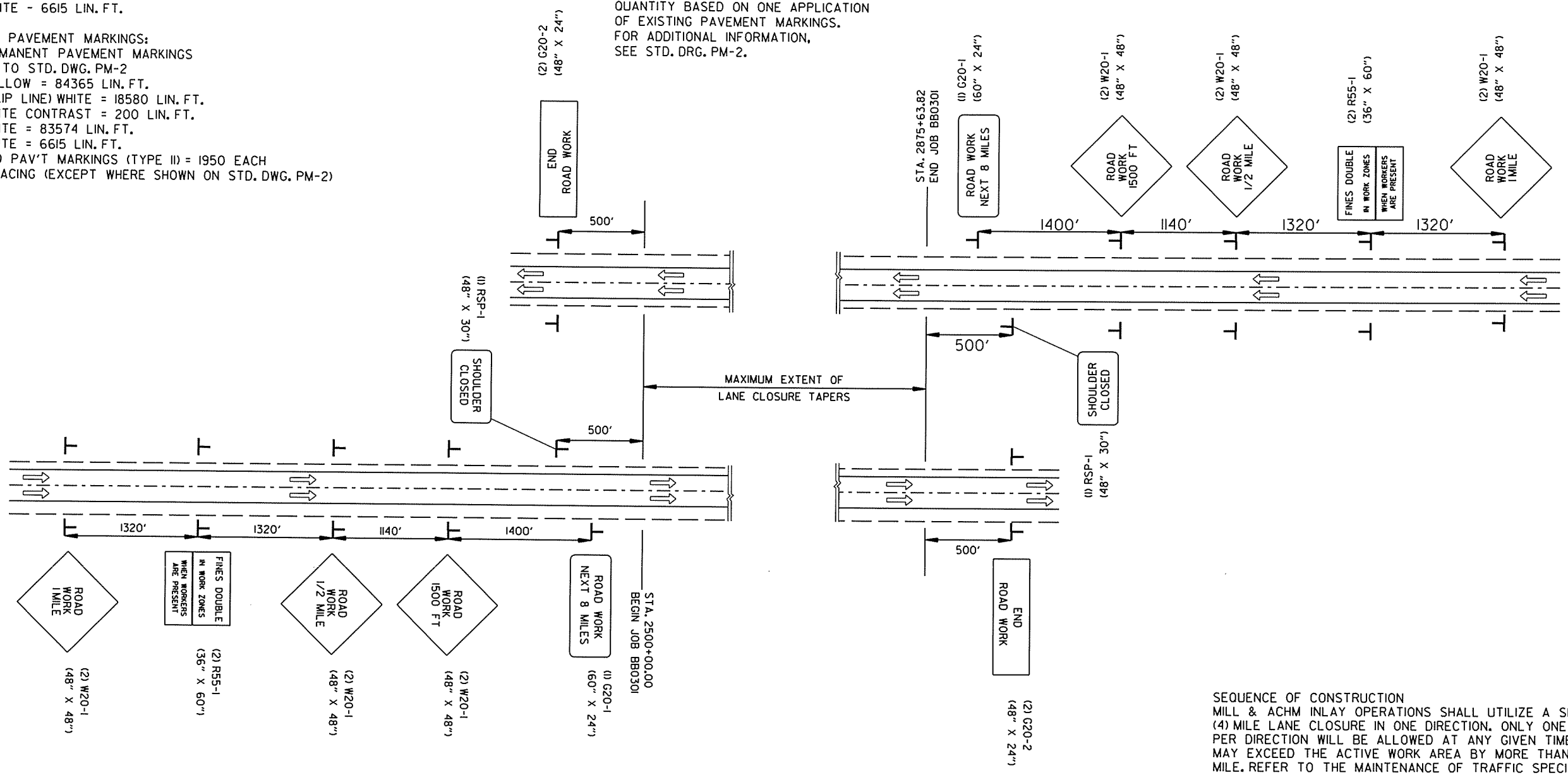
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		24	66
JOB NO. BB0301								

② MAINTENANCE OF TRAFFIC



PORTABLE CHANGEABLE MESSAGE SIGN
 PLACED AS DIRECTED BY THE ENGINEER

PORTABLE CHANGEABLE MESSAGE SIGN
 PLACED AS DIRECTED BY THE ENGINEER



SEQUENCE OF CONSTRUCTION
 MILL & ACHM INLAY OPERATIONS SHALL UTILIZE A SINGLE FOUR (4) MILE LANE CLOSURE IN ONE DIRECTION. ONLY ONE LANE CLOSURE PER DIRECTION WILL BE ALLOWED AT ANY GIVEN TIME. NO LANE CLOSURE MAY EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE. REFER TO THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION.

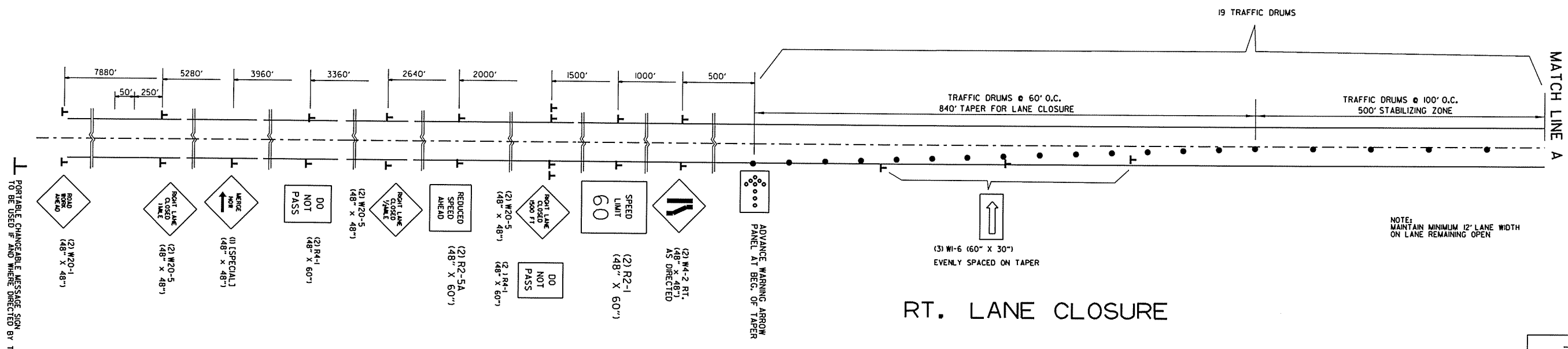
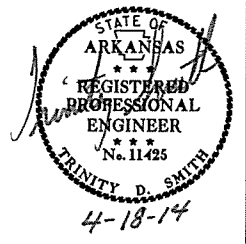
MILL & ACHM INLAY OPERATIONS FOR THE WESTBOUND MAIN LANES SHALL BE COMPLETED BEFORE MILL AND ACHM INLAY OPERATIONS CAN BEGIN ON THE EASTBOUND MAIN LANES. A TWO (2) MILE LANE CLOSURE WILL BE PERMITTED IN THE OPPOSITE MAIN LANES FOR CONSTRUCTION OTHER THAN MILL & INLAY OPERATIONS AS LONG AS NO OTHER LANE CLOSURE EXISTS IN THAT SET OF LANES.

FOR POLYMER OVERLAY OPERATIONS, A SINGLE LANE CLOSURE OF NO MORE THAN TWO (2) MILES WILL BE PERMITTED. POLYMER OVERLAY OPERATIONS MAY BEGIN WHEN MILL & INLAY OPERATIONS FOR A SET OF LANES HAS BEEN COMPLETED. ONLY ONE LANE CLOSURE PER SET OF MAIN LANES WILL BE ALLOWED AND SHALL NOT EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE.

MAINTENANCE OF TRAFFIC
 ADVANCE SIGNS AT JOB ENDS

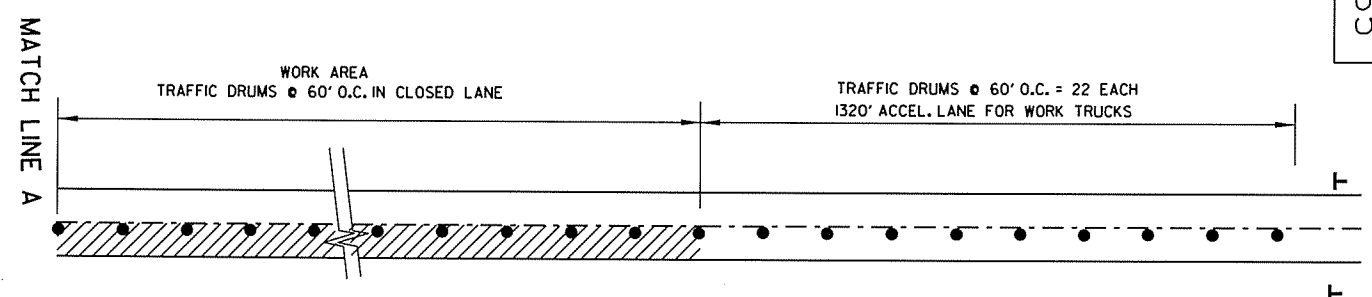
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	25	66

② MAINTENANCE OF TRAFFIC



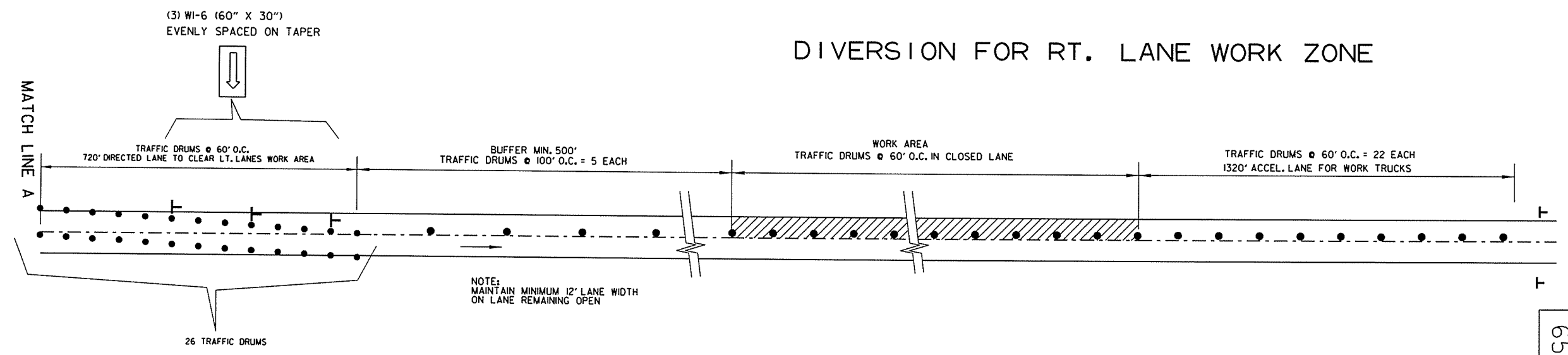
PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

SPEED LIMIT SIGNS ARE ALSO PROVIDED FOR PLACEMENT PAST ENTRANCE RAMPS WITHIN THE WORK ZONE.



SPEED LIMIT	70
TRUCKS	65

(2) R2-1 (48" X 60")
(2) R2-2 (48" X 48")



SPEED LIMIT	70
TRUCKS	65

(2) R2-1 (48" X 60")
(2) R2-2 (48" X 48")

DIVERSION FOR LT. LANE WORK ZONE

MAINTENANCE OF TRAFFIC LANE CLOSURE

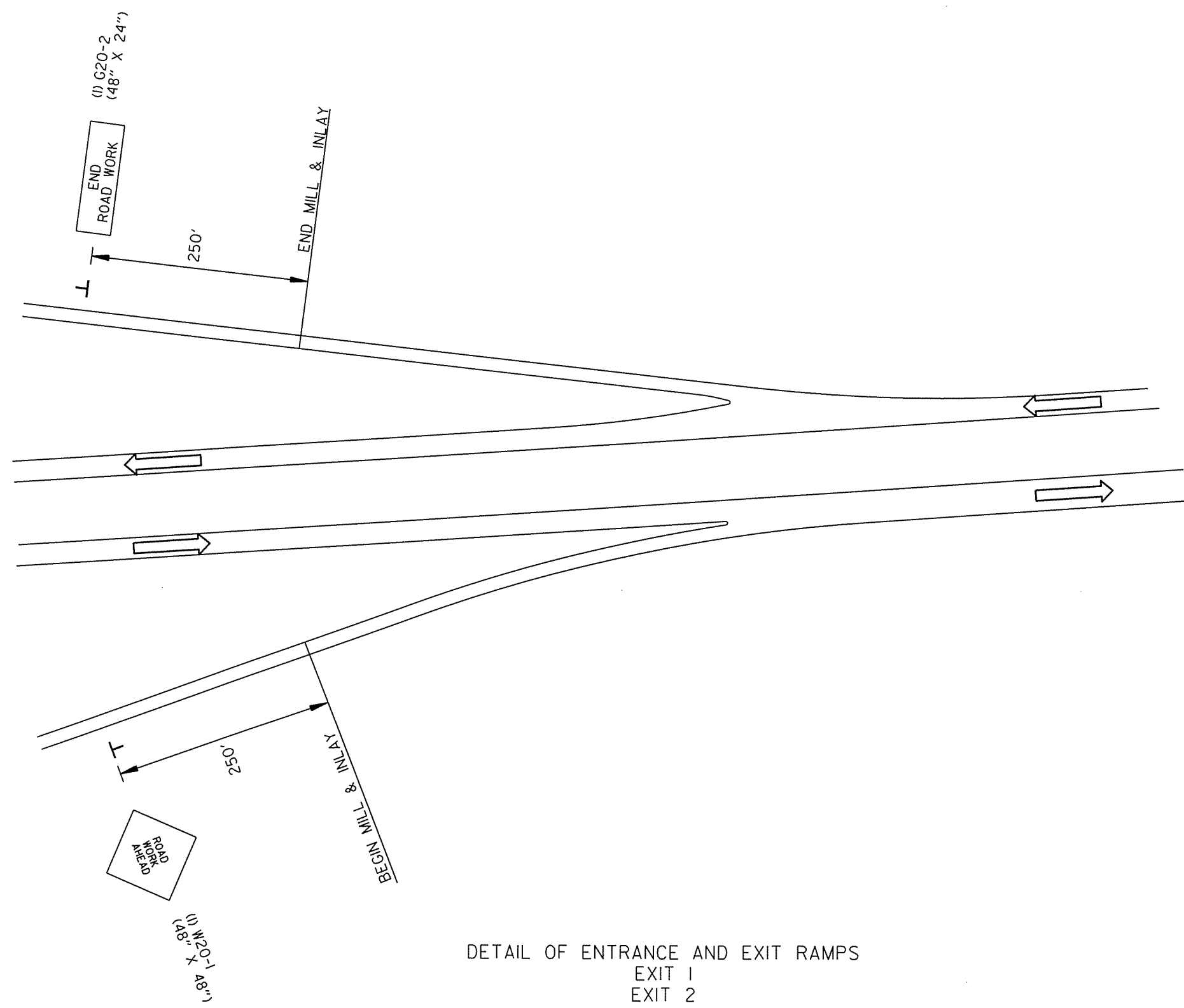
4/17/2014

RB0301.DCN

ADVANCE WARNING SIGNS FOR ENTRANCE AND EXIT RAMP
 ROAD WORK AHEAD (6) = 96 SQ. FT.
 END ROAD WORK (5) = 40 SQ. FT.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							26	66

② MAINTENANCE OF TRAFFIC

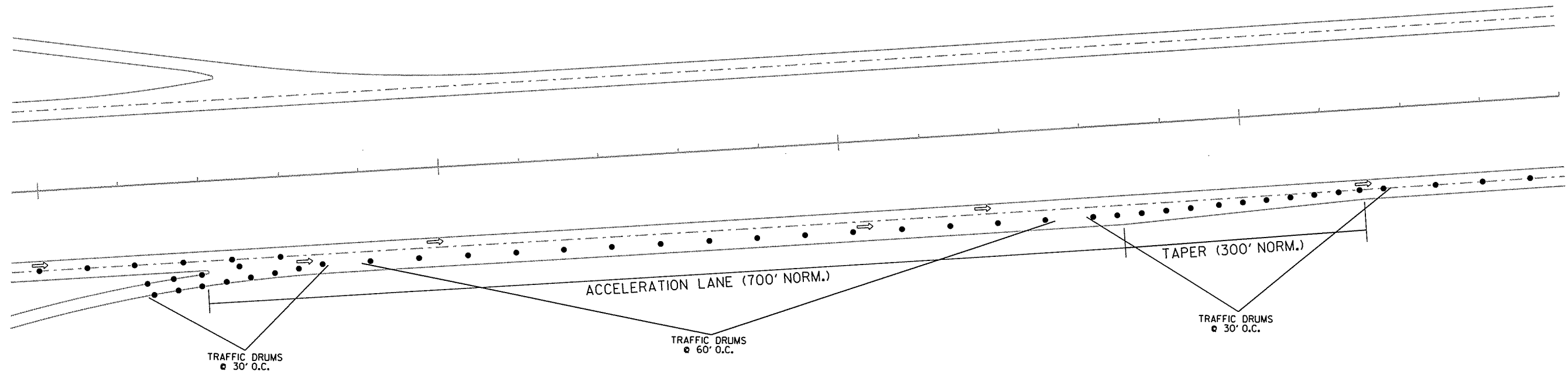
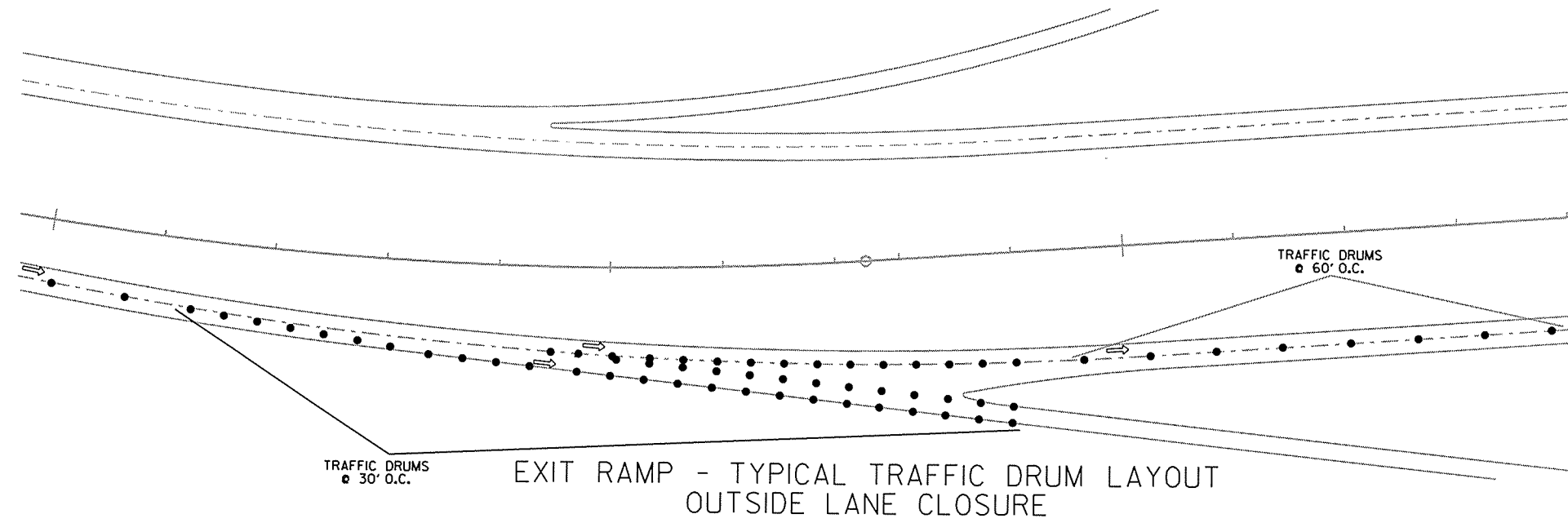


DETAIL OF ENTRANCE AND EXIT RAMP
 EXIT 1
 EXIT 2
 EXIT 3
 EXIT 7

MAINTENANCE OF TRAFFIC
 DETAIL OF RAMPS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							27	66

② MAINTENANCE OF TRAFFIC



ENTRANCE RAMP - TYPICAL TRAFFIC DRUM LAYOUT
ACCELERATION LANE CLOSURE

EXIT 1:
EASTBOUND EXIT = 40 TRAFFIC DRUMS
EASTBOUND ENTRANCE = 17 TRAFFIC DRUMS

WESTBOUND EXIT = 40 TRAFFIC DRUMS
WESTBOUND ENTRANCE = 17 TRAFFIC DRUMS

EXIT 2:
EASTBOUND EXIT = 40 TRAFFIC DRUMS
EASTBOUND ENTRANCE = 17 TRAFFIC DRUMS

WESTBOUND EXIT = 40 TRAFFIC DRUMS
WESTBOUND ENTRANCE = 17 TRAFFIC DRUMS

EXIT 3:
EASTBOUND EXIT = 40 TRAFFIC DRUMS
EASTBOUND ENTRANCE = 17 TRAFFIC DRUMS

WESTBOUND EXIT = 40 TRAFFIC DRUMS
WESTBOUND ENTRANCE = 17 TRAFFIC DRUMS

EXIT 7:
EASTBOUND EXIT = 40 TRAFFIC DRUMS
EASTBOUND ENTRANCE = 17 TRAFFIC DRUMS

WESTBOUND EXIT = 40 TRAFFIC DRUMS
WESTBOUND ENTRANCE = 17 TRAFFIC DRUMS

MAINTENANCE OF TRAFFIC
DETAIL OF RAMPS WITH LANE CLOSURE

4/15/2014

RB0301.DGN

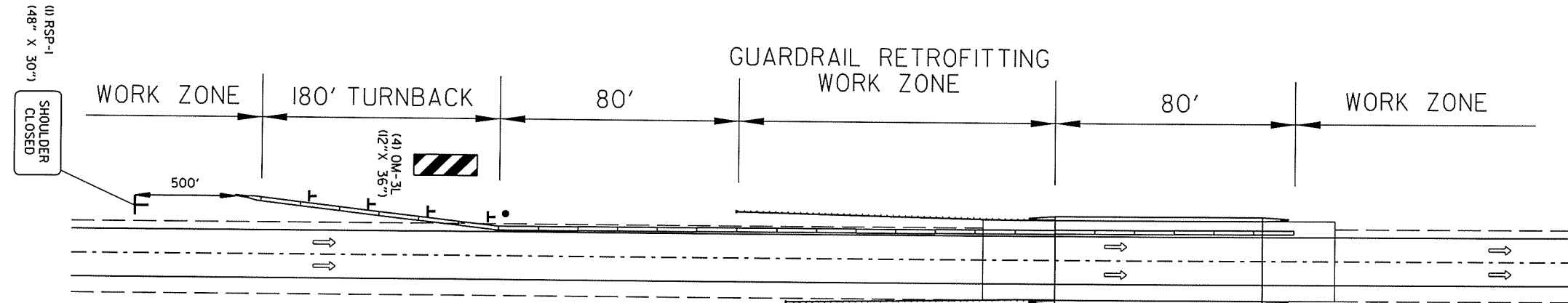
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							28	66

② MAINTENANCE OF TRAFFIC



PRECAST CONCRETE BARRIER WALL OUTSIDE SHOULDER (4 LOCATIONS PER SIDE)
 (1) FURNISH AND INSTALL = 2412 LIN. FT. (4 LOCATIONS)
 (1) RELOCATE = 2412 LIN. FT. TOTAL (4 LOCATIONS)
 (573-633 LIN. FT.) PER LOCATION

PRECAST CONCRETE BARRIER WALL INSIDE SHOULDER (3 LOCATIONS PER SIDE)
 (1) RELOCATE = 3838 LIN. FT. TOTAL (6 LOCATIONS)
 (573-673 LIN. FT.) PER LOCATION

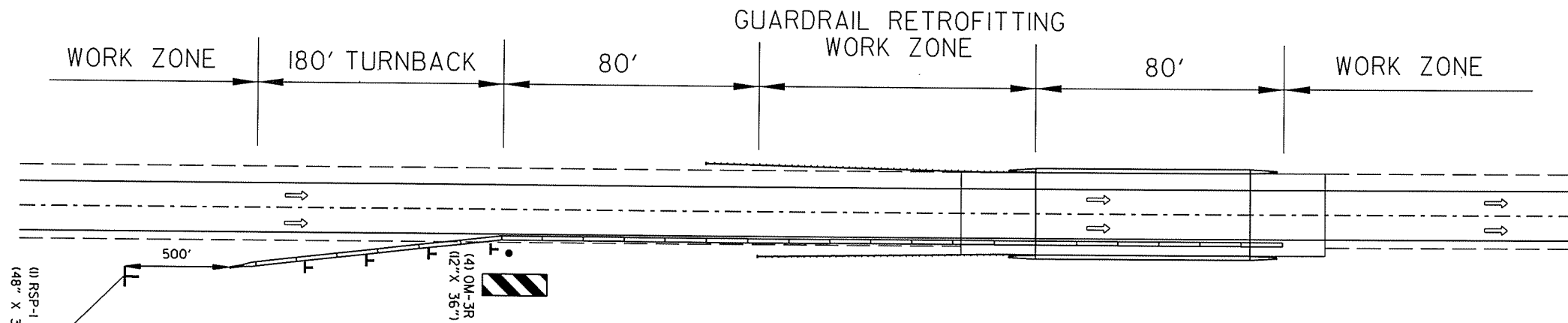


NOTE: OM-3L SIGNS SHALL BE EQUALLY SPACED ALONG P.C.C.B. TURNBACK.

REFER TO STANDARD DRAWING TC-5 FOR DETAILS OF PLACEMENT OF P.C.C.B. TURNBACKS.

TYPICAL LAYOUT OF PRECAST BARRIER INSIDE SHOULDER

3 SETS OF THIS NEEDED FOR JOB BB0301.



REFER TO STANDARD DRAWING TC-5 FOR DETAILS OF PLACEMENT OF P.C.C.B. TURNBACKS.

4 SETS OF THIS NEEDED FOR JOB BB0301.

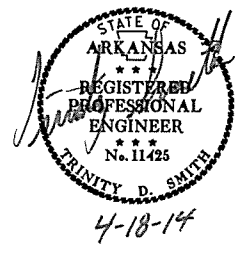
TYPICAL LAYOUT OF PRECAST BARRIER OUTSIDE SHOULDER

NOTE: OM-3R SIGNS SHALL BE EQUALLY SPACED ALONG P.C.C.B. TURNBACK.

MAINTENANCE OF TRAFFIC DETAILS
 WORK ZONE - BRIDGE DECK REHABILITATION

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0301	29
							66	

② QUANTITIES



CONSTRUCTION PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS

DESCRIPTION	ENTIRE PROJECT LIN. FT. - EACH	REMOVAL OF PERMANENT PAVEMENT MARKINGS LIN. FT.	CONSTRUCTION PAVEMENT MARKINGS LIN. FT.	RAISED PAVEMENT MARKERS	HIGH PERFORMANCE CONTRAST PAVEMENT MARKING	HIGH PERFORMANCE PAVEMENT MARKING				
				TYPE II (WHITE/RED) EACH	4" WHITE LIN. FT.	4"		8"		
						(SKIP LINE) WHITE	WHITE	YELLOW	WHITE	
REMOVAL OF PERMANENT PAVEMENT MARKINGS	1808	1808								
CONSTRUCTION PAVEMENT MARKINGS	193334		193334							
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED)	1950			1950						
HIGH PERFORMANCE CONTRAST PAVEMENT MARKING WHITE (4")	200				200					
HIGH PERFORMANCE PAVEMENT MARKING (SKIP LINE) WHITE (4")	18580					18580				
HIGH PERFORMANCE PAVEMENT MARKING WHITE (4")	83574						83574			
HIGH PERFORMANCE PAVEMENT MARKING YELLOW (4")	84365							84365		
HIGH PERFORMANCE PAVEMENT MARKING WHITE (8")	6615								6615	
TOTALS:		1808	193334	1950	200	18580	83574	84365	6615	

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	ENTIRE PROJECT LIN. FT. - EACH	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS EACH	FURNISHING & INSTALLING PRECAST CONC. BARRIER LIN. FT.	RELOCATING PRECAST CONCRETE BARRIER LIN. FT.	ADVANCE WARNING ARROW PANEL DAY	PORTABLE CHANGEABLE MESSAGE SIGN WEEK	PORTABLE CHANGEABLE MESSAGE SIGN (TRUCK MOUNTED) WEEK	FURNISH AND OPERATION OF MOBILE SPEED NOTIFICATION SYSTEM EACH
					NO.	SQ. FT.							
W20-1	ROAD WORK 1500 FT.	48"x48"	4	4	4	64.0							
W20-1	ROAD WORK 1/2 MILE	48"x48"	4	4	4	64.0							
W20-1	ROAD WORK 1 MILE	48"x48"	4	4	4	64.0							
W20-1	ROAD WORK AHEAD	48"x48"	10	10	10	160.0							
G20-2	END ROAD WORK	48"x24"	9	9	9	72.0							
G20-1	ROAD WORK NEXT XX MILES	60"x24"	2	2	2	20.0							
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	4	4	4	64.0							
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	4	4	4	64.0							
W20-5	RIGHT LANE CLOSED 1500 FT.	48"x48"	4	4	4	64.0							
SPECIAL	MERGE NOW W/ ARROW	48"x48"	2	2	2	32.0							
R2-5A	REDUCED SPEED AHEAD	48"x60"	4	4	4	80.0							
R55-1	FINES DOUBLE IN WORK ZONES	36"x60"	4	4	4	60.0							
OM-3L	OBJECT MARKER	12"x36"	12	12	12	36.0							
OM-3R	OBJECT MARKER	12"x36"	16	16	16	48.0							
W1-6	LARGE ARROW	48"x24"	12	12	12	96.0							
R4-1	DO NOT PASS	48"x60"	8	8	8	160.0							
R2-1	SPEED LIMIT 60 MPH	48"x60"	9	9	9	180.0							
R2-1	SPEED LIMIT 70 MPH	48"x60"	4	4	4	80.0							
R2-2	TRUCKS SPEED LIMIT 65 MPH	48"x60"	4	4	4	80.0							
W4-2 RT.	MERGE RIGHT	48"x48"	4	4	4	64.0							
RSP-1	SHOULDER CLOSED	48"x30"	6	6	6	60.0							
	TRAFFIC DRUMS		1031	1031			1031						
	FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER		2412	2412			2412						
	RELOCATING PRECAST CONCRETE BARRIER		6250	6250				6250					
	ADVANCE WARNING ARROW PANEL		2	2					60				
	PORTABLE CHANGEABLE MESSAGE SIGN		4	4						32			
	PORTABLE CHANGEABLE MESSAGE SIGN (TRUCK MOUNTED)		1	1							8		
	FURNISH AND OPERATION OF MOBILE SPEED NOTIFICATION SYSTEM		2	2									2
TOTALS:						1612.0	1031	2412	6250	60	32	8	2

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

NOTE: THE QUANTITY OF TRAFFIC DRUMS PROVIDED IS FOR BOTH SIDES OF THE ROADWAY FOR ONE WORK AREA OF TWO MILES AND ONE WORK AREA OF FOUR MILES. HOWEVER, THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

4/17/2014

RB0301.DGN

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
5-19-14				6	ARK.			
						JOB NO. BB0301	30	66

② QUANTITIES



REMOVAL AND DISPOSAL OF ITEMS

STATION	STATION	LOCATION	GUARDRAIL	IMPACT ATTENUATION BARRIER
			LIN. FT.	EACH
2536+29.35	2539+79.35	LEFT OF RIGHT MAIN LANES	350	
2544+70.35	2548+20.13	LEFT OF RIGHT MAIN LANES	350	
2539+34.41	2542+84.41	RIGHT OF LEFT MAIN LANES	350	
2547+80.41	2551+30.41	RIGHT OF LEFT MAIN LANES	350	
2567+92.39	2568+42.39	LEFT OF RIGHT MAIN LANES	50	
2601+48.71	2604+98.71	LEFT OF RIGHT MAIN LANES	350	
2610+96.60	2614+46.60	LEFT OF RIGHT MAIN LANES	350	
2604+50.71	2608+00.71	RIGHT OF LEFT MAIN LANES	350	
2614+98.31	2618+48.31	RIGHT OF LEFT MAIN LANES	350	
2727+03.77	2729+03.77	LEFT OF RIGHT MAIN LANES	200	
2727+40.62	2729+40.62	RIGHT OF RIGHT MAIN LANES	200	
2728+47.15	2730+47.15	LEFT OF LEFT MAIN LANES	200	
2728+83.99	2730+83.99	RIGHT OF LEFT MAIN LANES	200	
2757+36.01	2759+36.01	LEFT OF RIGHT MAIN LANES	200	
2757+57.95	2759+57.95	RIGHT OF RIGHT MAIN LANES	200	
2760+04.26	2762+04.26	LEFT OF LEFT MAIN LANES	200	
2760+26.20	2762+26.20	RIGHT OF LEFT MAIN LANES	200	
2796+17.34	2798+17.34	LEFT OF RIGHT MAIN LANES	200	
2796+39.34	2798+39.34	RIGHT OF RIGHT MAIN LANES	200	
2798+87.90	2800+87.90	LEFT OF LEFT MAIN LANES	200	
2799+09.39	2801+09.39	RIGHT OF LEFT MAIN LANES	200	
2841+94.68	2843+94.68	RIGHT OF RIGHT MAIN LANES	200	
2844+13.89	2846+13.89	LEFT OF LEFT MAIN LANES	200	
2543+67		JEFFERSON AVE. OVERPASS		2
2610+09		HWY. 245 OVERPASS		2
2674+59		HWY. 549 INTERCHANGE		2
2844+04		HWY. 108 OVERPASS		2
TOTALS:			5650	8

REMOVAL AND DISPOSAL OF PLOWABLE PAVEMENT MARKER

LOG MILE	LOG MILE	LOCATION	PLOWABLE PAVEMENT MARKER
			EACH
0.00	7.11	ENTIRE PROJECT	939
TOTAL:			939

GUARDRAIL

STATION	STATION	LOCATION	GUARDRAIL (TYPE A)	THRIE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 2)	TERMINAL ANCHOR POST (TYPE 1)
			LIN. FT.		EACH	
* 2536+29.35	2548+29.35	LEFT OF RIGHT MAIN LANES	1150		1	1
* 2539+30.41	2551+30.41	RIGHT OF LEFT MAIN LANES	1150		1	1
2567+92.39	2568+42.39	LEFT OF RIGHT MAIN LANES			1	
* 2601+48.71	2615+48.71	LEFT OF RIGHT MAIN LANES	1350		1	1
* 2604+48.31	2618+48.31	RIGHT OF LEFT MAIN LANES	1350		1	1
2670+28.25	2676+53.25	LEFT OF RIGHT MAIN LANES	575		1	1
2672+89.90	2679+14.90	RIGHT OF LEFT MAIN LANES	575		1	1
2726+85.02	2729+03.77	LEFT OF RIGHT MAIN LANES	150	1	1	
2727+21.87	2729+40.62	RIGHT OF RIGHT MAIN LANES	150	1	1	
2728+47.15	2730+65.90	LEFT OF LEFT MAIN LANES	150	1	1	
2728+83.99	2731+02.74	RIGHT OF LEFT MAIN LANES	150	1	1	
2756+17.26	2759+36.01	LEFT OF RIGHT MAIN LANES	250	1	1	
2756+89.20	2759+57.95	RIGHT OF RIGHT MAIN LANES	200	1	1	
2760+04.26	2762+73.01	LEFT OF LEFT MAIN LANES	200	1	1	
2760+26.20	2763+44.95	RIGHT OF LEFT MAIN LANES	250	1	1	
2794+98.59	2798+17.34	LEFT OF RIGHT MAIN LANES	250	1	1	
2795+70.59	2798+39.34	RIGHT OF RIGHT MAIN LANES	200	1	1	
2798+87.90	2801+56.65	LEFT OF LEFT MAIN LANES	200	1	1	
2799+09.39	2802+28.14	RIGHT OF LEFT MAIN LANES	250	1	1	
2840+30.08	2845+30.08	LEFT OF RIGHT MAIN LANES	450		1	1
2841+75.93	2843+94.68	RIGHT OF RIGHT MAIN LANES	150	1	1	
2842+78.46	2847+78.46	RIGHT OF LEFT MAIN LANES	450		1	1
2844+13.89	2846+32.64	LEFT OF LEFT MAIN LANES	150	1	1	
TOTALS:			9750	14	23	8

* REMOVE CONCRETE DITCH PAVING AS REQUIRED FOR GUARDRAIL POST INSTALLATION AND CONCRETE PATCH AROUND POST, PAYMENT INCLUDED IN PRICE BID FOR GUARD RAIL (TYPE A).

