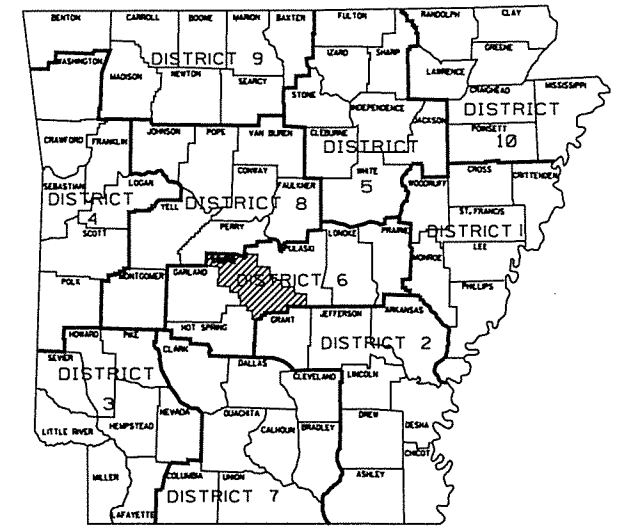


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				6	ARK.			
						JOB NO.	BB0602	1
						HWY. 70 - WEST (WESTBOUND LANES) (F)		

"A FULLY CONTROLLED ACCESS FACILITY"
ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT
CONSTRUCTION PLANS FOR STATE HIGHWAY
HWY. 70-WEST (WESTBOUND LANES) (F)
SALINE COUNTY
JOB BB0602
F.A.P. BIM-PEN-B30-0(211)
ROUTE 30 SECTION 22



ARK. HWY. DIST. NO. 6

DESIGN TRAFFIC DATA

DESIGN YEAR-----	2035
2015 ADT-----	39,000
2035 ADT-----	53,000
2035 DHV-----	5,830
DIRECTIONAL DISTRIBUTION-----	60%
TRUCKS-----	40%
DESIGN SPEED-----	70 MPH

EQUATION (C WB I-30)

▲ STA. 5969+05.40 BK. =
STA. 236+32.50 AHD.

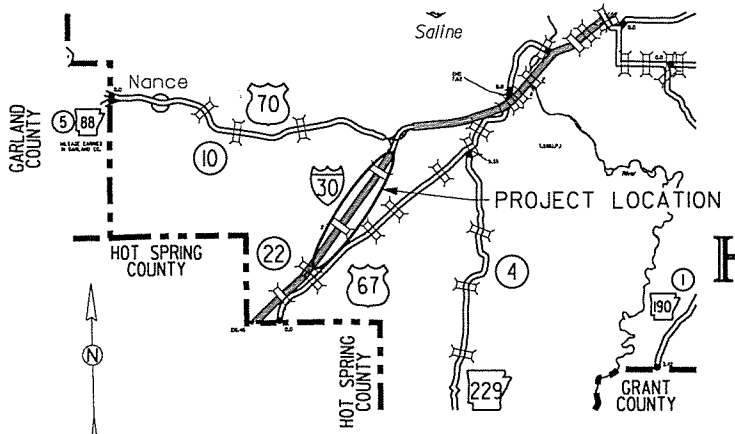
EQUATION (C EB I-30)

▲ STA. 5851+89.00 BK. =
STA. 5851+73.40 AHD.

▲ STA. 5969+35.80 BK. =
STA. 236+32.50 AHD.

C WB I-30 STA. 5964+88.46
END JOB BB0602
(LOG MILE 110.56)

C WB I-30 STA. 5811+71.49
BEGIN JOB BB0602
(LOG MILE 107.64)



VICINITY MAP

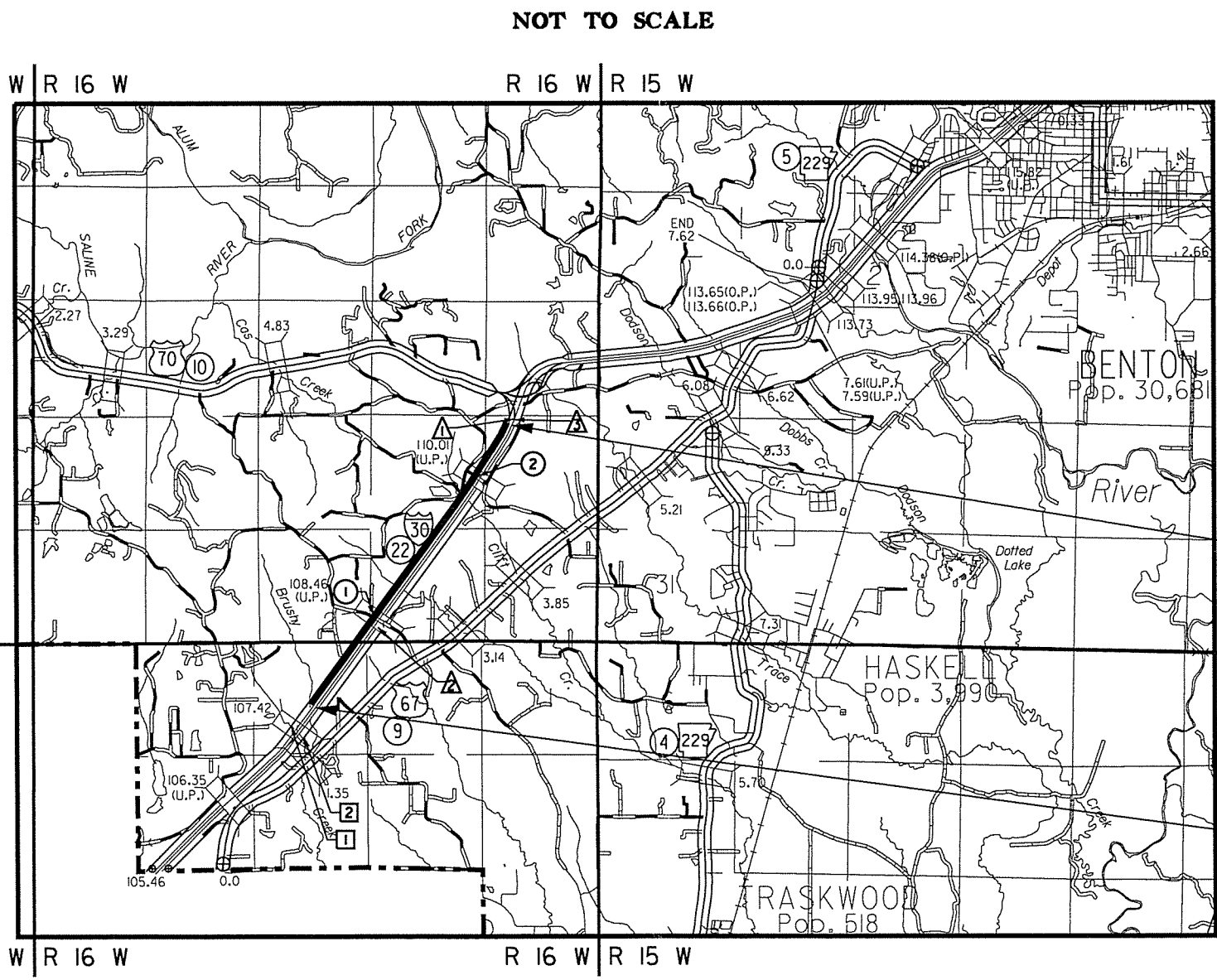
BRIDGE STRUCTURES

- ① C POINT VIEW RD. STA. 11+41.90 BRIDGE END
127'-2" R.C. SLAB SPANS
STEEL BEAMS, SIMPLE SPANS 38' - 66'
BRIDGE NO. 03248
(NO SKEW)
26' CLEAR ROADWAY WIDTH
C POINT VIEW RD. STA. 12+69.06 BRIDGE END
RETAIN
- ② C CROSSROADS STA. 13+28.22 BRIDGE END
210'-2" R.C. SLAB SPAN
STEEL BEAMS, SIMPLE SPANS 46' - 87'
BRIDGE NO. 03249
(NO SKEW)
24' CLEAR ROADWAY WIDTH
C CROSSROADS STA. 15+22.38 BRIDGE END
RETAIN

**STRUCTURES OVER
20'-0" SPAN**

- ① C WB I-30 STA. 5801+81 IN PLACE
DOUBLE 8'x8' R.C. BOX CULVERT
(LENGTH AND SKEW UNKNOWN)
SPAN = 23.81'
RETAIN
- ② C EB I-30 STA. 5799+81 IN PLACE
DOUBLE 8'x8' R.C. BOX CULVERT
(LENGTH AND SKEW UNKNOWN)
SPAN = 23.81'
RETAIN

BEGIN-POINT OF PROJECT
 LAT. 34°29'45" N
 LONG. 92°42'33" W
MID-POINT OF PROJECT
 LAT. 34°30'47" N
 LONG. 92°41'39" W
END-POINT OF PROJECT
 LAT. 34°31'48" N
 LONG. 92°40'46" W



LENGTH COMPUTED ALONG C WB I-30

GROSS LENGTH OF PROJECT	15,316.97 FT. OR	2.901 MI.
NET LENGTH OF ROADWAY	15,316.97 FT. OR	2.901 MI.
NET LENGTH OF BRIDGES	0.00 FT. OR	0.000 MI.
NET LENGTH OF PROJECT	15,316.97 FT. OR	2.901 MI.

P.E. JOB BB0602

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRINAZ
 11-19-2014

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 11/19/2014
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1-12-15				6	ARK.			
1-15-15								
				JOB NO.		BBO602	2	114

INDEX, GOVERNING SPECS., & GENERAL NOTES

SHEET NO.	TITLE	DWG. NO.	DATE
1	TITLE SHEET		
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES		
3 - 6	TYPICAL SECTIONS OF IMPROVEMENT		
7 - 11	SPECIAL DETAILS		
12 - 17	TEMPORARY EROSION CONTROL DETAILS		
18 - 26	MAINTENANCE OF TRAFFIC		
27 - 61	MAINTENANCE OF TRAFFIC DETAILS		
62 - 65	PERMANENT PAVEMENT MARKING DETAILS		
66 - 76	QUANTITIES		
77 - 78	SUMMARY OF QUANTITIES AND REVISIONS		
79 - 84	PLAN SHEETS		
85	CONCRETE DITCH PAVING	CDP-1	11/17/10
86	TRANSVERSE & LONGITUDINAL JOINTS FOR CONCRETE PAVEMENT (NON-REINFORCED)	CPTJ-6A	5/25/06
87	GUARD RAIL DETAILS	GR-8	7/14/10
88	GUARD RAIL DETAILS	GR-8A	7/14/10
89	GUARD RAIL DETAILS	GR-9	4/17/08
90	GUARD RAIL DETAILS	GR-9A	4/17/08
91	GUARD RAIL DETAILS	GR-10	7/14/10
92	GUARD RAIL DETAILS	GR-10A	7/14/10
93	CONCRETE BARRIER WALL (PIER PROTECTION TYPE A)	GR-11	7/14/10
94	GUARD RAIL DETAILS	GRT-1	7/14/10
95	IMPACT ATTENUATION BARRIER	IB-1	10/15/09
96	PAVEMENT MARKING DETAILS	PM-1	9/12/13
97	PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS	PM-2	9/12/13
98	TABLES AND METHOD OF SUPERELEVATION FOR ONE-WAY TRAFFIC	SE-1	1/9/87
99	STANDARD HIGHWAY SIGNS AND SUPPORT ASSEMBLIES	SHS-1	9/12/13
100	U-CHANNEL POST ASSEMBLIES	SHS-2	2/27/14
101	DETAIL OF BREAKAWAY SIGN SUPPORTS FOR GUIDE SIGNS	SHS-3	9/12/13
102	DETAIL OF BREAKAWAY SIGN SUPPORTS FOR STANDARD SIGNS	SHS-4	9/12/13
103	DETAILS OF GUIDE SIGN PANELS	SHS-5	9/12/13
104	MOUNTING DETAILS FOR DEMOUNTABLE LEGEND ON GUIDE SIGNS	SHS-6	9/12/13
105	DETAIL OF OMNI-DIRECTIONAL BREAKAWAY SIGN SUPPORTS	SHS-7	9/12/13
106	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-1	12/15/11
107	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-2	9/12/13
108	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-3	10/15/09
109	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER	TC-4	2/27/14
110	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER	TC-5	10/15/09
111	TEMPORARY EROSION CONTROL DEVICES	TEC-1	12/15/11
112	TEMPORARY EROSION CONTROL DEVICES	TEC-2	6/2/94
113	TEMPORARY EROSION CONTROL DEVICES	TEC-3	11/3/94
114	DETAILS OF STANDARD TURNOUT FOR ENTRANCE & EXIT RAMP (NON-REINFORCED)	TR-1A	8/22/02

GENERAL NOTES

- GRADE LINE DENOTES FINISHED GRADE WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A FENCE TO CONTROL LIVESTOCK IN AREAS WHERE PASTURES ARE SEVERED. WIRE FENCE MAY BE CONSTRUCTED INITIALLY, OR IN LIEU THEREOF, THE CONTRACTOR AT HIS OWN EXPENSE, MAY ELECT TO PROVIDE TEMPORARY FENCING SUITABLE TO CONTAIN LIVESTOCK.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210-UNCLASSIFIED EXCAVATION.

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - TRAINING PROGRAM - JOB BBO602
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL - AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
108-1	LIQUIDATED DAMAGES
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
620-1	MULCH COVER
JOB BBO602	ASPHALT CONCRETE HOT MIX PATCHING OF EXISTING SHOULDERS
JOB BBO602	ASSESSMENT OF WORKING DAYS - REVISED "CALENDAR DAY"
JOB BBO602	AUTOMATED WORK ZONE INFORMATION SYSTEM
JOB BBO602	BORROW
JOB BBO602	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB BBO602	BROADBAND INTERNET SERVICE FOR FIELD OFFICE
JOB BBO602	CONCRETE DITCH PAVING
JOB BBO602	CULVERT CLEAN OUT
JOB BBO602	DISADVANTAGED BUSINESS ENTERPRISE BIDDER'S RESPONSIBILITIES
JOB BBO602	EMPLOYMENT REPORTING
JOB BBO602	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB BBO602	HIGH PERFORMANCE PAVEMENT MARKING
JOB BBO602	MAINTENANCE OF TRAFFIC
JOB BBO602	MANDATORY USE OF INTERNET BIDDING
JOB BBO602	PARTNERING REQUIREMENTS
JOB BBO602	PORTABLE CAMERA ASSEMBLY
JOB BBO602	PROSECUTION AND PROGRESS
JOB BBO602	REMOVAL AND DISPOSAL OF GUARDRAIL
JOB BBO602	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIERS
JOB BBO602	REMOVING EXISTING PORTLAND CEMENT CONCRETE PAVEMENT
JOB BBO602	ROADWAY CONSTRUCTION CONTROL
JOB BBO602	RUMBLE STRIP REMOVAL
JOB BBO602	SEQUENCE OF CONSTRUCTION
JOB BBO602	SITE USE (A+C METHOD)
JOB BBO602	SOIL STABILIZATION
JOB BBO602	STORM WATER POLLUTION PREVENTION PLAN
JOB BBO602	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB BBO602	TEMPORARY PORTABLE RUMBLE STRIPS
JOB BBO602	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB BBO602	TRAFFIC CONTROL SUPERVISOR
JOB BBO602	TRENCHING AND SHOULDER PREPARATION
JOB BBO602	UTILITY ADJUSTMENTS
JOB BBO602	VALUE ENGINEERING
JOB BBO602	WARM MIX ASPHALT
JOB BBO602	WIRE ROPE SAFETY FENCE (POST REPAIR)
JOB BBO602	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS
JOB BBO602	WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS
JOB BBO602	WRSF TRAINING WORKSHOP

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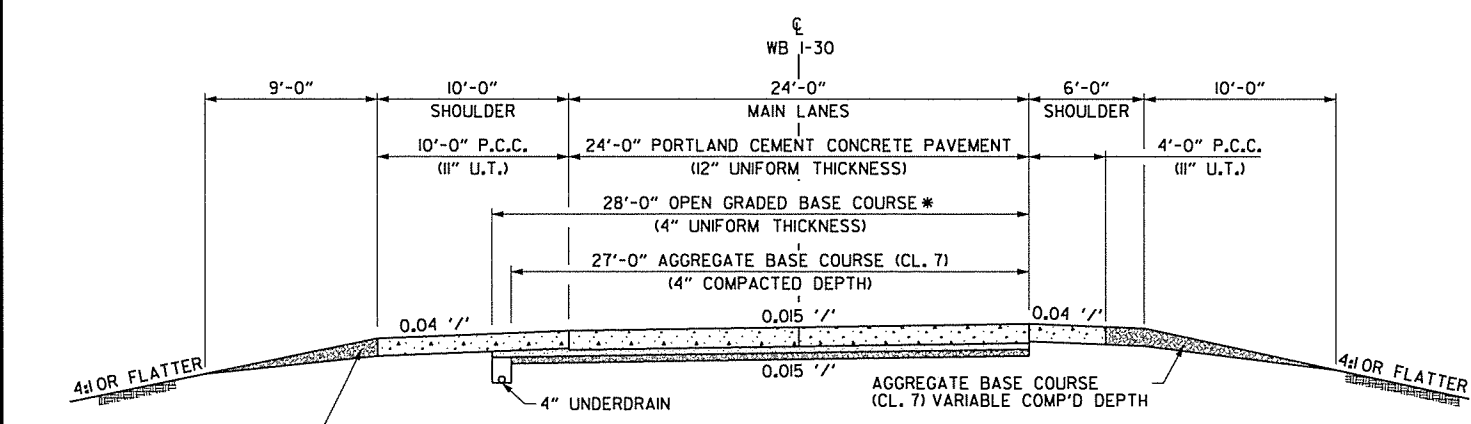
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				JOB NO.	BB0602	4	114	

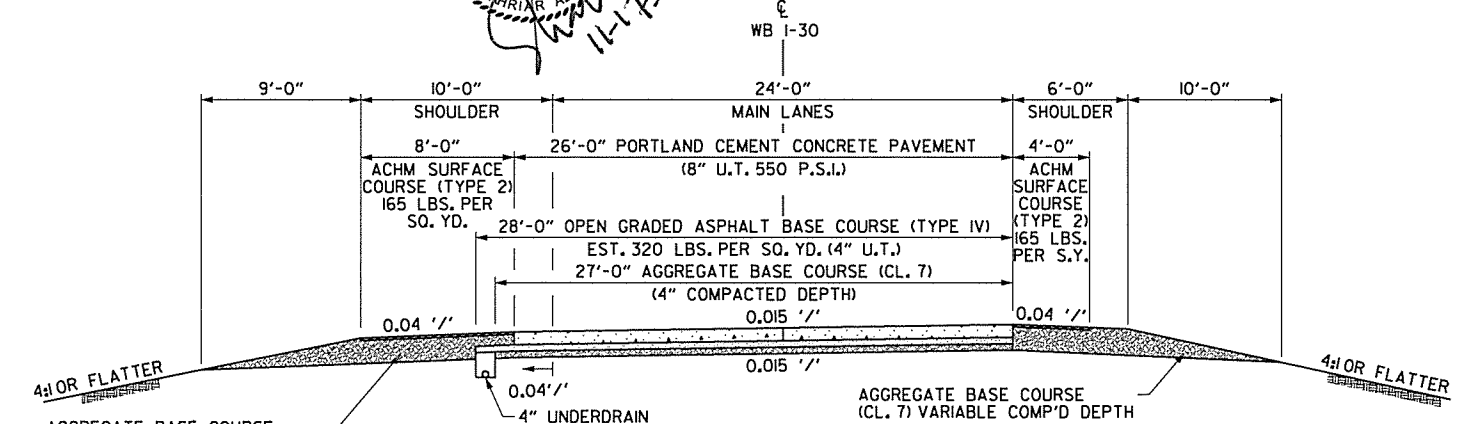
② TYPICAL SECTIONS OF IMPROVEMENT



AGGREGATE BASE COURSE (CL. 7) VARIABLE COMP'D DEPTH

* OPEN GRADED BASE COURSE IS ONE OF THE FOLLOWING ALTERNATIVES:
 ALT. 1 - OPEN GRADED ASPHALT BASE COURSE (TYPE IV) @ 4" U.T. - EST. 320 LBS. PER SQ. YD.
 ALT. 2 - OPEN GRADED PORTLAND CEMENT CONCRETE BASE COURSE @ 4" U.T.