WIRE ROPE SAFETY FENCE

STATION	STATION	LOCATION	WIRE ROPE SAFETY FENCE	*WRSF ANCHOR	WRSF MAINTENANCE MATERIALS	**WRSF POST REPAIR
			LIN. FT.	EACH	LUMP SUM	EACH
2500+74.00	2539+81.00	RIGHT OF LEFT MAIN LANES	3907.00	2		
2547+70.00	2601+99.00	LEFT OF RIGHT MAIN LANES	5429.00	2		
2617+98.00	2673+40.00	RIGHT OF LEFT MAIN LANES	5542.00	2		
2678+65.00	2729+36.00	RIGHT OF LEFT MAIN LANES	5071.00	2		
2731+39.00	2758+96.00	RIGHT OF LEFT MAIN LANES	2757.00	2		
2762+95.00	2797+80.00	RIGHT OF LEFT MAIN LANES	3485.00	2		
2801+79.00	2843+28.00	RIGHT OF LEFT MAIN LANES	4149.00	2		
ENTIRE	PROJECT				1.00	50
TOTALS:			30340.00	14	1.00	50

* SHOWN FOR INFORMATION ONLY
 ** QUANTITY ESTIMATED
 SEE SECTION 104.03 OF THE STD. SPECS.

5/19/2014

RB0301.DGN

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0301	31
								66

2 QUANTITIES



EARTHWORK

STATION	STATION	LOCATION / DESCRIPTION	UNCLASSIFIED EXCAVATION	COMPACTED EMBANKMENT
			CU. YD.	
2535+86	2543+31	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT JEFFERSON AVE. INTERCHANGE	147	
2538+72	2543+41	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT JEFFERSON AVE. INTERCHANGE	93	
2543+93	2548+87	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT JEFFERSON AVE. INTERCHANGE	98	
2544+04	2551+73	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT JEFFERSON AVE. INTERCHANGE	152	
2600+91	2609+73	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 245 INTERCHANGE	174	
2604+05	2609+82	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 245 INTERCHANGE	114	
2610+35	2615+92	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 245 INTERCHANGE	110	
2610+44	2619+06	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 245 INTERCHANGE	170	
2669+85	2676+96	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 549 INTERCHANGE	141	
2672+32	2679+73	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 549 INTERCHANGE	146	
2726+42	2729+04	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 296 OVERPASS	26	
2726+79	2729+41	RT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 296 OVERPASS	26	
2728+47	2731+09	LT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 296 OVERPASS	26	
2728+84	2731+10	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 296 OVERPASS	22	
2755+74	2759+36	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. B3569	36	
2756+46	2759+58	RT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. B3569	31	
2760+04	2763+16	LT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. A3569	31	
2760+26	2763+88	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. A3569	36	
2794+56	2798+17	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. B3570	36	
2795+28	2798+39	RT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. B3570	31	
2798+88	2802+00	LT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. A3570	31	
2799+09	2802+71	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT BR. NO. A3570	36	
2839+87	2845+73	LT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 108 OVERPASS	116	
2841+33	2843+95	RT. OF RT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 108 OVERPASS	26	
2842+20	2848+21	LT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 108 OVERPASS	119	
2844+14	2846+76	RT. OF LT. MAIN LANES SHLDR. WIDENING FOR GUARDRAIL AT HWY. 108 OVERPASS	26	
2708+00		MEDIAN CROSSOVER (REMOVAL)	115	
2731+20		MEDIAN CROSSOVER	15	260
ENTIRE	PROJECT	TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER		200
TOTALS:			2130	460

* QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

NOTE: EARTHWORK QUANTITIES SHOWN ABOVE SHALL BE PAID AS PLAN QUANTITY.

EROSION CONTROL

STATION	STATION	LOCATION	PERMANENT EROSION CONTROL					TEMPORARY EROSION CONTROL		
			SEEDING	LIME	MULCH COVER	WATER	SECOND SEEDING APPLICATION	SAND BAG DITCH CHECKS (E-5)	DROP INLET SILT FENCE (E-7)	*SEDIMENT REMOVAL & DISPOSAL
			ACRE	TON	ACRE	M.GAL.	ACRE	BAG	LIN. FT.	CU. YD.
ENTIRE	PROJECT	MAIN LANES	7.00	14.00	7.00	714.0	7.00	836	850	69
*ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.			1.75	3.50	1.75	178.5	1.75	209	213	8
TOTALS:			8.75	17.50	8.75	892.5	8.75	1045	1063	77

BASIS OF ESTIMATE:
LIME2 TONS / ACRE OF SEEDING
WATER.....102.0 M.G. / ACRE OF SEEDING.
SAND BAG DITCH CHECKS.....22 BAGS / LOCATION
DROP INLET SILT FENCE.....25 LIN. FT./LOCATION

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

*QUANTITIES ARE ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

RUMBLE STRIPS IN ASPHALT SHOULDERS

STATION	STATION	LOCATION	* RUMBLE STRIPS IN ASPHALT SHOULDERS
			LIN. FT.
2500+00	2507+69	RT. OF RT. MAIN LANES	769
2509+44	2539+00	RT. OF RT. MAIN LANES	2956
2539+01	2549+38	RT. OF RT. MAIN LANES	1037
2549+41	2600+97	RT. OF RT. MAIN LANES	5156
2600+64	2620+37	RT. OF RT. MAIN LANES	1973
2620+39	2645+24	RT. OF RT. MAIN LANES	2485
2645+26	2683+30	RT. OF RT. MAIN LANES	3804
2683+31	2698+20	RT. OF RT. MAIN LANES	1489
2698+21	2759+16	RT. OF RT. MAIN LANES	6095
2761+09	2797+99	RT. OF RT. MAIN LANES	3690
2799+92	2834+35	RT. OF RT. MAIN LANES	3443
2836+11	2852+98	RT. OF RT. MAIN LANES	1687
2853+00	2874+56	RT. OF RT. MAIN LANES	2156
2874+58	2875+64	RT. OF RT. MAIN LANES	106
2500+00	2759+02	LT. OF RT. MAIN LANES	25902
2760+95	2797+85	LT. OF RT. MAIN LANES	3690
2799+79	2875+64	LT. OF RT. MAIN LANES	7585
2500+00	2505+77	LT. OF LT. MAIN LANES	577
2505+78	2538+60	LT. OF LT. MAIN LANES	3282
2538+25	2554+90	LT. OF LT. MAIN LANES	1665
2554+56	2599+93	LT. OF LT. MAIN LANES	4537
2599+95	2619+95	LT. OF LT. MAIN LANES	2000
2619+60	2651+98	LT. OF LT. MAIN LANES	3238
2651+99	2696+92	LT. OF LT. MAIN LANES	4493
2696+94	2758+54	LT. OF LT. MAIN LANES	6160
2760+47	2797+37	LT. OF LT. MAIN LANES	3690
2799+31	2835+08	LT. OF LT. MAIN LANES	3577
2835+10	2851+97	LT. OF LT. MAIN LANES	1687
2853+73	2875+29	LT. OF LT. MAIN LANES	2156
2500+00	2758+67	RT. OF LT. MAIN LANES	25867
2760+60	2797+51	RT. OF LT. MAIN LANES	3691
2799+45	2875+29	RT. OF LT. MAIN LANES	7584
TOTAL:			148227

* QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

4/17/2014

RB0301.DGN

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							BB0301	32	66

COLD MILLING ASPHALT PAVEMENT

STATION	STATION	LOCATION	AVG. WIDTH	COLD MILLING ASPHALT PAVEMENT
			FEET	SQ. YD.
MAIN LANES				
2500+00.00	2758+58.73	LT. MAIN LANES	30	86195.77
2760+51.73	2797+42.15	LT. MAIN LANES	30	12301.40
2799+35.15	2875+28.96	LT. MAIN LANES	30	25312.70
2500+00.00	2759+10.40	RT. MAIN LANES	30	86368.00
2761+03.40	2797+91.87	RT. MAIN LANES	30	12294.90
2799+84.87	2875+63.82	RT. MAIN LANES	30	25263.17
ADDITIONAL FOR ENTRANCE AND EXIT RAMP				
2500+00.00	2509+44.01	EXIT 1 RT. MAIN LANES - TURN OUT	VARIES	1804.52
2539+00.33	2543+06.68	EXIT 1 RT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	428.46
2549+39.72	2559+37.82	EXIT 1 RT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	1148.24
2543+88.02	2549+38.02	EXIT 1 RAMP 2 - ENTRANCE RAMP	15.5	947.22
2554+54.47	2557+79.44	EXIT 1 LT. MAIN LANES - TURN OUT	VARIES	293.16
2549+05.41	2554+55.41	EXIT 1 RAMP 3 - EXIT RAMP	15.5	947.22
2538+23.75	2543+51.10	EXIT 1 LT. MAIN LANES - TURN OUT	VARIES	415.24
2500+00.00	2505+78.24	EXIT 1 LT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	827.31
2597+95.03	2601+00.06	EXIT 2 RT. MAIN LANES - TURN OUT	VARIES	263.99
2600+98.56	2606+48.56	EXIT 2 RAMP 1 - EXIT RAMP	15.5	947.22
2620+37.09	2630+34.32	EXIT 2 RT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	1157.76
2614+87.91	2620+37.91	EXIT 2 RAMP 2 - ENTRANCE RAMP	15.5	947.22
2619+57.74	2622+55.71	EXIT 2 LT. MAIN LANES - TURN OUT	VARIES	266.77
2614+09.11	2619+59.11	EXIT 2 RAMP 3 - EXIT RAMP	15.5	947.22
2589+99.86	2599+94.09	EXIT 2 LT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	1163.73
2599+94.09	2605+44.09	EXIT 2 RAMP 4 - ENTRANCE RAMP	15.5	947.22
2642+46.50	2645+25.66	EXIT 3 RT. MAIN LANES - TURN OUT	VARIES	217.39
2683+30.09	2694+97.70	EXIT 3 RT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	1339.26
2698+19.98	2708+12.57	EXIT 3 RT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	1149.37
2696+91.99	2700+14.69	EXIT 3 LT. MAIN LANES - TURN OUT	VARIES	254.89
2639+66.34	2651+98.56	EXIT 3 LT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	1441.84
2829+47.28	2836+11.08	EXIT 7 RT. MAIN LANES - TURN OUT	VARIES	925.42
2852+98.43	2874+58.47	EXIT 7 RT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	2927.71
2851+97.48	2858+62.70	EXIT 7 LT. MAIN LANES - TURN OUT	VARIES	925.46
2825+10.32	2835+10.12	EXIT 7 LT. MAIN LANES - ACCELERATION LANE AND TAPER	VARIES	1156.43
434+37.98	447+43.80	EXIT 7 RAMP 1 - EXIT RAMP	15.5	2248.91
447+43.80	447+55.80	EXIT 7 RAMP 1 - HWY. 108 INTERSECTION	VARIES	625.28
443+67.59	452+99.43	EXIT 7 RAMP 2 - ENTRANCE RAMP	15.5	1604.84
438+64.77	443+67.59	EXIT 7 RAMP 2 - HWY. 108 INTERSECTION SOUTH END	VARIES	1454.79
440+55.13	443+67.59	EXIT 7 RAMP 2 - HWY. 108 INTERSECTION NORTH END	VARIES	900.95
440+60.86	453+70.53	EXIT 7 RAMP 3 - EXIT RAMP	15.5	2255.54
440+47.99	440+60.86	EXIT 7 RAMP 3 - HWY. 108 INTERSECTION	VARIES	498.58
435+09.12	444+42.45	EXIT 7 RAMP 4 - ENTRANCE RAMP	15.5	1607.40
444+42.45	447+40.71	EXIT 7 RAMP 4 - HWY. 108 INTERSECTION SOUTH END	VARIES	870.34
444+42.45	448+52.56	EXIT 7 RAMP 4 - HWY. 108 INTERSECTION NORTH END	VARIES	1138.13
TOTAL:				284730.97

NOTE: AVERAGE MILLING DEPTH 2".

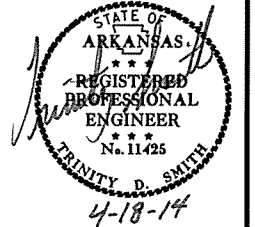
CONCRETE DITCH PAVING

STATION	STATION	LOCATION	LENGTH	"W"	CONC. DITCH PAVING (TYPE B)	SOLID SODDING	WATER
			LIN. FT.	FEET	SQ. YD.	SQ. YD.	M. GAL.
2500+74	2539+20	RIGHT OF LEFT MAIN LANES	3846.00	4	1709.33	1709.33	21.54
2548+30	2601+38	LEFT OF RIGHT MAIN LANES	5308.00	4	2359.11	2359.11	29.72
2618+58	2672+80	LEFT OF RIGHT MAIN LANES	5422.00	4	2409.78	2409.78	30.36
2679+18	2729+36	LEFT OF RIGHT MAIN LANES	5018.00	4	2230.22	2230.22	28.10
2731+19	2758+96	LEFT OF RIGHT MAIN LANES	2777.00	4	1234.22	1234.22	15.55
2762+95	2797+80	LEFT OF RIGHT MAIN LANES	3485.00	4	1548.89	1548.89	19.52
2801+79	2842+68	LEFT OF RIGHT MAIN LANES	4089.00	4	1817.33	1817.33	22.90
TOTALS:					13308.88	13308.88	167.69

BASIS OF ESTIMATE:

WATER.....12.6 GAL. / SQ. YD. OF SOLID SODDING.

QUANTITIES



ACHM PATCHING OF EXISTING ROADWAY

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	100
TOTAL:	100

NOTE: QUANTITY IS ESTIMATED
SEE SECTION 104.03 OF THE STD. SPECS.

BRIDGE QUANTITIES

UNIT OF STRUCTURE	JOINT REHABILITATION (TYPE A)	POLYMER OVERLAY	BRIDGE DECK REPAIR
	LIN. FT.	SQ. YD.	SQ. FT.
BRIDGE NO. A3569	231	534	250
BRIDGE NO. B3569	231	534	250
BRIDGE NO. A3570	231	534	250
BRIDGE NO. B3570	231	534	250
TOTALS:	924	2136	1000

NOTE: EXISTING BRIDGE DECKS HAVE NO ASPHALTIC OVERLAY.

FOG SEAL

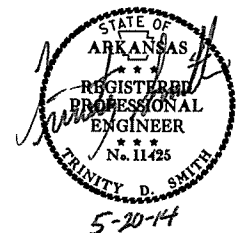
STATION	STATION	LOCATION	LENGTH	AVERAGE WIDTH	FOG SEAL	
			LIN. FT.	FEET	SQ. YD.	GALLON
2500+00	2507+69	RT. OF RT. MAIN LANES	769	8	683.56	68.4
2509+44	2539+00	RT. OF RT. MAIN LANES	2956	8	2627.56	262.8
2539+01	2549+38	RT. OF RT. MAIN LANES	1037	8	921.78	92.2
2549+41	2600+97	RT. OF RT. MAIN LANES	5156	8	4583.11	458.3
2600+64	2620+37	RT. OF RT. MAIN LANES	1973	8	1753.78	175.4
2620+39	2645+24	RT. OF RT. MAIN LANES	2485	8	2208.89	220.9
2645+26	2683+30	RT. OF RT. MAIN LANES	3804	8	3381.33	338.1
2683+31	2698+20	RT. OF RT. MAIN LANES	1489	8	1323.56	132.4
2698+21	2759+16	RT. OF RT. MAIN LANES	6095	8	5417.78	541.8
2761+09	2797+99	RT. OF RT. MAIN LANES	3690	8	3280.00	328.0
2799+92	2834+35	RT. OF RT. MAIN LANES	3443	8	3060.44	306.0
2836+11	2852+98	RT. OF RT. MAIN LANES	1687	8	1499.56	150.0
2853+00	2874+56	RT. OF RT. MAIN LANES	2156	8	1916.44	191.6
2874+58	2875+64	RT. OF RT. MAIN LANES	106	8	94.22	9.4
2500+00	2505+77	LT. OF LT. MAIN LANES	577	8	512.89	51.3
2505+78	2538+60	LT. OF LT. MAIN LANES	3282	8	2917.33	291.7
2538+25	2554+90	LT. OF LT. MAIN LANES	1665	8	1480.00	148.0
2554+56	2599+93	LT. OF LT. MAIN LANES	4537	8	4032.89	403.3
2599+95	2619+95	LT. OF LT. MAIN LANES	2000	8	1777.78	177.8
2619+60	2651+98	LT. OF LT. MAIN LANES	3238	8	2878.22	287.8
2651+99	2696+92	LT. OF LT. MAIN LANES	4493	8	3993.78	399.4
2696+94	2758+54	LT. OF LT. MAIN LANES	6160	8	5475.56	547.6
2760+47	2797+37	LT. OF LT. MAIN LANES	3690	8	3280.00	328.0
2799+31	2835+08	LT. OF LT. MAIN LANES	3577	8	3179.56	318.0
2835+10	2851+97	LT. OF LT. MAIN LANES	1687	8	1499.56	150.0
2853+73	2875+29	LT. OF LT. MAIN LANES	2156	8	1916.44	191.6
TOTALS:					65696.02	6569.8

FOG SEAL SHALL BE TYPE SS-1H
BASIS OF ESTIMATE: FOG SEAL = 0.10 GALLON PER SQ. YD.

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
5-19-14				6	ARK.			
						JOB NO. BB0301	34	66

2 SUMMARY OF QUANTITIES AND REVISIONS



SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	9050	BIM-B30-0(201)	QUANTITY	UNIT
SP & 202	REMOVAL AND DISPOSAL OF GUARDRAIL				
210	UNCLASSIFIED EXCAVATION		5650	5650	LIN. FT.
210	COMPACTED EMBANKMENT		2130	2130	CU. YD.
303	AGGREGATE BASE COURSE (CLASS 7)		460	460	CU. YD.
401	TACK COAT		3322	3322	TON
SP, SS, & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")		28473	28473	GAL.
SP, SS, & 407	ASPHALT BINDER (PG 76-22) IN ACHM SURFACE COURSE (1/2")		30614	30614	TON
412	COLD MILLING ASPHALT PAVEMENT		1679	1679	TON
SP & 415	ACHM PATCHING OF EXISTING ROADWAY		284731	284731	SQ. YD.
601	MOBILIZATION		100	100	TON
SP & 603	MAINTENANCE OF TRAFFIC		1.00	1.00	LUMP SUM
604	SIGNS		1.00	1.00	LUMP SUM
604	TRAFFIC DRUMS		1612	1612	SQ. FT.
604	FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER		1031	1031	EACH
604	RELOCATING PRECAST CONCRETE BARRIER		2412	2412	LIN. FT.
604	CONSTRUCTION PAVEMENT MARKINGS		6250	6250	LIN. FT.
604	ADVANCE WARNING ARROW PANEL		193334	193334	LIN. FT.
SP & 604	PORTABLE CHANGEABLE MESSAGE SIGN		60	60	DAY
SP & 604	PORTABLE CHANGEABLE MESSAGE SIGN (TRUCK MOUNTED)		32	32	WEEK
604	REMOVAL OF PERMANENT PAVEMENT MARKINGS		8	8	WEEK
SP & 605	CONCRETE DITCH PAVING (TYPE B)		1808	1808	LIN. FT.
617	GUARDRAIL (TYPE A)	13309		13309	SQ. YD.
617	GUARDRAIL TERMINAL (TYPE 2)		9750	9750	LIN. FT.
617	THREE BEAM GUARDRAIL TERMINAL		23	23	EACH
617	TERMINAL ANCHOR POSTS (TYPE 1)		14	14	EACH
620	LIME		8	8	EACH
620	SEEDING		18	18	TON
SS & 620	MULCH COVER		8.75	8.75	ACRE
620	WATER		8.75	8.75	ACRE
621	SAND BAG DITCH CHECKS	167.7	892.5	1060.2	M.GAL.
621	DROP INLET SILT FENCE		1045	1045	BAG
621	SEDIMENT REMOVAL AND DISPOSAL		1063	1063	LIN. FT.
623	SECOND SEEDING APPLICATION		77	77	CU. YD.
624	SOLID SODDING		8.75	8.75	ACRE
635	ROADWAY CONSTRUCTION CONTROL	13309		13309	SQ. YD.
642	RUMBLE STRIPS IN ASPHALT SHOULDERS		1.00	1.00	LUMP SUM
SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING (SKIP LINE) WHITE (4")		148227	148227	LIN. FT.
SP	HIGH PERFORMANCE MARKING TAPE (SKIP LINE) WHITE (4")		18580	18580	LIN. FT.
SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING WHITE (4")		18580	18580	LIN. FT.
SP	HIGH PERFORMANCE MARKING TAPE WHITE (4")		83574	83574	LIN. FT.
SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING YELLOW (4")		83574	83574	LIN. FT.
SP	HIGH PERFORMANCE MARKING TAPE YELLOW (4")		84365	84365	LIN. FT.
SP & 719	INVERTED PROFILE THERMOPLASTIC CONTRAST PAVEMENT MARKING WHITE (4")		84365	84365	LIN. FT.
SP	HIGH PERFORMANCE CONTRAST MARKING TAPE WHITE (4")		200	200	LIN. FT.
SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING WHITE (8")		200	200	LIN. FT.
SP	HIGH PERFORMANCE MARKING TAPE WHITE (8")		6615	6615	LIN. FT.
721	RAISED PAVEMENT MARKERS (TYPE II)		6615	6615	LIN. FT.
SP	FOG SEAL		1950	1950	EACH
SP	FURNISH AND OPERATION OF MOBILE SPEED NOTIFICATION SYSTEM		6570	6570	GALLON
SP	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIERS		2	2	EACH
SP	REMOVAL AND DISPOSAL OF PLOWABLE PAVEMENT MARKER		8	8	EACH
SP	WIRE ROPE SAFETY FENCE (POST REPAIR)		939	939	EACH
SP	WIRE ROPE SAFETY FENCE	50		50	EACH
SP	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS	30340		30340	LIN. FT.
		1.00		1.00	LUMP SUM
STRUCTURES OVER 20' SPAN					
SP & 509	JOINT REHABILITATION (TYPE A)				
636	BRIDGE CONSTRUCTION CONTROL		924	924	LIN. FT.
SP	BRIDGE DECK REPAIR		1.00	1.00	LUMP SUM
SP	POLYMER OVERLAY		1000	1000	SQ. FT.
			2136	2136	SQ. YD.

* DENOTES ALTERNATE BID ITEMS

REVISIONS

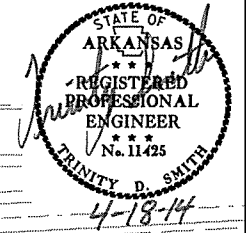
DATE	REVISION	SHEET NUMBER
5/19/2014	ADDED ITEM AND SPECIAL PROVISION "REMOVAL AND DISPOSAL OF PLOWABLE PAVEMENT MARKER"	2, 30, 34

5/19/2014

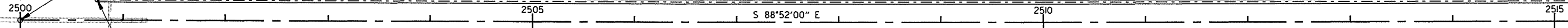
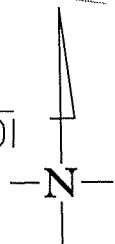
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							35	66

2 PLAN SHEETS



STA. 2500+00.00
BEGIN JOB BB0301
LOG MILE 0.00

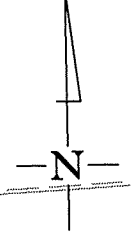


STA. 2500+74
BEGIN WRSF

EXISTING OVERHEAD SIGN

TEXAS
ARKANSAS

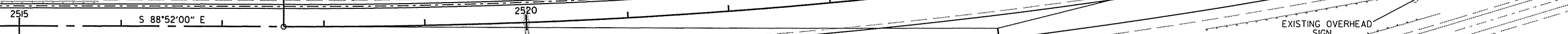
STA. 2520+00 IN PLACE-LT. FTG. RD.
36"X40" R.C. PIPE CULVERT
D.A. 28 AC. C=0.6
RETAIN



STA. 2520+00 IN PLACE-L.M.L.
36"X80" R.C. PIPE CULVERT
TYPE "H" DROP INLET IN MED.
4.5'X4.5'X"H"=5'-0" 2-OPENINGS
RETAIN

EXISTING OVERHEAD SIGN

WRSF & CONCRETE
DITCH PAVING



PC 2517+60.59

PI 2524+64.09

STA. 2520+00 IN PLACE-R.M.L.
36"X80" R.C. PIPE CULVERT
D.A. 28 AC. C=0.6
RETAIN

EXISTING OVERHEAD SIGN

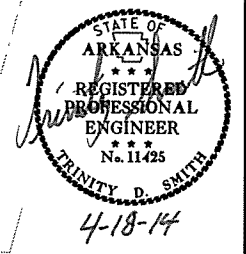
STA. 2520+00 IN PLACE-RT. FTG. RD.
36"X40" R.C. PIPE CULVERT
D.A. 28 AC. C=0.6
RETAIN

4/17/2014

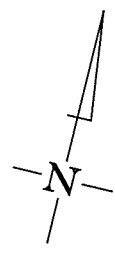
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO. BB0301	36 66

2 PLAN SHEETS



STA. 2539+00 IN PLACE-R.M.L.
TYPE "H" DROP INLET IN MEDIAN
4'X4'X"H"=3'-9" I-OPENING
CONST. 24"X84' R.C. PIPE OUTLET
RETAIN



REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIER
STA. 2543+67 C.L. OF I-30 = 2 EACH

STA. 2539+81
END WRSF

JEFFERSON AVE.



STA.	STA.	SIDE	GUARDRAIL (TYPE A)
2536+29.35	2548+29.35	R.M.L. - LT.	1150 LIN. FT.
2539+30.41	2551+30.41	L.M.L. - RT.	1150 LIN. FT.

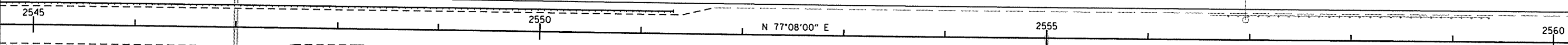
REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 2536+29.35 TO STA. 2539+79.35 LT. OF RT. LANES = 350 LIN. FT.
 STA. 2544+70.35 TO STA. 2548+20.13 LT. OF RT. LANES = 350 LIN. FT.
 STA. 2539+34.41 TO STA. 2542+84.41 RT. OF LT. LANES = 350 LIN. FT.
 STA. 2547+80.41 TO STA. 2551+30.41 RT. OF LT. LANES = 350 LIN. FT.

STA. 2543+31 - 2544+04
REMOVE CONCRETE DITCH PAVING AS
REQUIRED FOR GUARDRAIL POST
INSTALLATION AND CONCRETE PATCH
AROUND POST, PAYMENT INCLUDED
IN PRICE BID FOR GUARD RAIL
(TYPE A).

STA. 2547+00 IN PLACE
4'X3'X168' R.C. BOX CULVERT
D.A. 50 AC. C=0.6
TYPE "K" DROP INLET IN MED.
4'X4'X"H"=1'-6" 2-OPENINGS
RETAIN



EXISTING OVERHEAD SIGN



STA. 2547+70
BEGIN WRSF

STA. 2555+00 IN PLACE-R.M.L.
TYPE "H" DROP INLET IN MEDIAN
4'X4'X"H"=3'-9" I-OPENING
CONST. 24"X80' R.C. PIPE OUTLET
RETAIN

4/17/2014

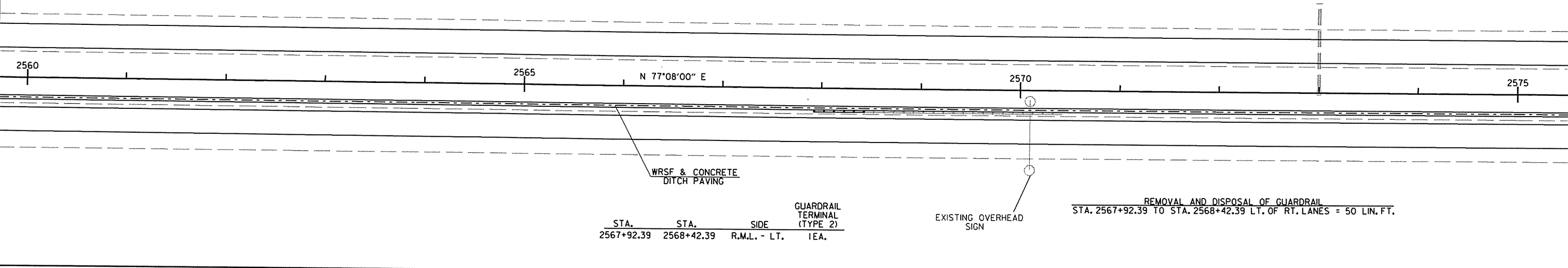
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	37	66

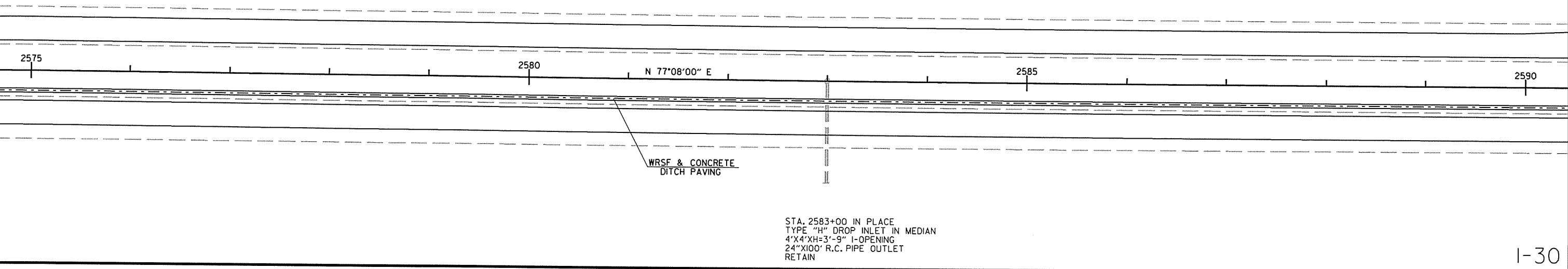
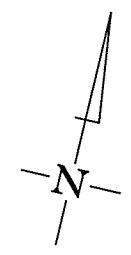
2 PLAN SHEETS



STA. 2573+00 IN PLACE-L.M.L.
 TYPE "H" DROP INLET IN MEDIAN
 4'X4'XH= 3'-9" I-OPENING
 24"X88' R.C. PIPE OUTLET
 RETAIN



STA. 2558+00 TO STA. 2590+00
 REMOVAL OF PERMANENT PAVEMENT MARKINGS
 8" WHITE - 208 LIN. FT.
 4" WHITE - 1600 LIN. FT.