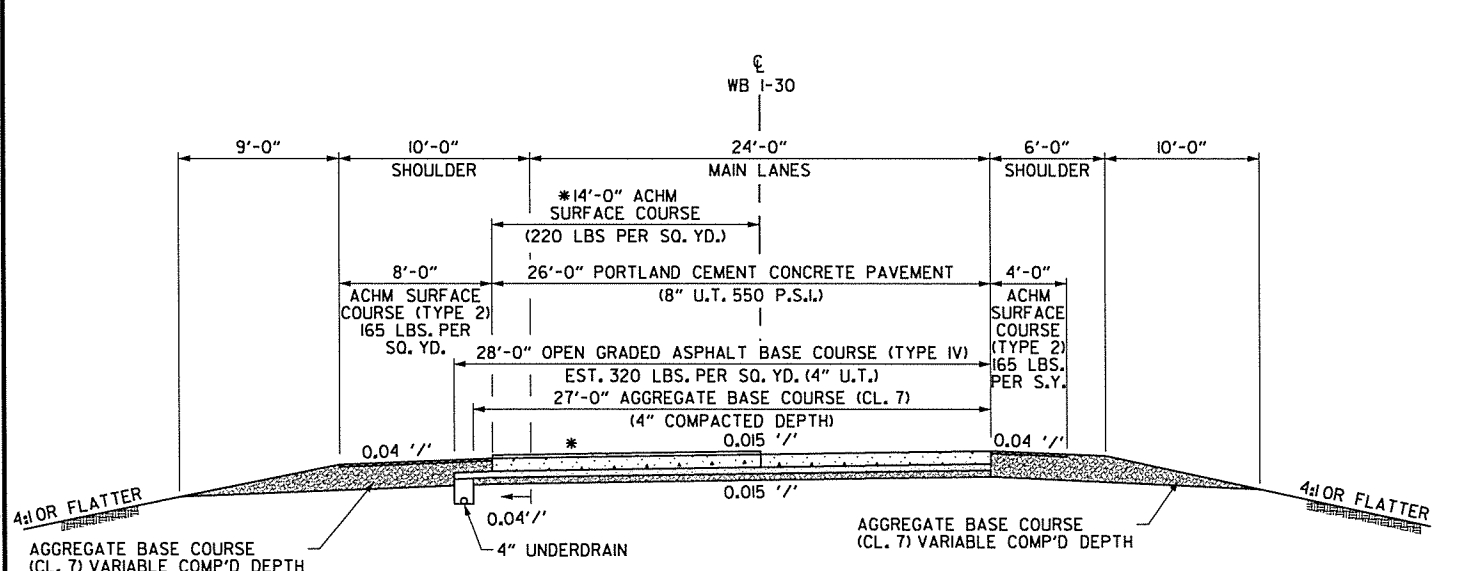
EXISTING TYPICAL SECTION
CL WB I-30 STA. 5811+71.49 TO STA. 5819+50.00
 SCALE: N.T.S.



AGGREGATE BASE COURSE (CL. 7) VARIABLE COMP'D DEPTH

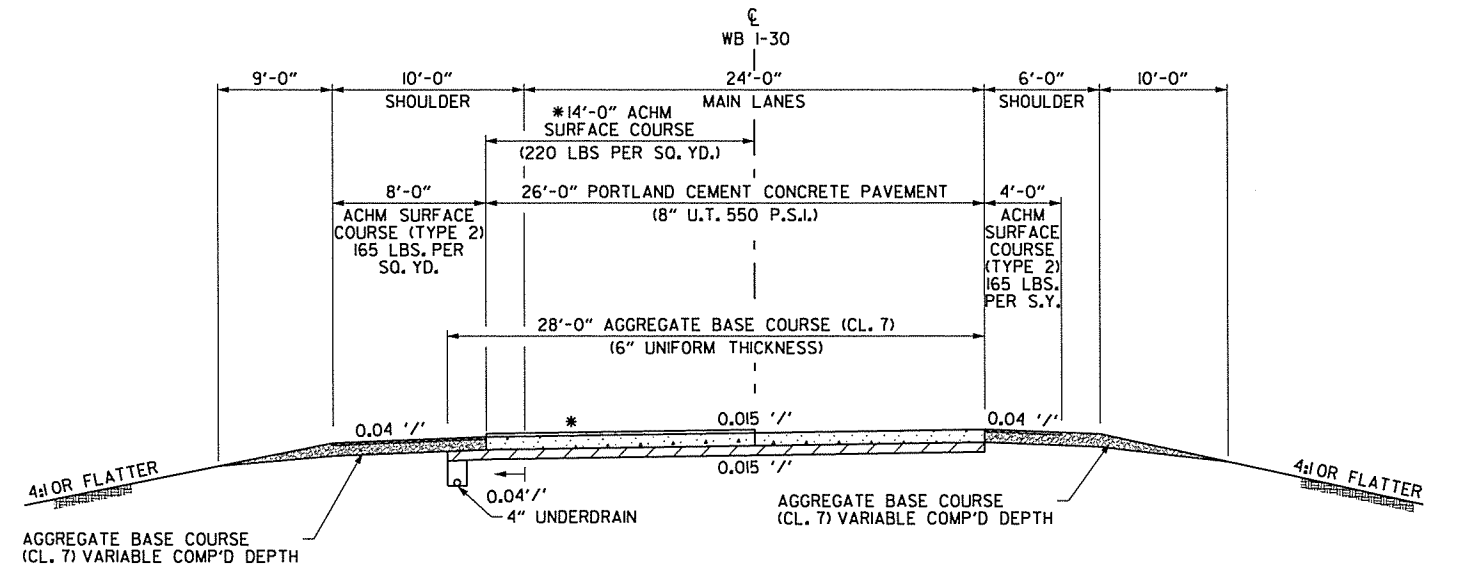
EXISTING TYPICAL SECTION
CL WB I-30 STA. 5819+50.00 TO STA. 5831+50.00
TRANSITION FROM CL WB I-30 STA. 5831+50 TO STA. 5844+21.00
 SCALE: N.T.S.

NOTE: THE EXISTING PAVEMENT SECTIONS ARE BASED ON AVAILABLE INFORMATION AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION.



AGGREGATE BASE COURSE (CL. 7) VARIABLE COMP'D DEPTH

EXISTING TYPICAL SECTION
CL WB I-30 STA. 5844+21.00 TO STA. 5847+00.00
 SCALE: N.T.S.



AGGREGATE BASE COURSE (CL. 7) VARIABLE COMP'D DEPTH

EXISTING TYPICAL SECTION
CL WB I-30 STA. 5847+00.00 TO STA. 5853+51.00
 SCALE: N.T.S.

* 2" ASPHALT PAVEMENT ADDED AFTER VISUAL INSPECTION OF EXISTING PAVEMENT. ACCURACY SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION.

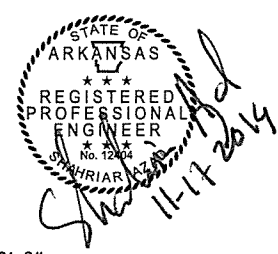
* 2" ASPHALT PAVEMENT ADDED AFTER VISUAL INSPECTION OF EXISTING PAVEMENT. ACCURACY SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION.

TYPICAL SECTIONS OF IMPROVEMENT

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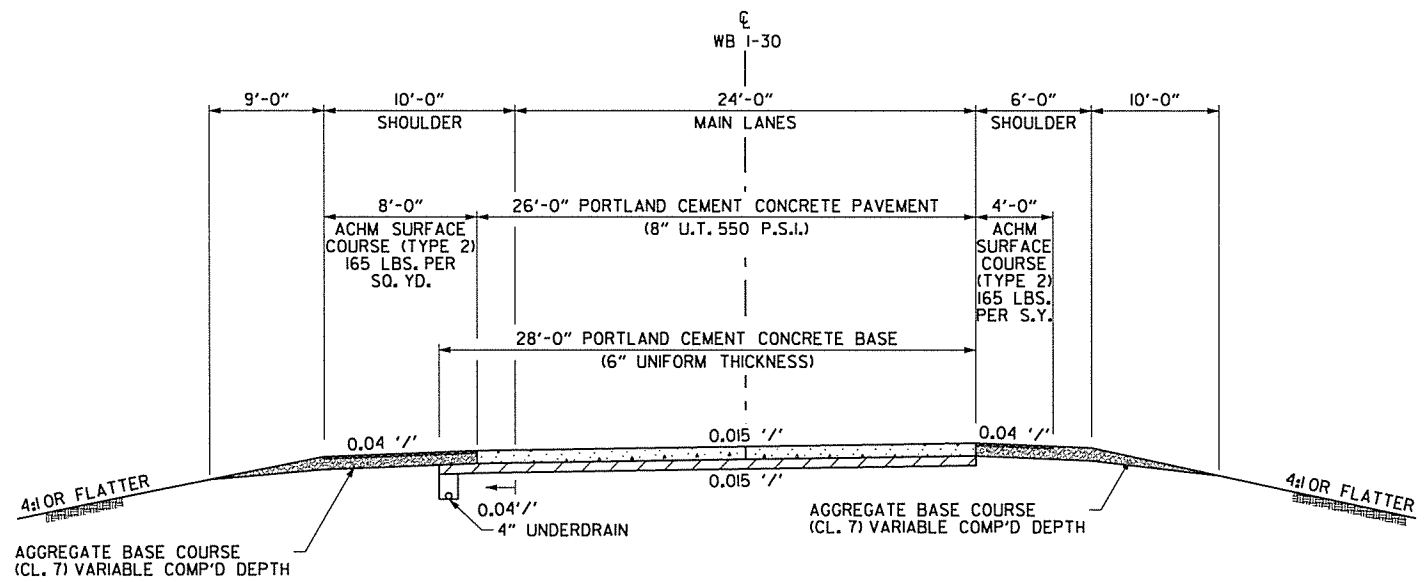
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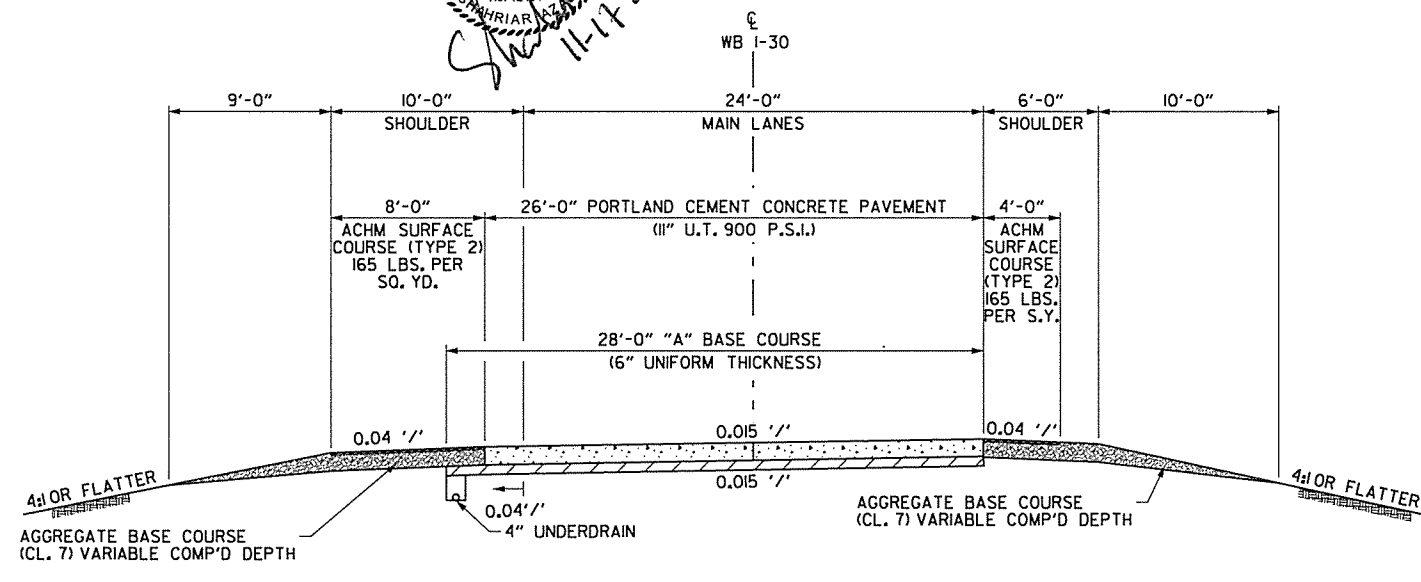


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				6	ARK.			
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② TYPICAL SECTIONS OF IMPROVEMENT



EXISTING TYPICAL SECTION
 TRANSITION CL WB I-30 STA. 5853+51.00 TO STA. 5857+00.00
 CL WB I-30 STA. 5881+00.00 TO STA. 5887+00.00
 TRANSITION CL WB I-30 STA. 5887+00.00 TO STA. 5896+00.00
 SCALE: N.T.S.

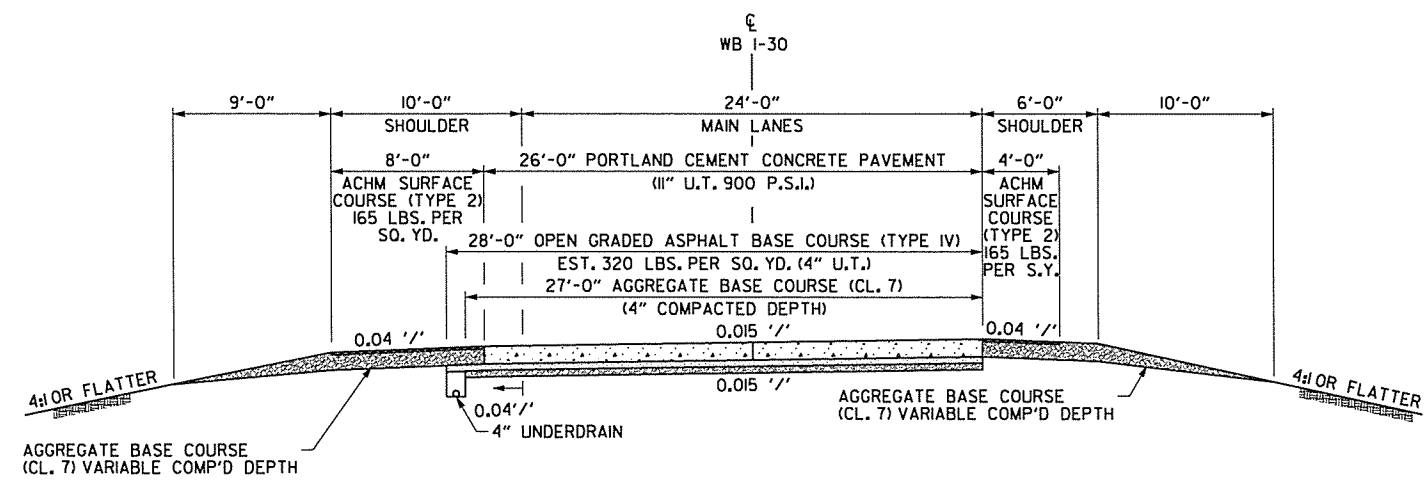


EXISTING TYPICAL SECTION
 CL WB I-30 STA. 5857+00.00 TO STA. 5863+00.00
 CL WB I-30 STA. 5872+00.00 TO STA. 5878+00.00
 SCALE: N.T.S.

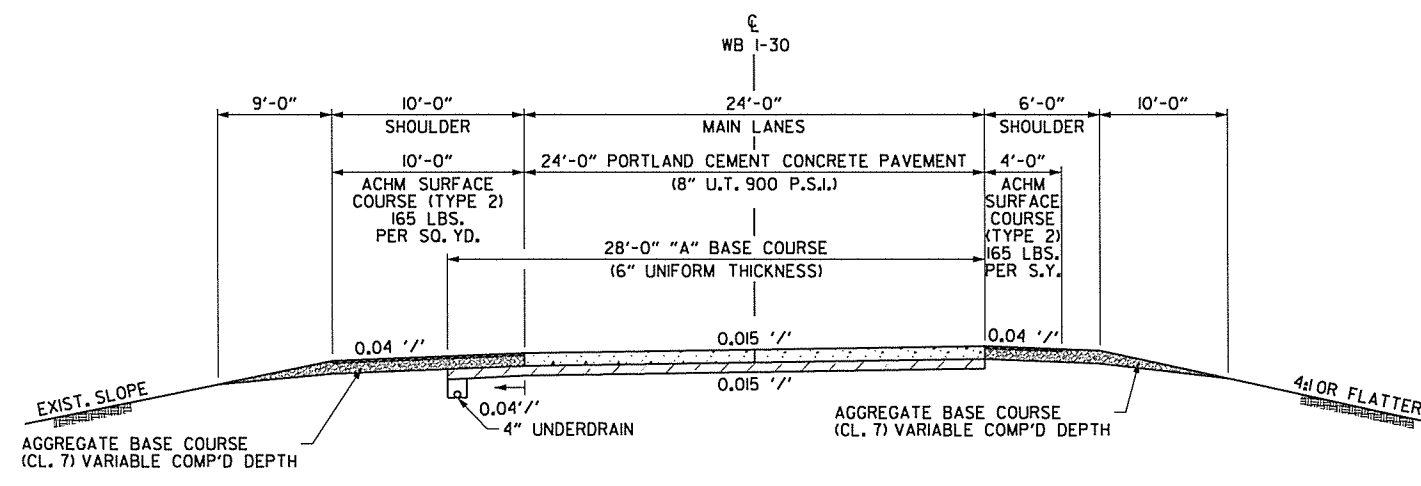
NOTE: THE EXISTING PAVEMENT SECTIONS ARE BASED ON AVAILABLE INFORMATION AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION.

TABLE OF VARIABLES

STATION	STATION	A
TRANSITION 5857+00	5863+00	AGGR. BASE (CL. 7)
TRANSITION 5863+00	5865+00	
TRANSITION 5871+00	5872+00	
TRANSITION 5872+00	5878+00	PORT. CEM. CONC.
TRANSITION 5878+00	5881+00	



EXISTING TYPICAL SECTION
 CL WB I-30 STA. 5865+00.00 TO STA. 5871+00.00
 SCALE: N.T.S.



EXISTING TYPICAL SECTION
 CL WB I-30 STA. 5896+00.00 TO STA. 5902+00.00
 CL WB I-30 STA. 5928+50.00 TO STA. 5934+50.00
 SCALE: N.T.S.

TABLE OF VARIABLES

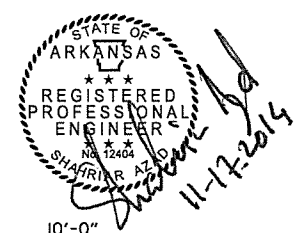
STATION	STATION	A
TRANSITION 5896+00	5902+00	PORT. CEM. CONC.
TRANSITION 5902+00	5905+00	
TRANSITION 5928+50	5934+50	AGGR. BASE (CL. 7)
TRANSITION 5934+50	5946+00	

TYPICAL SECTIONS OF IMPROVEMENT

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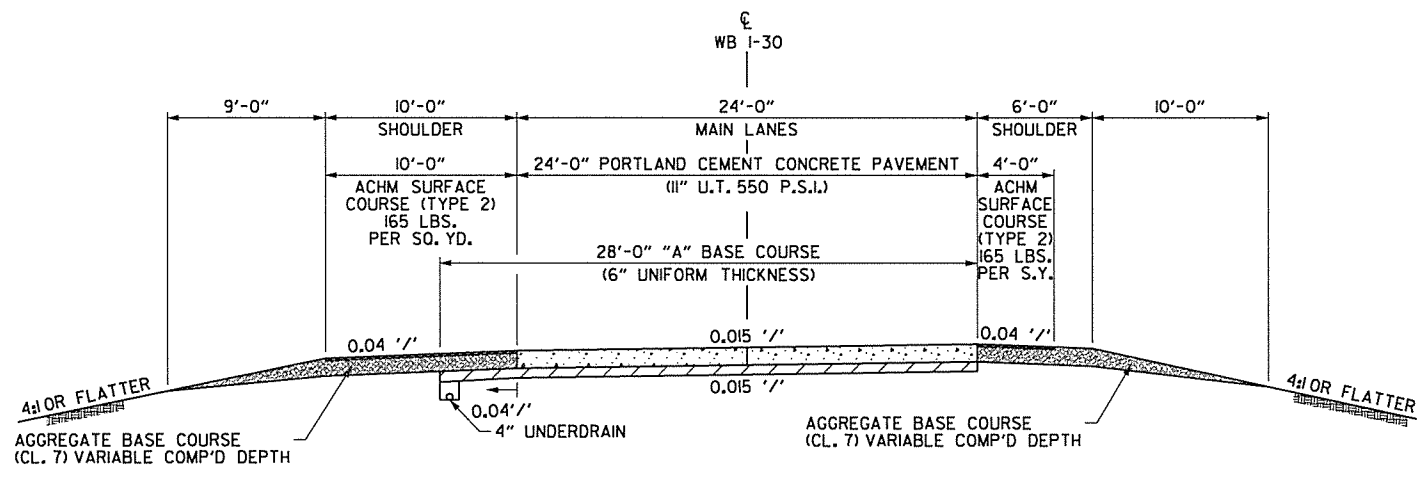
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				6	ARK.		6	114
				JOB NO.		BBO602		

② TYPICAL SECTIONS OF IMPROVEMENT

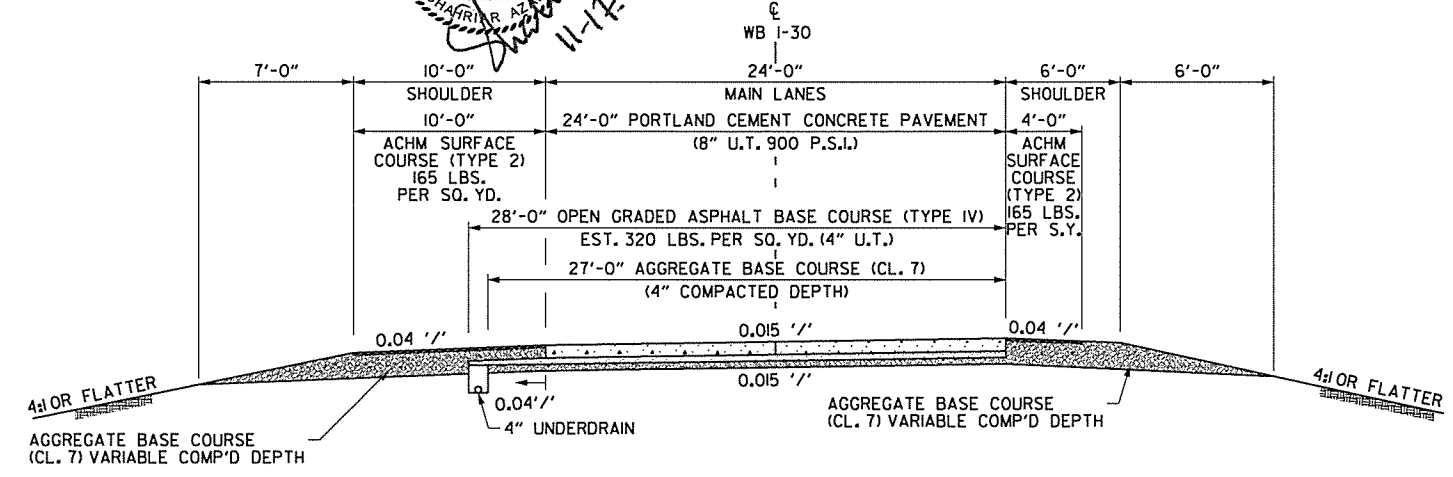


EXISTING TYPICAL SECTION
CL WB I-30 STA. 5905+00.00 TO STA. 5911+00.00
CL WB I-30 STA. 5954+00.00 TO STA. 5960+00.00
 SCALE: N.T.S.

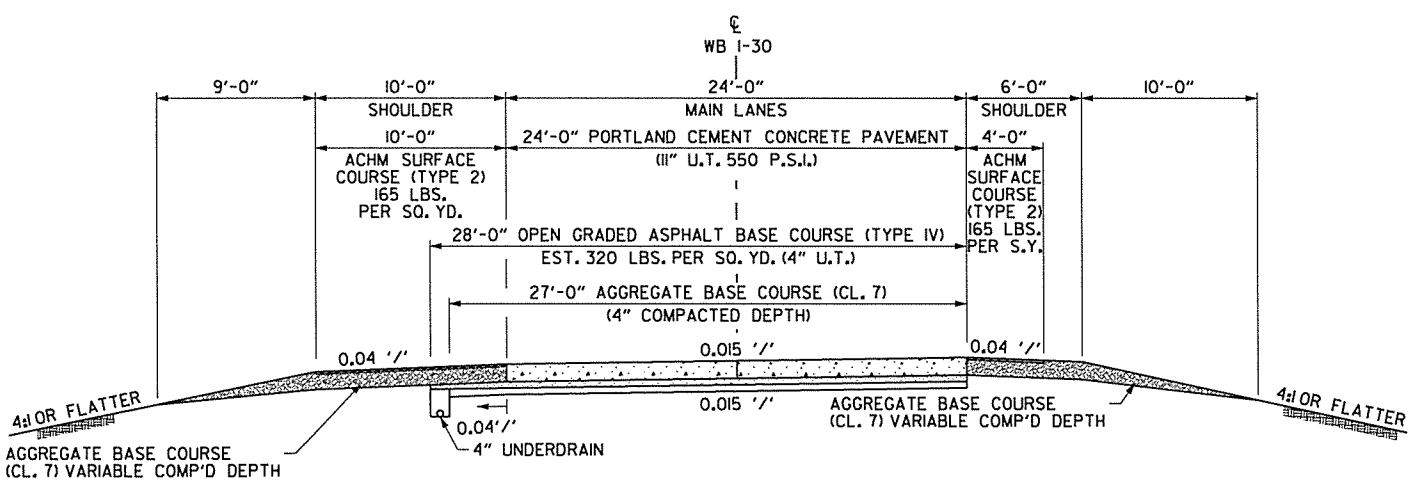
TABLE OF VARIABLES

STATION	STATION	A
5905+00	5911+00	PORT. CEM. CONC.
TRANSITION	5911+00	5918+50
	5954+00	5960+00
TRANSITION	5960+00	5961+00

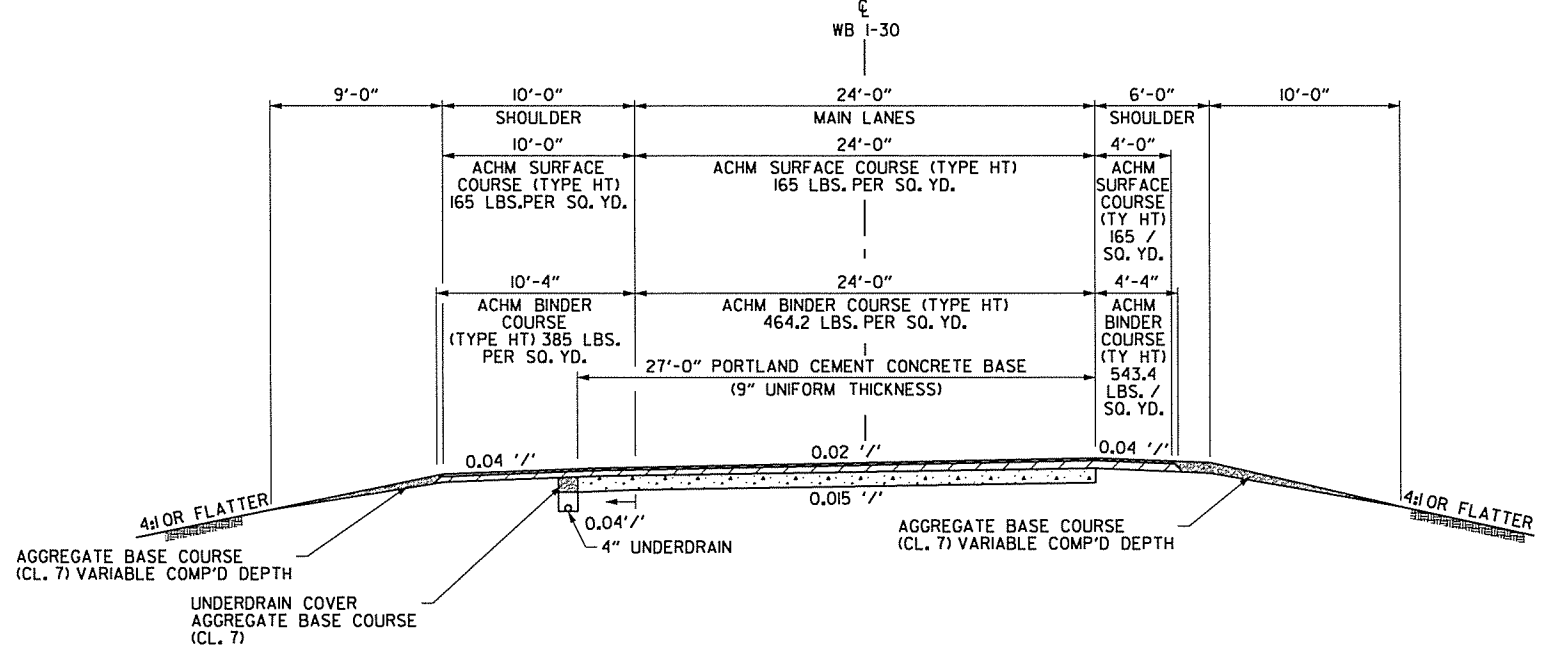
NOTE: THE EXISTING PAVEMENT SECTIONS ARE BASED ON AVAILABLE INFORMATION AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION.



EXISTING TYPICAL SECTION
CL WB I-30 STA. 5918+50.00 TO STA. 5924+50.00
TRANSITION CL WB I-30 STA. 5924+50.00 TO STA. 5928+50.00
 SCALE: N.T.S.



EXISTING TYPICAL SECTION
CL WB I-30 STA. 5946+00.00 TO STA. 5952+00.00
TRANSITION CL WB I-30 STA. 5952+00.00 TO STA. 5954+00.00
 SCALE: N.T.S.

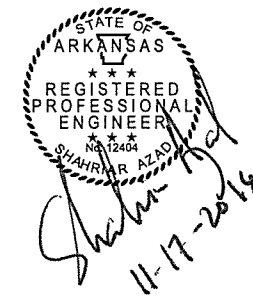


EXISTING TYPICAL SECTION
CL WB I-30 STA. 5961+00.00 TO STA. 5964+88.46
 SCALE: N.T.S.

TYPICAL SECTIONS OF IMPROVEMENT

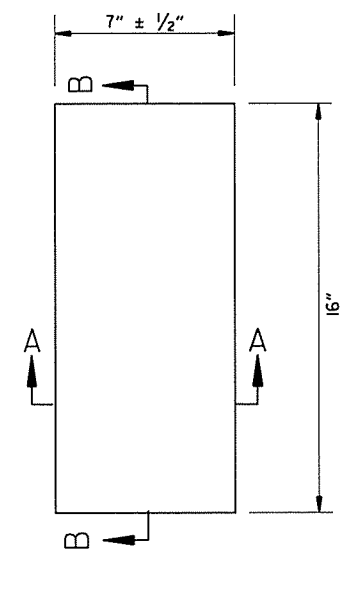
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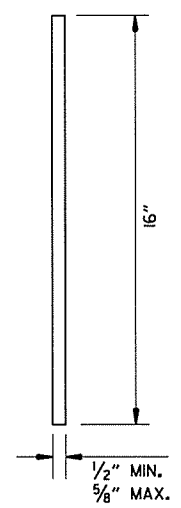


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				6	ARK.			
				JOB NO.	BB0602		7	114

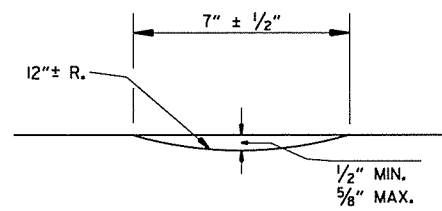
② SPECIAL DETAILS



PLAN

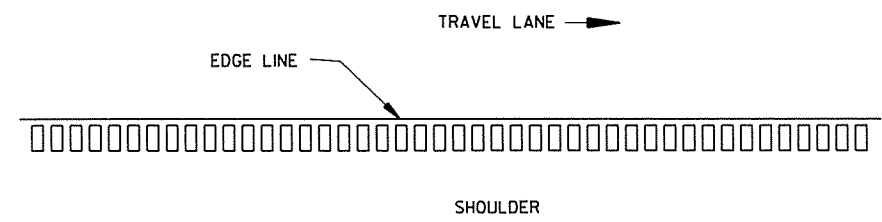
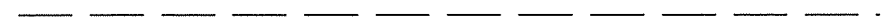
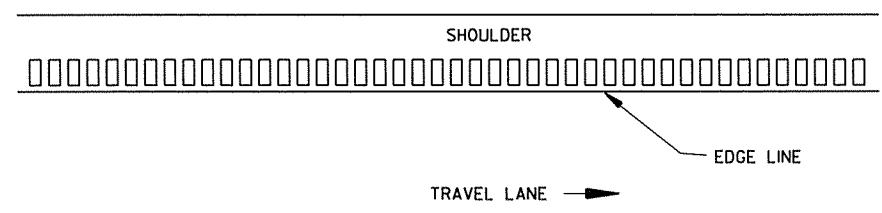


SECTION B-B

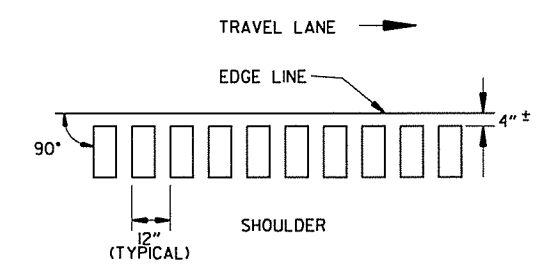


SECTION A-A

DETAILS OF RUMBLE STRIPS

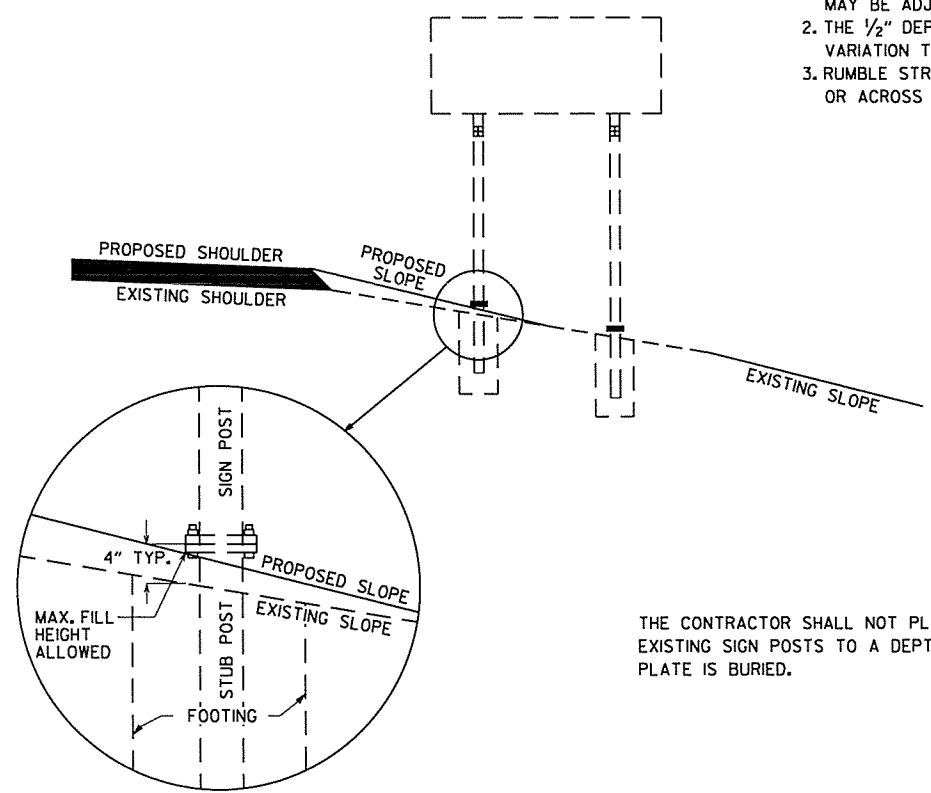


PLAN VIEW



LOCATION PLAN OF RUMBLE STRIPS LEFT OR RIGHT SHOULDER

- NOTES:
1. ALIGNMENT OF RUMBLE STRIPS SHALL GENERALLY BE STRAIGHT AND OFFSET APPROXIMATELY 4" FROM THE OUTER EDGE OF THE EDGE LINE. THIS OFFSET MAY BE ADJUSTED TO ACCOMMODATE VARIATIONS IN THE EDGE LINE.
 2. THE 1/2" DEPTH SHALL GENERALLY APPLY FOR THE ENTIRE 16' LENGTH. SOME VARIATION TO SUIT SHOULDER SLOPE BREAKS MAY BE NECESSARY.
 3. RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS, APPROACH SLABS, OR ACROSS TRANSVERSE JOINTS OF CONCRETE SHOULDERS.



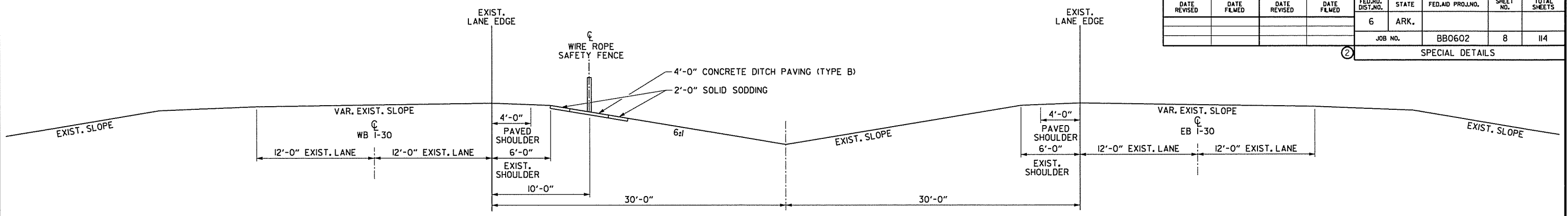
THE CONTRACTOR SHALL NOT PLACE EMBANKMENT AROUND THE EXISTING SIGN POSTS TO A DEPTH SUCH THAT THE STUB POST BASE PLATE IS BURIED.