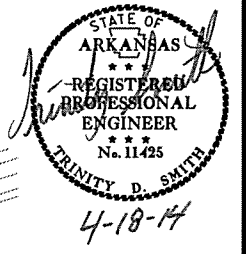


4/21/2014

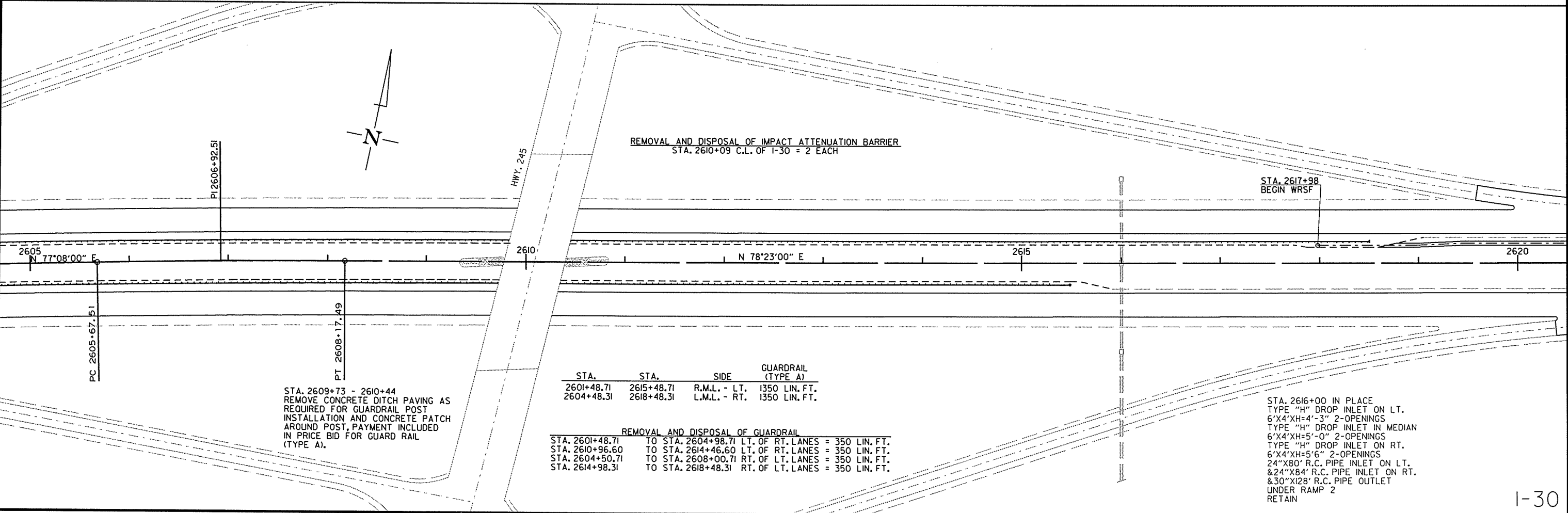
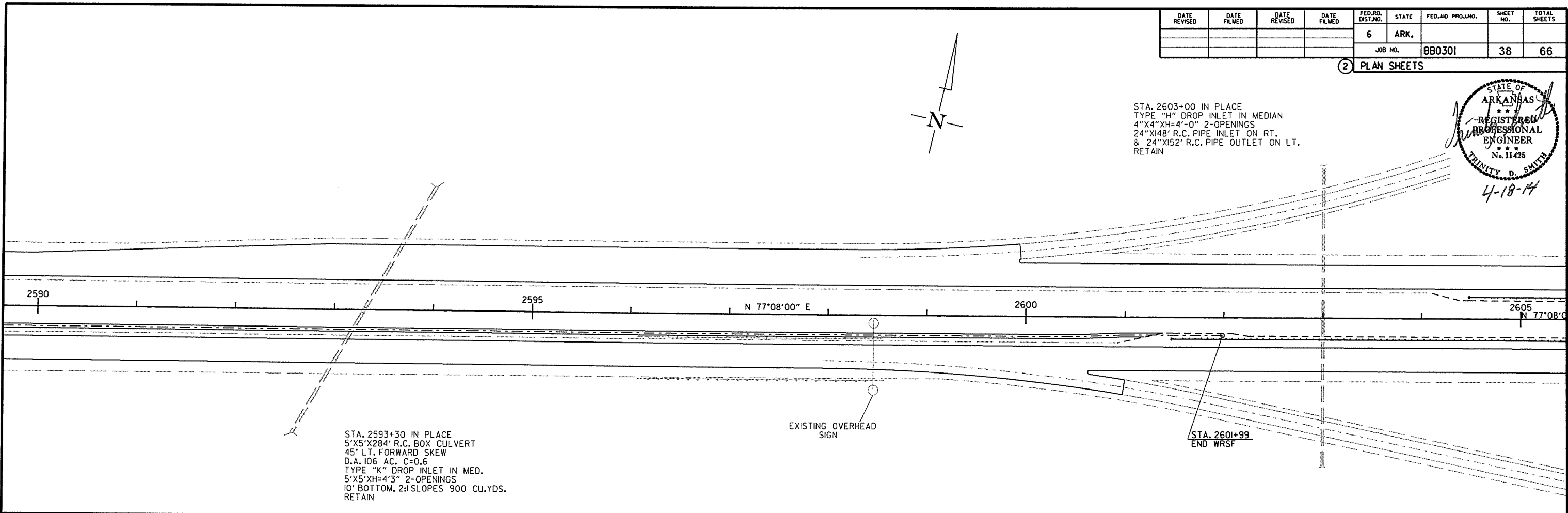
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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							38	66

2 PLAN SHEETS



STA. 2603+00 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4"X4"XH=4'-0" 2-OPENINGS
 24"X148' R.C. PIPE INLET ON RT.
 & 24"X152' R.C. PIPE OUTLET ON LT.
 RETAIN



4/17/2014
 RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	39	66

② PLAN SHEETS



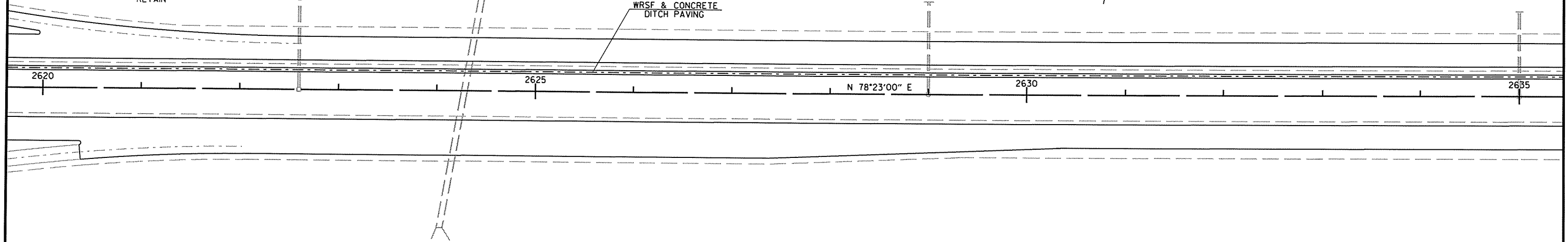
STA. 2624+25 IN PLACE
6'X8'X283' R.C. BOX CULVERT
10' LT. FORWARD SKEW
D.A. 365 AC. C=0.8
RETAIN

STA. 2629+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=4'-6" 2-OPENINGS
24"X92' R.C. PIPE CULVERT ON LT.
RETAIN

STA. 2622+60 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=4'-0" 2-OPENINGS
24"X92' R.C. PIPE CULVERT ON LT.
RETAIN

STA. 2635+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=4'-0" 1-OPENING
24"X84' R.C. PIPE CULVERT ON LT.
RETAIN

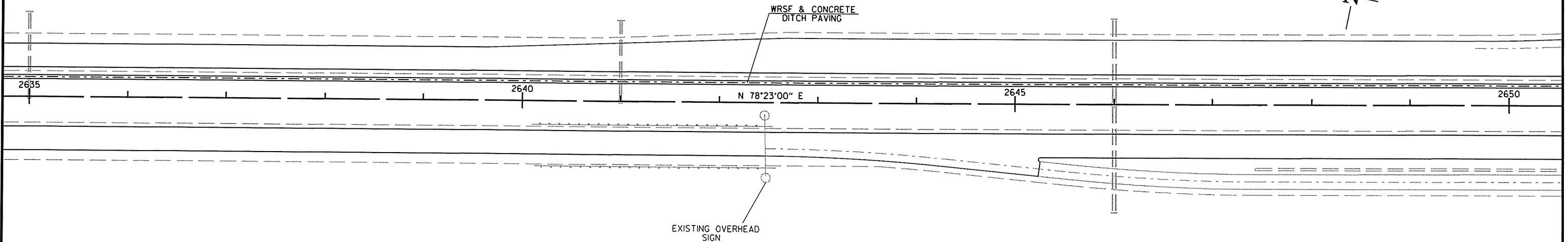
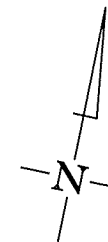
WRSF & CONCRETE
DITCH PAVING



STA. 2641+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=3'-9" 1-OPENING
24"X80' R.C. PIPE CULVERT ON LT.
RETAIN

STA. 2646+00 IN PLACE
42"X191' R.C. PIPE CULVERT
D.A. 26 AC. C=0.8
TYPE "H" DROP INLET IN MED.
5'X5'XH=6'-0" 1-OPENING
RETAIN

WRSF & CONCRETE
DITCH PAVING

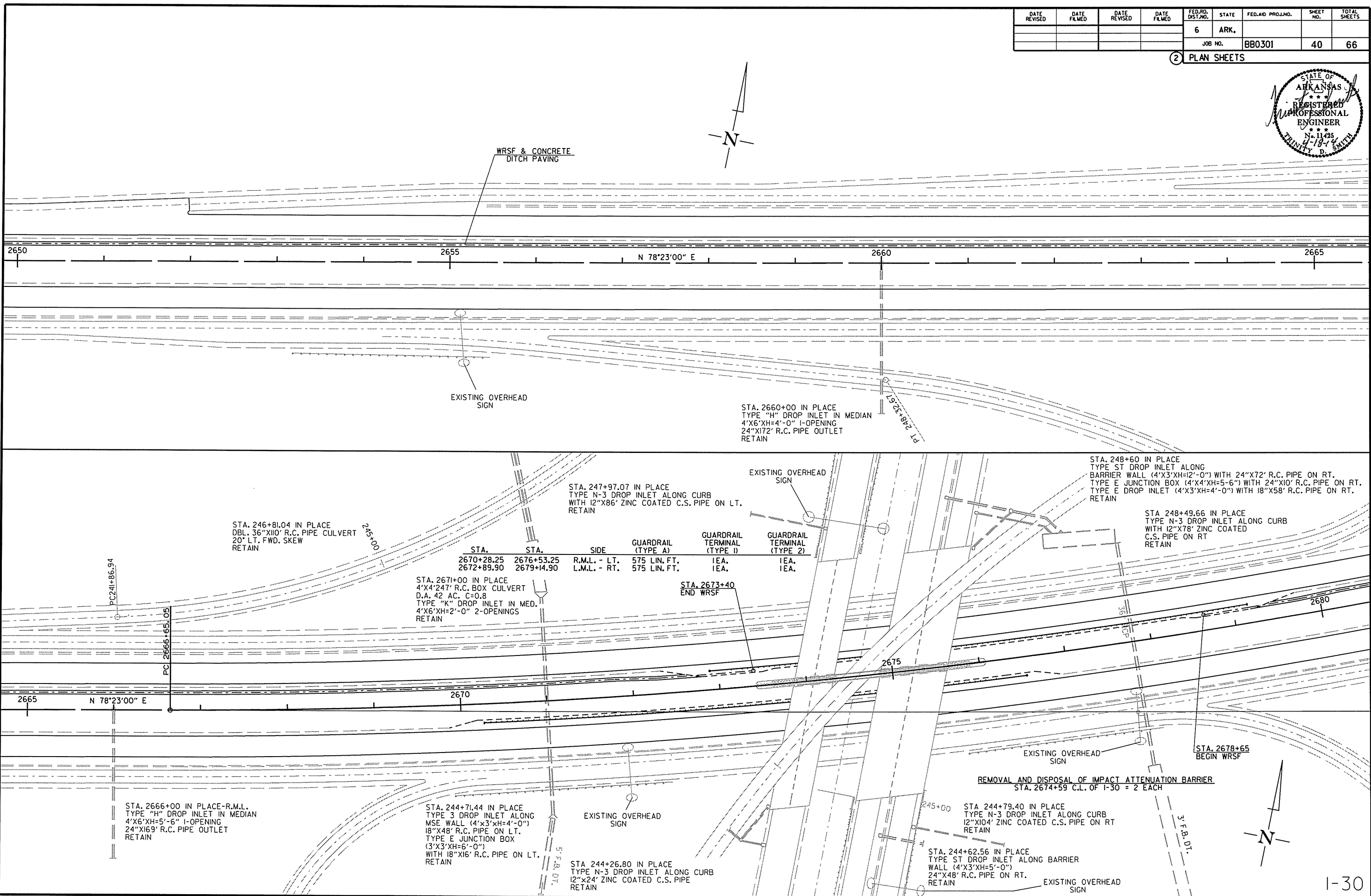


4/17/2014

RB00301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							40	66

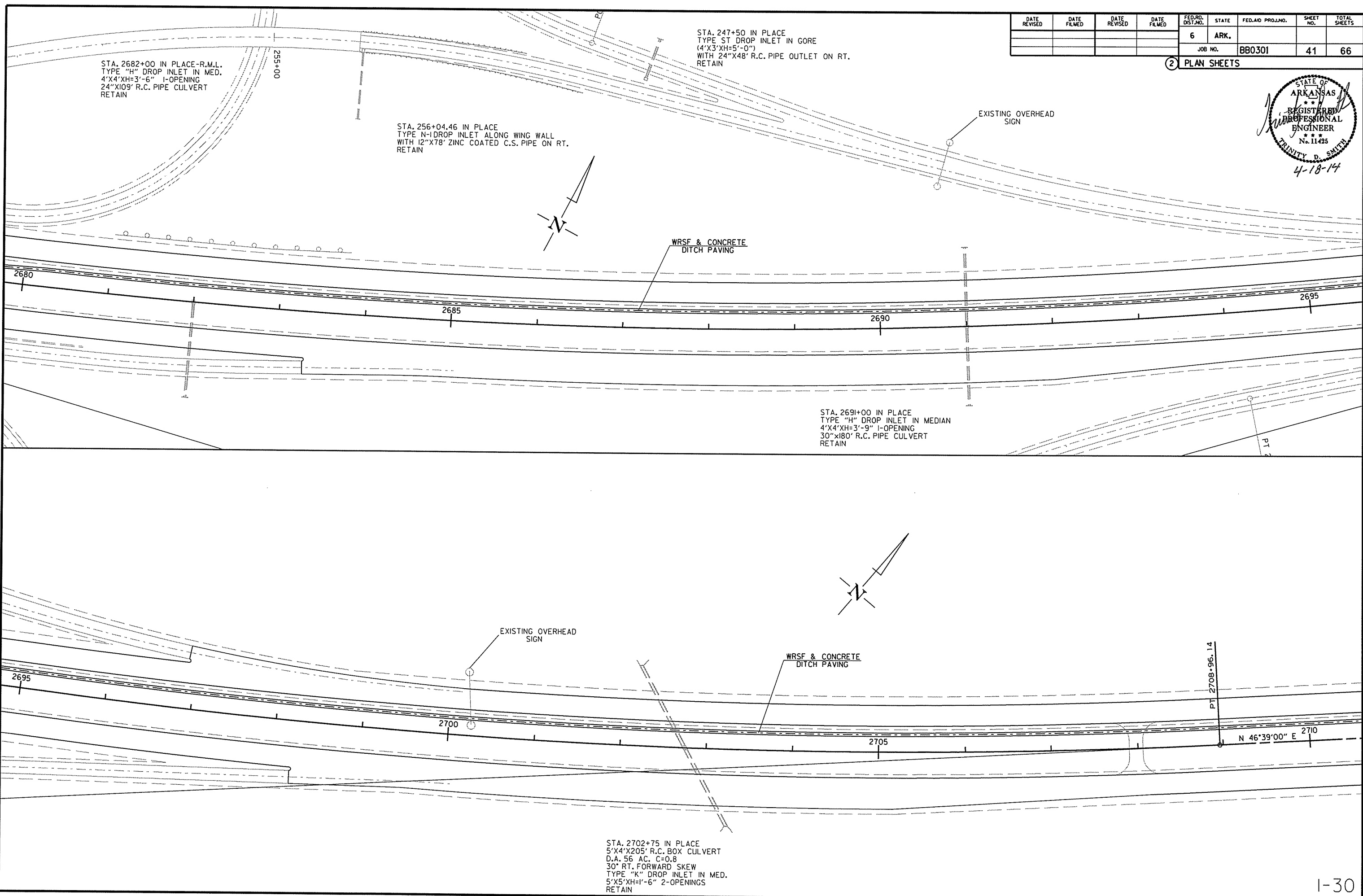
2 PLAN SHEETS



4/17/2014
RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	41	66

2 PLAN SHEETS



4/17/2014

RB0301.DCN

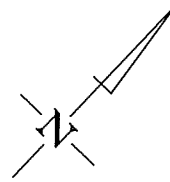
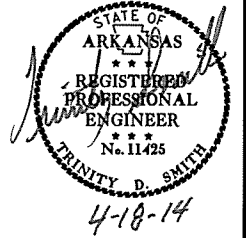
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							42	66

2 PLAN SHEETS

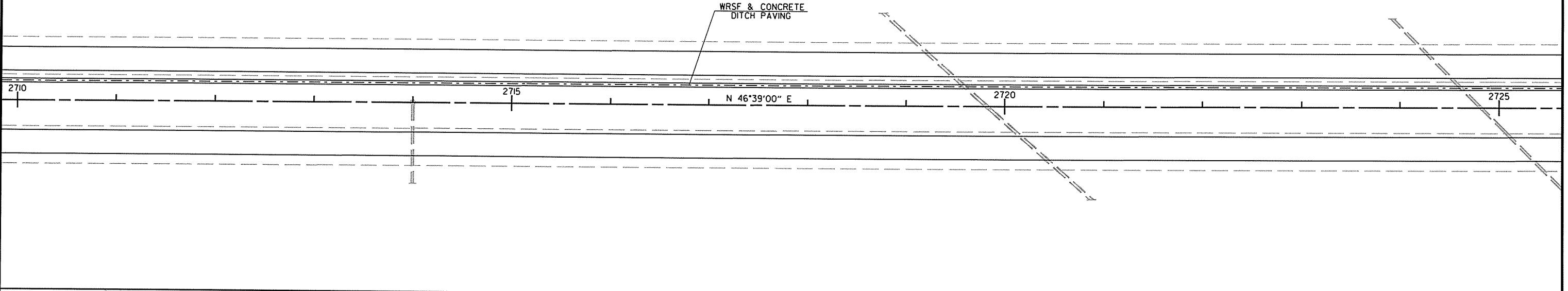
STA. 2714+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=4'-0" I-OPENING
24"X80' R.C. PIPE OUTLET IN R.M.L.
RETAIN

STA. 2719+82 IN PLACE
48"X276' R.C. PIPE CULVERT
D.A. 35 AC. C=0.8
45° RT. FORWARD SKEW
TYPE "H" DROP INLET IN MED.
7'X4'XH=7'-6" I-OPENING
RETAIN

STA. 2724+82 IN PLACE
36"X248' R.C. PIPE CULVERT
D.A. 15 AC. C=0.8
45° RT. FORWARD SKEW
TYPE "H" DROP INLET IN MED.
6'X4'XH=7'-0" 2-OPENINGS
RETAIN

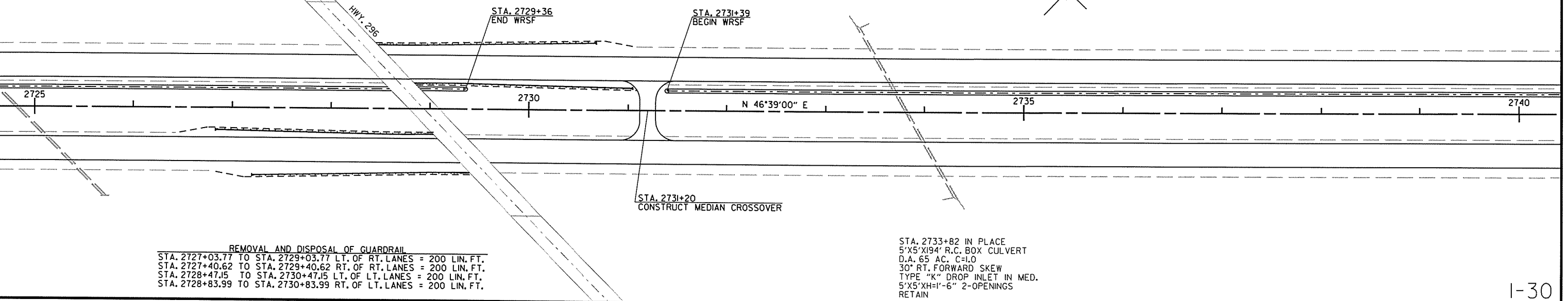
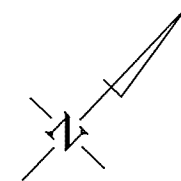


WRSF & CONCRETE
DITCH PAVING



STA.	STA.	SIDE	GUARDRAIL (TYPE A)	THREE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 2)
2726+85.02	2729+03.77	R.M.L. - LT.	150 LIN. FT.	1EA.	1EA.
2727+21.87	2729+40.62	R.M.L. - RT.	150 LIN. FT.	1EA.	1EA.
2728+47.15	2730+65.90	L.M.L. - LT.	150 LIN. FT.	1EA.	1EA.
2728+83.99	2731+02.74	L.M.L. - RT.	150 LIN. FT.	1EA.	1EA.

REFER TO TYPICAL LAYOUT OF GUARDRAIL
AT OVERPASSES SPECIAL DETAIL



REMOVAL AND DISPOSAL OF GUARDRAIL
STA. 2727+03.77 TO STA. 2729+03.77 LT. OF RT. LANES = 200 LIN. FT.
STA. 2727+40.62 TO STA. 2729+40.62 RT. OF RT. LANES = 200 LIN. FT.
STA. 2728+47.15 TO STA. 2730+47.15 LT. OF LT. LANES = 200 LIN. FT.
STA. 2728+83.99 TO STA. 2730+83.99 RT. OF LT. LANES = 200 LIN. FT.

STA. 2733+82 IN PLACE
5'X5'X194' R.C. BOX CULVERT
D.A. 65 AC. C=1.0
30° RT. FORWARD SKEW
TYPE "K" DROP INLET IN MED.
5'X5'XH=1'-6" 2-OPENINGS
RETAIN

4/17/2014

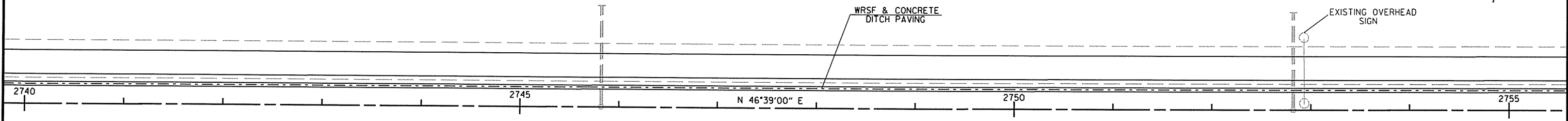
RB0301.DCN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							43	66

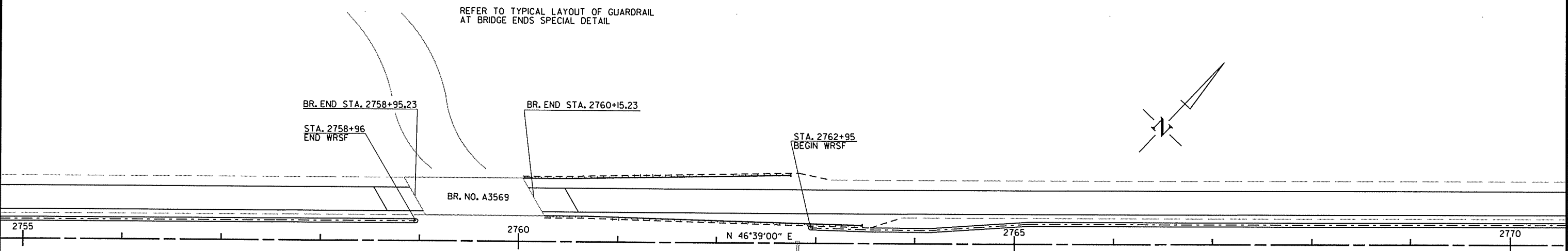
2 PLAN SHEETS

STA. 2745+82 IN PLACE-L.M.L.
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=4'-0" 2-OPENINGS
24"X100' R.C. PIPE OUTLET
RETAIN

STA 2752+82 IN PLACE-L.M.L.
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=5'-6" 2-OPENINGS
24"X96' R.C. PIPE OUTLET
RETAIN



REFER TO TYPICAL LAYOUT OF GUARDRAIL
AT BRIDGE ENDS SPECIAL DETAIL



STA.	STA.	SIDE	GUARDRAIL (TYPE A)	THREE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 2)
2756+17.26	2759+36.01	R.M.L. - LT.	250 LIN. FT.	IEA.	IEA.
2756+89.20	2759+57.95	R.M.L. - RT.	200 LIN. FT.	IEA.	IEA.
2760+04.26	2762+73.01	L.M.L. - LT.	200 LIN. FT.	IEA.	IEA.
2760+26.20	2763+44.95	L.M.L. - RT.	250 LIN. FT.	IEA.	IEA.

REMOVAL AND DISPOSAL OF GUARDRAIL

STA. 2757+36.01 TO STA. 2759+36.01 LT. OF RT. LANES = 200 LIN. FT.
STA. 2757+57.95 TO STA. 2759+57.95 RT. OF RT. LANES = 200 LIN. FT.
STA. 2760+04.26 TO STA. 2762+04.26 LT. OF LT. LANES = 200 LIN. FT.
STA. 2760+26.20 TO STA. 2762+26.20 RT. OF LT. LANES = 200 LIN. FT.

STA. 2762+82 IN PLACE-R.M.L.
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=4'-0" 2-OPENINGS
24"X84' R.C. PIPE OUTLET
RETAIN

MCKINNEY BAYOU

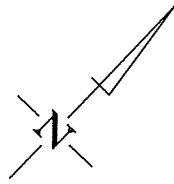
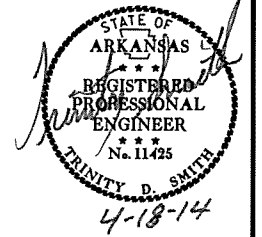
4/17/2014

RB0301.DGN

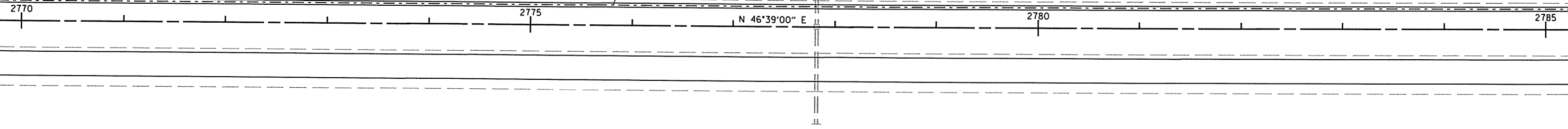
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	44	66

② PLAN SHEETS

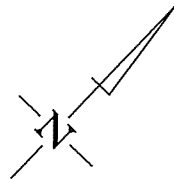
STA. 2777+82 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 5'X5'XH=7'-6" 2-OPENINGS
 36"X192' R.C. PIPE
 RETAIN



WRSF & CONCRETE
 DITCH PAVING



REFER TO TYPICAL LAYOUT OF GUARDRAIL
 AT BRIDGE ENDS SPECIAL DETAIL

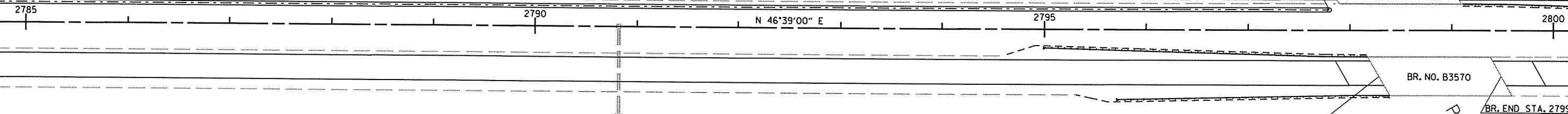


BR. END STA. 2797+78.65

BR. END 2798+98.65

STA. 2797+80
 END WRSF

BR. NO. A3570



BR. NO. B3570

BR. END STA. 2798+28.37

BR. END STA. 2799+48.37

STA. 2790+82 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4'X4'XH+3'-9" 2-OPENINGS
 24'X84' R.C. PIPE OUTLET
 RETAIN

STA.	STA.	SIDE	GUARDRAIL (TYPE A)	THREE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 2)
2794+98.59	2798+17.34	R.M.L. - LT.	250 LIN. FT.	IEA.	IEA.
2795+70.59	2798+39.34	R.M.L. - RT.	200 LIN. FT.	IEA.	IEA.
2798+87.90	2801+56.65	L.M.L. - LT.	200 LIN. FT.	IEA.	IEA.
2799+09.39	2802+28.14	L.M.L. - RT.	250 LIN. FT.	IEA.	IEA.

REMOVAL AND DISPOSAL OF GUARDRAIL

STA. 2796+17.34 TO STA. 2798+17.34 LT. OF RT. LANES = 200 LIN. FT.
 STA. 2796+39.34 TO STA. 2798+39.34 RT. OF RT. LANES = 200 LIN. FT.
 STA. 2798+87.90 TO STA. 2800+87.90 LT. OF LT. LANES = 200 LIN. FT.
 STA. 2799+09.39 TO STA. 2801+09.39 RT. OF LT. LANES = 200 LIN. FT.

PAUPS SPUR BAYOU

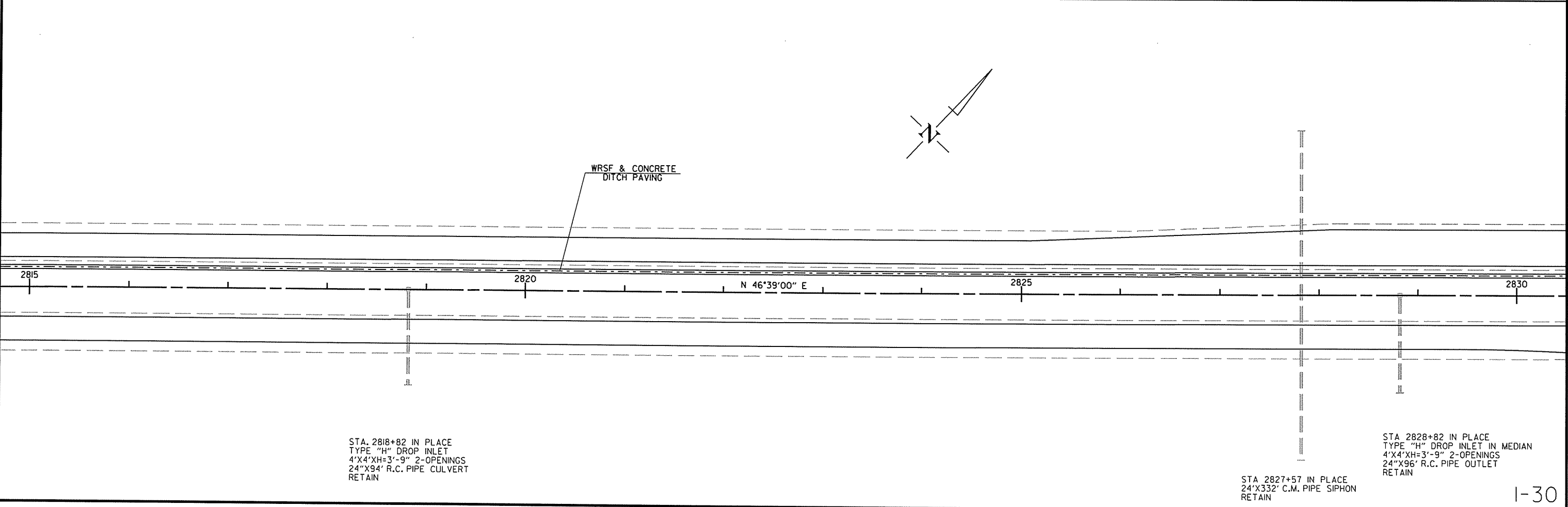
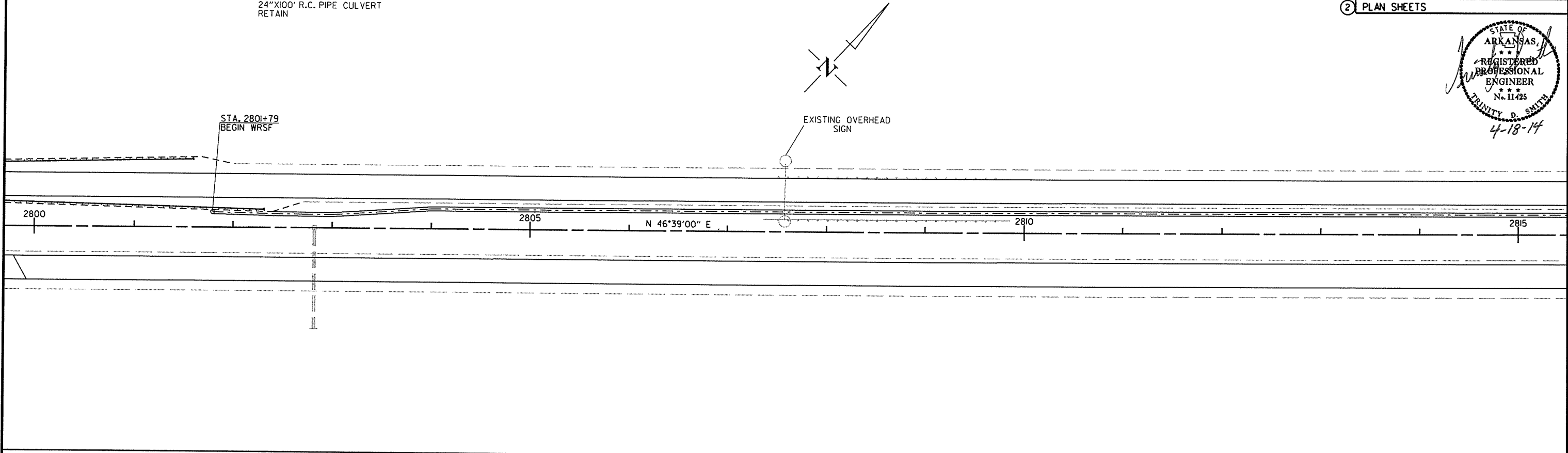
4/17/2014

RB80301.DGN

STA. 2802+82 IN PLACE
 TYPE "H" DROP INLET ON MEDIAN
 4'X4'XH=5'-0" 2-OPENINGS
 24"X100' R.C. PIPE CULVERT
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0301	45
						66		

② PLAN SHEETS



STA. 2818+82 IN PLACE
 TYPE "H" DROP INLET
 4'X4'XH=3'-9" 2-OPENINGS
 24"X94' R.C. PIPE CULVERT
 RETAIN

STA 2828+82 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4'X4'XH=3'-9" 2-OPENINGS
 24"X96' R.C. PIPE OUTLET
 RETAIN

STA 2827+57 IN PLACE
 24'X332' C.M. PIPE SIPHON
 RETAIN

4/17/2014

RB0301.DCN

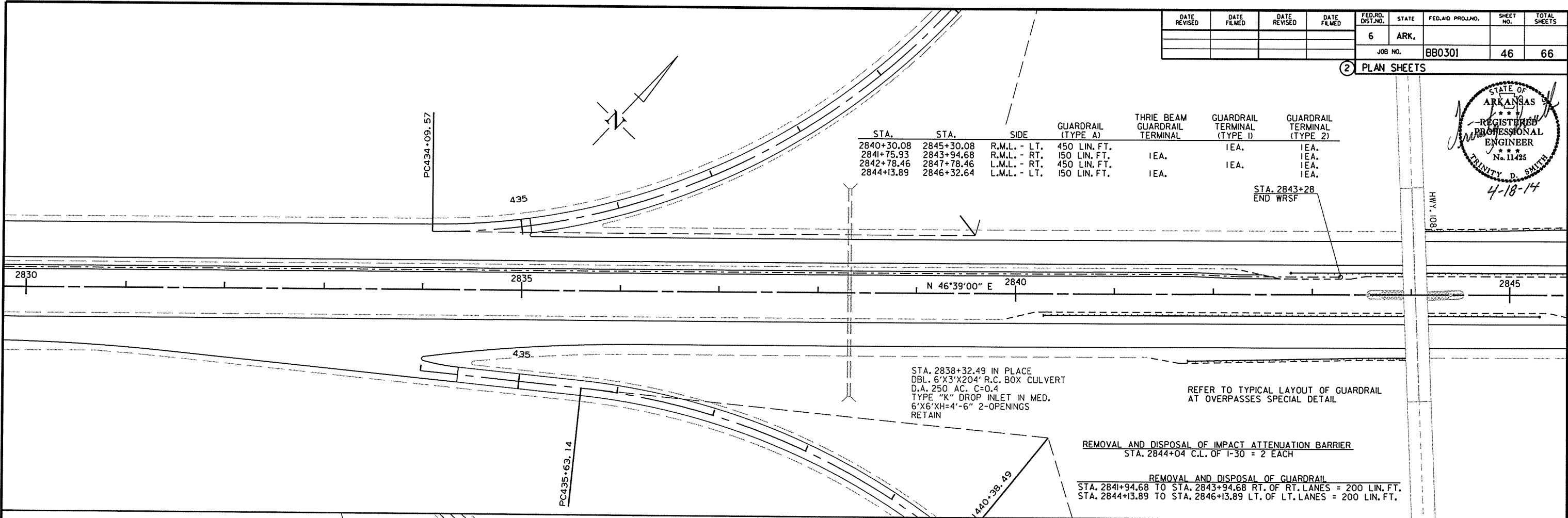
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	46	66

2 PLAN SHEETS



STA.	STA.	SIDE	GUARDRAIL (TYPE A)	THRIE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 1)	GUARDRAIL TERMINAL (TYPE 2)
2840+30.08	2845+30.08	R.M.L. - LT.	450 LIN. FT.		IEA.	IEA.
2841+75.93	2843+94.68	R.M.L. - RT.	150 LIN. FT.	IEA.	IEA.	IEA.
2842+78.46	2847+78.46	L.M.L. - RT.	450 LIN. FT.		IEA.	IEA.
2844+13.89	2846+32.64	L.M.L. - LT.	150 LIN. FT.	IEA.	IEA.	IEA.

STA. 2843+28
END WRSF

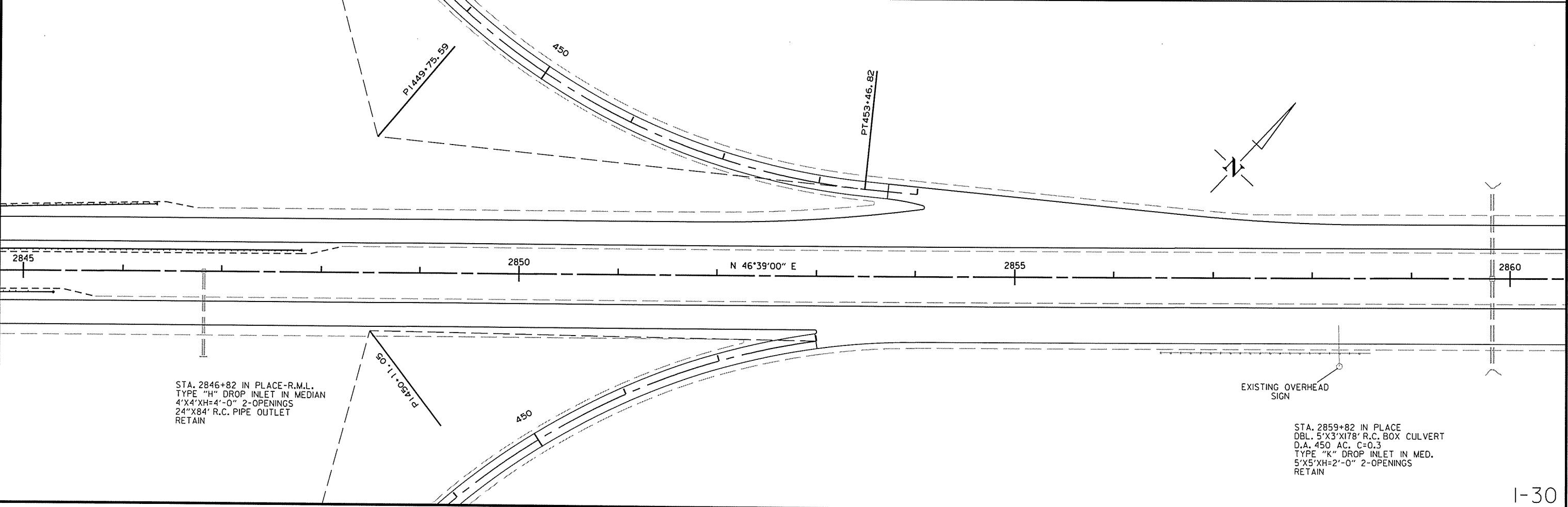


STA. 2838+32.49 IN PLACE
DBL. 6'X3'X204' R.C. BOX CULVERT
D.A. 250 AC. C=0.4
TYPE "K" DROP INLET IN MED.
6'X6'XH=4'-6" 2-OPENINGS
RETAIN

REFER TO TYPICAL LAYOUT OF GUARDRAIL
AT OVERPASSES SPECIAL DETAIL

REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIER
STA. 2844+04 C.L. OF I-30 = 2 EACH

REMOVAL AND DISPOSAL OF GUARDRAIL
STA. 2841+94.68 TO STA. 2843+94.68 RT. OF RT. LANES = 200 LIN. FT.
STA. 2844+13.89 TO STA. 2846+13.89 LT. OF LT. LANES = 200 LIN. FT.



STA. 2846+82 IN PLACE-R.M.L.
TYPE "H" DROP INLET IN MEDIAN
4'X4'XH=4'-0" 2-OPENINGS
24"X84' R.C. PIPE OUTLET
RETAIN

EXISTING OVERHEAD
SIGN

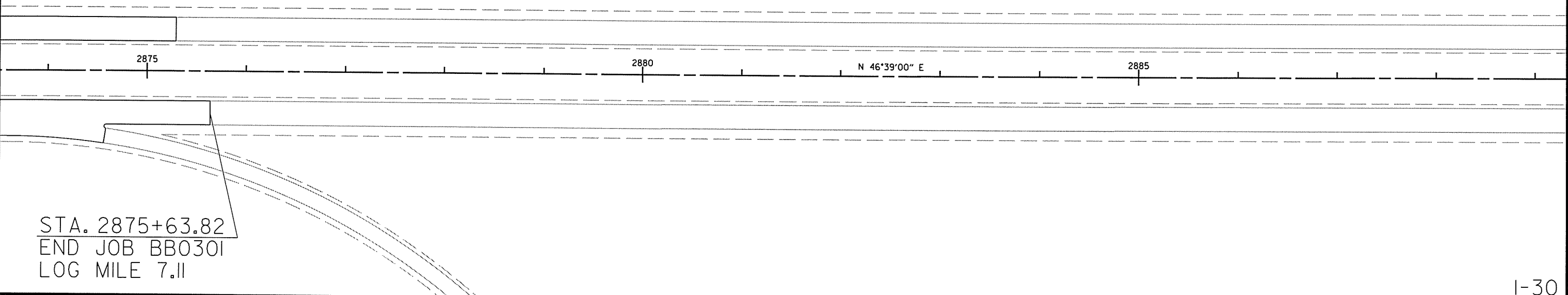
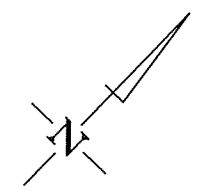
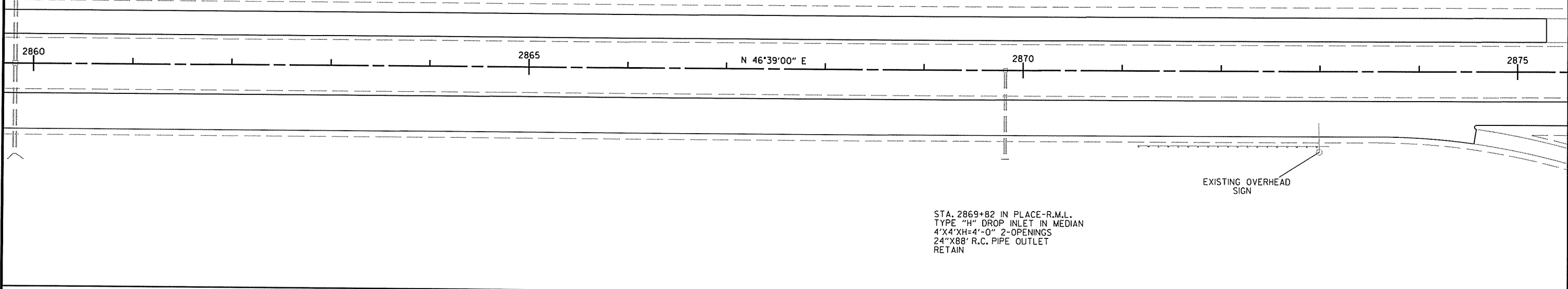
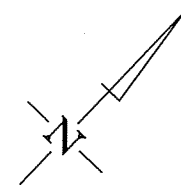
STA. 2859+82 IN PLACE
DBL. 5'X3'X178' R.C. BOX CULVERT
D.A. 450 AC. C=0.3
TYPE "K" DROP INLET IN MED.
5'X5'XH=2'-0" 2-OPENINGS
RETAIN

4/17/2014

RB0301.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0301	47	66

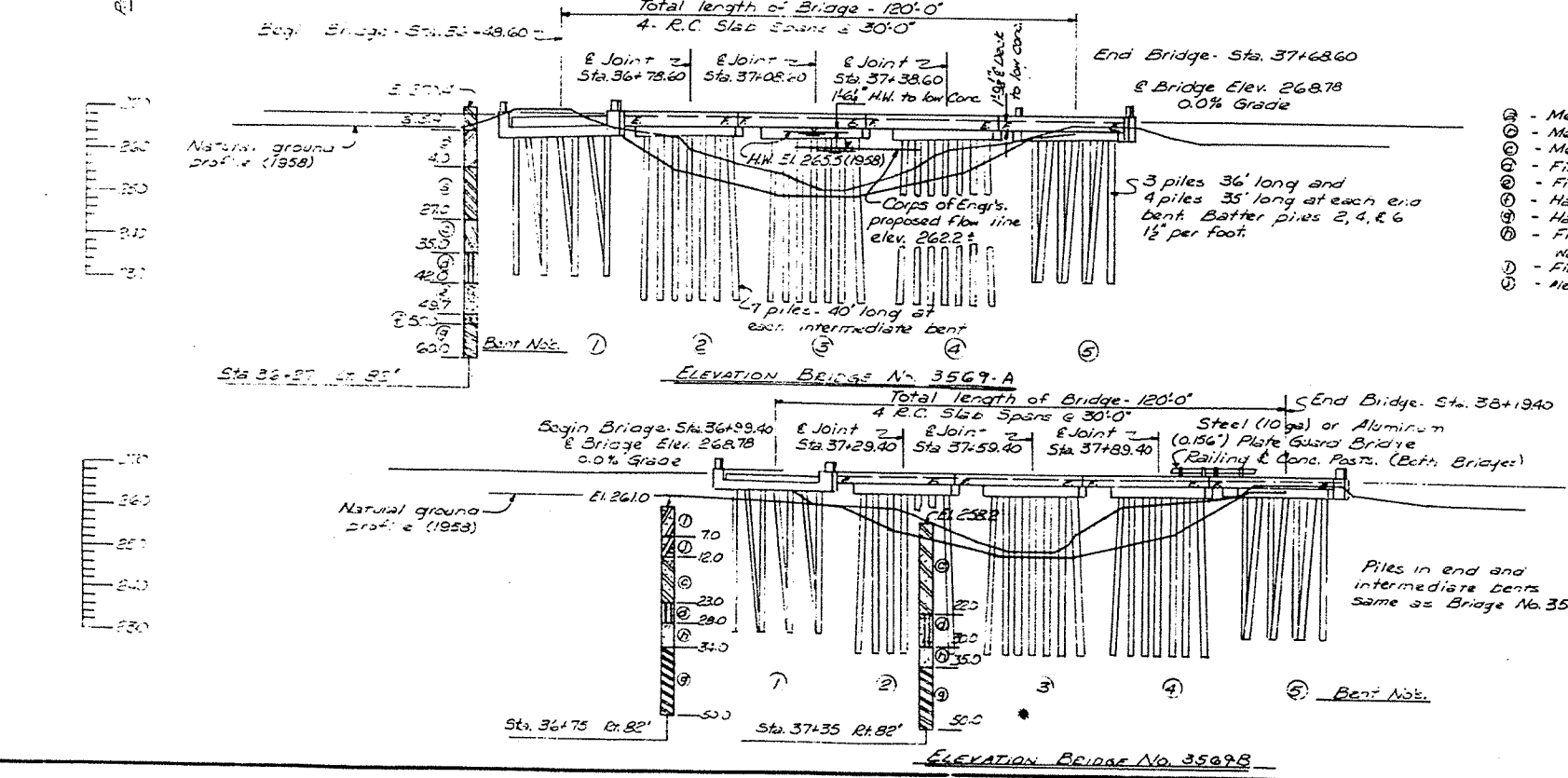
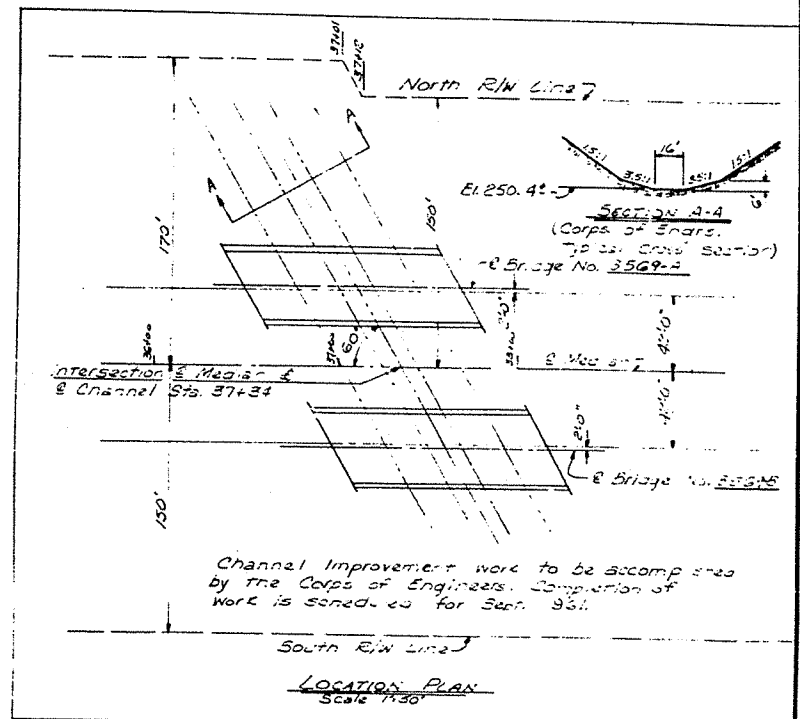
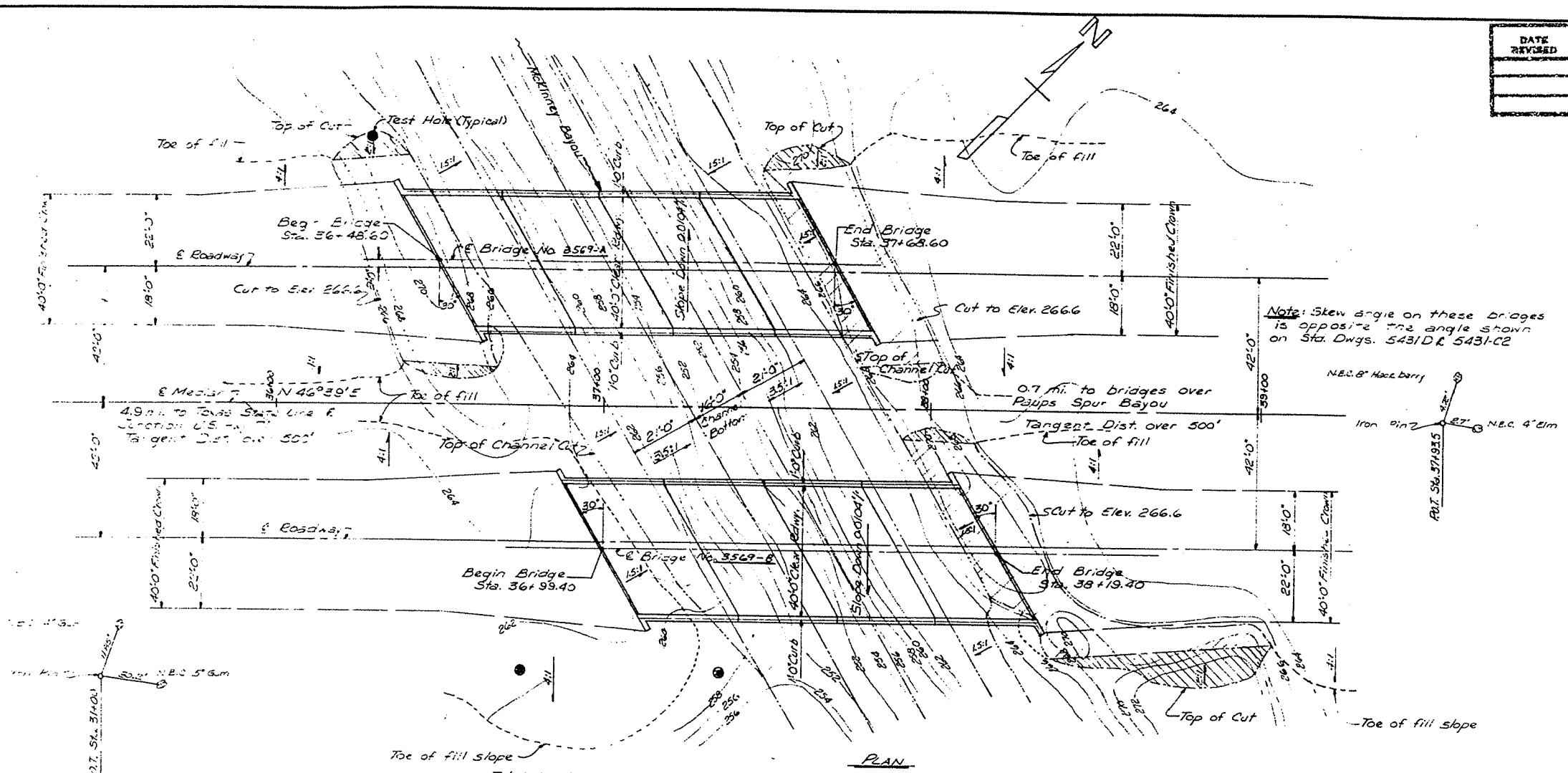
② PLAN SHEETS



4/17/2014

RB0301.DCN

DATE REVISED	DATE FILLED	DATE REVISED	DATE FILLED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0301							48	60
A&B3569 LAYOUT							55535	



- BORING LEGEND**
- ⊙ - Med. firm red clay - Moist
 - ⊙ - Med. soft red clay - Wet
 - ⊙ - Med. soft brown sandy clay - Wet
 - ⊙ - Fine brown silty sand - Med. Comp.
 - ⊙ - Fine gray water bearing sand - Comp.
 - ⊙ - Hard blue sandy clay & gravel
 - ⊙ - Hard blue clay
 - ⊙ - Fine brown sand, some silt, water bearing - Comp.
 - ⊙ - Firm brown sandy clay - Dry
 - ⊙ - Med. firm brown sandy clay - Moist

Drainage Area: McKinney Bayou and Paups Spur Bayou have a combined drainage area of 51 square miles with 0.5. See Dwg. No. 1:633 for layout of bridges over Paups Spur Bayou.

GENERAL NOTES

Bench Mark - nail in 8" Ash root 65'ft. of Station 39+00. Elevation 267.26.

All piling to be 16" octagonal precast concrete piles driven to a minimum bearing capacity of 35 tons. Lengths of piling shown are for estimating purposes only; actual lengths to be determined in the field. Drive one 45' test pile at Bents 2 and 4 of each bridge.

Piling in end bents are to be driven after embankment is in place.

For details of superstructure see Drawing No. 5431D.

For details of substructure see Drawing No. 5431C2.

For details of precast concrete piling see Drawing No. 2382.

Loading: H20-S16 AASHO 1957 and Special Interstate Loading; two 24,000 lb. axles at 4' centers.

Stresses: Class 5 Concrete (n=10) 1,200 psi
Reinforcing Steel 20,000 psi

SPECIFICATIONS: Arkansas State Highway Commission Standard Specifications for Highway Construction, Edition of 1959.

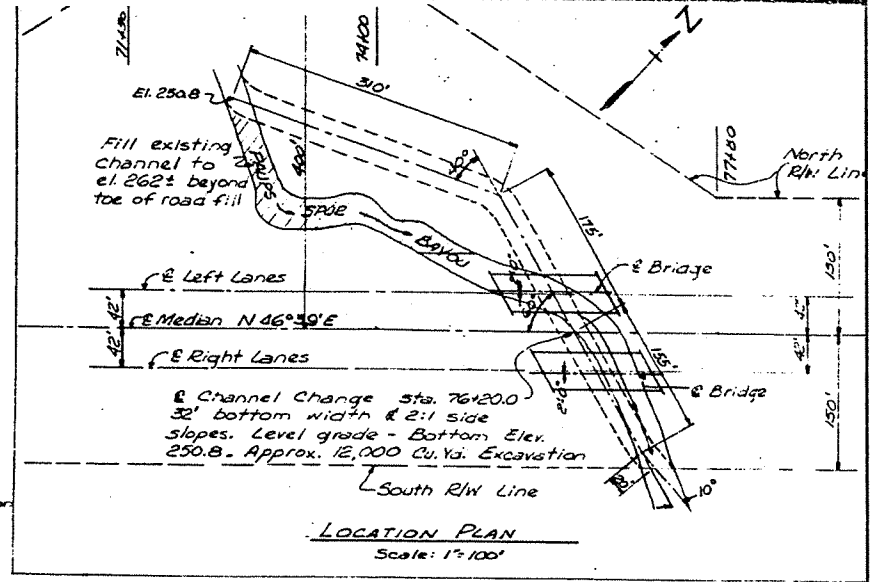
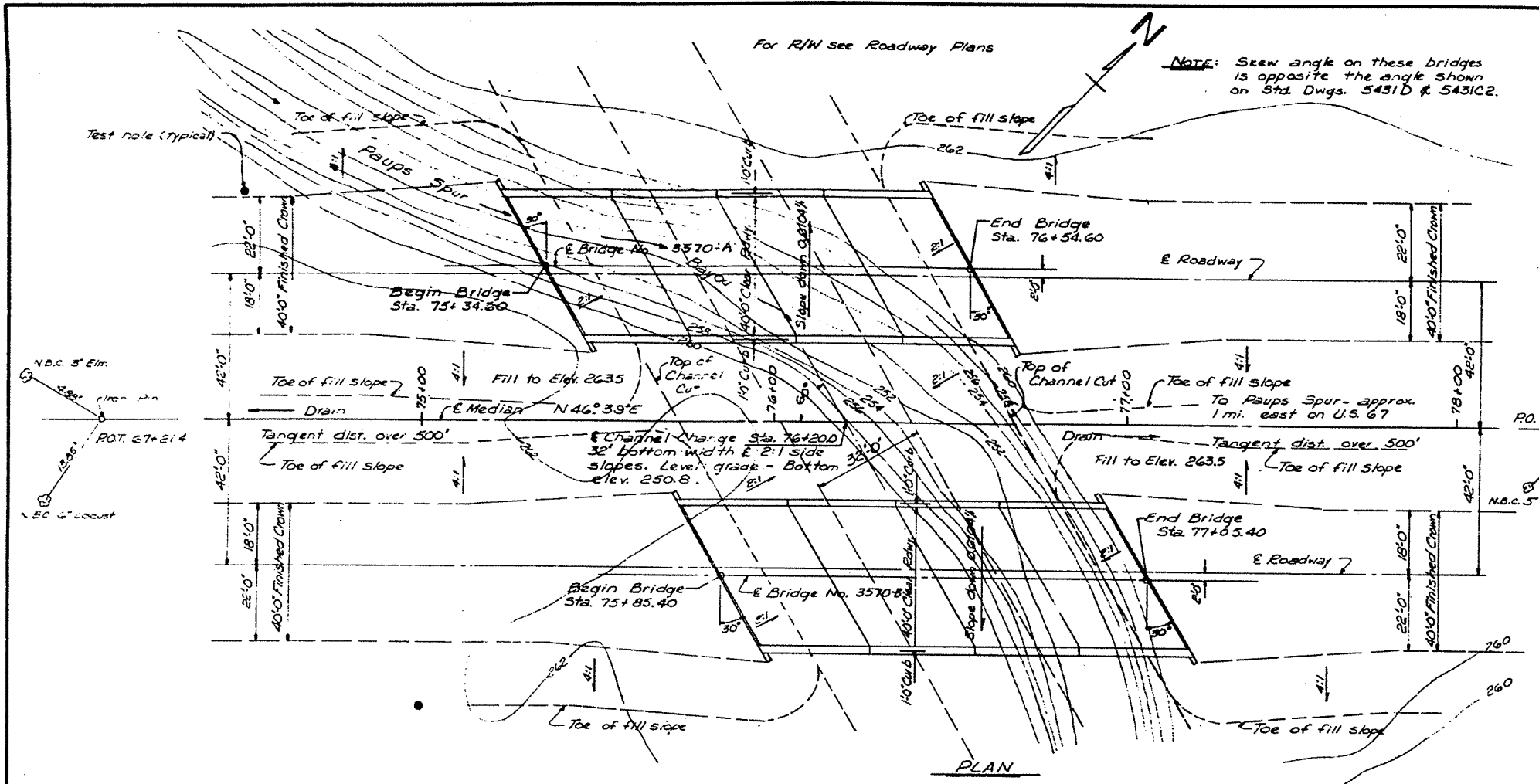
FOR INFORMATION ONLY

LAYOUT OF BRIDGES
OVER MCKINNEY BAYOU
TEXAS STATE LINE-HOMAN
MILLER COUNTY
INT. ROUTE 30 SEC. 1
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: C.E.V. DATE: 7-25-51
TRACED BY: DATE: 7-27-51
CHECKED BY: E.R.A. DATE: 7-27-51

BRIDGE NO. A&B3569 DRAWING NO. 55535

DATE REVISED	DATE FILLED	DATE REVISED	DATE FILLED	FED. ROAD DIST. NO.	STATION	PRO. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. A&B3570							49	66
LAYOUT							55536	



GENERAL NOTES

Bench Mark - Nail in root of 36" Pecan tree 340' right Station 73+12. Elevation 260.37.

All piling to be 16" octagonal precast concrete piles driven to a minimum bearing of 35 tons per pile. Lengths of piling shown are assumed for estimating quantities only. Actual lengths to be determined in the field. Drive one 40" test pile in Bent No. 2 and Bent No. 4 of each bridge. Piles in end bents are to be driven after embankment is in place.

For details of superstructure see Drawing No. 5431D.

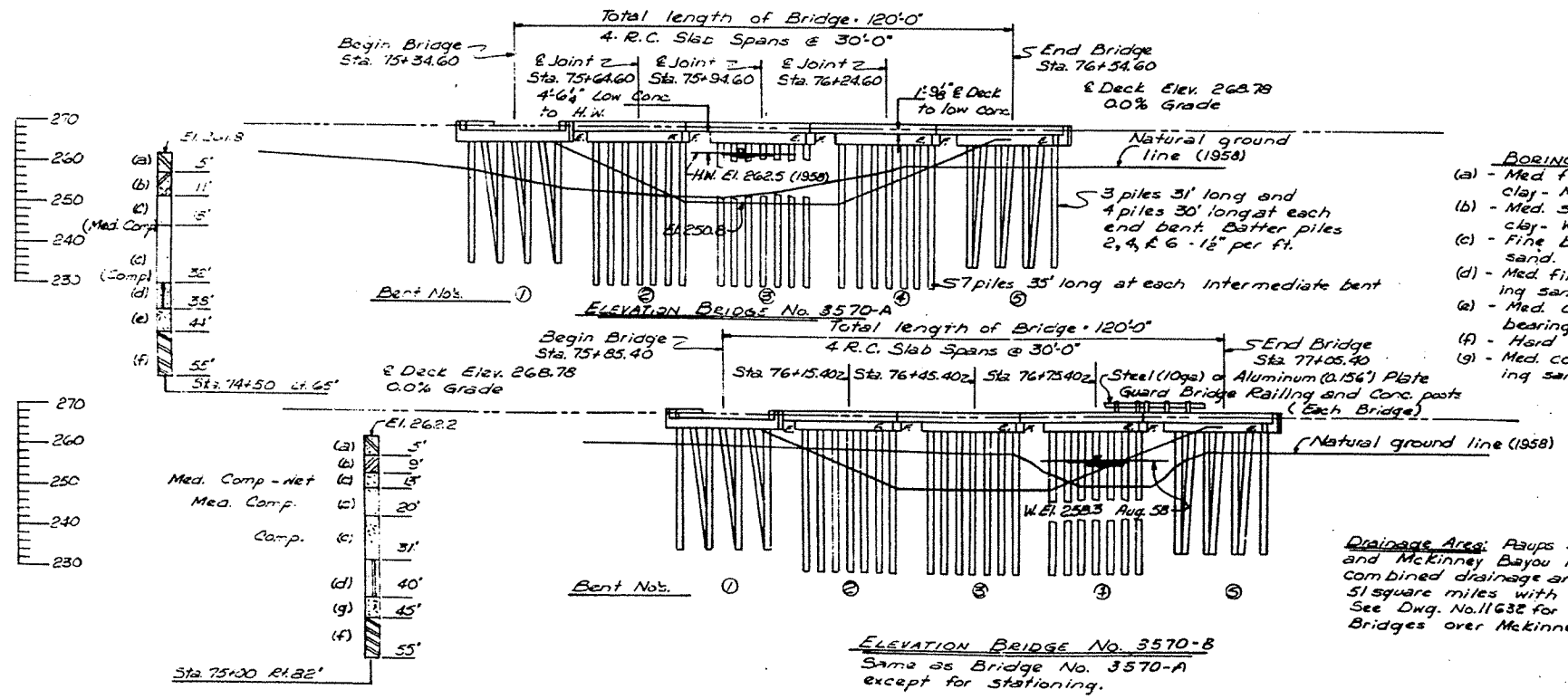
For details of substructure see Drawing No. 5431C2.