DETAILS FOR MAINTENANCE OF EXISTING BREAKAWAY SIGN STRUCTURES

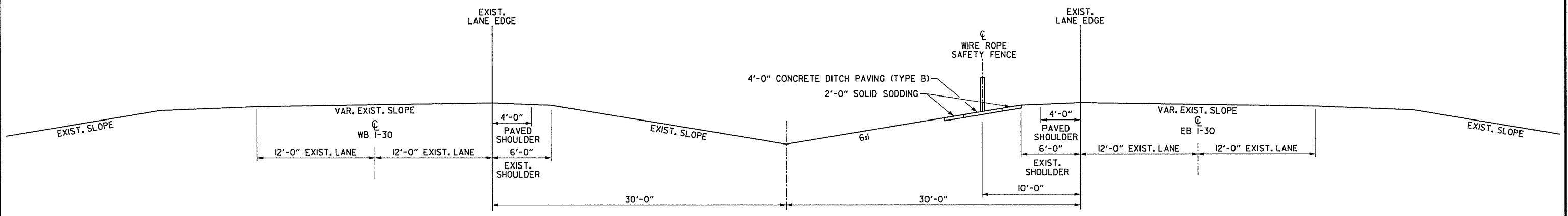
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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0602	8	114

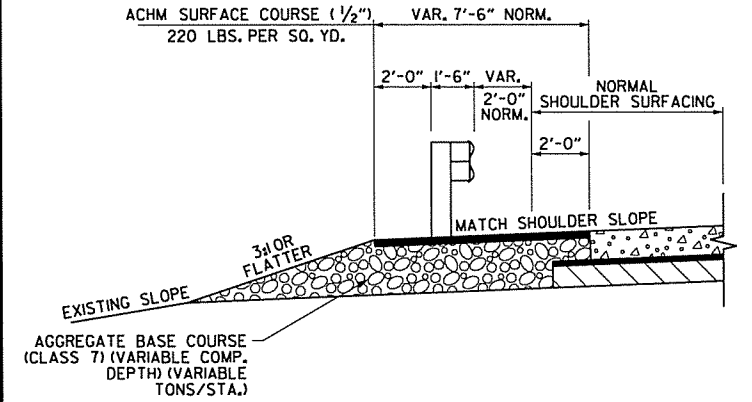
SPECIAL DETAILS



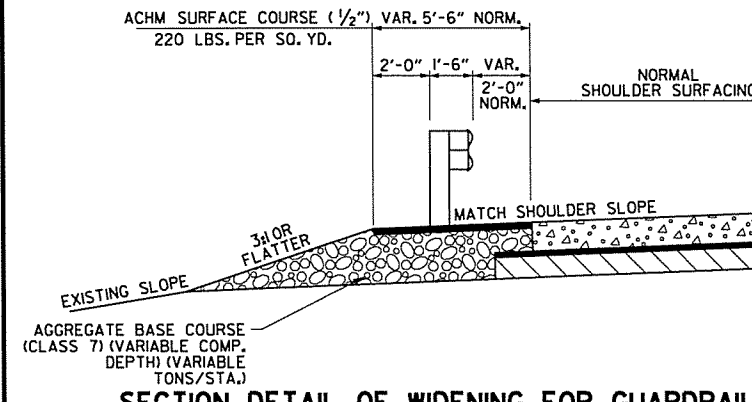
WIRE ROPE SAFETY FENCE ON WESTBOUND LANES FORESLOPE



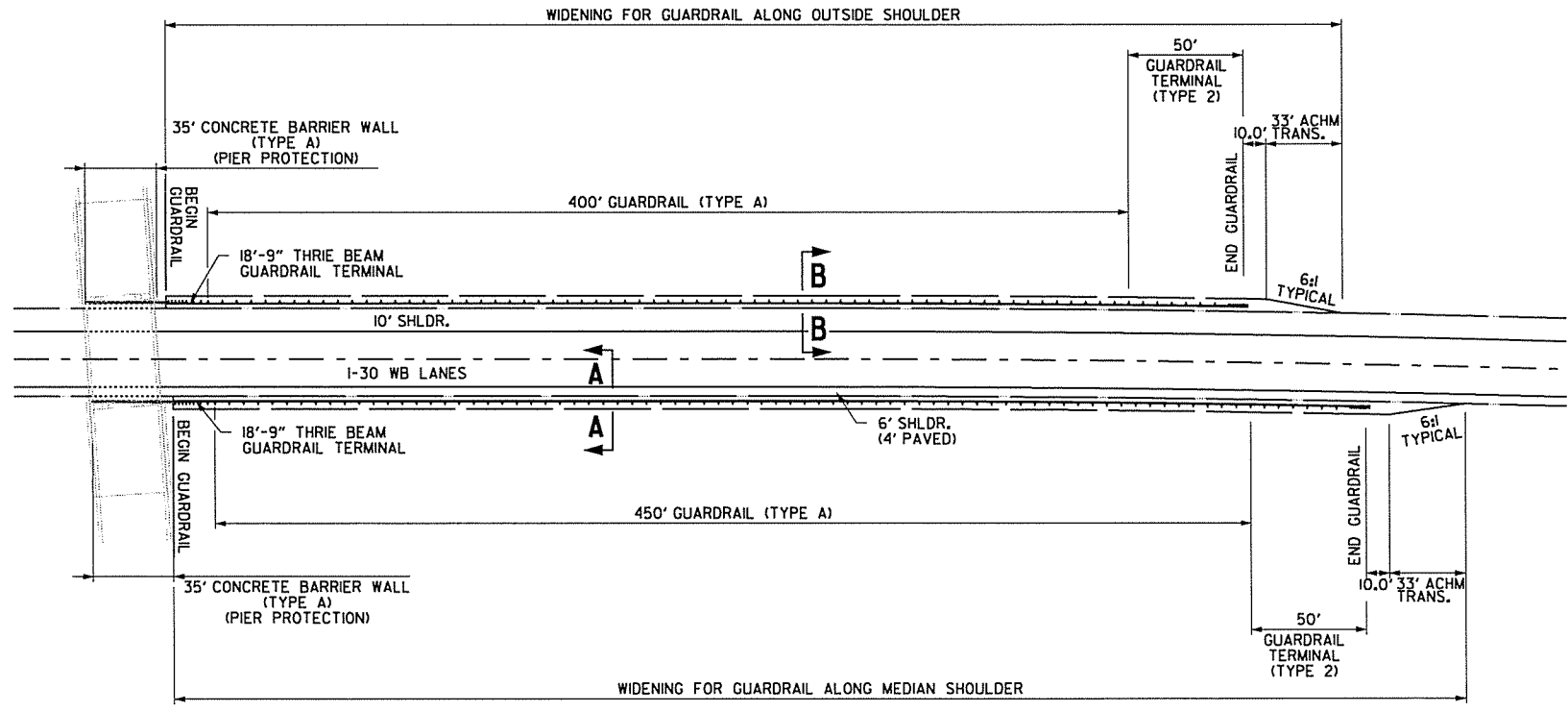
WIRE ROPE SAFETY FENCE ON EASTBOUND LANES FORESLOPE



SECTION DETAIL OF WIDENING FOR GUARDRAIL SECTION A-A



SECTION DETAIL OF WIDENING FOR GUARDRAIL SECTION B-B



DETAIL AT POINT VIEW RD.

- NOTES:
1. REFER TO STD. DWG GR-9A FOR SLOPE REQUIREMENTS BEHIND GUARDRAIL.
 2. REFER TO STANDARD DRAWINGS GR-8, GR-8A, GR-9, GR-9A, GR-10, GR-10A, GR-11 & GRT-1 FOR ADDITIONAL INFORMATION

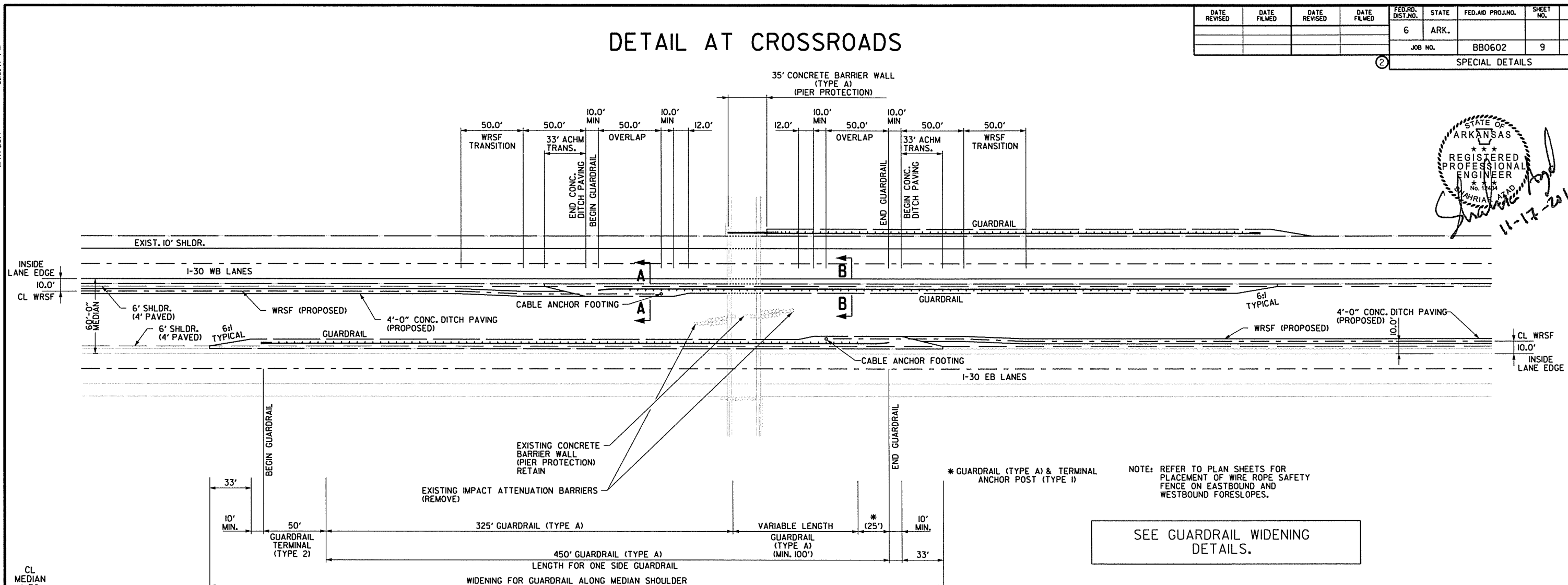
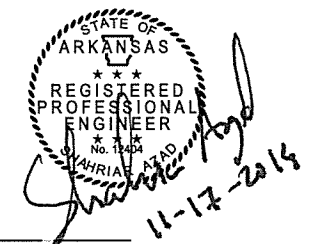
STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 18104
 SHARIK AZAD
 11-17-2014

SPECIAL DETAILS

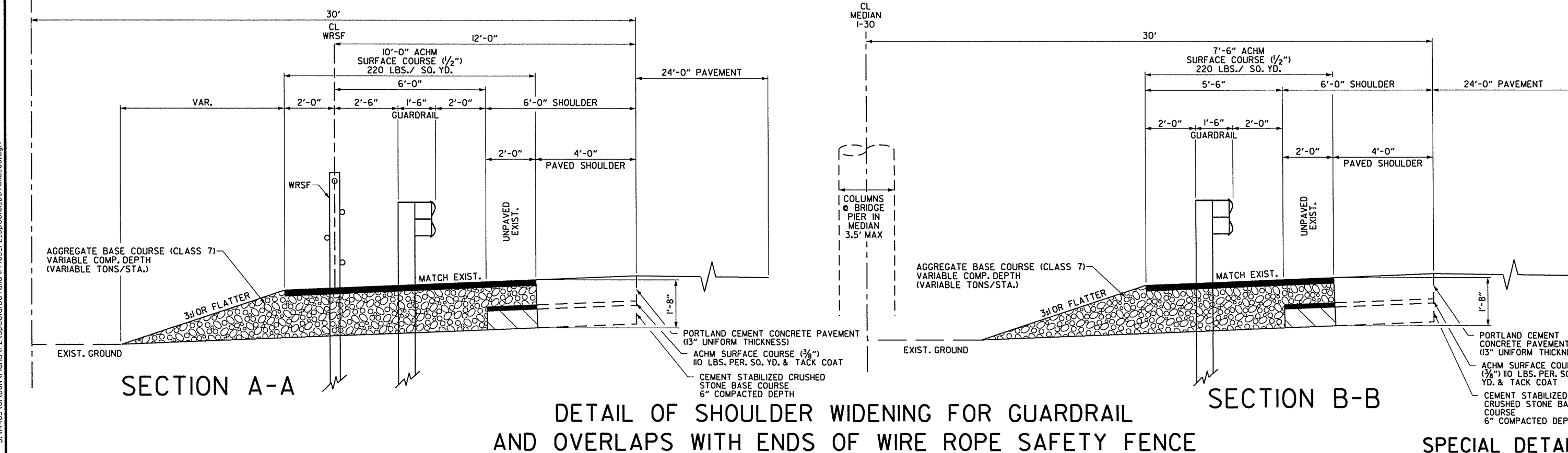
DETAIL AT CROSSROADS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		BB0602	9	114

② SPECIAL DETAILS



SEE GUARDRAIL WIDENING DETAILS.



DETAIL OF SHOULDER WIDENING FOR GUARDRAIL AND OVERLAPS WITH ENDS OF WIRE ROPE SAFETY FENCE

SPECIAL DETAILS

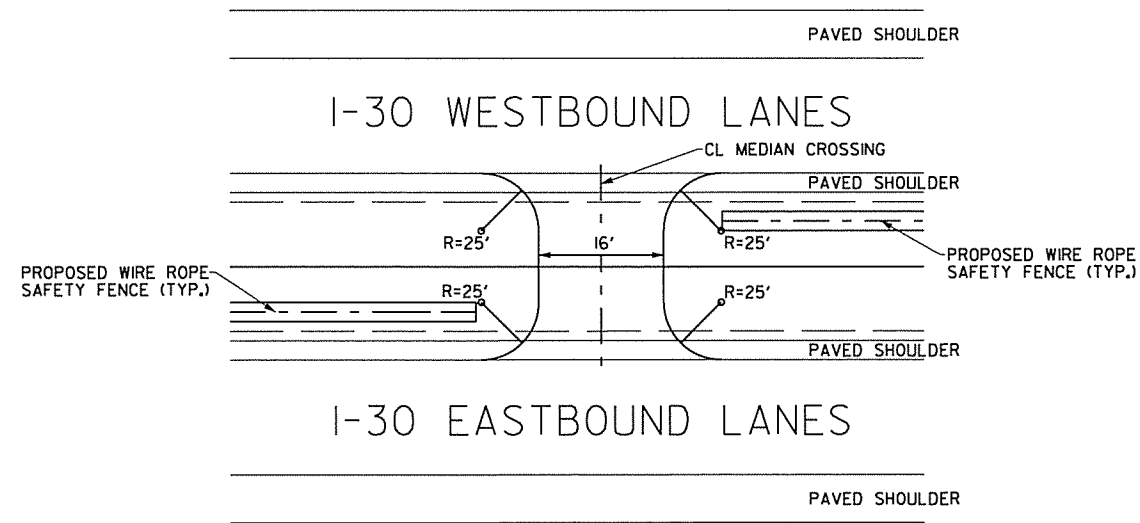
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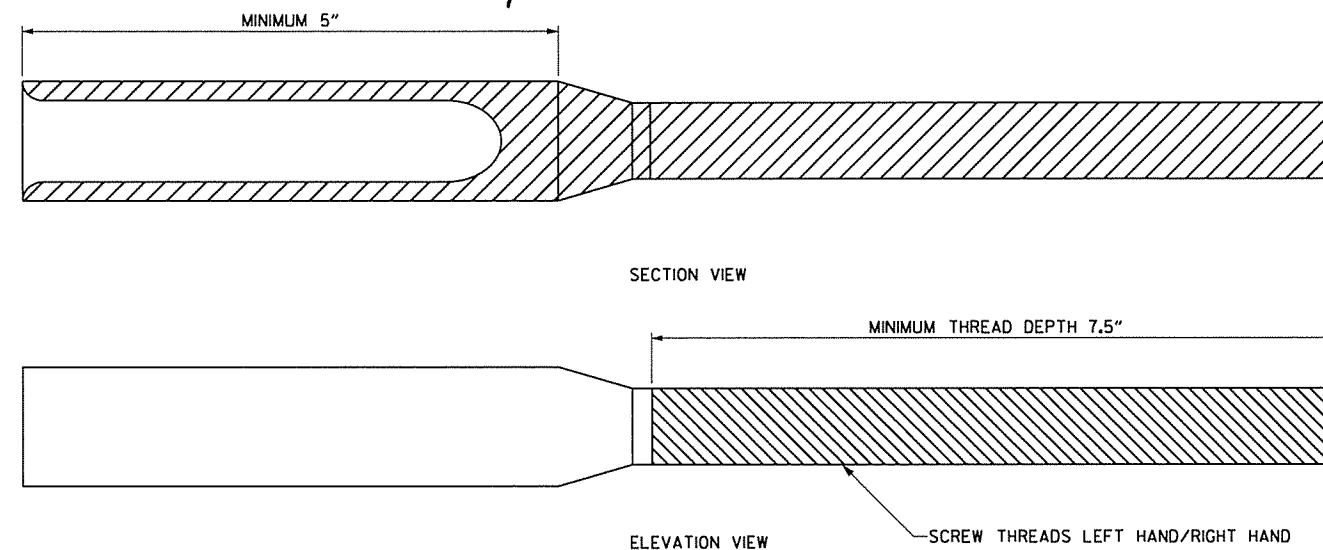


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				6	ARK.		II	II4
				JOB NO.		BBO602	SPECIAL DETAILS	



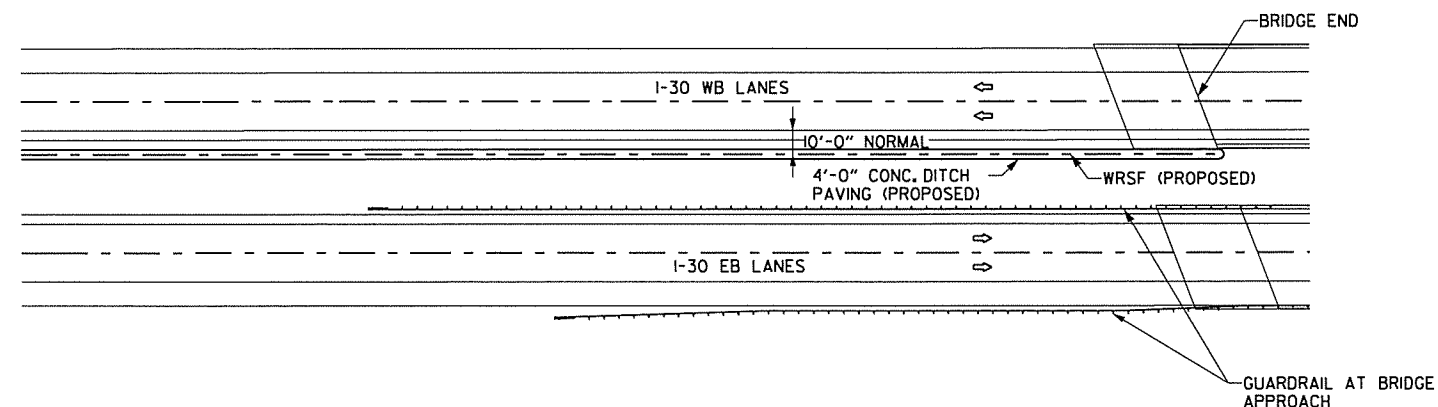
DETAIL AT MEDIAN CROSSING

- NOTES: 1. MEDIAN CROSSING TO BE CONSTRUCTED OF AGGREGATE BASE COURSE (CLASS 7) - 7" COMPACTED DEPTH & ACHM SURFACE COURSE (1/2") - 220 LBS. PER. SQ. YD.
2. REFER TO PLAN SHEETS FOR LOCATIONS OF PROPOSED WIRE ROPE SAFETY FENCE.



THREADED TERMINAL DETAIL

- NOTES: 1. REFER TO PLAN SHEETS FOR PLACEMENT OF WIRE ROPE SAFETY FENCE ON EASTBOUND AND WESTBOUND FORESLOPES.
2. REFER TO "WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS" SPECIAL PROVISION FOR ADDITIONAL REQUIREMENTS.



WRSF AND GUARDRAIL ON OPPOSITE SIDES OF MEDIAN

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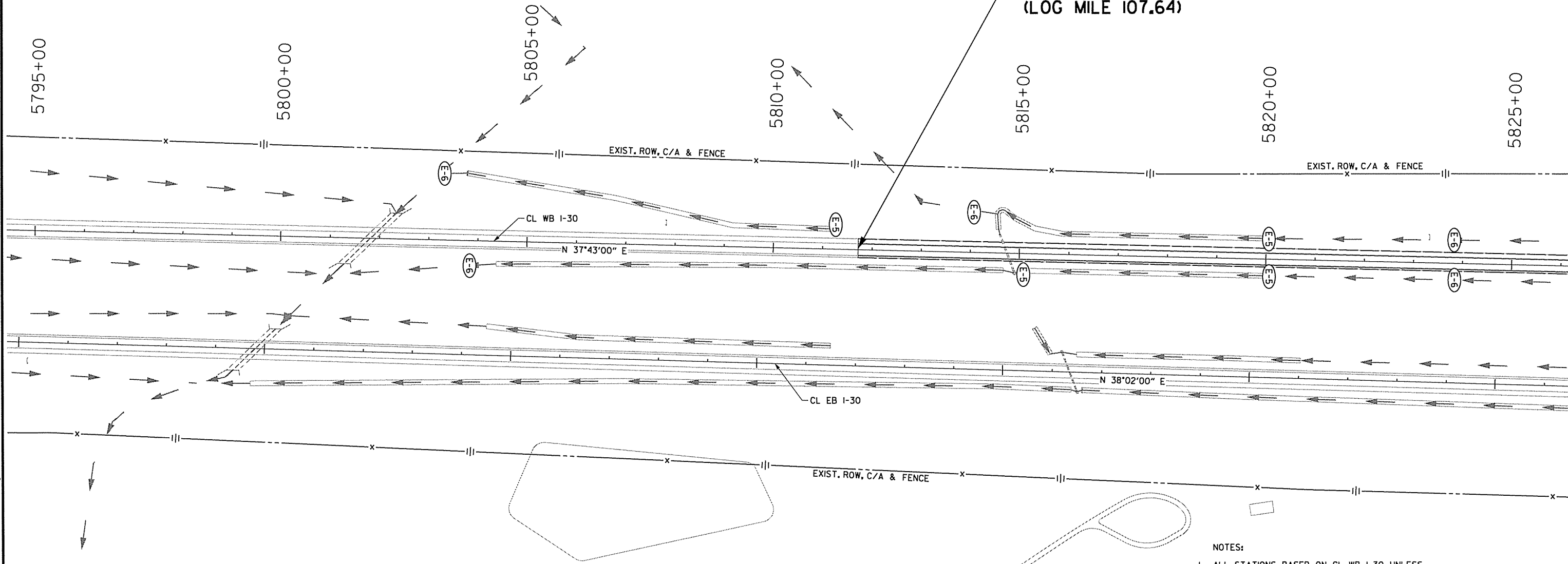
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STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 12104
 SAHRIAN AZAD
 11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			

② TEMPORARY EROSION CONTROL DETAILS

STA. 5811+71.49
 BEGIN JOB BB0602
 WESTBOUND ONLY
 (LOG MILE 107.64)



EROSION CONTROL LEGEND

(E-5)	SAND BAG DITCH CHECK
(E-6)	ROCK DITCH CHECK
←	WATER FLOW DIRECTION

REVISIONS

DATE	DESCRIPTION

- NOTES:**
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
 - REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR STAGE CONSTRUCTION SEQUENCE.
 - ALL TEMPORARY PAVEMENT IS SHOWN ON TEMPORARY EROSION CONTROL PLANS. REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR PHASING AND INDIVIDUAL CONFIGURATIONS.
 - ALL FLOWLINES ESTIMATED BASED ON MOST RECENT USGS TOPOGRAPHIC MAPPING AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION (SUBSIDIARY TO OTHER ITEMS).

TEMPORARY EROSION CONTROL DETAILS
 I-30

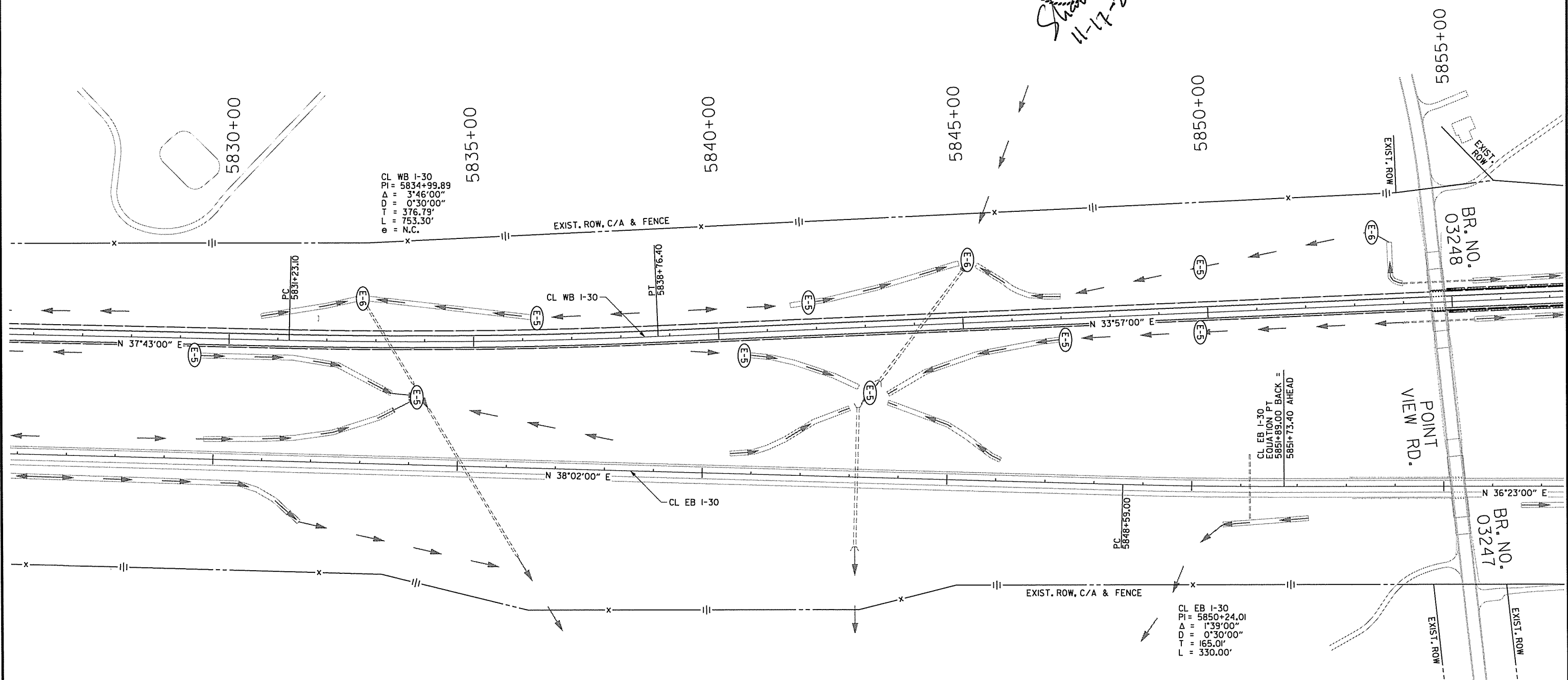
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STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHARIAR
11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	13	114	

TEMPORARY EROSION CONTROL DETAILS



EROSION CONTROL LEGEND

(E-5)	SAND BAG DITCH CHECK
(E-6)	ROCK DITCH CHECK
←	WATER FLOW DIRECTION

REVISIONS

DATE	DESCRIPTION

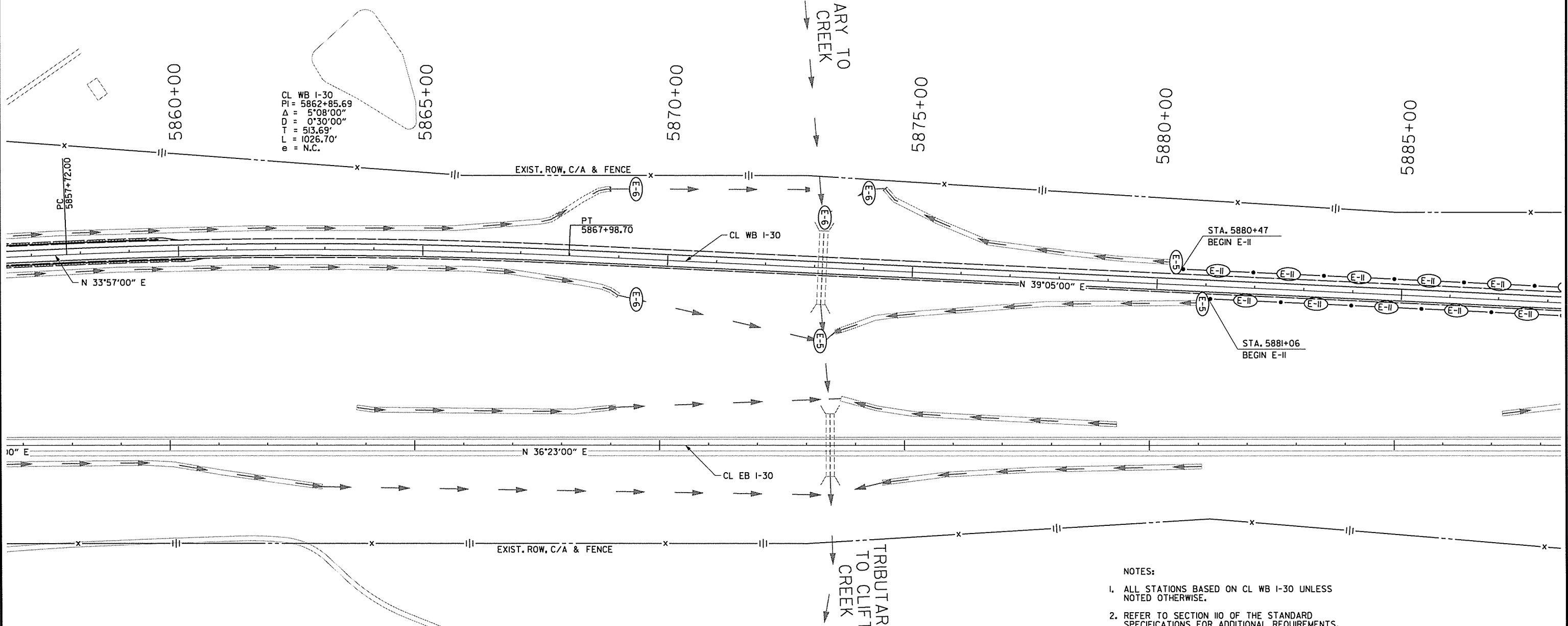
- NOTES:**
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
 - REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR STAGE CONSTRUCTION SEQUENCE.
 - ALL TEMPORARY PAVEMENT IS SHOWN ON TEMPORARY EROSION CONTROL PLANS. REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR PHASING AND INDIVIDUAL CONFIGURATIONS.
 - ALL FLOWLINES ESTIMATED BASED ON MOST RECENT USGS TOPOGRAPHIC MAPPING AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION (SUBSIDIARY TO OTHER ITEMS).

TEMPORARY EROSION CONTROL DETAILS
I-30

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STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRIAR AZAD
11-17-2014

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0602	14
						TEMPORARY EROSION CONTROL DETAILS		



CL WB I-30
PI = 5862+85.69
Δ = 5°08'00"
D = 0°30'00"
T = 513.69'
L = 1026.70'
e = N.C.

EROSION CONTROL LEGEND

(E-5)	SAND BAG DITCH CHECK
(E-6)	ROCK DITCH CHECK
●(E-II)●	SILT FENCE
←	WATER FLOW DIRECTION

REVISIONS

DATE	DESCRIPTION

- NOTES:**
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
 - REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR STAGE CONSTRUCTION SEQUENCE.
 - ALL TEMPORARY PAVEMENT IS SHOWN ON TEMPORARY EROSION CONTROL PLANS. REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR PHASING AND INDIVIDUAL CONFIGURATIONS.
 - ALL FLOWLINES ESTIMATED BASED ON MOST RECENT USGS TOPOGRAPHIC MAPPING AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION (SUBSIDIARY TO OTHER ITEMS).

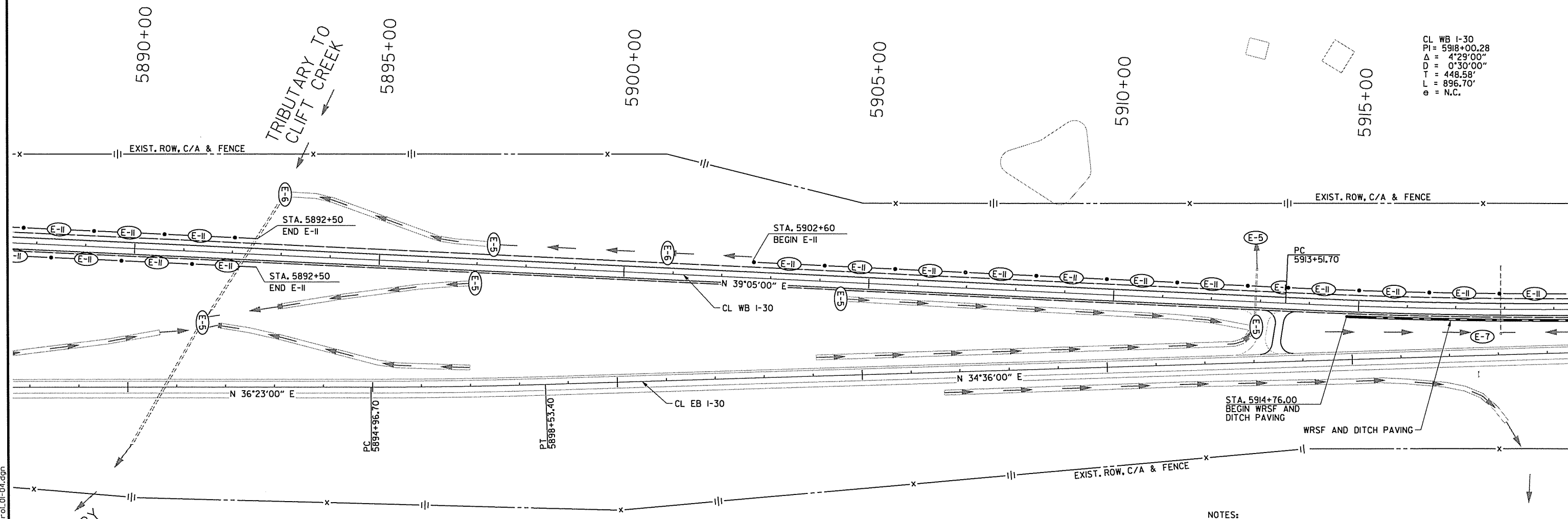
TEMPORARY EROSION CONTROL DETAILS
I-30

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STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRAR AZAD
11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0602	15
						TEMPORARY EROSION CONTROL DETAILS		



CL EB I-30
PI = 5896+75.06
Δ = 1°47'00\"
D = 0°30'00\"
T = 178.36'
L = 356.70'

EROSION CONTROL LEGEND

(E-5)	SAND BAG DITCH CHECK
(E-6)	ROCK DITCH CHECK
(E-7)	DROP INLET SILT FENCE
● (E-II) ●	SILT FENCE
←	WATER FLOW DIRECTION

REVISIONS

DATE	DESCRIPTION

- NOTES:**
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
 - REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR STAGE CONSTRUCTION SEQUENCE.
 - ALL TEMPORARY PAVEMENT IS SHOWN ON TEMPORARY EROSION CONTROL PLANS. REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR PHASING AND INDIVIDUAL CONFIGURATIONS.
 - ALL FLOWLINES ESTIMATED BASED ON MOST RECENT USGS TOPOGRAPHIC MAPPING AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION (SUBSIDIARY TO OTHER ITEMS).

TEMPORARY EROSION CONTROL DETAILS
I-30

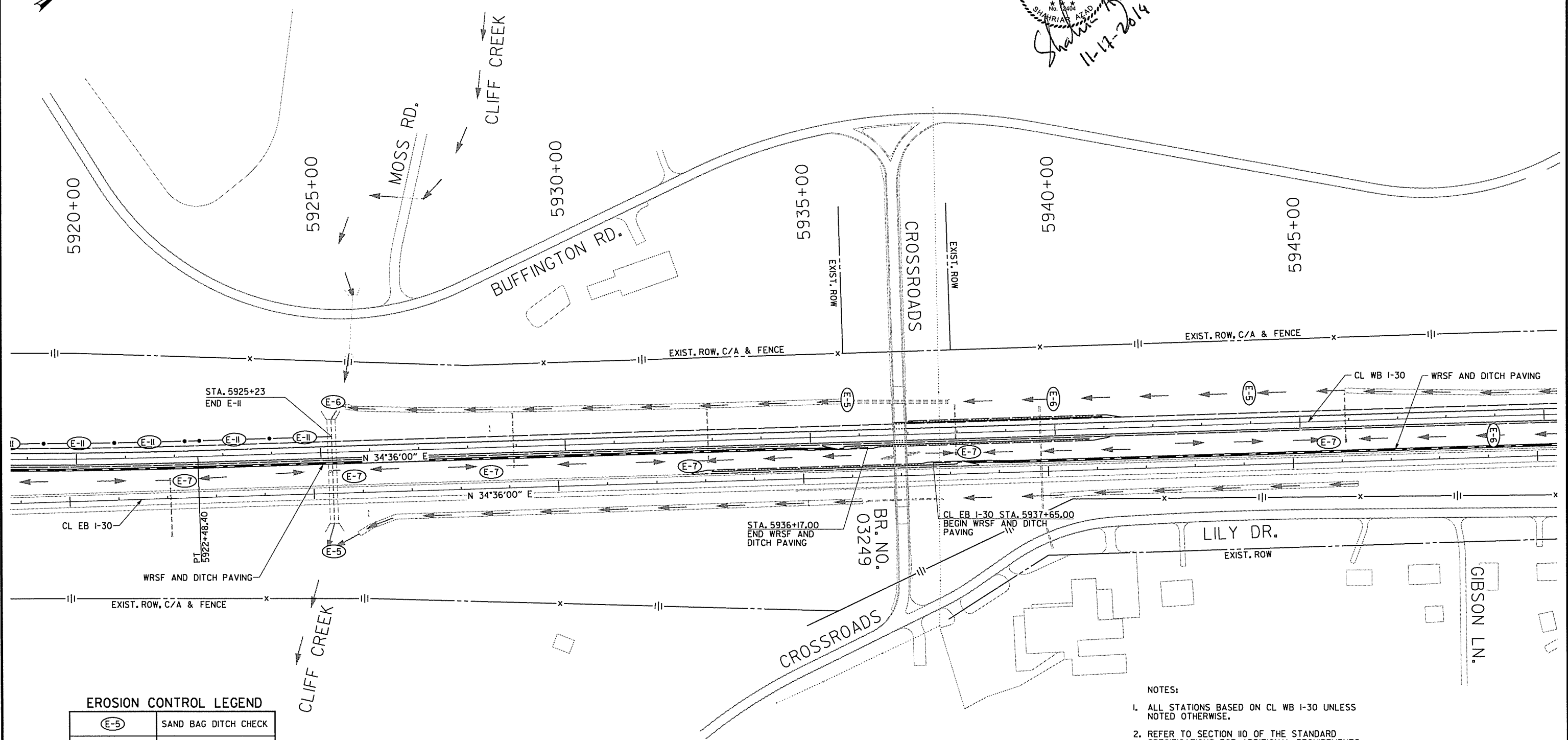
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STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 03249
 SHARVIA ALLEN
 11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	16	114	

② TEMPORARY EROSION CONTROL DETAILS



EROSION CONTROL LEGEND

(E-5)	SAND BAG DITCH CHECK
(E-6)	ROCK DITCH CHECK
(E-7)	DROP INLET SILT FENCE
(E-11)	SILT FENCE
←	WATER FLOW DIRECTION

REVISIONS

DATE	DESCRIPTION

- NOTES:**
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
 - REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR STAGE CONSTRUCTION SEQUENCE.
 - ALL TEMPORARY PAVEMENT IS SHOWN ON TEMPORARY EROSION CONTROL PLANS. REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR PHASING AND INDIVIDUAL CONFIGURATIONS.
 - ALL FLOWLINES ESTIMATED BASED ON MOST RECENT USGS TOPOGRAPHIC MAPPING AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION (SUBSIDIARY TO OTHER ITEMS).