For details of precast piles see Drawing No. 2382.

Loading: H20-S16 AASHTO - 1957 and Special Interstate Loading of two 24,000 lb. axles 4' on centers.

Unit Stresses:
Class 3 Concrete (nw10) 1,200 psi
Reinforcing Steel 20,000 psi

SPECIFICATIONS: Arkansas State Highway Commission Standard Specifications for Highway Construction, Edition of 1959.



FOR INFORMATION ONLY

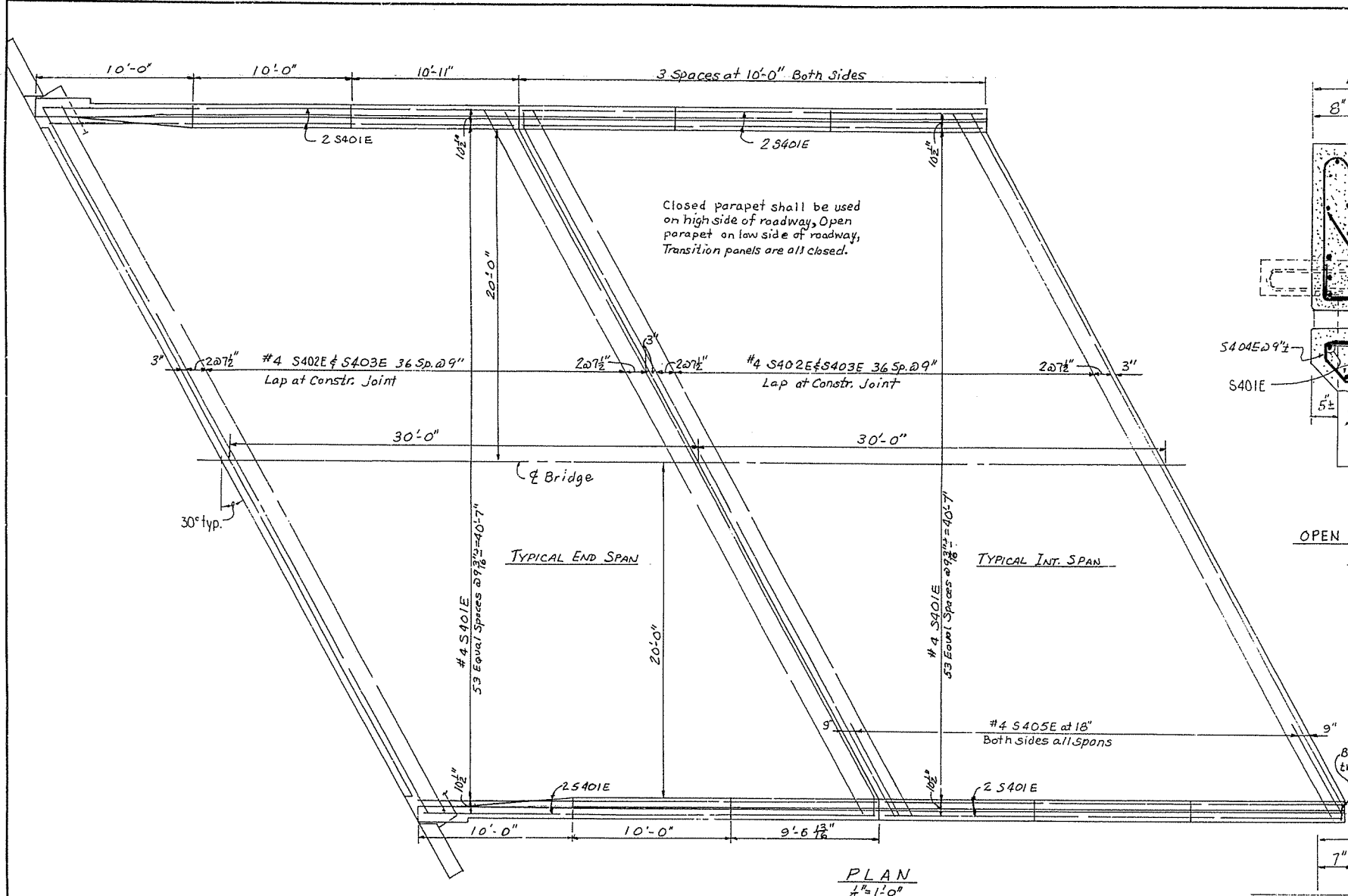
LAYOUT OF BRIDGES OVER
PAUPS SPUR BAYOU
TEXAS STATE LINE-HOMAN
MILLER COUNTY
INT. ROUTE 30 SEC. I
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: C.E.V. DATE: 7-25-61
TRACED BY: DATE: SCALE: 1" = 20'
CHECKED BY: ERB DATE: 7-27-61

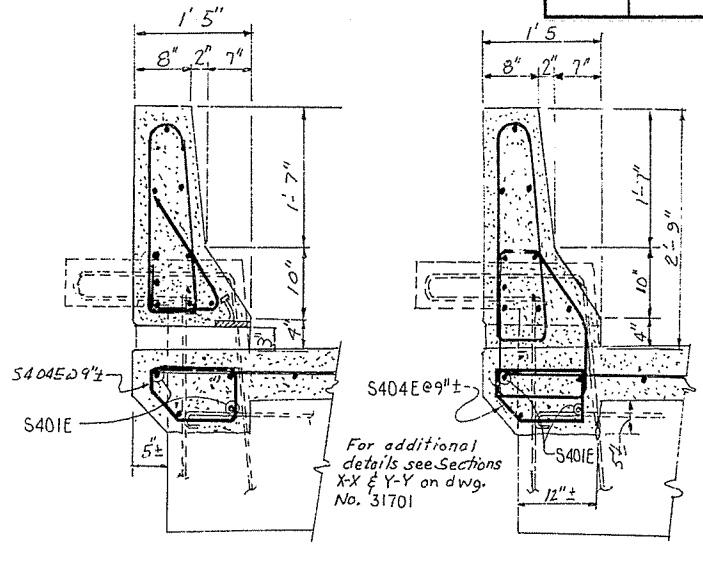
BRIDGE NO. A&B3570 DRAWING NO. 55536

BRIDGE ENGINEER

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0301	SG	lab	
				A&B3569, A&B3570	R.C. SLAB SPAN		55537	

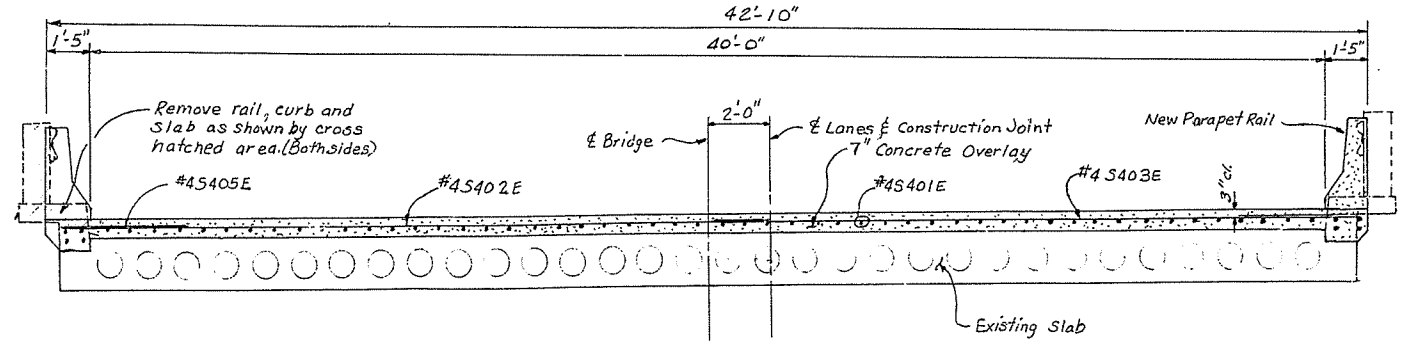


PLAN
1/4" = 1'-0"



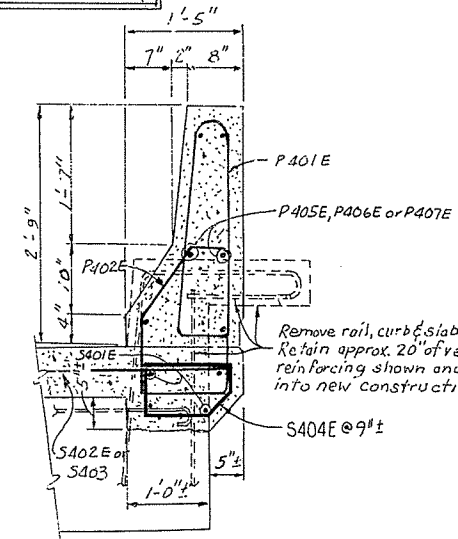
OPEN PARAPET SECTION
AT DRAIN
1" = 1'-0"

OPEN PARAPET SECTION
AT POST
1" = 1'-0"



ROADWAY SECTION
3/8" = 1'-0"

Before the concrete overlay is poured, the bridge deck shall be cleaned by sand blasting or water blasting to remove all dust, dirt, oil, grease, tar, loose concrete or other foreign material. Any joint material extending above the roadway surface shall be removed. Immediately before the concrete is poured the existing concrete shall be wetted.



CLOSED PARAPET SECTION
1" = 1'-0"

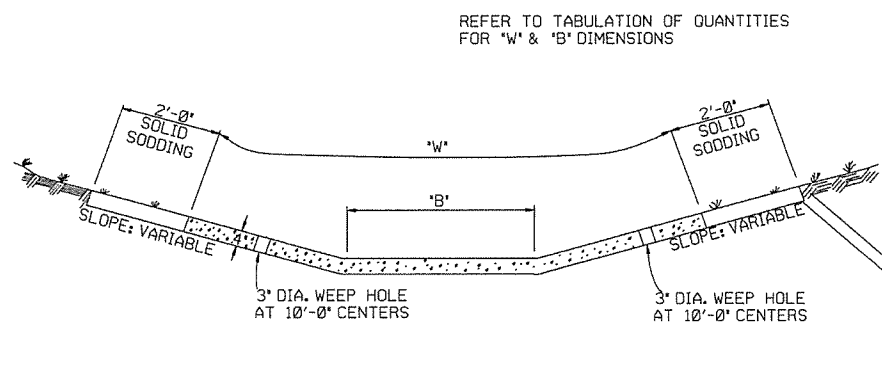
FOR INFORMATION ONLY
DETAILS OF MODIFICATIONS
30' R.C. SLAB SPANS

ROUTE 30 SEC. II
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

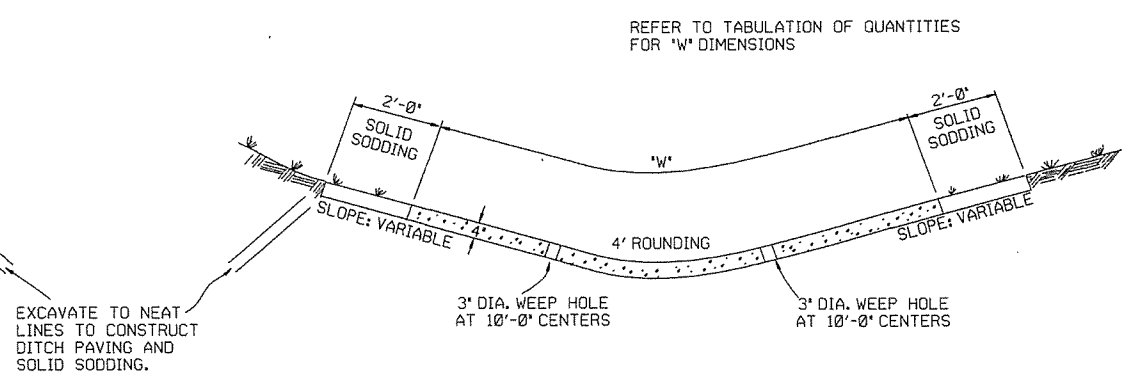
DRAWN BY: FMH DATE: 9-11-90
CHECKED BY: DFL DATE: 10-1-90 SCALE: AS NOTED
DESIGNED BY: DATE: _____

Michael P. ...
BRIDGE ENGINEER

BRIDGE NO. A&B3569
DRAWING NO. 55537

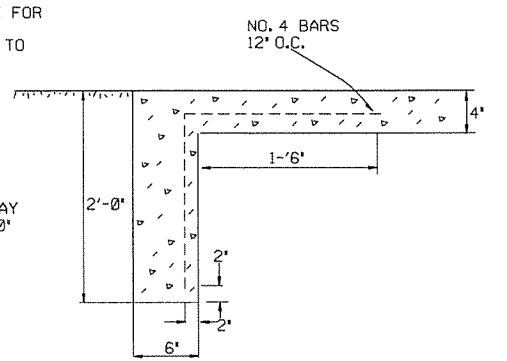


TYPE A



TYPE B

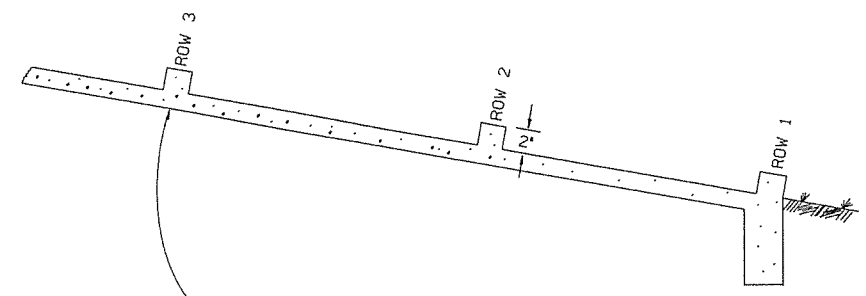
THE STEEL AND ADDITIONAL CONCRETE FOR THE WALLS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR 'CONCRETE DITCH PAVING.'



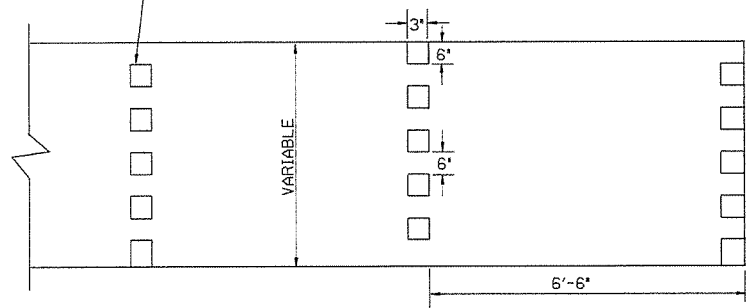
TOE WALL DETAIL FOR CONCRETE DITCH PAVING

GENERAL NOTES:

- THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.
- TOE WALLS TO BE CONSTRUCTED FULL WIDTH AT EACH END OF DITCH PAVING, AND POURED MONOLITHICALLY.
- SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.
- 1' WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.



ENERGY DISSIPATORS TO BE USED FOR THE ENTIRE LENGTH OF DITCH WHEN SLOPE OF DITCH PAVING EXCEEDS 7%. THE DISSIPATORS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE UNINCLUDED IN THE PRICE BID FOR CONCRETE DITCH PAVING.



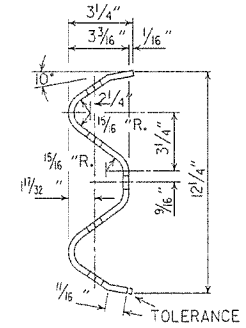
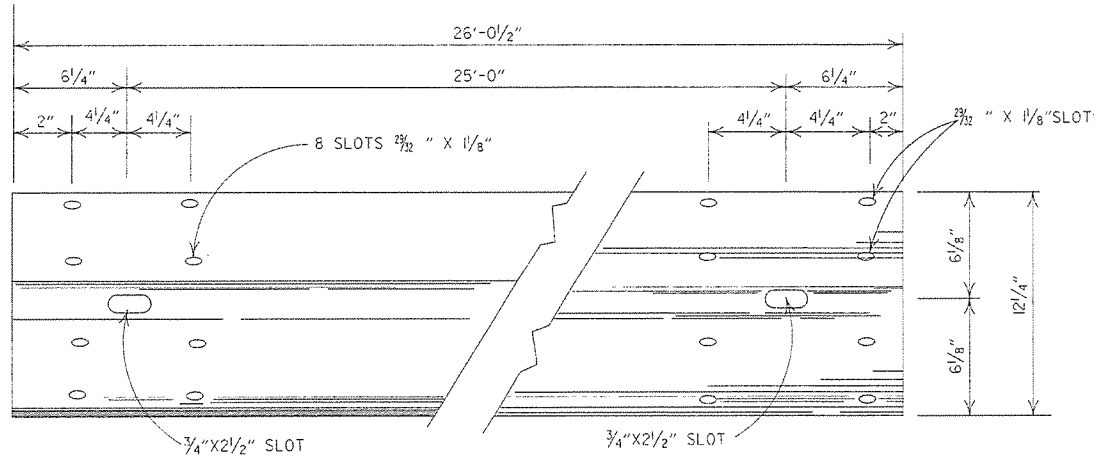
ENERGY DISSIPATORS
(NO SCALE)

DATE	REVISION	DATE FILED
11-17-10	ADDED GENERAL NOTE	
6-2-94	ADDED GENERAL NOTE ABOUT SOLID SODDING	
11-30-8	ELIMINATED MIN. ROWS OF ELEMENTS	111-30-89
7-15-88	REVISED DISSIPATOR NOTE	653-7-15-88
4-3-87	REVISED ENERGY DISSIPATOR	671-4-3-87
1-9-87	MODIFIED NOTE ON ENERGY DISS.	532-1-9-87
11-3-86	ADDED NOTE TO ENERGY DISS.	599-12-1-86
11-1-84	ENERGY DISSIPATOR DETAILS	508-11-1-84
11-1-84	ADDED	
11-1-84	EXCAVATION DETAILS ADDED	
	TYPED A & B	
10-2-72	REVISED AND REDRAWN	508-10-2-72

ARKANSAS STATE HIGHWAY COMMISSION

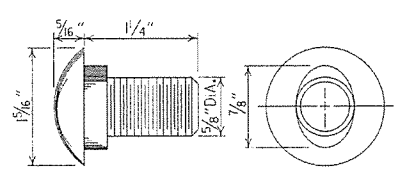
CONCRETE DITCH PAVING

STANDARD DRAWING CDP-1

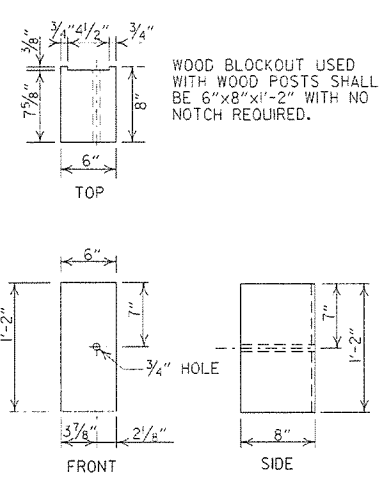
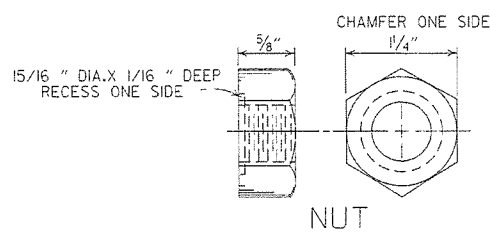
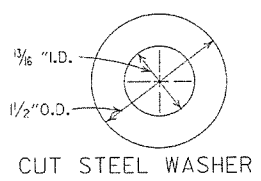


DETAILS OF W-BEAM GUARD RAIL

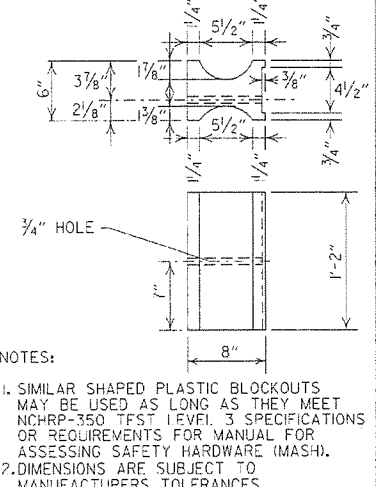
RAIL SECTION OF CLOSELY SIMILAR DIMENSIONS AND COMPARABLE STRENGTH MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.



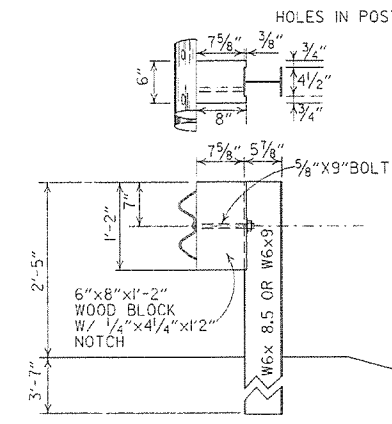
SPLICE BOLT
POST BOLT - SAME EXCEPT LENGTH



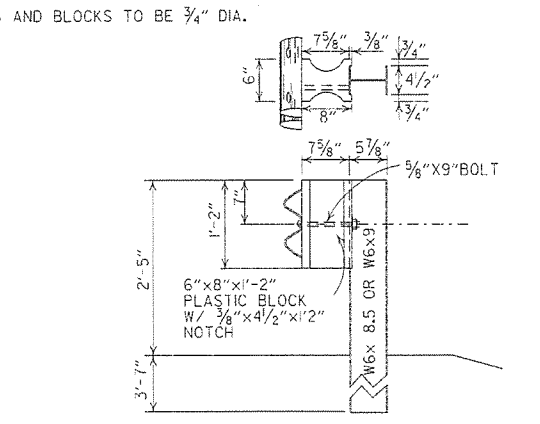
WOOD BLOCKOUT (W-BEAM)



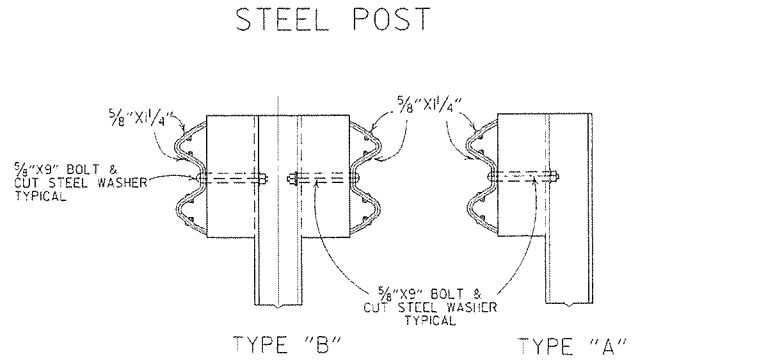
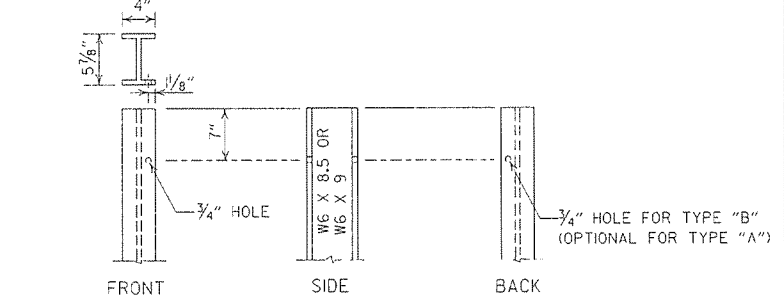
PLASTIC BLOCKOUT (W-BEAM)



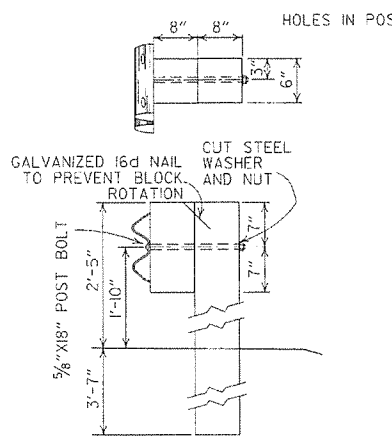
WOOD BLOCKOUT CONNECTIONS
DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



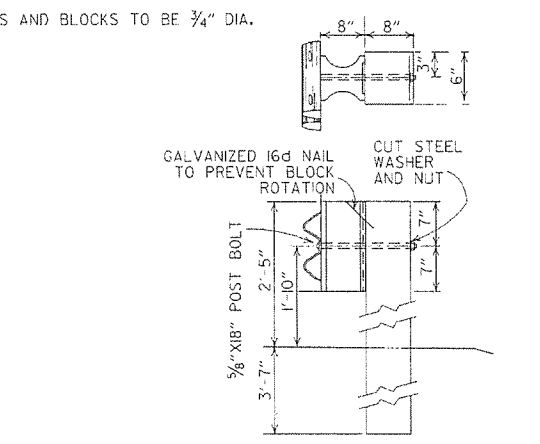
PLASTIC BLOCKOUT CONNECTIONS
DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



WOOD BLOCKOUT CONNECTIONS
DETAILS OF WOOD LINE POST CONNECTIONS (W-BEAM)



PLASTIC BLOCKOUT CONNECTIONS
DETAILS OF WOOD LINE POST CONNECTIONS (W-BEAM)

-GENERAL NOTES-

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.

WHERE W-BEAM GUARD RAIL CONTINUES, THE INTERMEDIATE SECTIONS SHALL HAVE A POST SPACING OF 6'-3" UNLESS OTHERWISE NOTED.

W-BEAM GUARD RAIL REPRESENTING INTERMEDIATE SECTIONS WILL BE MEASURED ALONG THE ROADWAY FACE FROM CENTERLINE OF POST TO CENTERLINE OF POST.

USE W-BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. FOR EXTENSIONS OR MODIFICATION OF EXISTING GUARD RAIL, W-BEAM GUARD RAIL COMPONENTS OF THE SAME TYPE AS THOSE EXISTING SHALL BE USED.

ANY BACKFILLING UNDER OR AROUND POST SHALL BE DAMP SAND THOROUGHLY TAMPED IN PLACE.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (1400 F) OR NO. 1 1350 F SOUTHERN PINE.

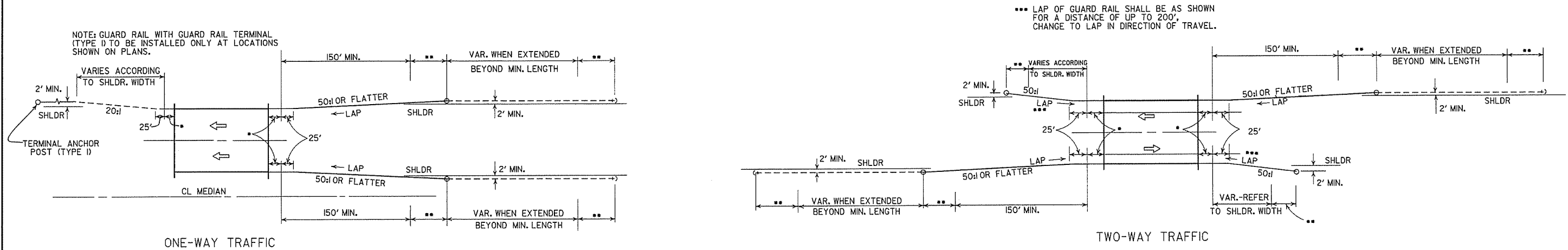
CONTRACTOR SHALL HAVE THE OPTION OF USING WOOD BLOCKOUTS FOR W-BEAM GUARD RAIL OR PLASTIC BLOCKOUTS, AS LONG AS BLOCKOUT USED MEETS NCHRP-350 TEST LEVEL 3 SPECIFICATIONS OR REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) FOR W-BEAM GUARD RAIL.

DATE	REVISION	DATE	FILM
7-4-40	RAISED HEIGHT OF GUARD RAIL 1"		
0-5-09	ADDED REFERENCE TO MASH		
4-10-03	REVISED GENERAL NOTES		
8-22-02	REVISED DIMENSION ON WOOD & PLASTIC BLOCKOUT CONNECTIONS & ON STEEL POST		
11-16-01	REVISED WOOD BLOCKOUT & DETAILS OF WOOD LINE POST CONNECTIONS		
3-30-00	REMOVED GUARD RAIL AT BRIDGE ENDS		
1-2-00	ADDED PLASTIC BLOCKOUT		
8-12-98	REV. BLOCKOUTS TO WOOD, DELETED CONC. POST & REV. GENERAL NOTE, DELETED DET. OF GUARD RAIL REPLACE BEHIND CURB & DET. OF POST PLACE IN SOLID ROCK, & ADDED DETAILS OF STEEL LINE POST CONN. REMOVED BACK-UP PLATE, REVISED HOLES IN STEEL POLES		
4-3-97	REMOVED "LAP IN DIRECTION OF TRAFFIC" NOTE & PLACED ARROWS ON WASHERS		
10-18-96	REVISED WOOD POST NOTE		
6-2-94	ADDF AT STEEL POST SIZE		
8-5-93	REVISED STEEL POST SIZE	8-5-93	
10-1-92	REDRAWN & REVISED	10-1-92	
8-15-91	REVISED WASHER NOTE	8-15-91	
8-2-90	REV. GEN. NOTE & DEP'T OF ANC. POST IN ROCK	8-2-90	
7-15-88	REVISED SECTION 3 & GENERAL NOTES		
3-4-88	REV. ANCHOR POST, ELEV. NOTES & POST IN ROCK	780-3-4-88	
10-30-87	REVISED WOOD LINE POST DETAIL	546-10-30-87	
10-9-87	REDRAWN & REVISED	802-10-9-87	

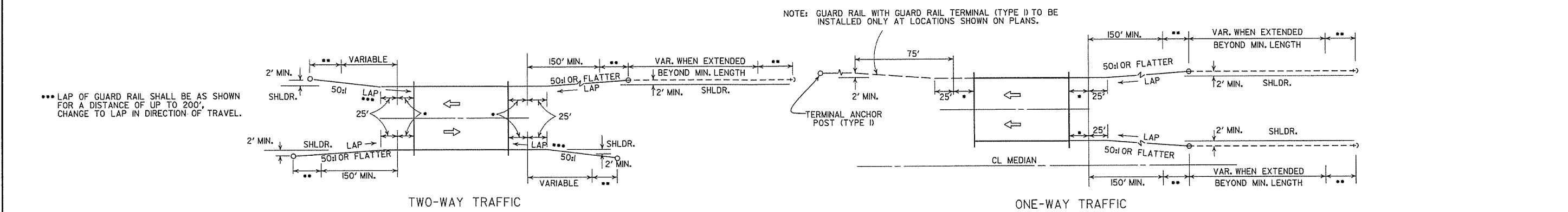
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

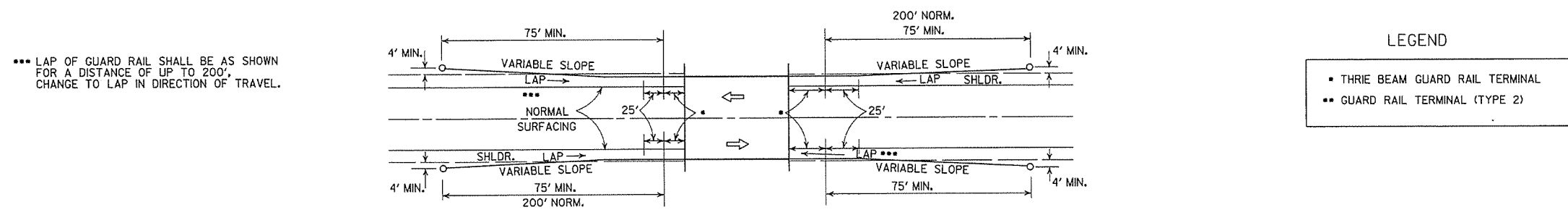
STANDARD DRAWING GR-8



METHODS OF INSTALLATION OF GUARD RAIL AT LESS THAN FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)