TEMPORARY EROSION CONTROL DETAILS
 I-30

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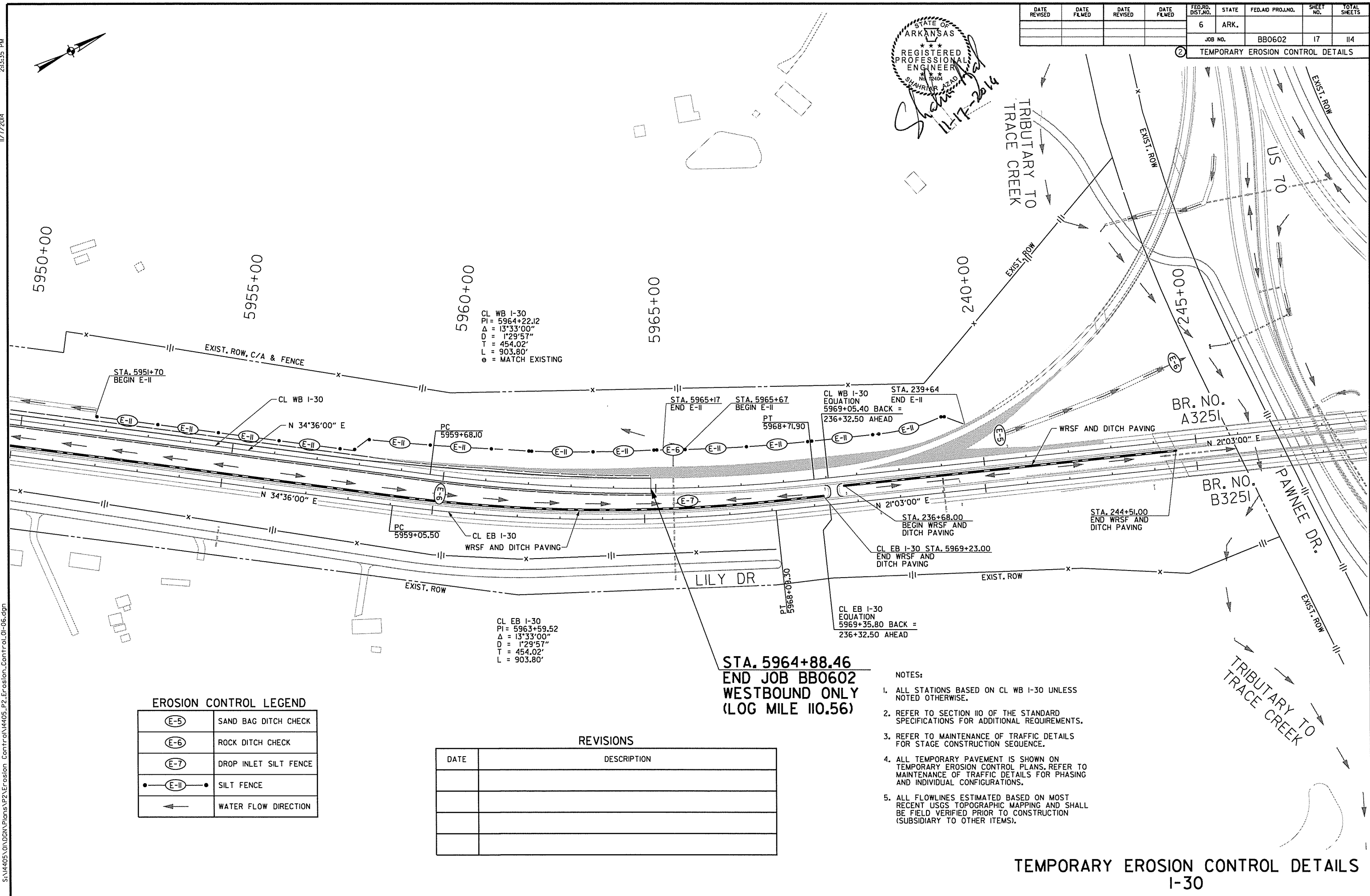
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STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRIN AZAD
 No. 2404
 12-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		17	114
				JOB NO.		BB0602		

TEMPORARY EROSION CONTROL DETAILS



CL WB I-30
 PI = 5964+22.12
 $\Delta = 13^{\circ}33'00''$
 $D = 1^{\circ}29'57''$
 $T = 454.02'$
 $L = 903.80'$
 e = MATCH EXISTING

CL EB I-30
 PI = 5963+59.52
 $\Delta = 13^{\circ}33'00''$
 $D = 1^{\circ}29'57''$
 $T = 454.02'$
 $L = 903.80'$

STA. 5964+88.46
END JOB BB0602
WESTBOUND ONLY
(LOG MILE 110.56)

EROSION CONTROL LEGEND

(E-5)	SAND BAG DITCH CHECK
(E-6)	ROCK DITCH CHECK
(E-7)	DROP INLET SILT FENCE
(E-II)	SILT FENCE
←	WATER FLOW DIRECTION

REVISIONS

DATE	DESCRIPTION

- NOTES:**
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
 3. REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR STAGE CONSTRUCTION SEQUENCE.
 4. ALL TEMPORARY PAVEMENT IS SHOWN ON TEMPORARY EROSION CONTROL PLANS. REFER TO MAINTENANCE OF TRAFFIC DETAILS FOR PHASING AND INDIVIDUAL CONFIGURATIONS.
 5. ALL FLOWLINES ESTIMATED BASED ON MOST RECENT USGS TOPOGRAPHIC MAPPING AND SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION (SUBSIDIARY TO OTHER ITEMS).

TEMPORARY EROSION CONTROL DETAILS
 I-30

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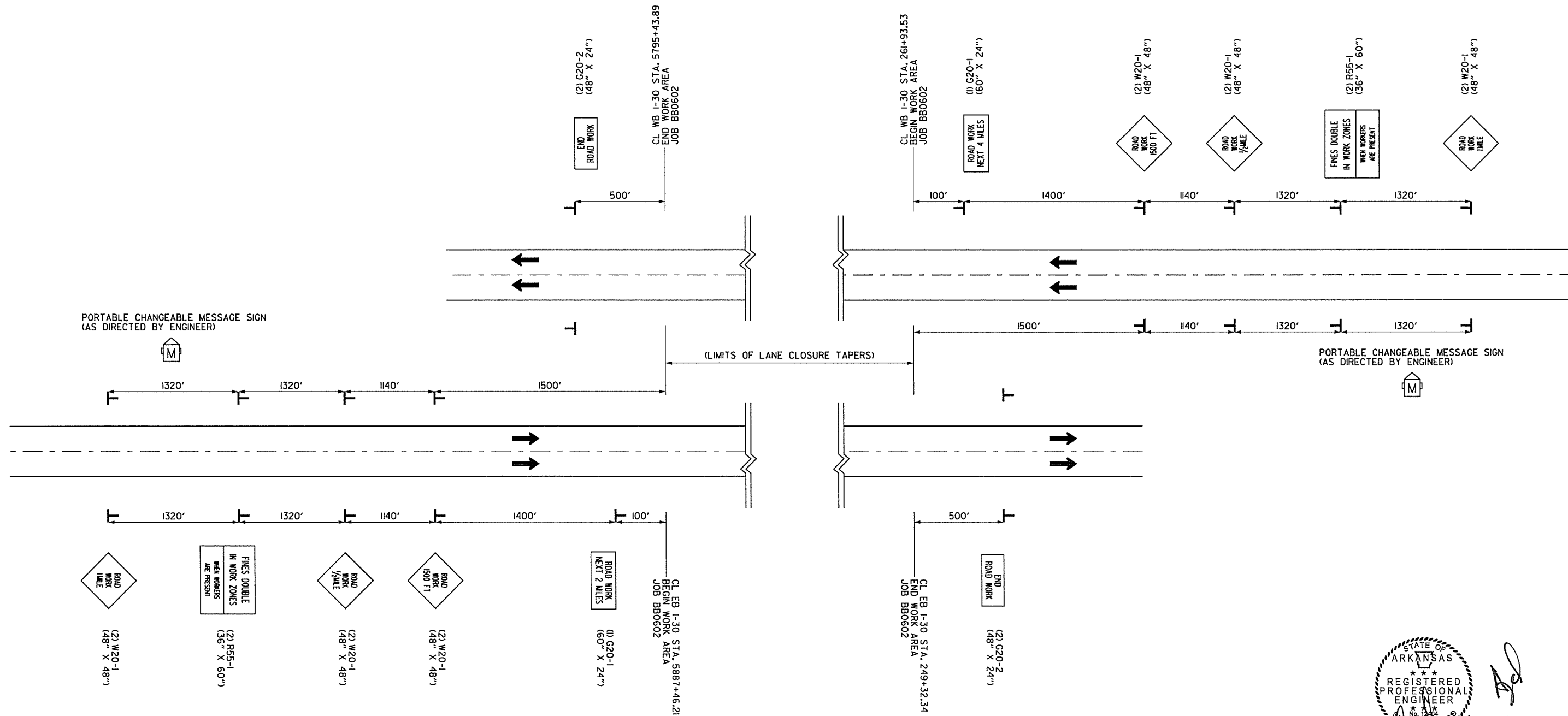
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				6	ARK.			
				JOB NO.	BBO602		18	114

② MAINTENANCE OF TRAFFIC

NOTE: THESE SIGNS MAY BE TEMPORARILY REPLACED BY SOME OF THE ADVANCE SIGNS FOR LANE CLOSURES WHILE WORK IS UNDERWAY IN THESE AREAS.



NOTE: THESE SIGNS MAY BE TEMPORARILY REPLACED BY SOME OF THE ADVANCE SIGNS FOR LANE CLOSURES WHILE WORK IS UNDERWAY IN THESE AREAS.

ADVANCE SIGNS AT BEGINNING AND END OF JOB BB0602

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 12404
 SAHRIAN AZAD
Signature
 11-17-2014

MAINTENANCE OF TRAFFIC ADVANCE WARNING

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11/7/2014

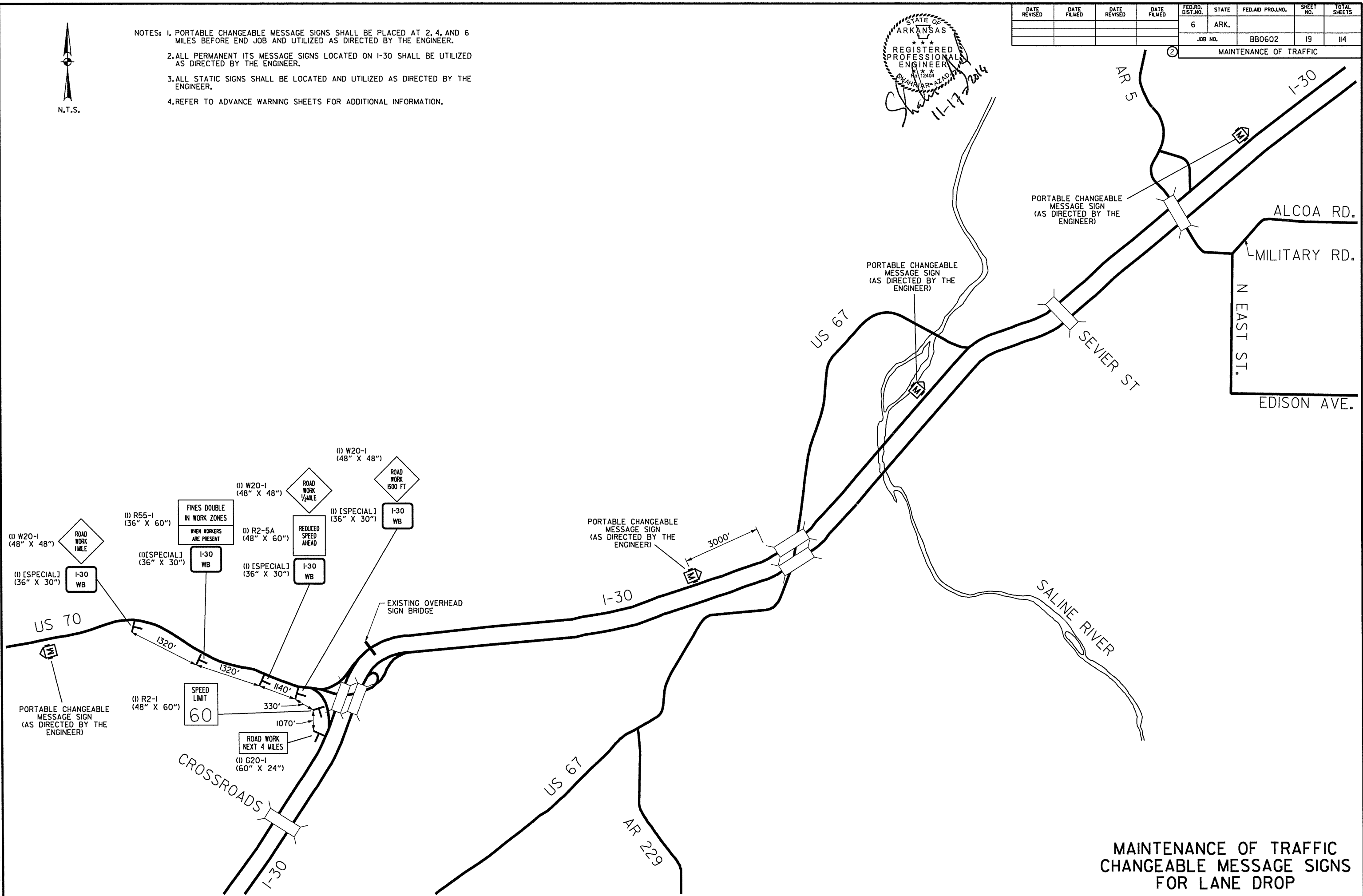


- NOTES: 1. PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AT 2, 4, AND 6 MILES BEFORE END JOB AND UTILIZED AS DIRECTED BY THE ENGINEER.
2. ALL PERMANENT ITS MESSAGE SIGNS LOCATED ON I-30 SHALL BE UTILIZED AS DIRECTED BY THE ENGINEER.
3. ALL STATIC SIGNS SHALL BE LOCATED AND UTILIZED AS DIRECTED BY THE ENGINEER.
4. REFER TO ADVANCE WARNING SHEETS FOR ADDITIONAL INFORMATION.



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0602						19	114	

MAINTENANCE OF TRAFFIC



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MAINTENANCE OF TRAFFIC
CHANGEABLE MESSAGE SIGNS
FOR LANE DROP

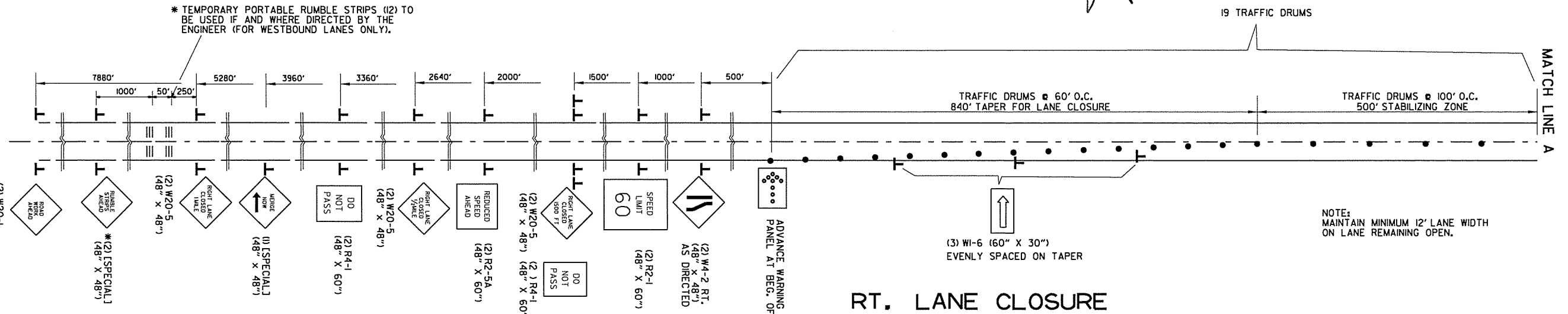
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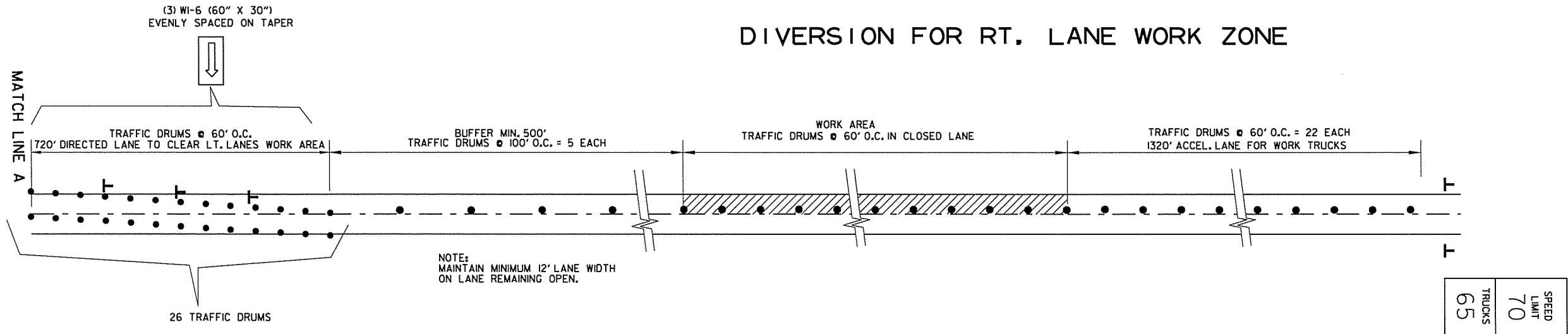
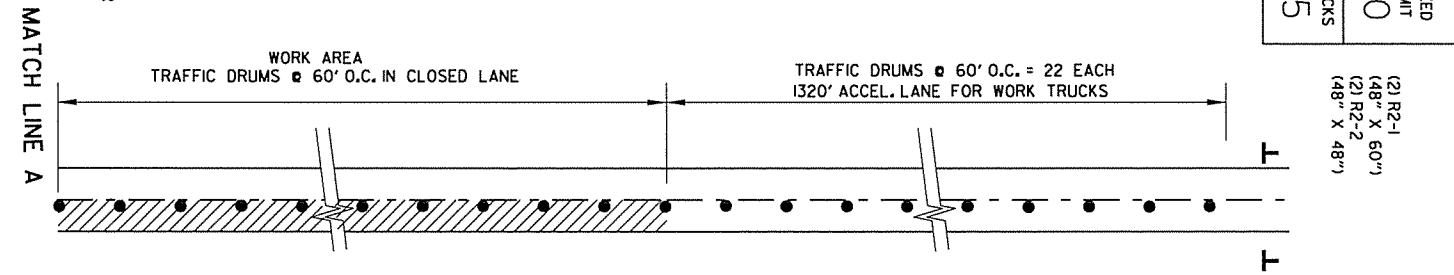


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				6	ARK.		20	114
				JOB NO.		BB0602		
② MAINTENANCE OF TRAFFIC								



PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

SPEED LIMIT SIGNS ARE ALSO PROVIDED FOR PLACEMENT PAST ENTRANCE RAMP WITHIN THE WORK ZONE.



MAINTENANCE OF TRAFFIC LANE CLOSURE

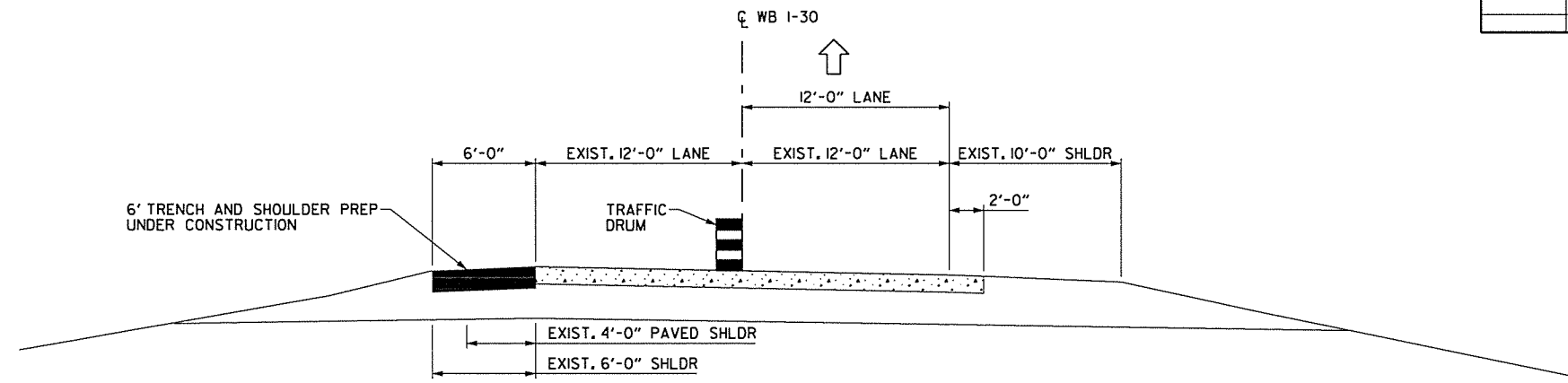
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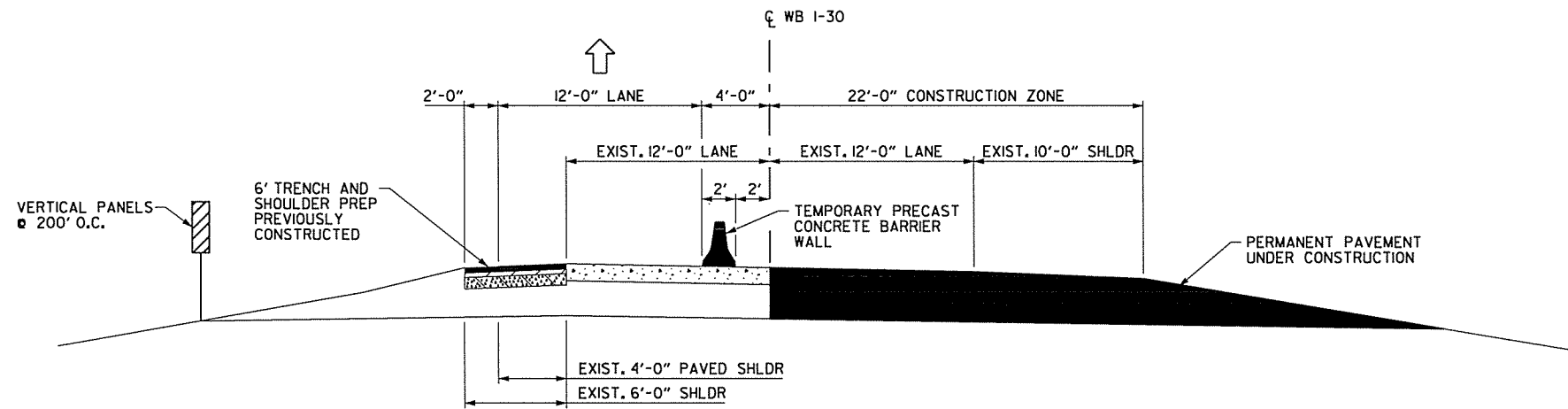
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				6	ARK.			
				JOB NO.	BB0602	21	114	

② MAINTENANCE OF TRAFFIC

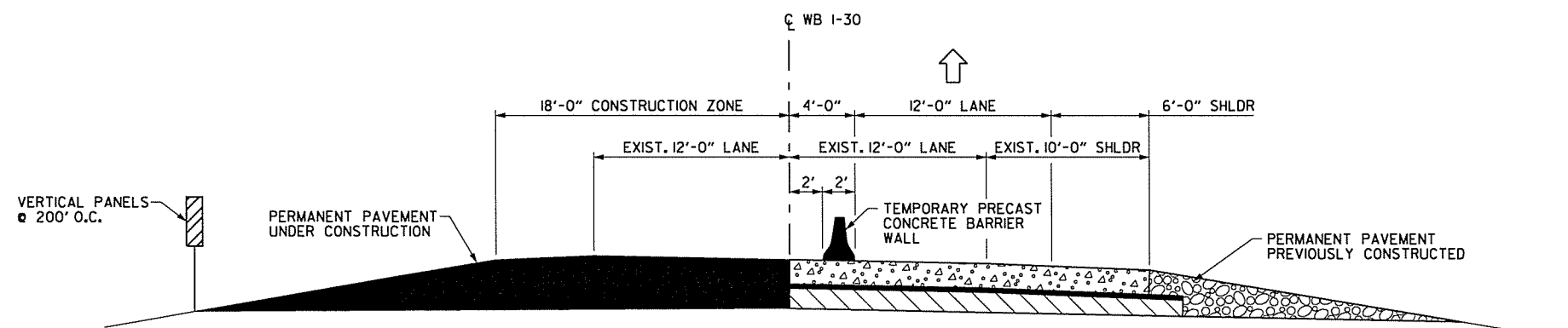
STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHAJAR-AZAD
No. 12404
11-17-2014



LOCATION OF TRAFFIC DRUMS FOR MAINTENANCE OF TRAFFIC
LT. MAIN LANES - STAGE 1A
2' TRENCH AND SHOULDER PREP AND RUMBLE STRIP REMOVAL CL WB 1-30 STA. 5808+65 TO STA. 5819+50
6' TRENCH AND SHOULDER PREP CL WB 1-30 STA. 5819+50 TO STA. 244+12.50
(SHOWN IN DIRECTION OF TRAFFIC)



LOCATION OF TEMPORARY PRECAST CONCRETE BARRIER WALL
FOR MAINTENANCE OF TRAFFIC
LT. MAIN LANES - STAGE 2A
(SHOWN IN DIRECTION OF TRAFFIC)



LOCATION OF TEMPORARY PRECAST CONCRETE BARRIER WALL
FOR MAINTENANCE OF TRAFFIC
LT. MAIN LANES - STAGE 2B
(SHOWN IN DIRECTION OF TRAFFIC)

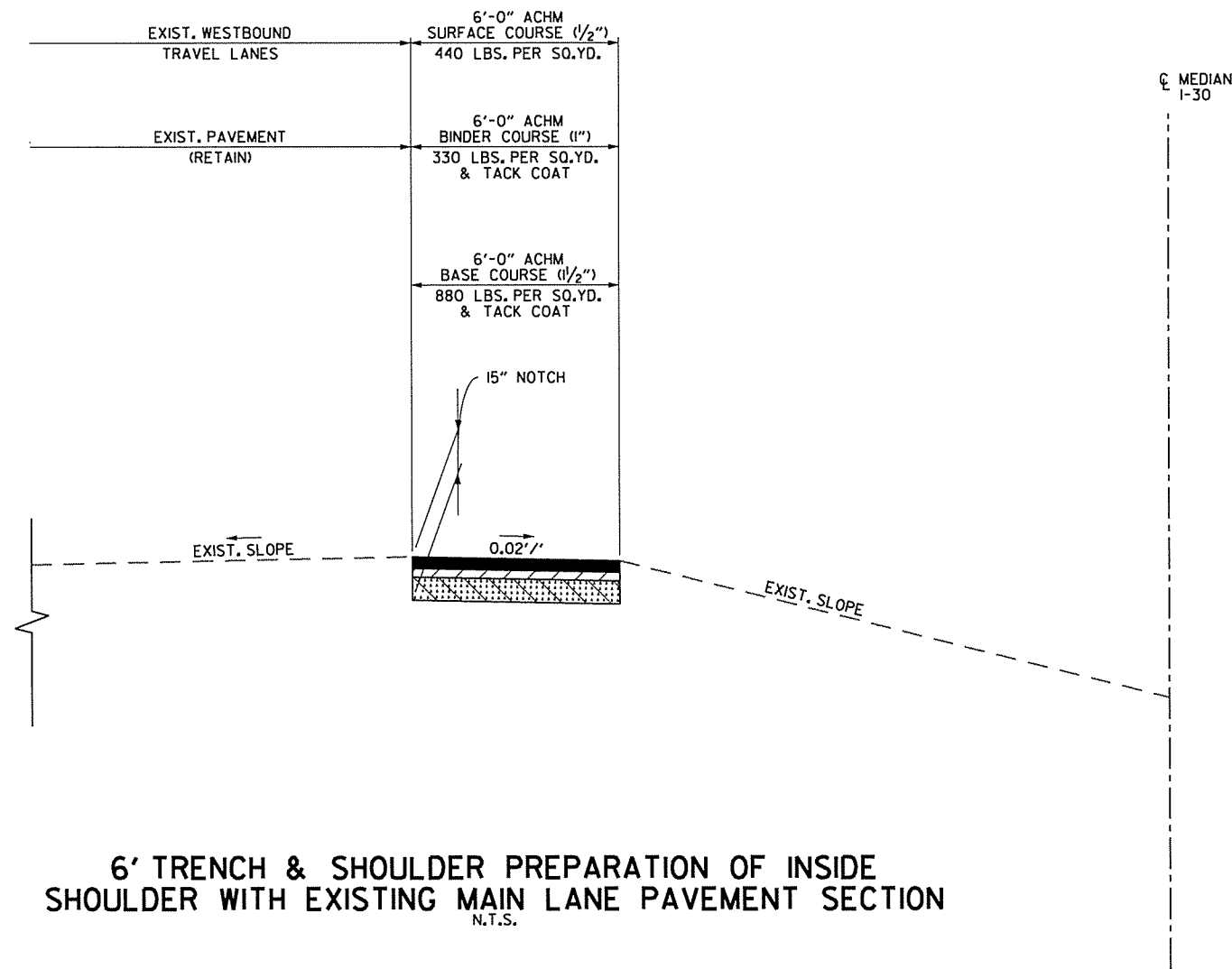
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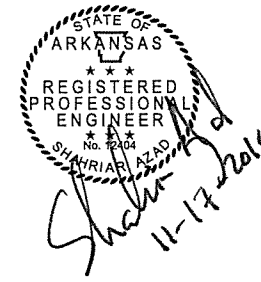


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602	23	114	
MAINTENANCE OF TRAFFIC								

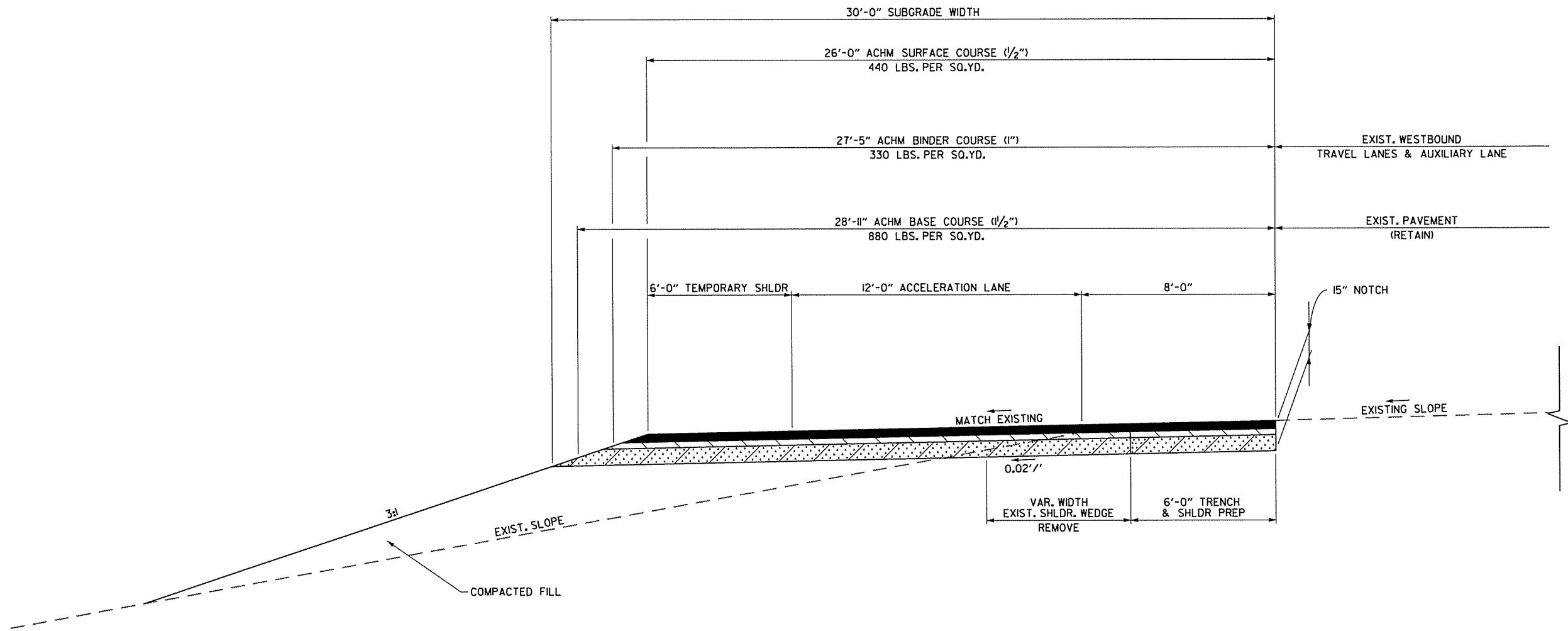


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11/7/2014

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS		
				6	ARK.					
							JOB NO.	BB0602	24	114
									MAINTENANCE OF TRAFFIC	



**TEMPORARY ACCELERATION LANE OUTSIDE WB AUXILIARY LANE
WITH EXISTING MAIN LANE PAVEMENT SECTION**
N.T.S.

NOTE:

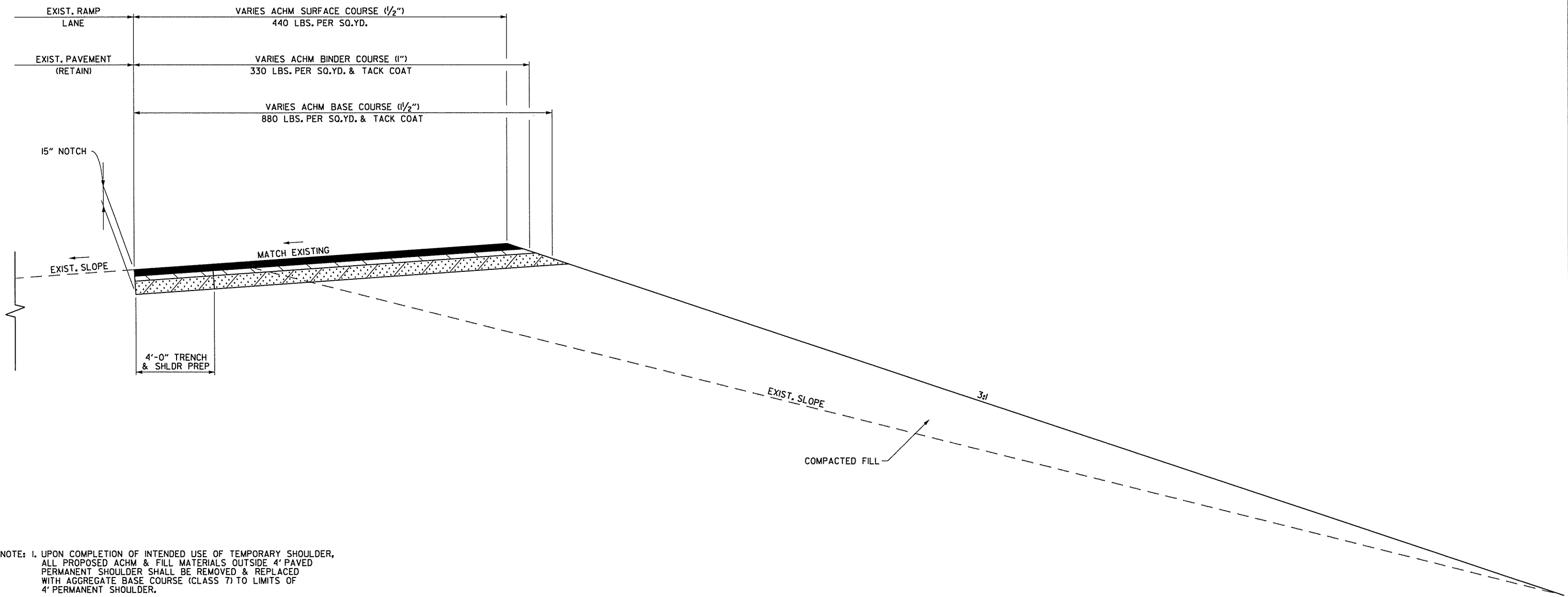
UPON COMPLETION OF INTENDED USE OF TEMPORARY ACCELERATION LANE & TEMPORARY SHOULDER, ALL PROPOSED ACHM & FILL MATERIALS OUTSIDE 6' PAVED PERMANENT SHOULDER SHALL BE REMOVED & REPLACED WITH AGGREGATE BASE COURSE (CLASS 7) TO LIMITS OF 6' PERMANENT SHOULDER AS SHOWN IN PERMANENT TYPICAL SECTIONS.

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11/7/2014

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRIAR AZAD
 No. 81004
 11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602	25	114	
② MAINTENANCE OF TRAFFIC								



NOTE: 1. UPON COMPLETION OF INTENDED USE OF TEMPORARY SHOULDER, ALL PROPOSED ACHM & FILL MATERIALS OUTSIDE 4' PAVED PERMANENT SHOULDER SHALL BE REMOVED & REPLACED WITH AGGREGATE BASE COURSE (CLASS 7) TO LIMITS OF 4' PERMANENT SHOULDER.

2. IN GORE AREA, ADJUST SLOPE OF TEMPORARY PAVEMENT TO MATCH BOTH EXISTING PAVEMENT EDGES (RAMP AND WB MAIN LANES) USING ONE CONSTANT SLOPE FOR THE TEMPORARY PAVEMENT.

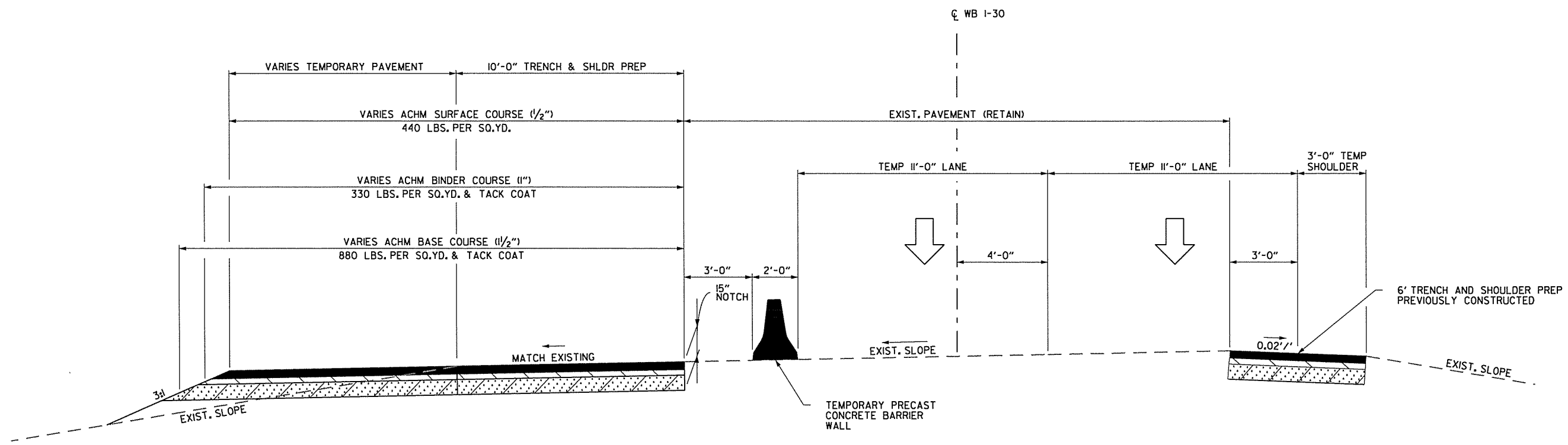
TEMPORARY WIDENING INSIDE WB ENTRANCE RAMP FROM US 70
 WITH EXISTING RAMP PAVEMENT SECTION
 N.T.S.