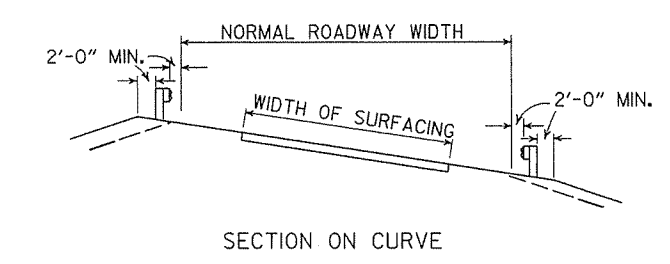
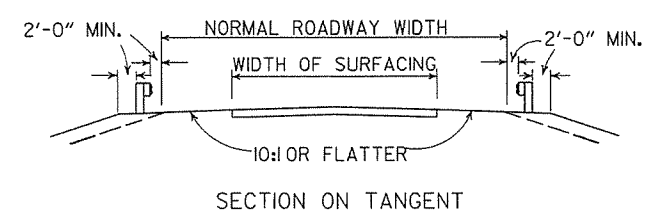
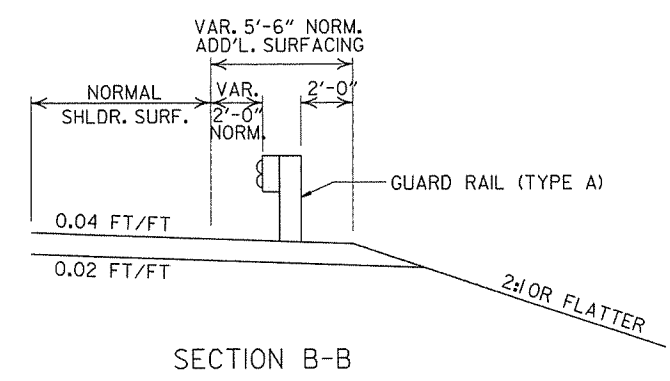
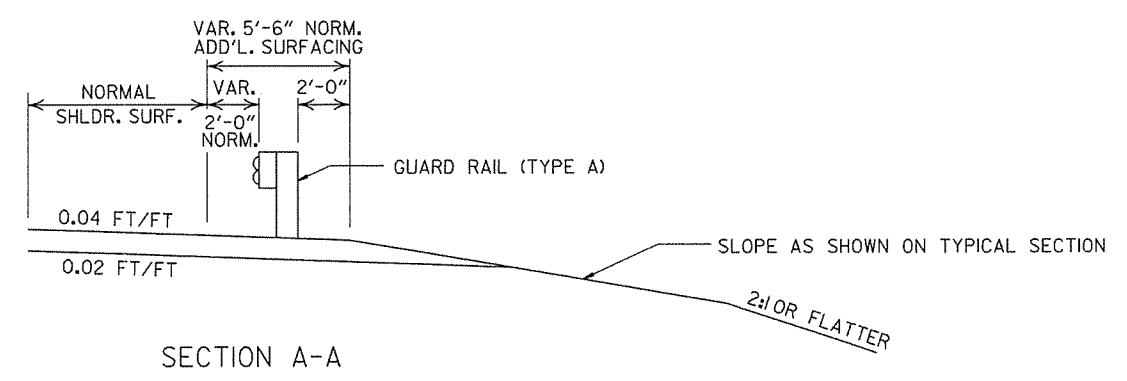
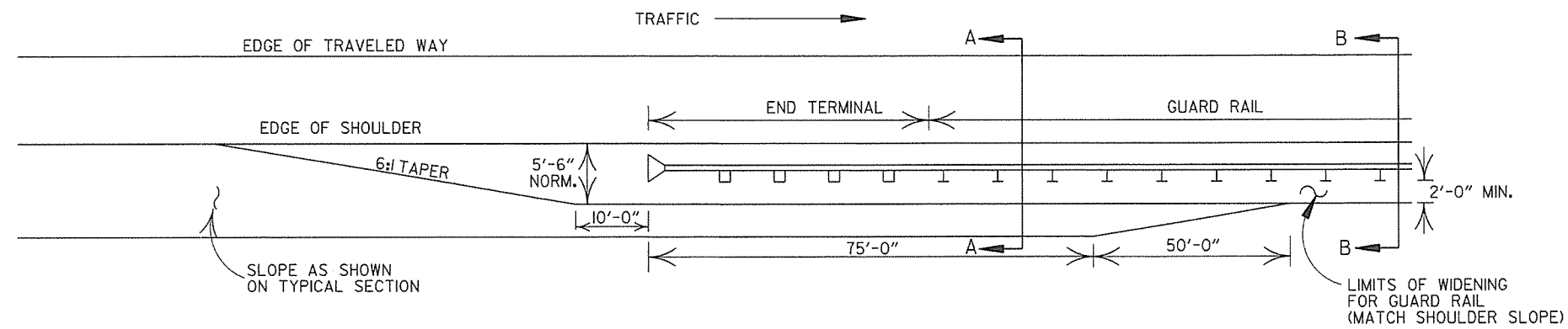


METHOD OF INSTALLATION OF GUARD RAIL AT FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)



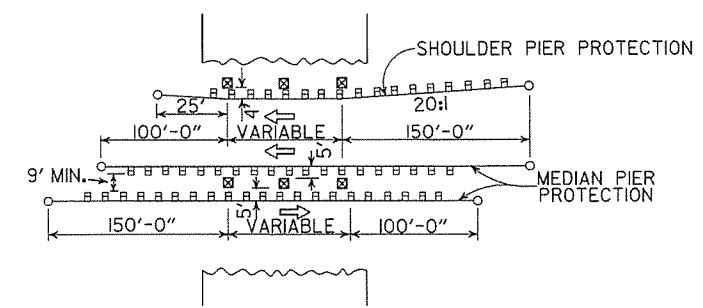
METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERMINAL (TYPE 1) (FULL SHOULDER WIDTH OR LESS BRIDGES)

			ARKANSAS STATE HIGHWAY COMMISSION
4-17-08	REVISED LAYOUTS		GUARD RAIL DETAILS
11-10-05	REMOVED GUARD RAIL NOTES AND DETAILS		
11-16-01	DELETED NOTE-METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERM. (TY. 1)		
1-12-00	ADDED CONSTRUCTION NOTE	1-12-00	
6-26-97	REVISED LAYOUT		
10-1-92	REDRAWN & REVISED	10-1-92	
	ADDED NOTE		
10-9-87	REDRAWN & REVISED		STANDARD DRAWING GR-9
DATE	REVISION	DATE FILM	



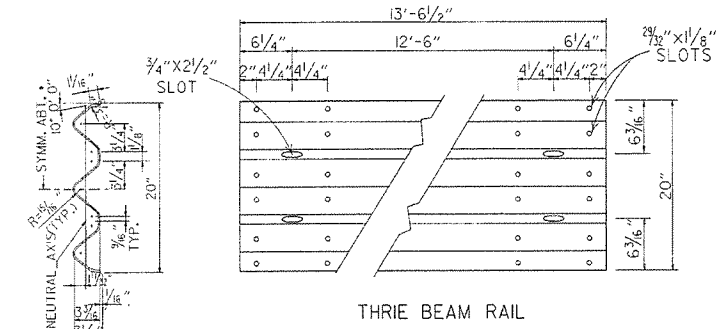
DETAILS OF WIDENING FOR GUARD RAIL

DETAILS SHOWING POSITION OF GUARD RAIL ON HIGHWAY

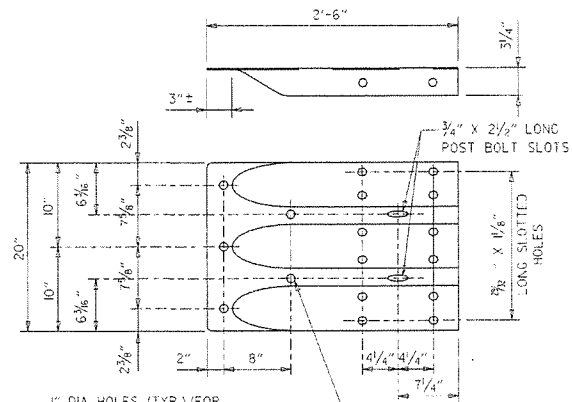


METHOD OF INSTALLATION OF GUARD RAIL AT FIXED OBSTACLE

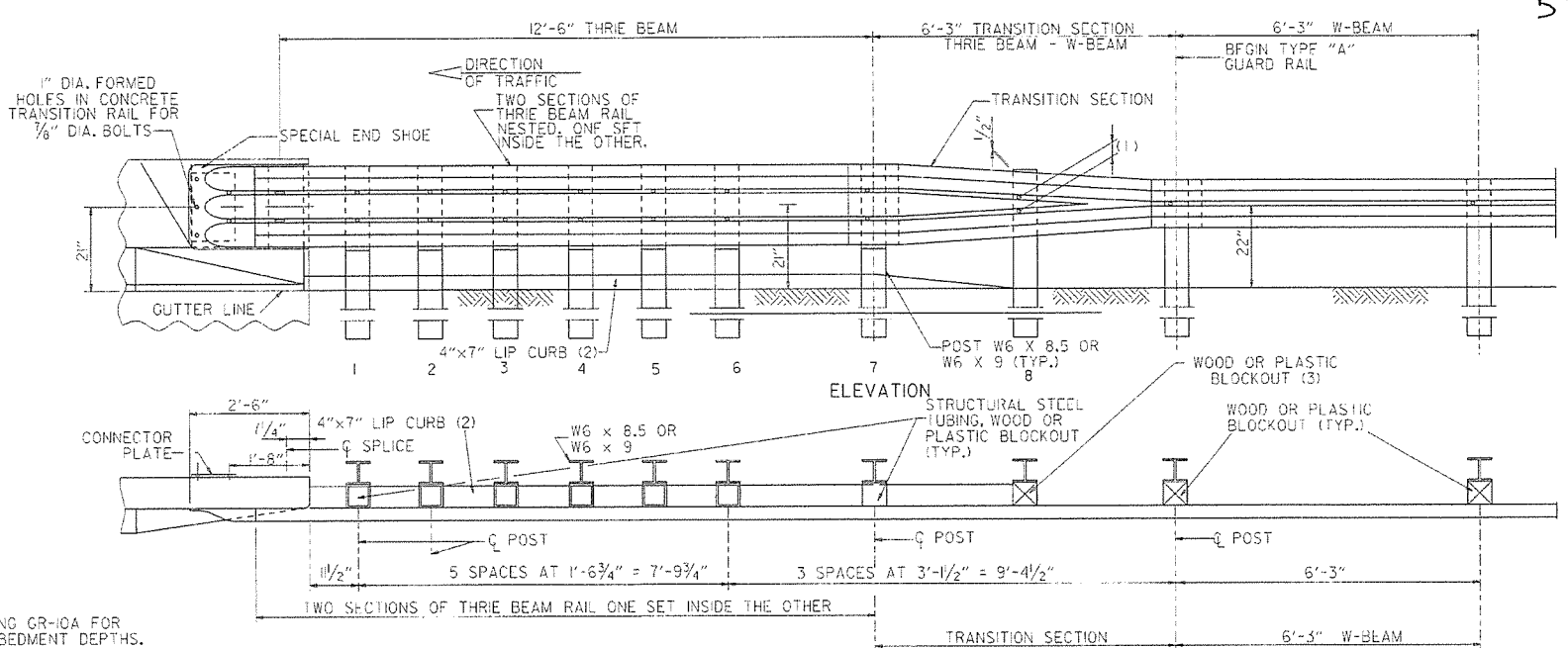
ARKANSAS STATE HIGHWAY COMMISSION			
GUARD RAIL DETAILS			
STANDARD DRAWING GR-9A			
4-17-08	MINOR REVISION		
11-10-05	DRAWN		
DATE	REVISION	DATE	FILM



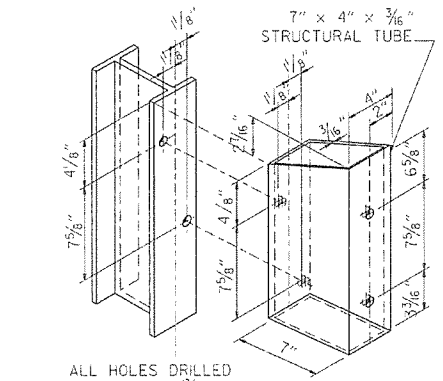
SECTION THRU THRIE BEAM RAIL



SPECIAL END SHOE

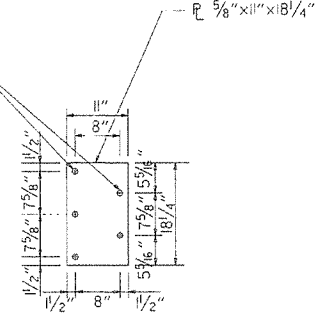


ELEVATION



ATTACH BLOCKOUT TO POST USING 5/8\"/>

1\"/>

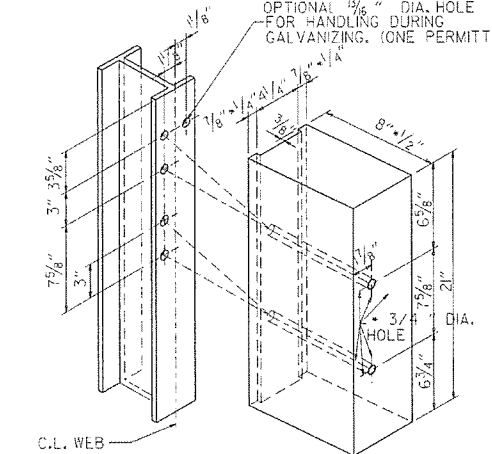


CONNECTOR PLATE

CONNECTOR PLATE SHALL BE AASHTO M270, GR. 36 AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING SHALL CONFORM TO SUBSECTION 807.19 OF THE STANDARD SPECIFICATIONS. CONNECTOR PLATE TO BE BOLTED TO SPECIAL END SHOE USING 7/8\"/>

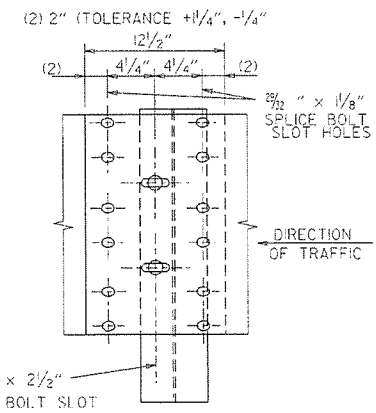
ALL HOLES DRILLED OR PUNCHED 3/8\"/>

STRUCTURAL STEEL TUBING BLOCKOUT DETAIL

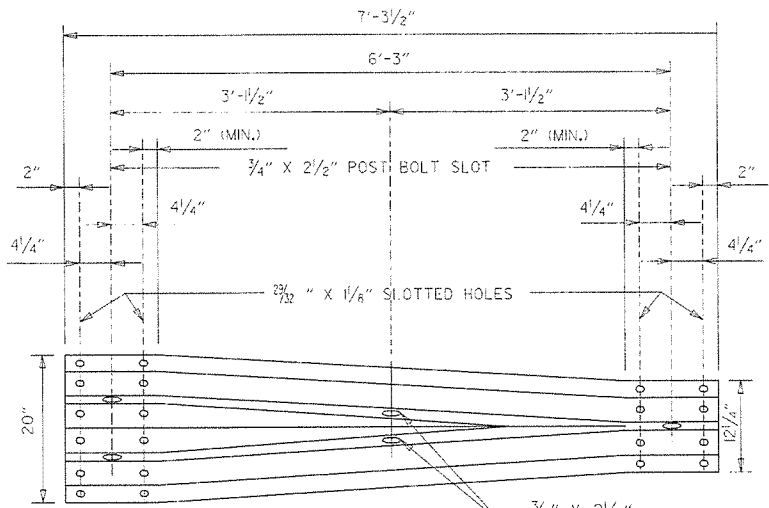


HOLE PUNCHING DETAIL FOR STEEL POST & WOOD OR PLASTIC BLOCKOUTS

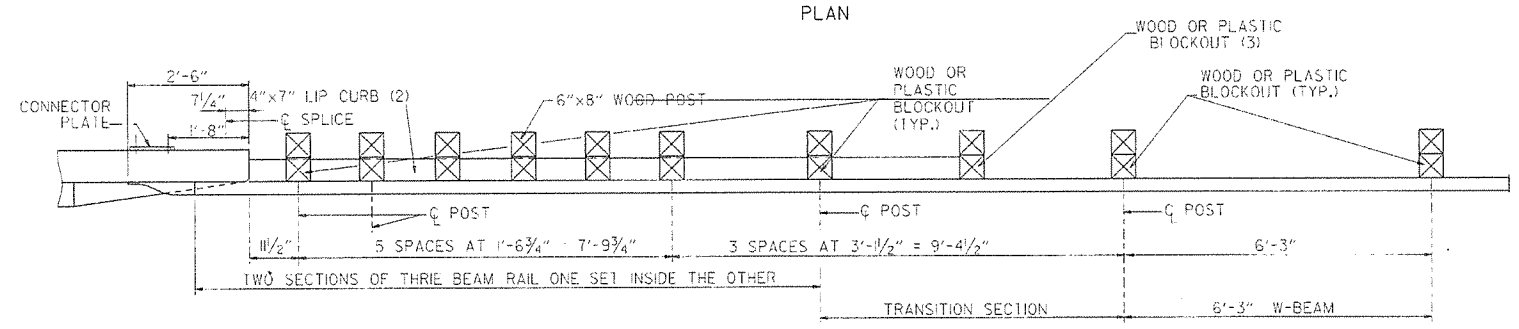
NOTE: BLOCKS SHALL BE THE SAME TYPE THROUGHOUT THE PROJECT LIMITS.



THRIE BEAM RAIL SPLICE AT POST



TRANSITION SECTION



PLAN

PLAN

- (1) VERIFY BOLT SPACING FROM RAIL TRANSITION PRODUCER.
- (2) REFER TO APPROACH GUTTER DETAILS.
- (3) LENGTH OF BLOCKOUT ON POST 8 TO BE MODIFIED TO FIT RAIL WIDTH.

THRIE BEAM GUARD RAIL CONNECTION AT BRIDGE ENDS

GENERAL NOTES:

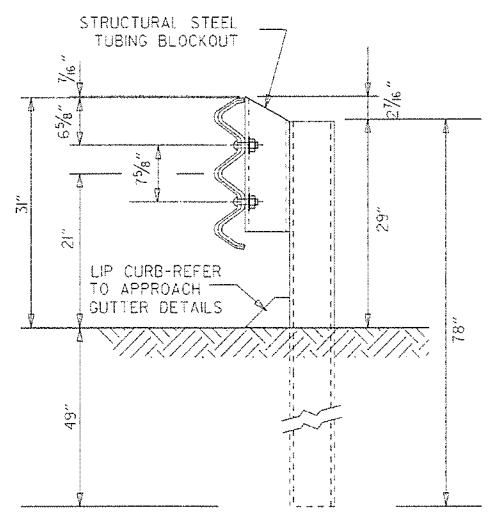
- THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE I.
- RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.
- ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4\"/>
- ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-9 & GR-11.
- WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1350 f SOUTHERN PINE.
- REFER TO STD. DRWG. GR-10A FOR POST DETAILS.
- USE THRIE BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB.
- THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W-BEAM POSTS FOR ENTIRE JOB.

DATE	REVISION	DATE FILED
7-14-10	RAISED HEIGHT OF W-BEAM 1"	
11-29-07	ADDED PLASTIC BLOCKOUTS	
11-10-05	ADDED NOTE FOR ATTACHING STEEL BLOCKOUT	
11-18-04	REVISED GENERAL NOTES	
10-9-03	REVISED GENERAL NOTES	
4-10-03	REVISED GENERAL NOTES	
8-22-02	REVISED NOTE (2)	
6-29-00	MOVED DIMENSION LINES	
5-18-00	ADDED NOTE	
3-30-00	DRAWN & ISSUED	

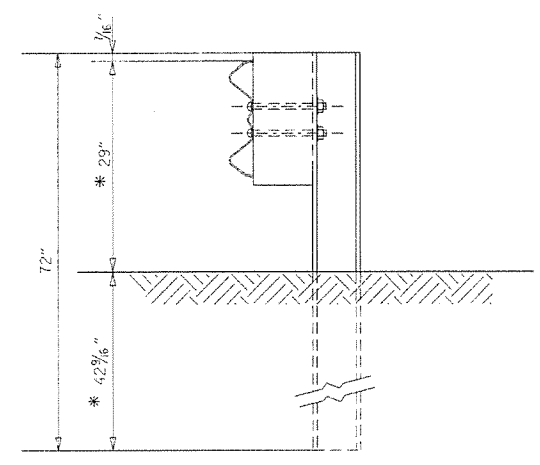
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

STANDARD DRAWING GR-10

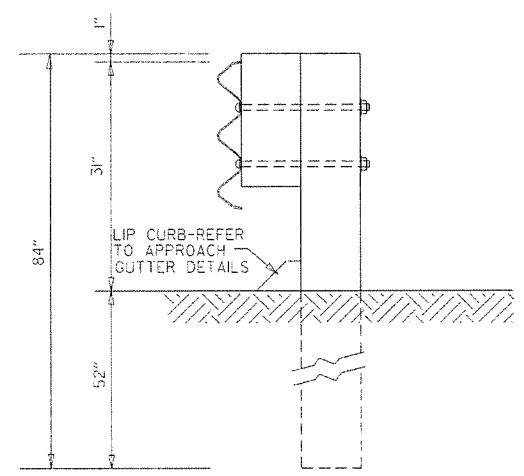


THREE BEAM RAIL WITH STEEL TUBING BLOCKOUT AND STEEL POST
POSTS 1-7

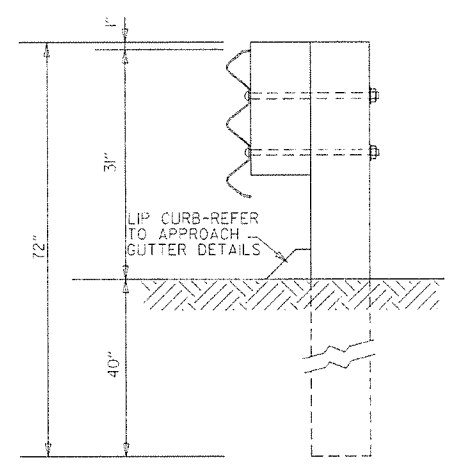


W-BEAM TO THREE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT AND STEEL POST
POST 8

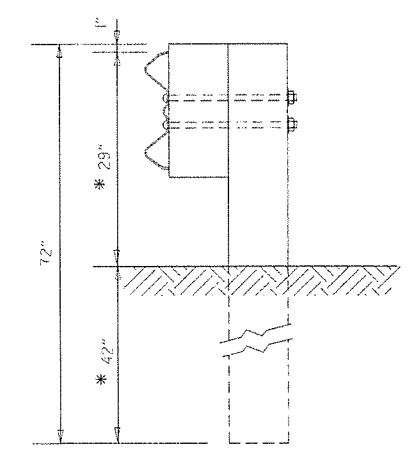
* NOTE:
THESE DIMENSIONS WILL NEED TO BE ADJUSTED IN THE FIELD TO MAKE THE TRANSITION FROM 21" MID POINT OF THREE BEAM TO 22" MID POINT OF W-BEAM.



THREE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUTS & WOOD POSTS
POSTS 1-6



THREE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 7

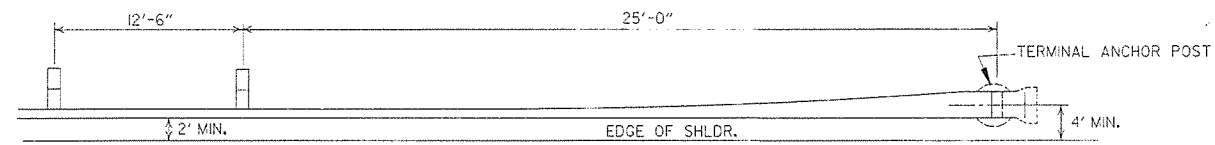


W-BEAM TO THREE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 8

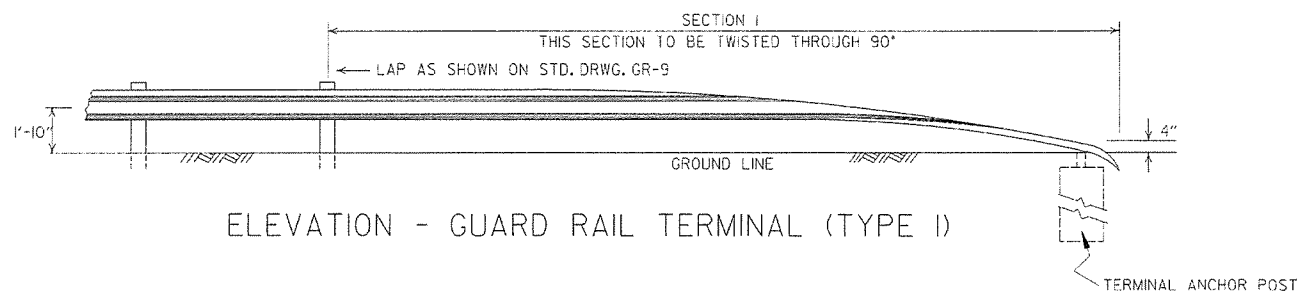
GENERAL NOTES:
RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.
WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (400 F) OR NO. 1 (350 F) SOUTHERN PINE.

DATE	REVISION	DATE FILM
7-14-10	REVISED POST 8 DIMENSIONS	
11-29-07	ADDED PLASTIC BLOCKOUTS	
8-22-02	REVISED LIP CURB NOTE	
3-30-00	DRAWN & ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION
GUARD RAIL DETAILS
STANDARD DRAWING GR-10A

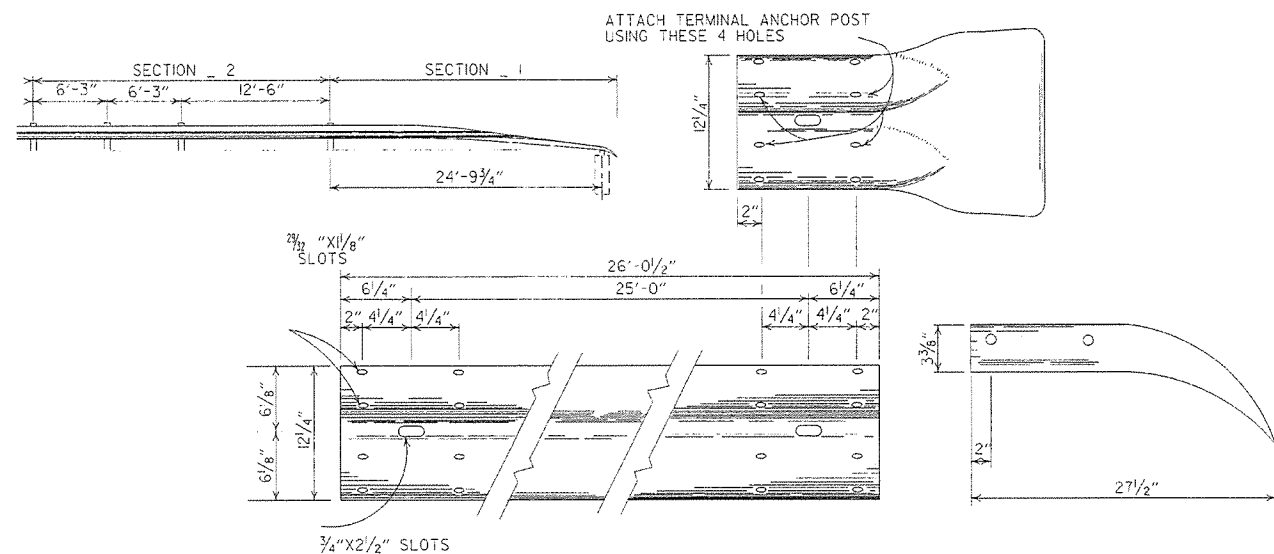


PLAN - GUARD RAIL TERMINAL (TYPE I)



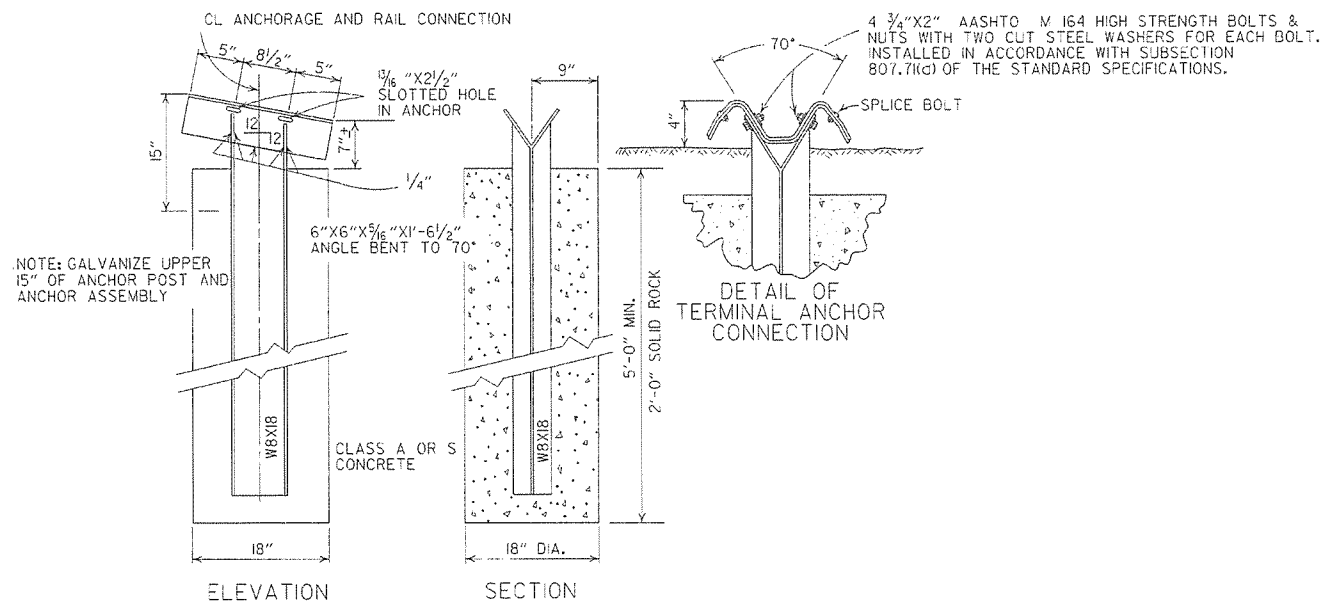
ELEVATION - GUARD RAIL TERMINAL (TYPE I)

NOTE:
SECTIONS 1 AND 2 OF GUARD RAIL TERMINAL SHALL BE PAID FOR AT THE PRICE BID PER LINEAR FOOT OF THE TYPE OF GUARD RAIL SPECIFIED.



SECTION 1

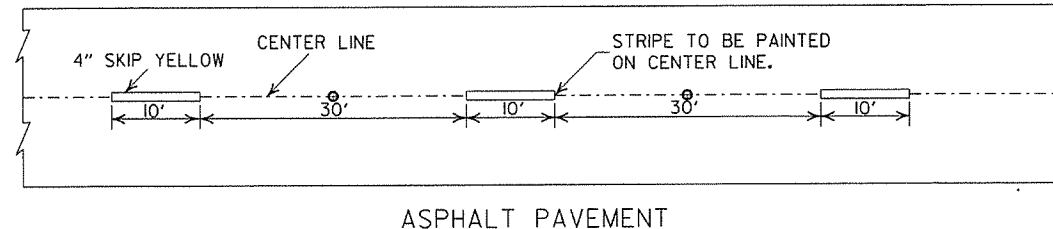
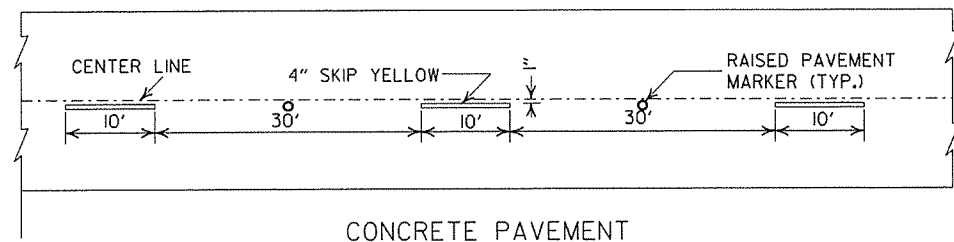
TERMINAL SECTION



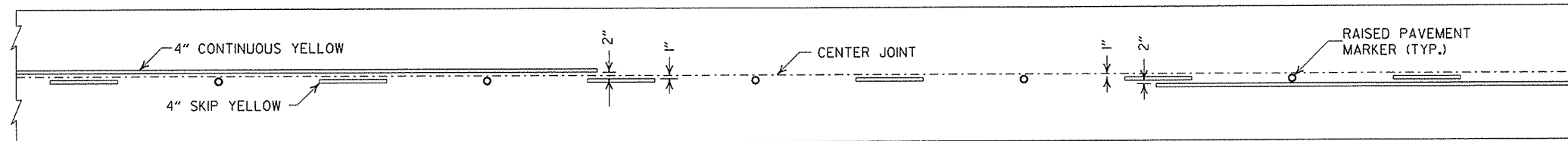
DETAIL OF TERMINAL ANCHOR POST (TYPE I)

NOTE: RAIL MEMBERS MAY BE BOLTED TO ANGLE AT TERMINAL ANCHOR AND THE TWO ASSEMBLIES POSITIONED TO PROPER ALIGNMENT PRIOR TO PLACING CONCRETE AROUND 8 W/ 17 POST IF CONTRACTOR SO DESIRES.

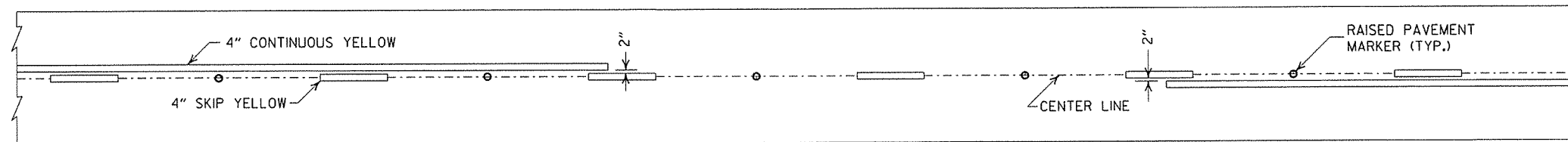
			ARKANSAS STATE HIGHWAY COMMISSION
			GUARD RAIL DETAILS
			STANDARD DRAWING GRT-1
7-14-90	RAISED HEIGHT OF GUARD RAIL 1"		
6-26-97	REVISED LAP NOTE		
10-18-96	REVISED ASTM REF. TO AASHTO		
11-3-94	DIMENSION TERMINAL DETAIL		
11-11-92	ADDED NOTE FOR PAYMENT	11-11-92	
10-1-92	DRAWN & ISSUED	10-1-92	
DATE	REVISION	DATE	FILM



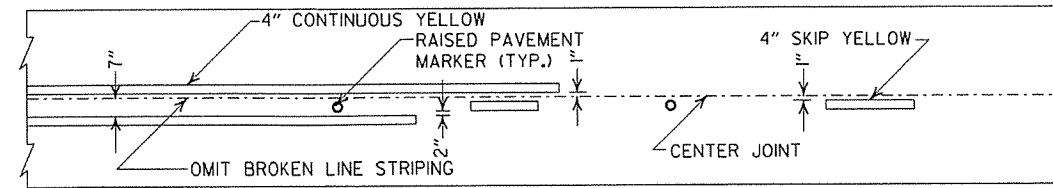
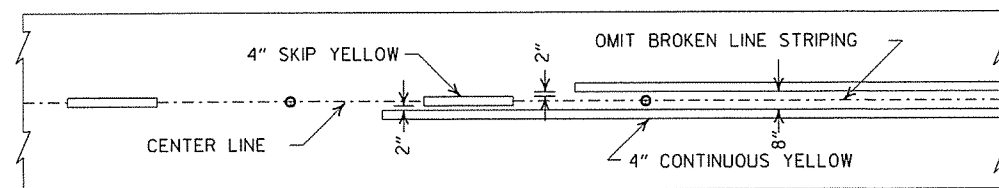
BROKEN LINE STRIPING



SOLID LINE STRIPING ON CONCRETE PAVEMENT



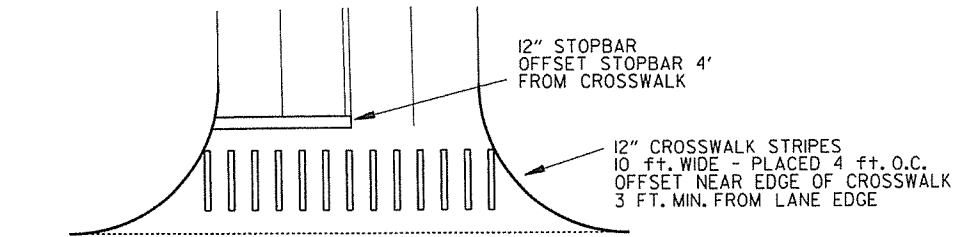
SOLID LINE STRIPING ON ASPHALT PAVEMENT



ASPHALT PAVEMENT

CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

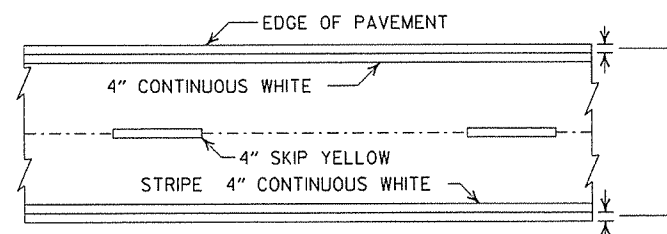


CROSSWALK AND STOPBAR DETAILS

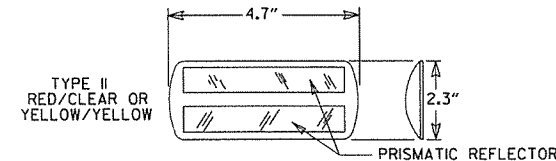
NOTES:

1. ALL LINES SHALL HAVE A WIDTH OF 4 INCHES.
2. THE THICKNESS AND RATE OF PAINT APPLICATION SHALL BE AS SPECIFIED IN SECTION 718 OF THE STANDARD SPECIFICATIONS.
3. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. RAISED PAVEMENT MARKERS SHALL BE CENTERED BETWEEN SKIP LINES ON 40 FEET SPACING UNLESS OTHERWISE SHOWN ON THE PLANS.

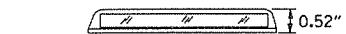
2" FOR ASPHALT OR CONCRETE PAVEMENT
6" FOR BITUMINOUS SURFACE TREATMENT



PAVEMENT EDGE LINE MARKING



NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

GENERAL NOTES:

THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND RAISED PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

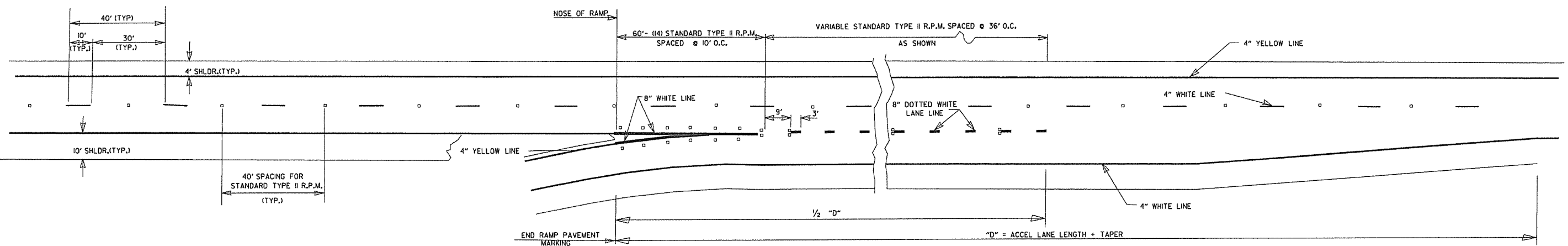
DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80

ARKANSAS STATE HIGHWAY COMMISSION	
PAVEMENT MARKING DETAILS	
STANDARD DRAWING PM-1	

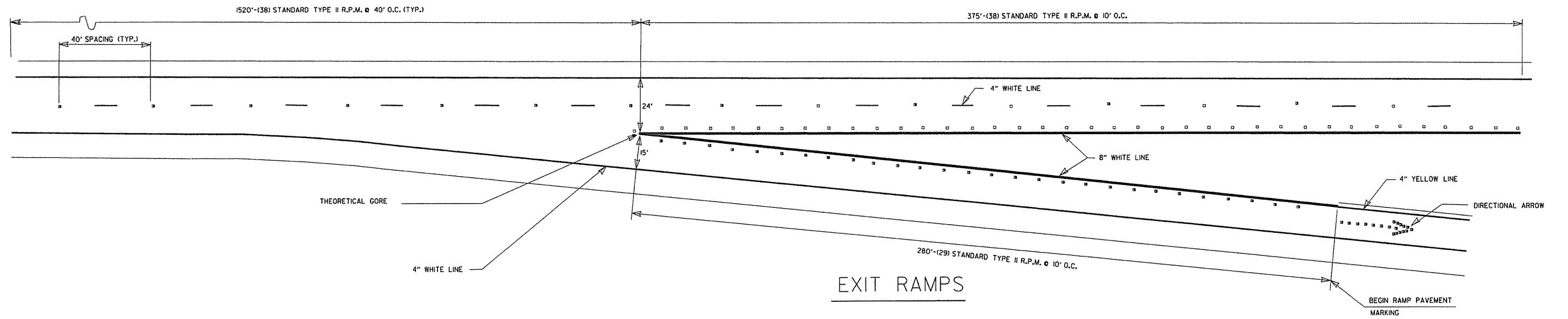
PAVEMENT MARKING QUANTITIES
(BASED ON 700' ACCEL. LANE + 300' TAPER)

ENTRANCE RAMP
8" WHITE = 228 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

EXIT RAMP
4" WHITE = 280 LIN. FT.
8" WHITE = 655 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH



ENTRANCE RAMPS

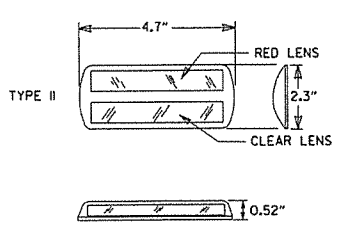


EXIT RAMPS

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

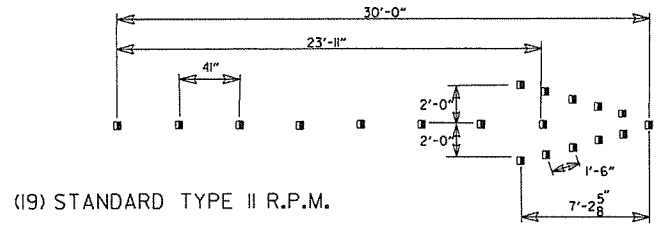
THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



DIRECTIONAL ARROWS

DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMPS	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95

ARKANSAS STATE HIGHWAY COMMISSION
PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS
STANDARD DRAWING PM-2

ADVANCE DISTANCES (XXXX)


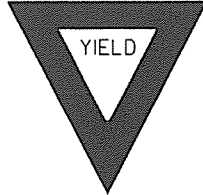
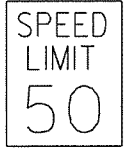




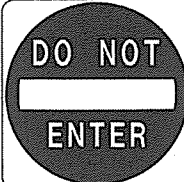
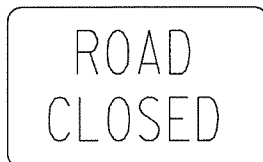
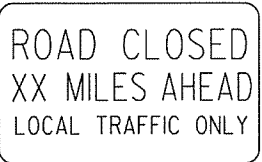
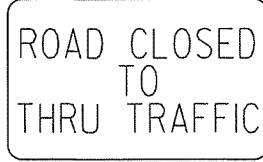
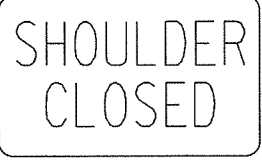
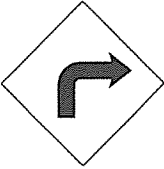
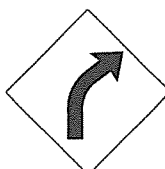
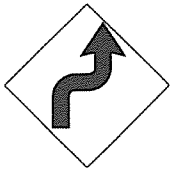
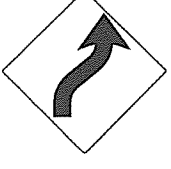
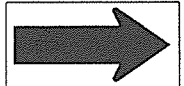
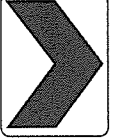
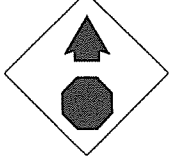
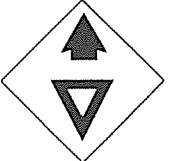
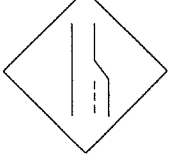

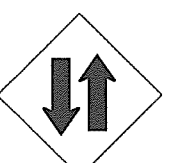

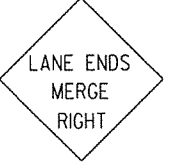









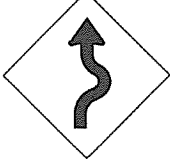
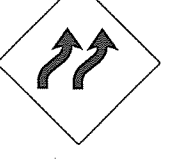


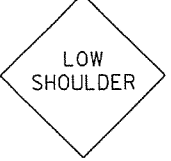
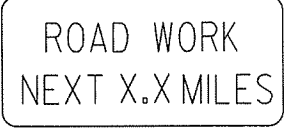
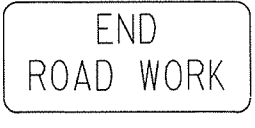
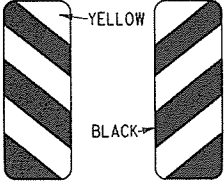


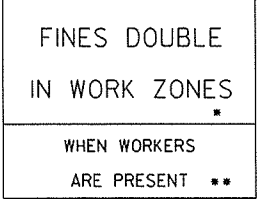
500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

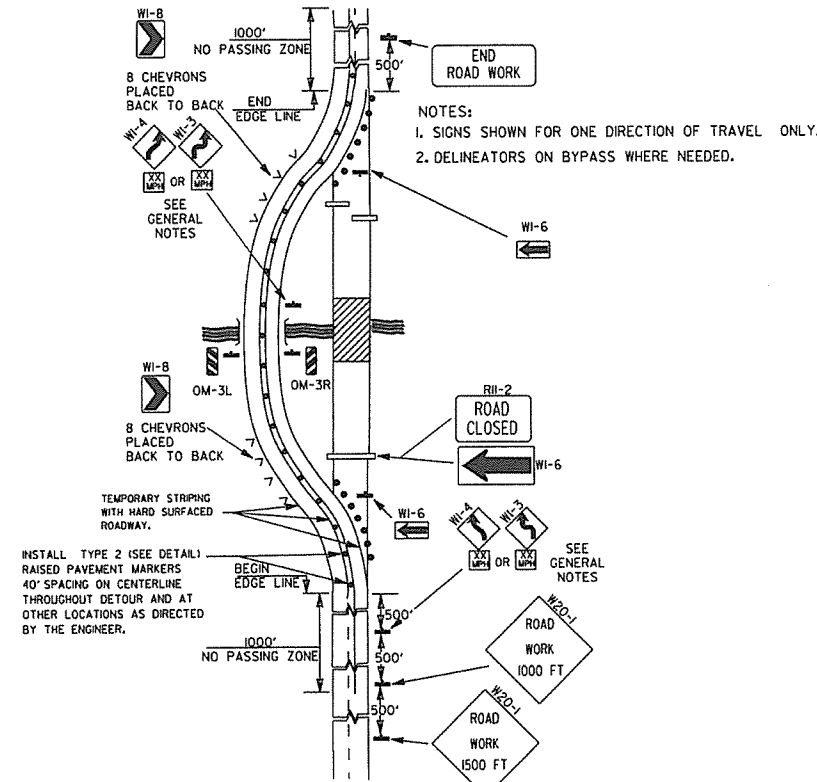
GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

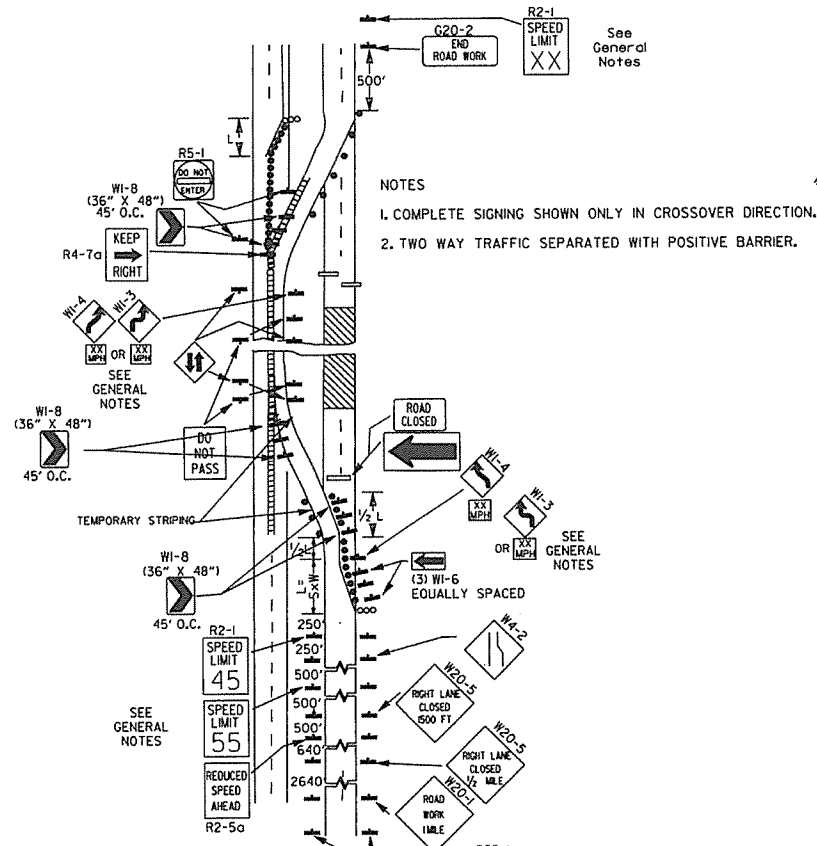
* NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

12-15-8	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

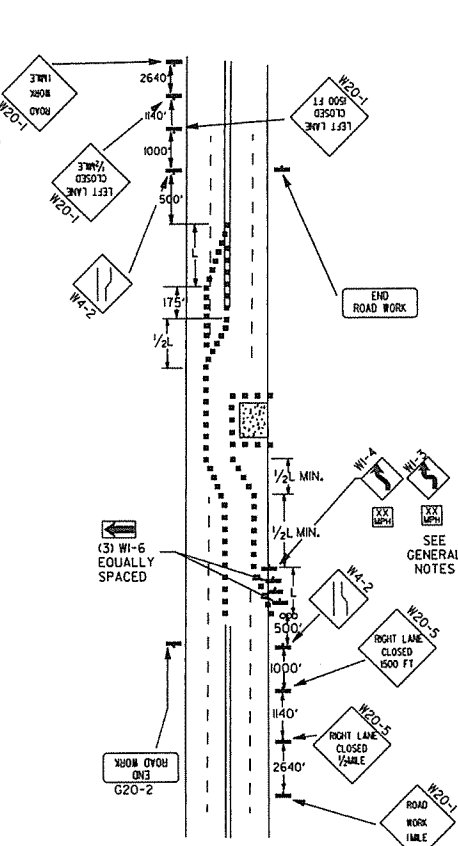
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>18" 500 FEET 24" W16-2</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>



(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.

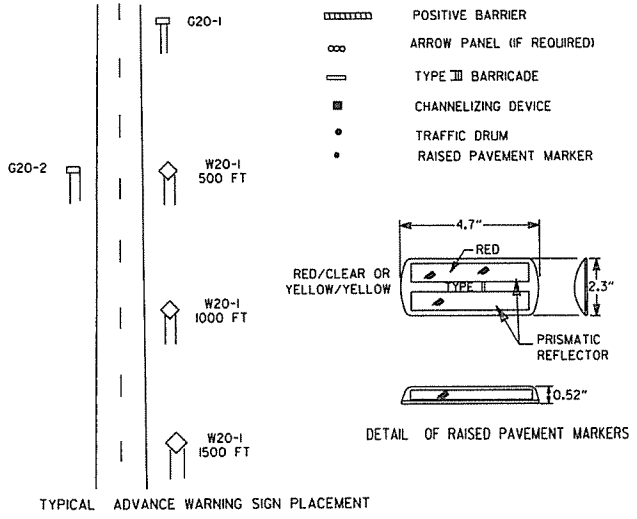


(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

- KEY:
- FLAGGER
 - POSITIVE BARRIER
 - ARROW PANEL (IF REQUIRED)
 - TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER

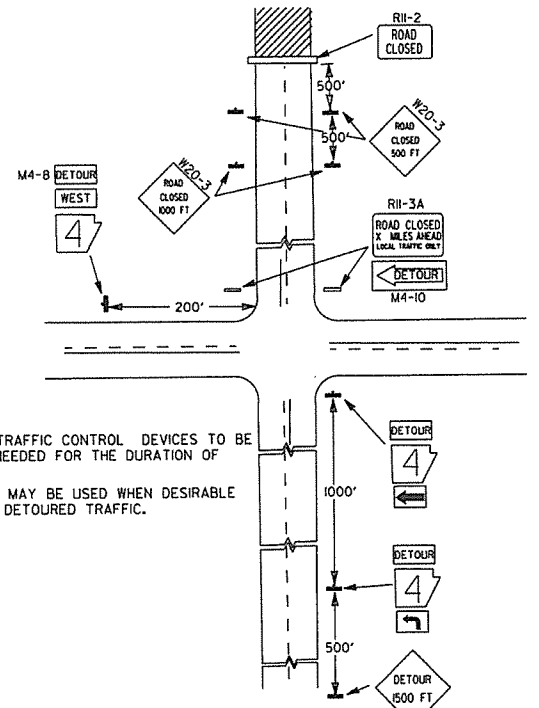


TAPER FORMULAE:
 $L = SXW$ FOR SPEEDS OF 45MPH OR MORE.
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
 WHERE:
 L = MINIMUM LENGTH OF TAPER.
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
 W = WIDTH OF OFFSET.

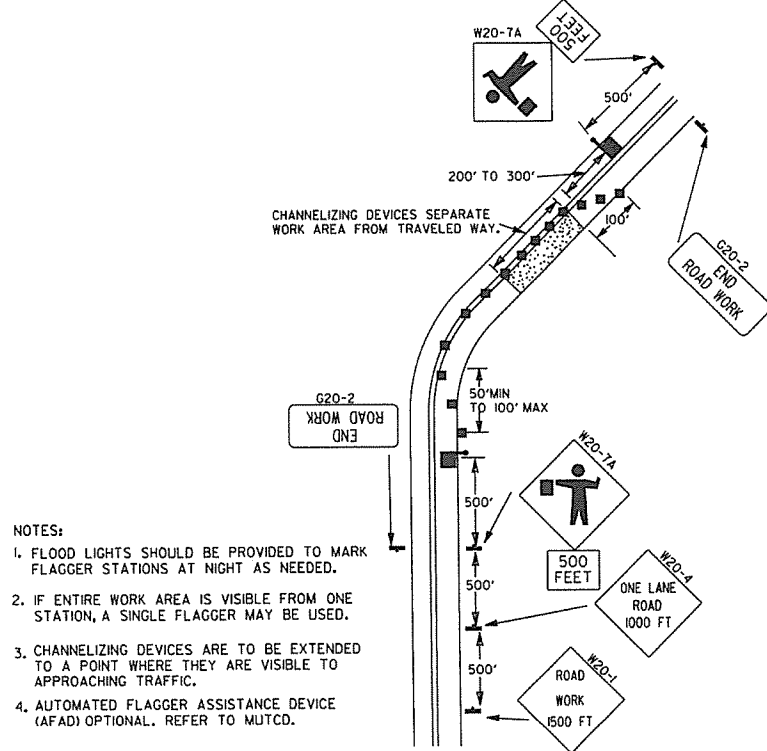
- GENERAL NOTES:
- ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 - WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE R2-5A SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 - WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 - PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 - TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.

DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-8-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

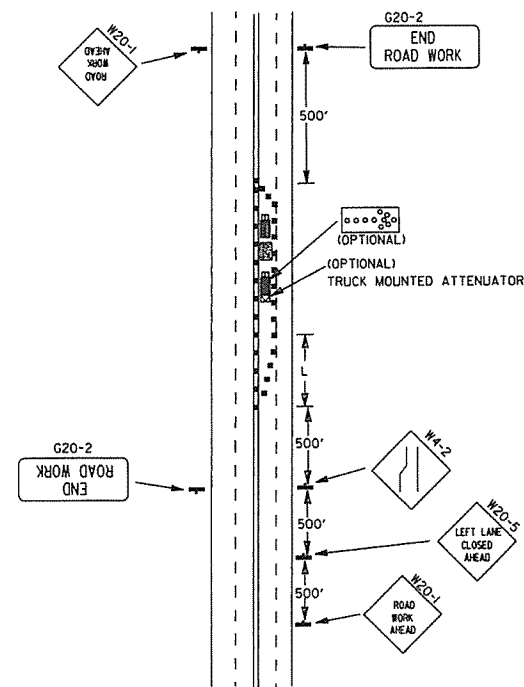
ARKANSAS STATE HIGHWAY COMMISSION
 STANDARD TRAFFIC CONTROLS
 FOR HIGHWAY CONSTRUCTION
 STANDARD DRAWING TC-2



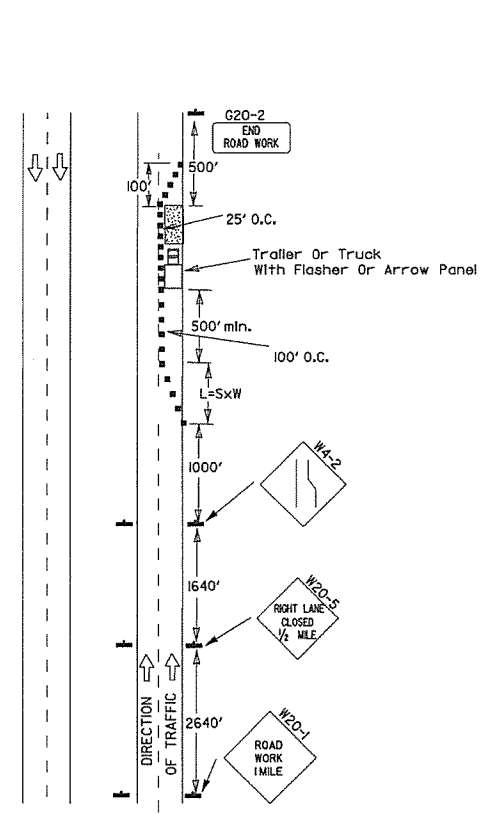
(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



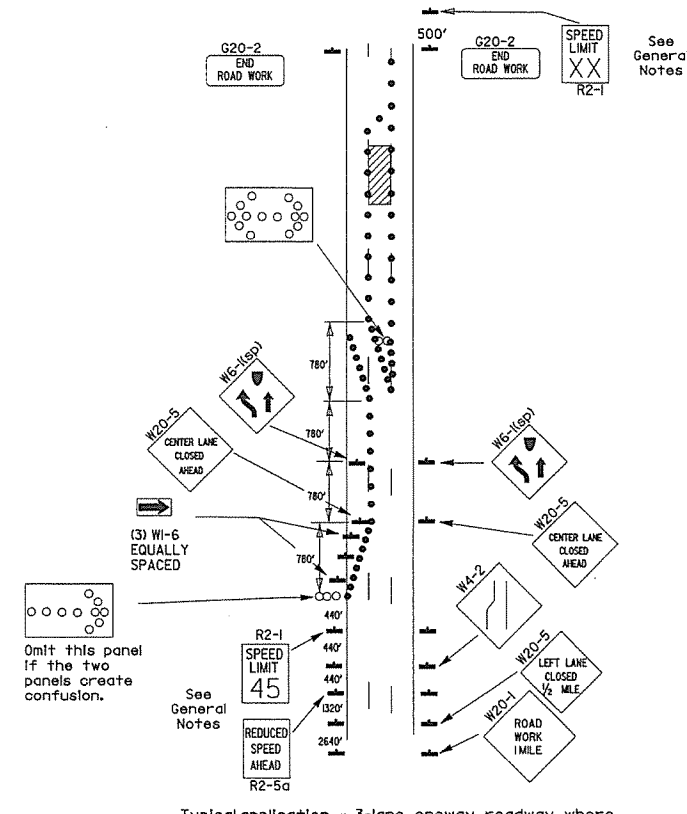
(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

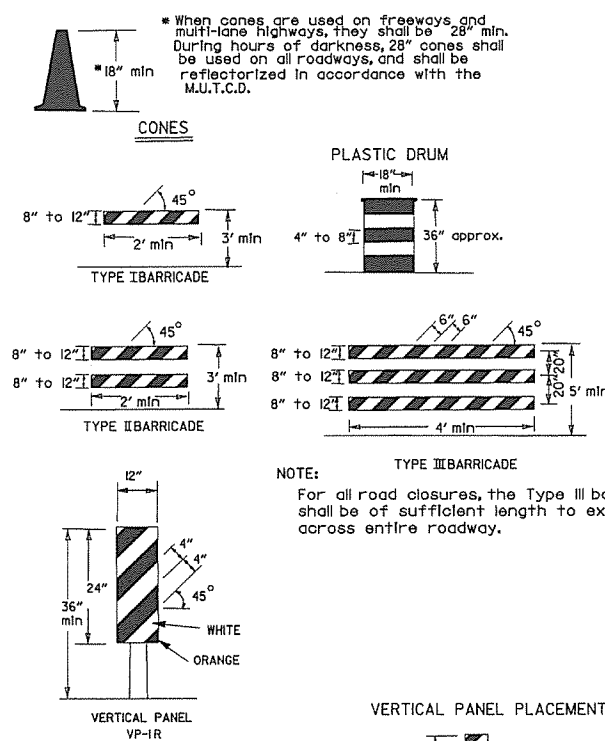


(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.



(B) Typical application - 3-lane oneway roadway where center lane is closed.

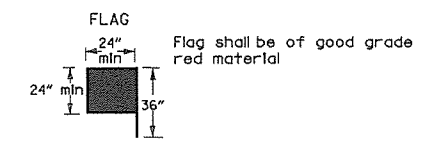
Channelizing devices



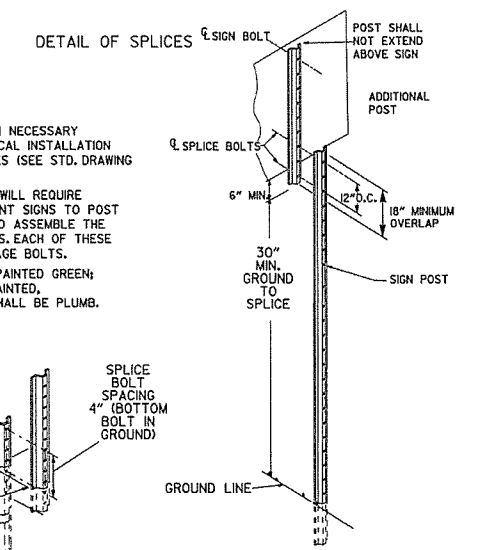
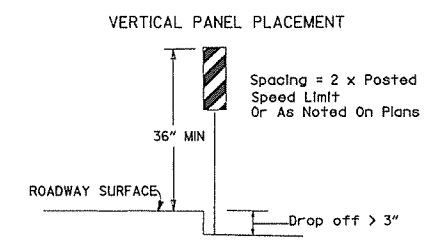
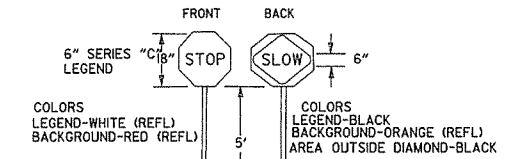
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-II
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-I and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

* When shown on the plans concrete barrier will be used.
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



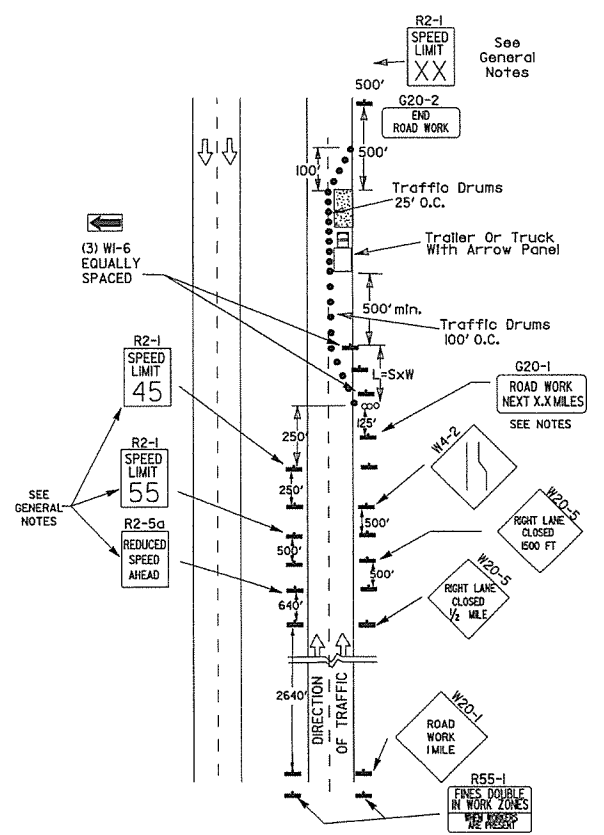
STOP SLOW PADDLE



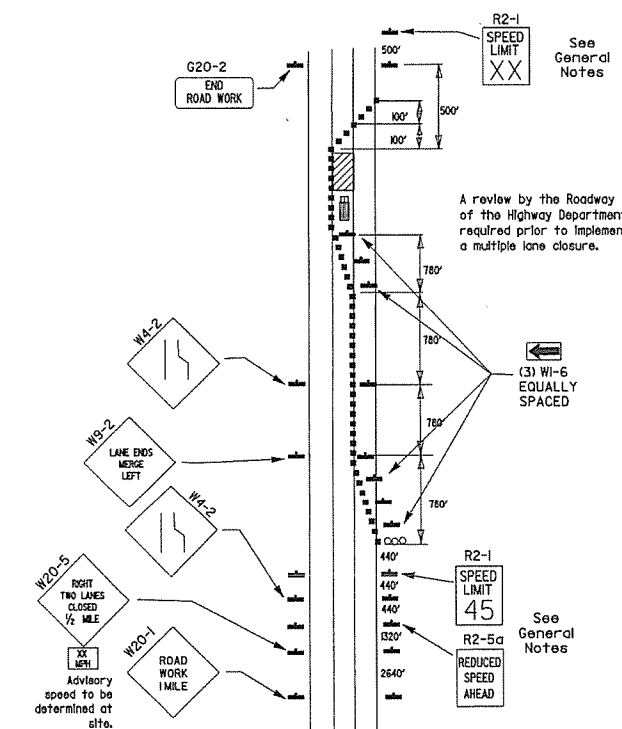
- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

GENERAL NOTES:

- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
- When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-1(45) speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- When the existing speed limit is 65mph and the plans require a speed limit of 45mph, the R2-1(65) shall be omitted. Additional R2-1(45) speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
- Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
- Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
- The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 1/2 mile in advance of the job limit. Additional W20-1 (1/2 MILE) signs are not required in advance of lane closures that begin inside the project limits.
- Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
- All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual for Assessing Safety Hardware (MASH).
- Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.



(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

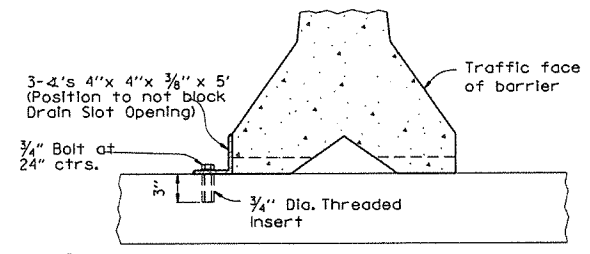
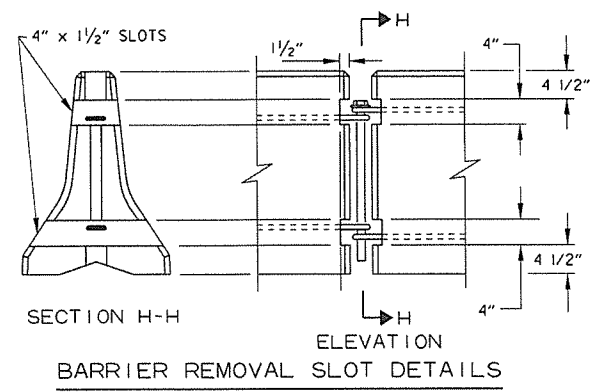
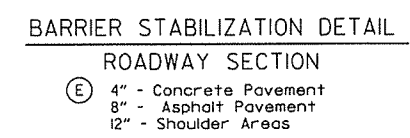
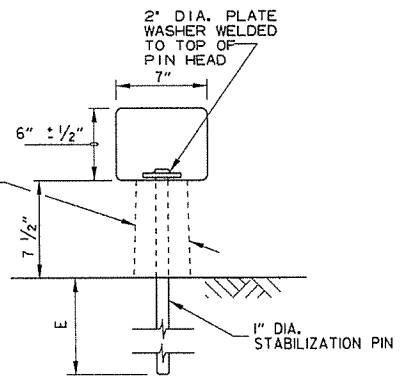
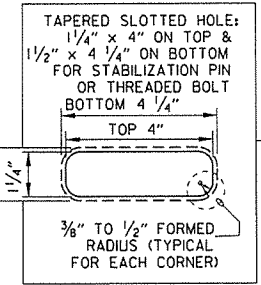
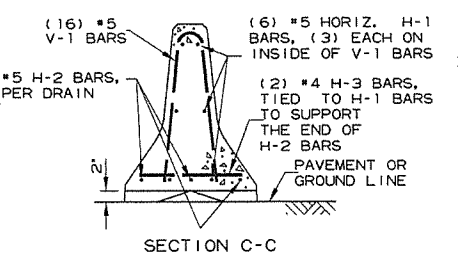
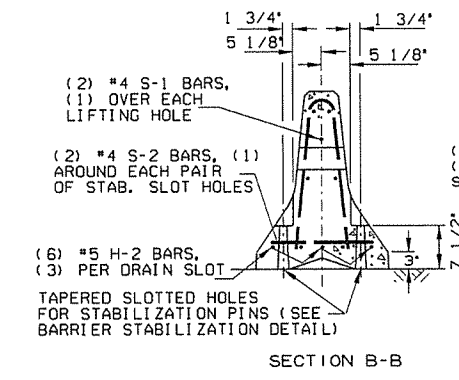
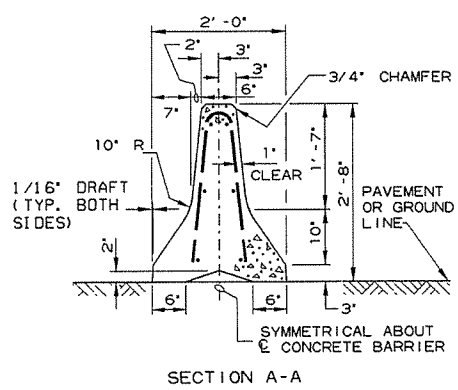
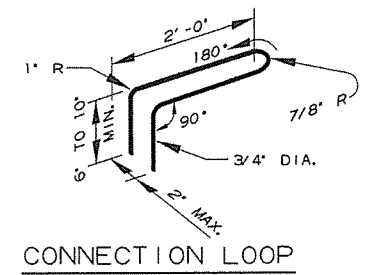
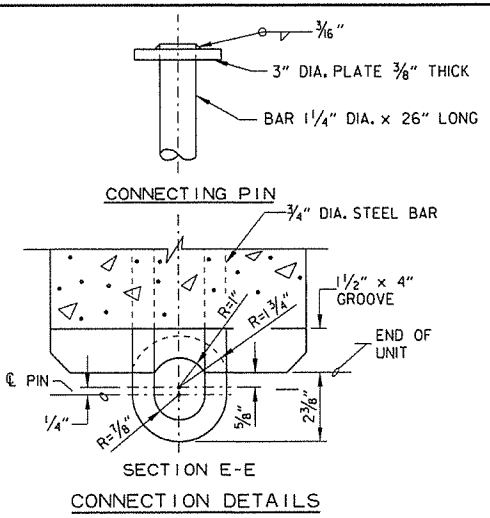


(D) Typical application - closing multiple lanes of a multilane highway.

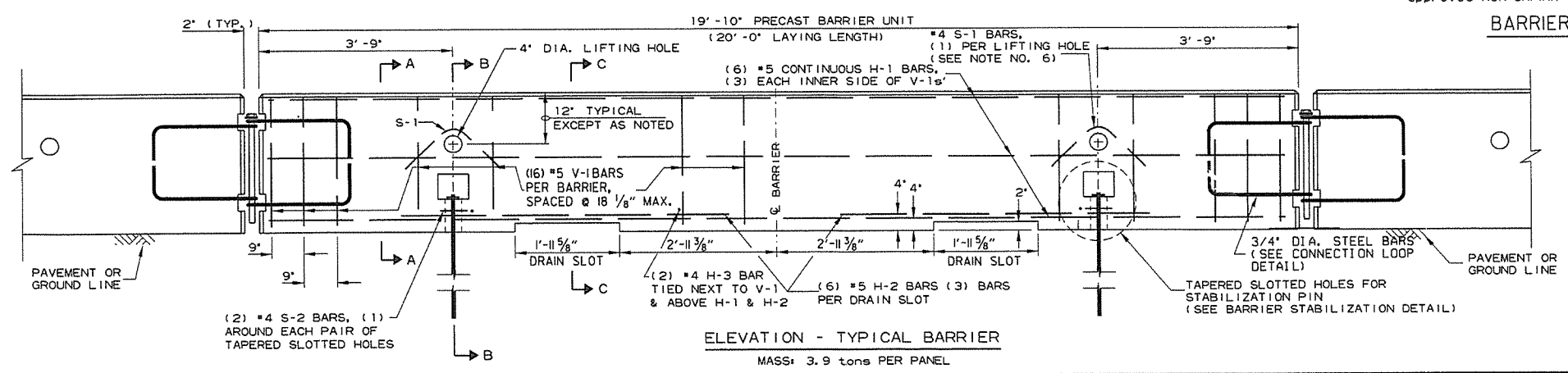
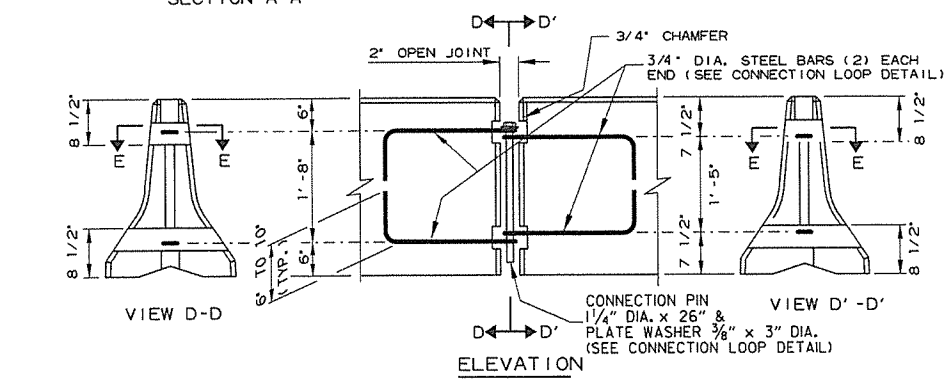
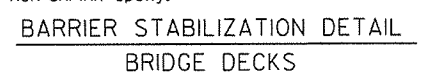
- NOTES:
- USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2)
 - NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.
 - SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.

DATE	REVISION	FILMED
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

REINFORCING BAR TABLE PER BARRIER UNIT			
MARK	LOCATION	BAR SIZE (NO. BARS)	SKETCH
H-1	HORIZONTAL IN BARRIER TIED INSIDE V-1 BARS	*5 (6)	19'-3"
H-2	CENTERED ABOVE DRAIN SLOTS LONG. & TRANSVERSELY	*5 (6)	6'-6"
H-3	TIED ABOVE H-1 BARS TO SUPPORT H-2, TIED TO V-1	*4 (2)	1'-6"
S-1	OVER LIFT HOLES	*4 (2)	
S-2	HORIZ. AROUND SLOTS BETWEEN V-1'S & DRAIN SLOTS	*4 (2)	
V-1	VERTICAL IN BARRIER (3) EACH END & (2) AT EACH DRAIN SLOTS	*5 (16)	



NOTE: 3/4" Threaded Inserts shall be cast in place for all new bridge decks and drilled and grouted for existing bridge decks. Inserts shall have a minimum ultimate load capacity of 8000 lbs. in tension. After removal of barrier, bolts, and angles, the inserts shall be filled with approved non-shrink epoxy.



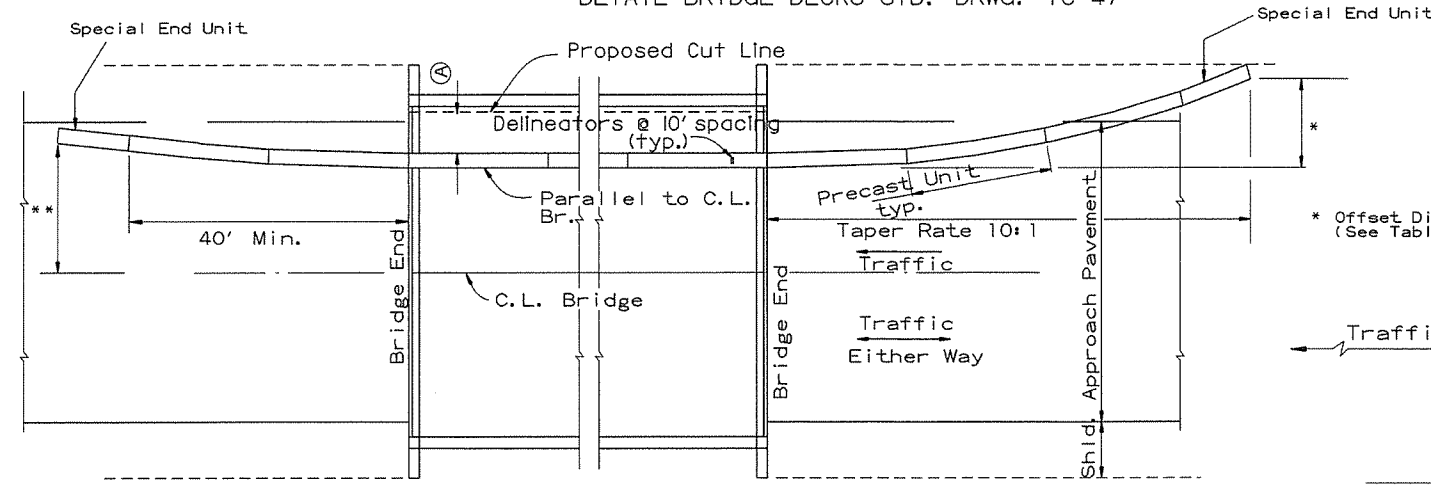
- General Notes**
- The contractor shall furnish the Precast Concrete Barrier Units and shall be responsible for the manufacture, shipment, storage, placement and removal. At the completion of the project, the precast units will remain the property of the contractor.
 - Materials shall meet the following minimum requirements:
Concrete: 2500 psi compressive strength at 28 days.
Reinforcing Steel: AASHTO M 31 or M 53, Grade 60
Structural Steel: AASHTO-M270 Grade 36 shall be used for the Connection Pin, Connection Loops, and Stabilization Pins. A One Piece Pin with a 3" rounded top may be used in place of the detailed Connection Pin. Delineators: Delineators shall be mounted at 10' spacing on top of precast barrier.

In applications where barrier walls within 6 feet of a traffic lane, additional delineators shall be placed on the barrier at 10' spacing approximately one (1) foot from the top of the barrier. Delineators shall be on the AHTD Qualified Products List for Construction Concrete Barrier Markers. Delineator color shall be in accordance with the Manual on Uniform Traffic Control Devices. Payment for delineators shall be considered included in the price bid per Lin. Ft. for "Furnishing and Installing Precast Concrete Barrier". The contractor shall certify to the Engineer that the material and the design used in the precast barrier units meets the requirements as shown on this standard drawing.
 - Other Precast Concrete Barriers that have been crash tested and approved by the Federal Highway Administration to meet the requirements of NCHRP-350 test level 3 or Manual for Assessing Safety Hardware (MASH) will be accepted in lieu of the barrier shown. Drain slots shall be provided as needed or as directed by the Engineer. The Contractor shall furnish a certification of NCHRP Report 350 or Manual for Assessing Safety Hardware (MASH) compliance for any other types of precast barrier to be used. The certification shall state that the precast concrete barrier meets the requirements of NCHRP Report 350 or Manual for Assessing Safety Hardware (MASH) and include a copy of the Federal Highway Administration's (FHWA) approval letter with all attachments. Precast concrete barrier units shall be fabricated and installed in accordance with crash testing and documentation provided in the FHWA approval letter. Mixing of shapes will not be allowed in a continuous line of units.
 - Dowel holes in pavement or bridge slabs that are to remain in place shall be filled. Holes in concrete pavement and bridge slabs shall be filled with an approved non-shrink epoxy grout. Holes in asphalt pavement shall be filled with an approved asphalt joint filler. Payment for drilling and filling holes to be included in the price for various barrier items.
 - Attach Units To Roadway Surface with Stabilization Pins and to Deck Slabs using bolts when required.
 - A 4" White PVC Sleeve may be used to form the Lifting Hole and if used the Sleeve is to be left in place.

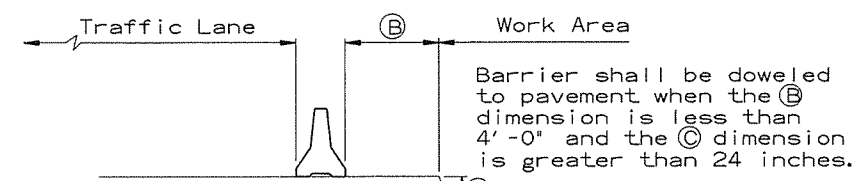
DATE	REVISION	FILMED
2-27-14	REVISED BARRIER STABILIZATION DETAIL	
10-15-09	ADDED REFERENCE TO MASH	
8-5-09	REV. NOTE 3 CONCERNING DRAIN SLOTS	
11-29-07	REVISED NOTE 3	
5-25-06	DELETED GENERAL NOTE 7	
11-18-04	REVISED BARRIER STABILIZATION DETAIL BRIDGE DECKS	
4-10-03	REVISED GENERAL NOTE 2	
8-22-02	ISSUED NEW DRAWING	

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER
STANDARD DRAWING TC-4

(A) 4 feet or greater preferred. If less than 4 feet, Precast Units shall be connected to slab (SEE BARRIER STABILIZATION DETAIL-BRIDGE DECKS STD. DRWG. TC-4)

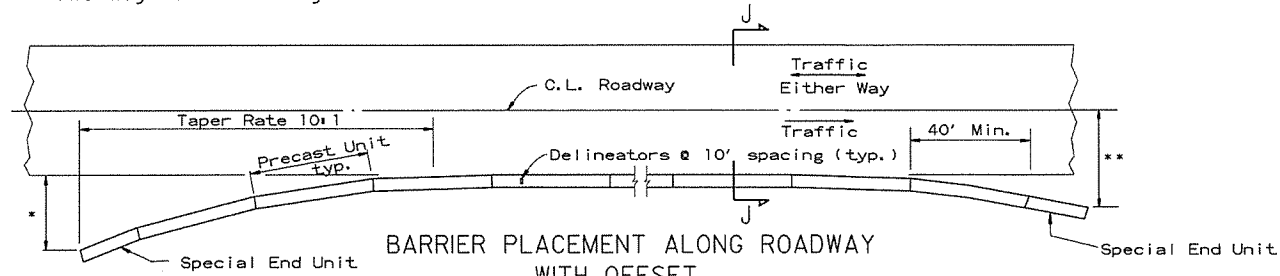


BARRIER PLACEMENT ALONG BRIDGE WITH OFFSET
No Scale



SECTION J-J
No Scale

** Offset Distance for Two Way Traffic Only



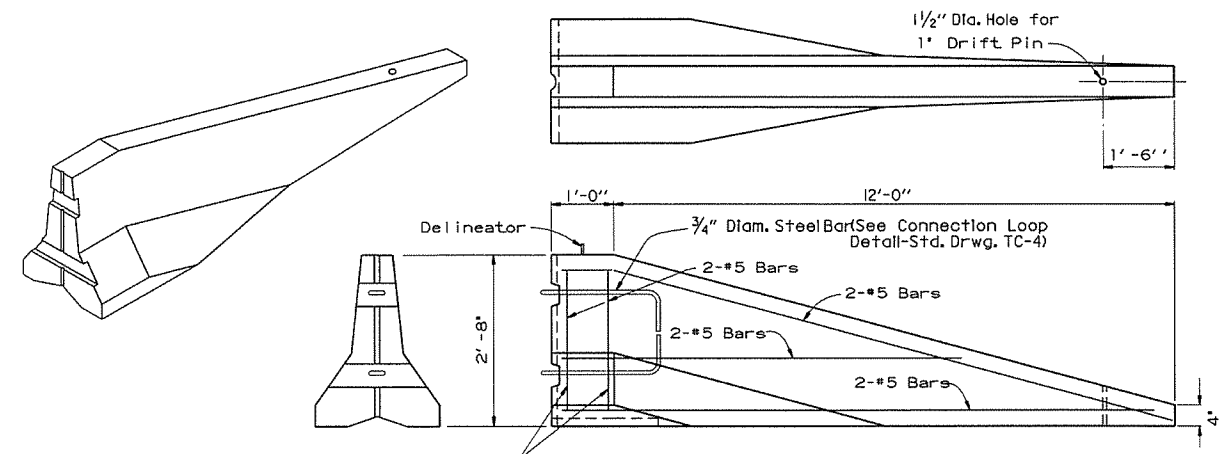
BARRIER PLACEMENT ALONG ROADWAY WITH OFFSET
No Scale

** Offset Distance For Two Way Traffic Only

* Offset Distance (See Table)

Speed (MPH)	Offset Distance (FT.)
≤ 45	12
> 45	18

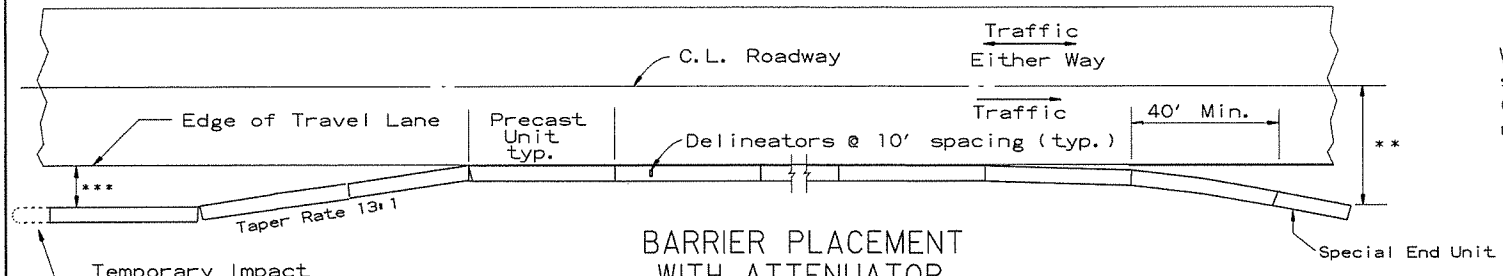
If offset distance is not attainable, then see 'Barrier Placement With Attenuator' Detail shown below.



SPECIAL END UNIT
No Scale

General Notes

When shown on the Plans, the ends of the Temporary Precast Concrete Barrier shall be protected with an NCHRP-350 or Manual For Assessing Safety Hardware (MASH) approved Crash Cushion. Payment for Crash Cushions shall be made under the item of "Temporary Impact Attenuation Barrier."



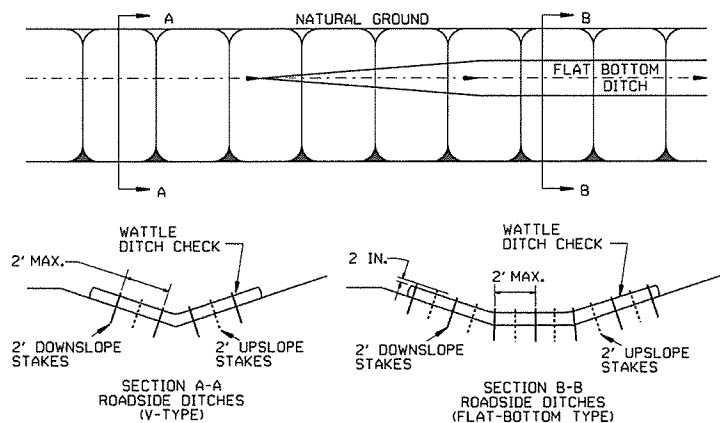
BARRIER PLACEMENT WITH ATTENUATOR
No Scale

** Offset Distance For Two Way Traffic Only

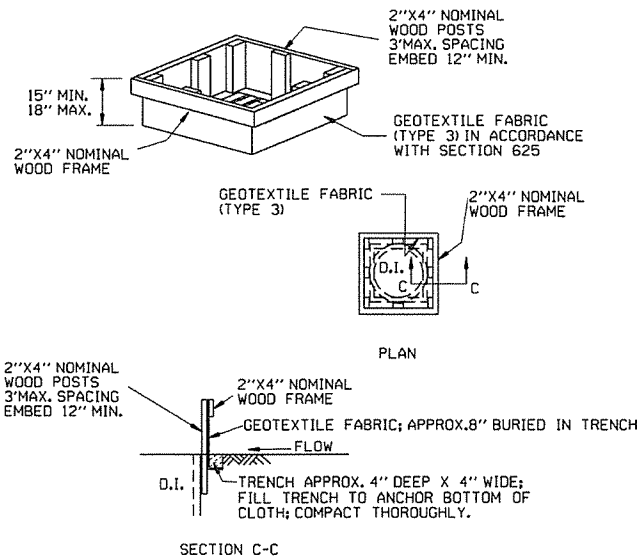
*** Min. 3'-0" From Edge of Travel Lane to Nearest Edge of Attenuator

			ARKANSAS STATE HIGHWAY COMMISSION
			STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER
			STANDARD DRAWING TC-5
10-15-09	ADDED REFERENCE TO MASH		
5-25-06	REVISED BARRIER PLACEMENT		
8-22-02	ISSUED NEW DRAWING		
DATE	REVISION	FILMED	

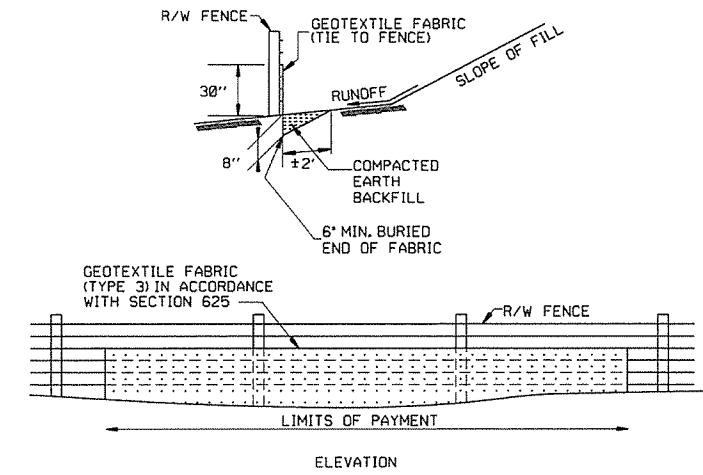
GENERAL NOTES
INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.



WATTLE DITCH CHECK (E-1)



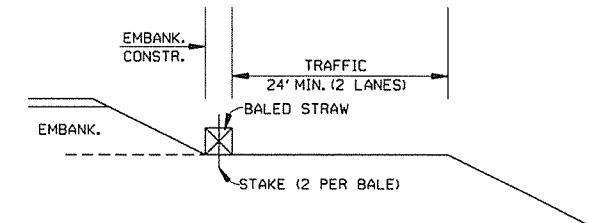
DROP INLET SILT FENCE (E-7)



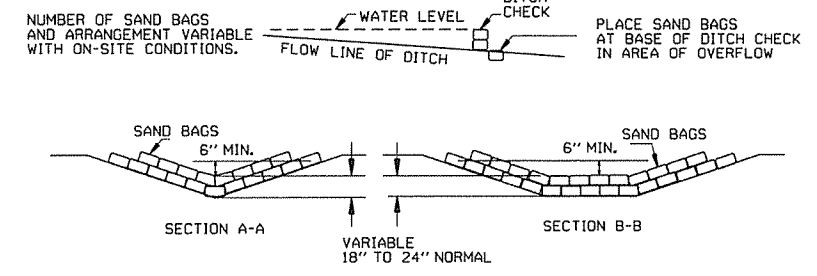
SILT FENCE ON R/W FENCE (E-4)

GENERAL NOTES
GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST. TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.

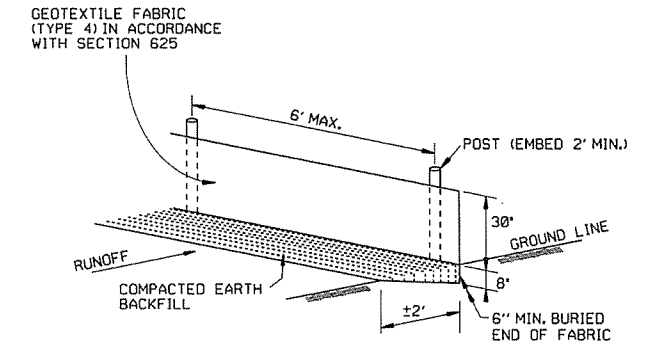
- GENERAL NOTES
1. STRAW BALES SHALL BE INSTALLED SO THAT THE BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES. THE BALES SHALL BE A MINIMUM OF 30 INCHES IN LENGTH.
 2. NO GAPS SHALL BE LEFT BETWEEN BALES.
 3. BALED STRAW FILTER BARRIERS COMPLETED AND ACCEPTED WILL BE MEASURED BY THE BALE IN PLACE AS AUTHORIZED BY THE ENGINEER AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER BALE FOR BALED STRAW DITCH CHECKS.



BALED STRAW FILTER BARRIER (E-2)

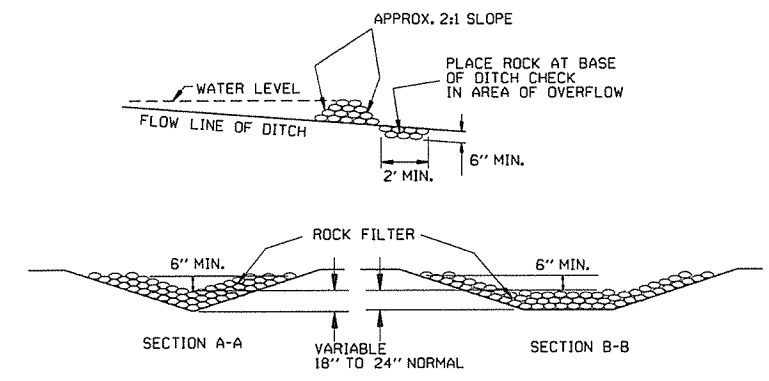


SAND BAG DITCH CHECK (E-5)



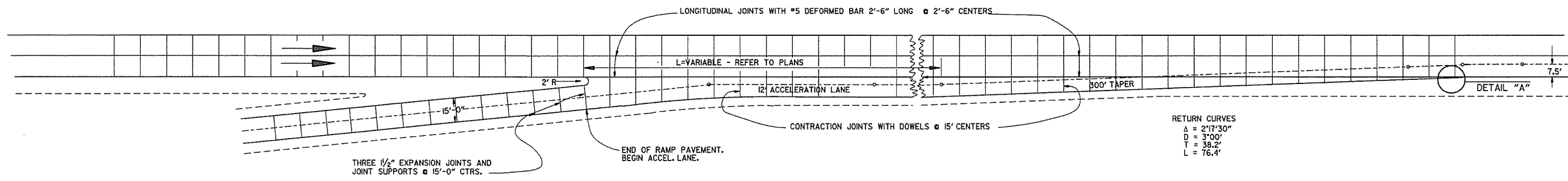
SILT FENCE (E-11)

GENERAL NOTES
GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



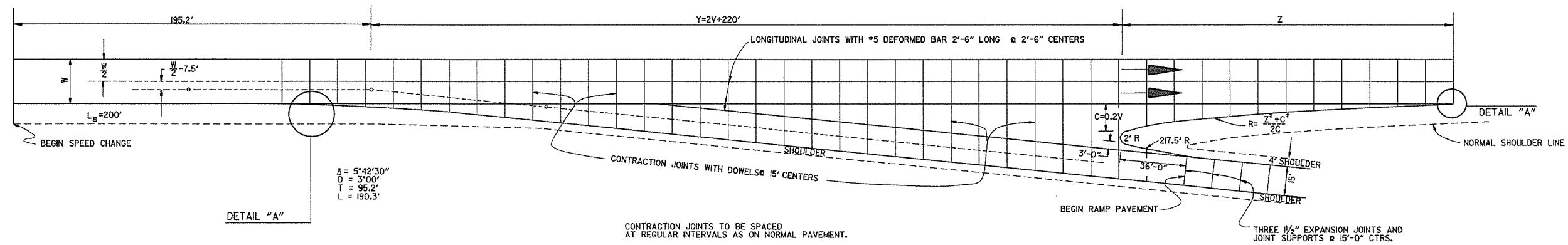
ROCK DITCH CHECK (E-6)

12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK		ARKANSAS STATE HIGHWAY COMMISSION
11-18-98	ADDED NOTES		
7-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)		
7-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95	TEMPORARY EROSION CONTROL DEVICES
7-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC		
6-2-94	REVISED E-1,4,7 & 11; DELETED E-2 & 3	6-2-94	
4-1-93	REDRAWN		
10-1-92	REDRAWN		
8-2-76	ISSUED R.D.M.	298-7-28-76	STANDARD DRAWING TEC-1
DATE	REVISION	FILMED	



ENTRANCE RAMP

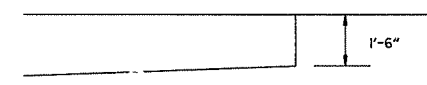
NOTE: JOINT SPACING ON THE MAIN LANES SHALL BE ADJUSTED AS NECESSARY TO CONFORM TO THESE JOINT LAYOUTS. THE MAIN LANE JOINT SPACING MAY BE REDUCED TO A 12' MINIMUM.



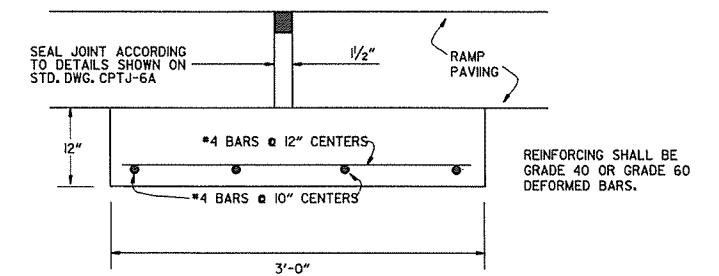
EXIT RAMP

EXIT RAMP

DESIGN SPEED V	Y	NOSE OFFSET C	LENGTH NOSE TAPER Z	RETURN RADIUS R	ADDITIONAL SURFACING SQ. YDS.
40	300.0	8.0	96.0	580.0	602.43
50	320.0	10.0	120.0	725.0	687.29
60	340.0	12.0	168.0	1182.0	790.55
70	360.0	14.0	210.0	1582.0	902.27



DETAIL "A"



DETAIL OF EXPANSION JOINT & JOINT SUPPORT

NOTE: THE EXPANSION JOINTS SHALL BE MEASURED AND PAID FOR AS P.C.C. PAVEMENT (RAMP THICKNESS). WHEN RAMP PAVING IS ASPHALT, EXPANSION JOINT IS NOT REQUIRED. THE JOINT SUPPORT MAY BE CONSTRUCTED WITH CLASS "A", "S", OR PAVING CONCRETE. PAYMENT FOR THE JOINT SUPPORT SHALL BE FOR THE CONTRACT UNIT PRICE BID FOR THE CLASS OF CONCRETE USED. ALL OTHER WORK AND MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE JOINT SUPPORT SHALL BE INCLUDED IN THE PRICE BID FOR THE ABOVE ITEMS.

DATE	REVISION	DATE FILM'D
8-22-02	DELETED NOTE	
11-16-01	CORRECTED SPELLING ON ENTRANCE RAMP NOTE	
5-13-99	ADDED, EDITED AND DELETED NOTES	
11-03-94	ADDED NOTE RE: REINF. BARS	
10-1-92	ADDED DETAIL A & OTHER MINOR CHANGES	10-1-92
1-25-90	REVISED EXPANSION JOINT	1-25-90
7-15-88	CONFORM TO 1988 SPECIFICATIONS	65C-7-15-88
3-2-81	ISSUED	511-10-2-72

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF STANDARD TURNOUT

FOR

ENTRANCE & EXIT RAMPS (NON-REINFORCED)

STANDARD DRAWING TR-1A