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11/7/2014



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	26	114	
② MAINTENANCE OF TRAFFIC								



**TEMPORARY PAVEMENT OUTSIDE WB MAIN LANES (IN GORE AREA)
WITH EXISTING MAIN LANE PAVEMENT SECTION**
N.T.S.

- NOTE: 1. UPON COMPLETION OF INTENDED USE OF TEMPORARY ACCELERATION LANE & TEMPORARY SHOULDER, ALL PROPOSED ACHM & FILL MATERIALS OUTSIDE 10' PAVED PERMANENT SHOULDER SHALL BE REMOVED & REPLACED WITH AGGREGATE BASE COURSE (CLASS 7) TO LIMITS OF 10' PERMANENT SHOULDER AS SHOWN IN PERMANENT TYPICAL SECTIONS.
2. IN GORE AREA, ADJUST SLOPE OF TEMPORARY PAVEMENT TO MATCH BOTH EXISTING PAVEMENT EDGES (RAMP AND WB MAIN LANES) USING ONE CONSTANT SLOPE FOR THE TEMPORARY PAVEMENT.

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1/21/2015

STAGE IA QUANTITIES
TRAFFIC DRUMS = 348 EACH
FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.

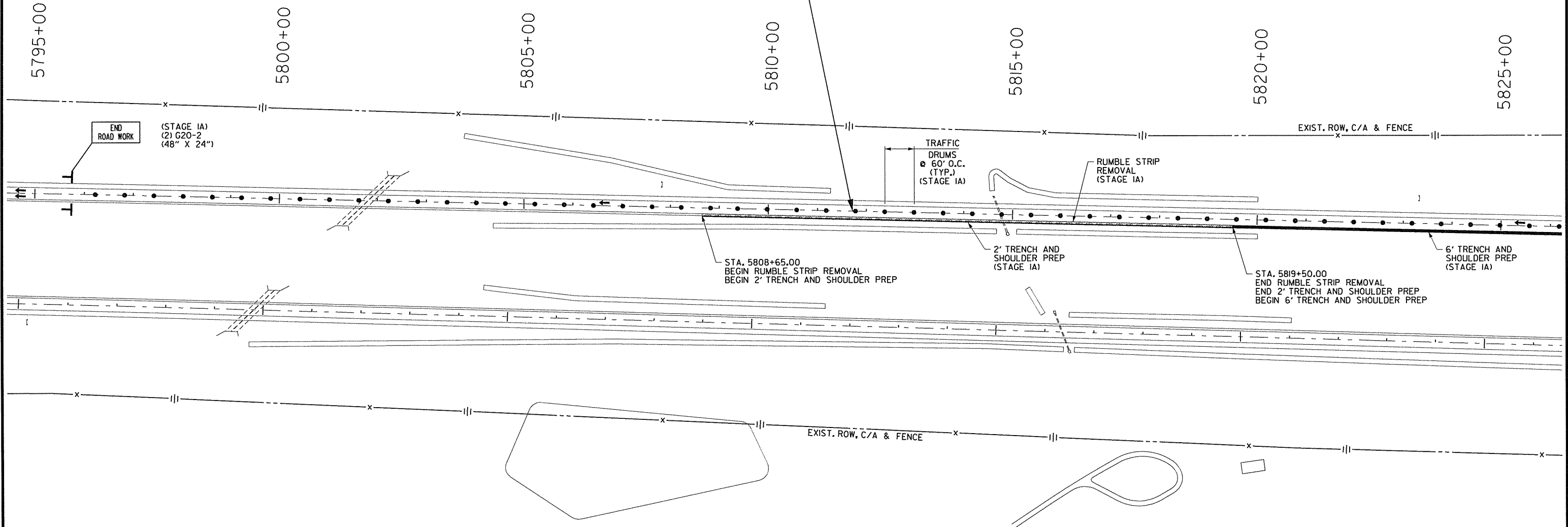
STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
No. 12448
WARRIAR AND
Shawhan
01-21-2015

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
						JOB NO.	BB0602	27
						TOTAL SHEETS		

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.

STA. 5811+71.49
BEGIN JOB BB0602
WESTBOUND ONLY
(LOG MILE 107.64)



- STAGE I
- TRAFFIC IN NORMAL PATTERN ON I-30.
 - PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE IA
- PLACE TEMPORARY OVERHEAD PLAQUES ON EXISTING SIGNS AT WB EXIT RAMP TO US 70 AND PLACE EXIT-ONLY DOTTED WHITE PAVEMENT MARKINGS.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 AND IN US 70 GORE AREA TO PLACE TRAFFIC IN THE OUTSIDE LANE TO PROTECT WORKERS IN MEDIUM.
 - PERFORM TRENCH AND SHOULDER PREP OF INSIDE 6' SHOULDER FROM STA. 5819+50.00 TO STA. 244+12.50.
 - PERFORM RUMBLE STRIP REMOVAL OF INSIDE 4' PAVED SHOULDER FROM STA. 5808+65.00 TO STA. 5819+50.00. ALSO PERFORM TRENCH AND SHOULDER PREP OF ADJACENT 2' UNPAVED INSIDE SHOULDER IN THIS AREA.
 - REMOVE EXISTING GUARDRAIL AND CONCRETE PIER PROTECTION INSIDE WB MAIN LANES AT BRIDGE NO. 03248 AND FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER.

LEGEND

- UNDER CONSTRUCTION
- RUMBLE STRIP REMOVAL
- STAGE IA TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGES I & IA

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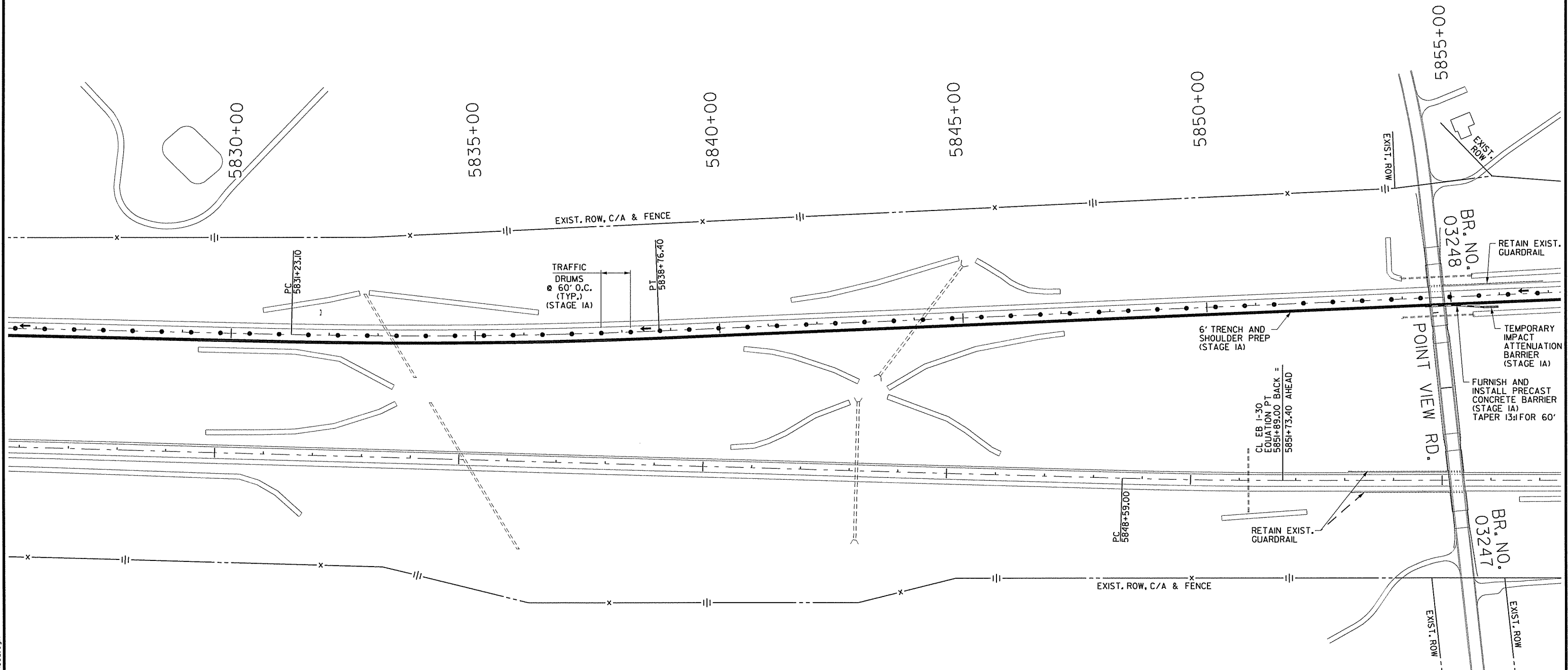
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1/21/2015

STAGE IA QUANTITIES
TRAFFIC DRUMS = 348 EACH
FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.

STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
No. 12904
SHAUNAR L. BELL
01-21-2015

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
						JOB NO.	BBO602	28
						② MAINTENANCE OF TRAFFIC DETAILS		

NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
2. REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE I
1. TRAFFIC IN NORMAL PATTERN ON I-30.
2. PLACE ADVANCE WARNING SIGNS AND DEVICES.

STAGE IA
1. PLACE TEMPORARY OVERHEAD PLAQUES ON EXISTING SIGNS AT WB EXIT RAMP TO US 70 AND PLACE EXIT-ONLY DOTTED WHITE PAVEMENT MARKINGS.
2. PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 AND IN US 70 GORE AREA TO PLACE TRAFFIC IN THE OUTSIDE LANE TO PROTECT WORKERS IN MEDIAN.
3. PERFORM TRENCH AND SHOULDER PREP OF INSIDE 6' SHOULDER FROM STA. 5819+50.00 TO STA. 244+12.50.
4. PERFORM RUMBLE STRIP REMOVAL OF INSIDE 4' PAVED SHOULDER FROM STA. 5808+65.00 TO STA. 5819+50.00. ALSO PERFORM TRENCH AND SHOULDER PREP OF ADJACENT 2' UNPAVED INSIDE SHOULDER IN THIS AREA.
5. REMOVE EXISTING GUARDRAIL AND CONCRETE PIER PROTECTION INSIDE WB MAIN LANES AT BRIDGE NO. 03248 AND FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER.

LEGEND
■ UNDER CONSTRUCTION
→ STAGE IA TRAFFIC
● TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGES I & IA

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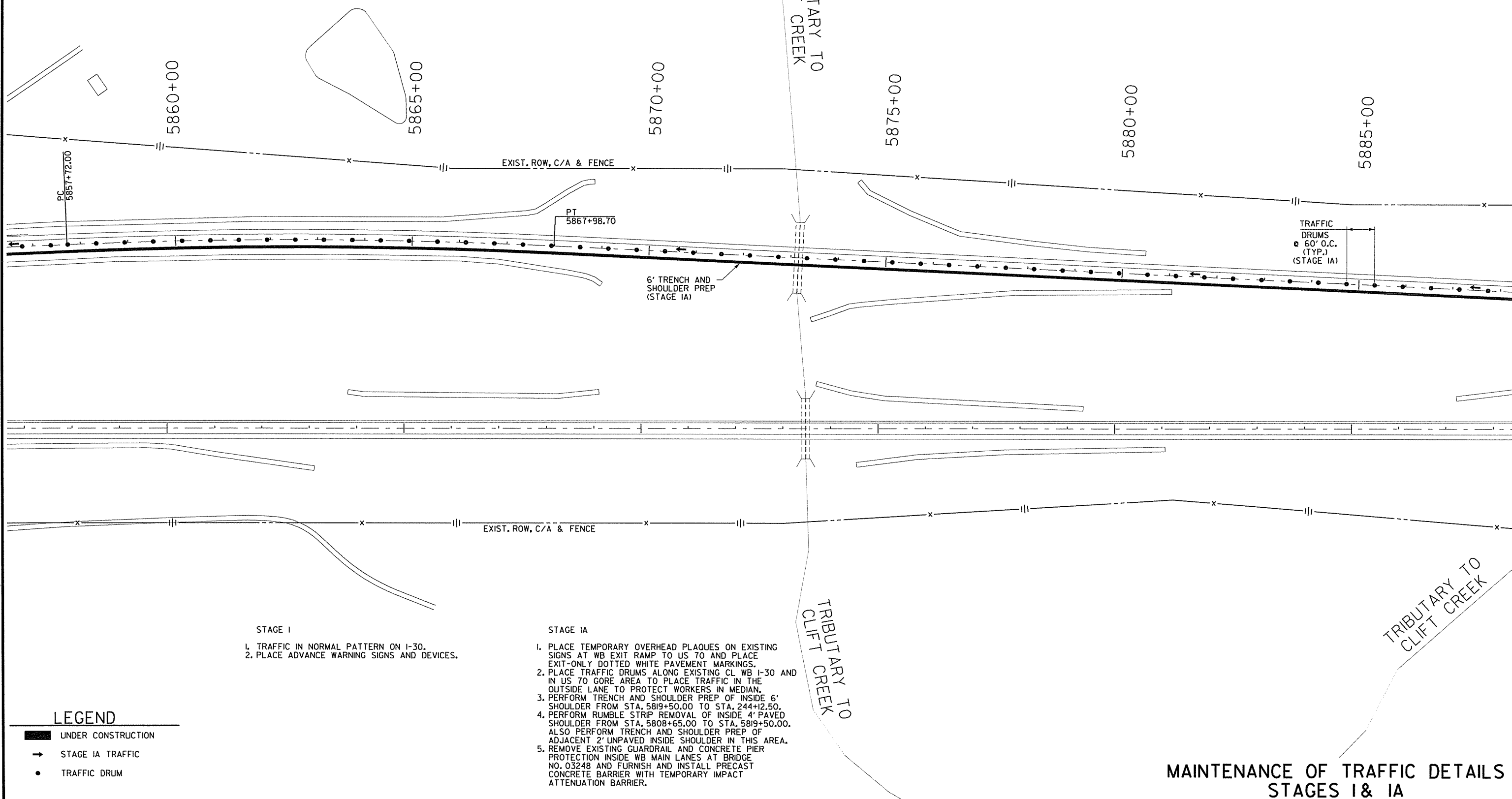
STAGE IA QUANTITIES
 TRAFFIC DRUMS = 348 EACH
 FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 12404
 SHAWRIAR, AZAD
Shawriar
 01-21-15

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
						JOB NO.	BB0602	29
						② MAINTENANCE OF TRAFFIC DETAILS		

NOTES:

- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
- REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



MAINTENANCE OF TRAFFIC DETAILS
 STAGES I & IA

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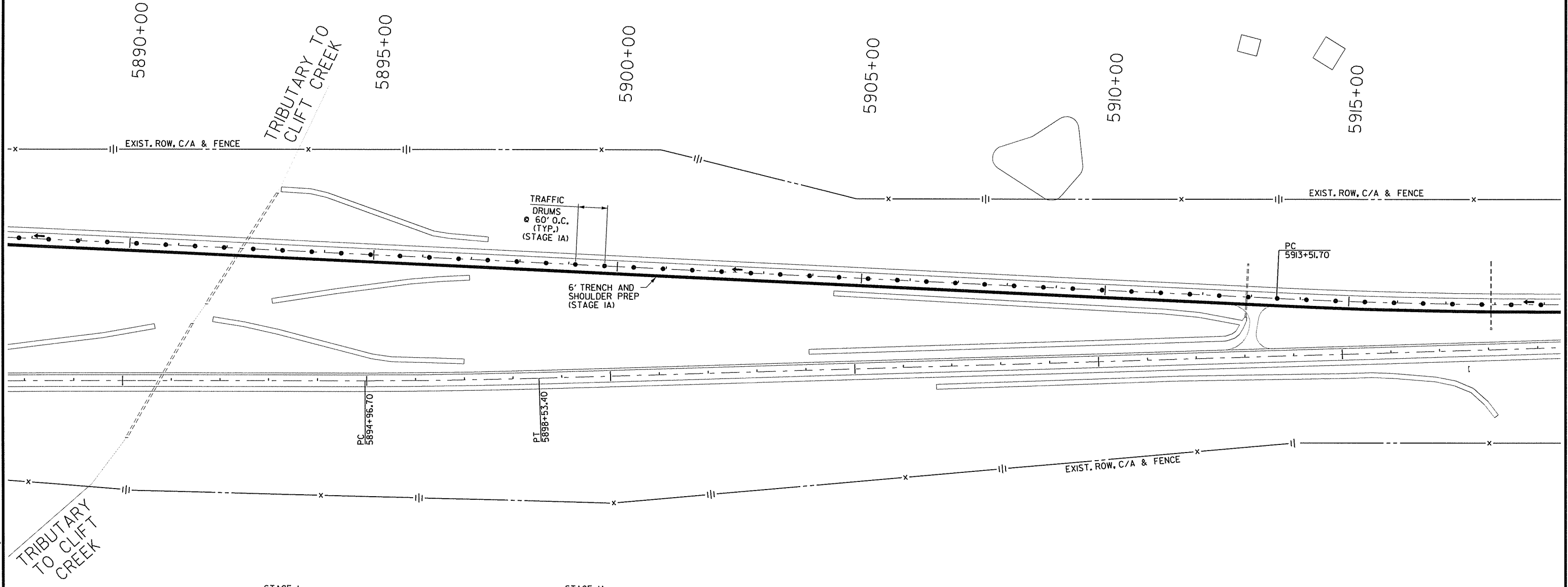
STAGE IA QUANTITIES
 TRAFFIC DRUMS = 348 EACH
 FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRIB AZAD
 No. 12814
 01-21-15

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
				JOB NO.		BB0602	30	114

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE I**
- TRAFFIC IN NORMAL PATTERN ON I-30.
 - PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE IA**
- PLACE TEMPORARY OVERHEAD PLAQUES ON EXISTING SIGNS AT WB EXIT RAMP TO US 70 AND PLACE EXIT-ONLY DOTTED WHITE PAVEMENT MARKINGS.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 AND IN US 70 GORE AREA TO PLACE TRAFFIC IN THE OUTSIDE LANE TO PROTECT WORKERS IN MEDIAN.
 - PERFORM TRENCH AND SHOULDER PREP OF INSIDE 6' SHOULDER FROM STA. 5819+50.00 TO STA. 244+12.50.
 - PERFORM RUMBLE STRIP REMOVAL OF INSIDE 4' PAVED SHOULDER FROM STA. 5808+65.00 TO STA. 5819+50.00. ALSO PERFORM TRENCH AND SHOULDER PREP OF ADJACENT 2' UNPAVED INSIDE SHOULDER IN THIS AREA.
 - REMOVE EXISTING GUARDRAIL AND CONCRETE PIER PROTECTION INSIDE WB MAIN LANES AT BRIDGE NO. 03248 AND FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER.

LEGEND

- UNDER CONSTRUCTION
- STAGE IA TRAFFIC
- TRAFFIC DRUM

**MAINTENANCE OF TRAFFIC DETAILS
STAGES I & IA**

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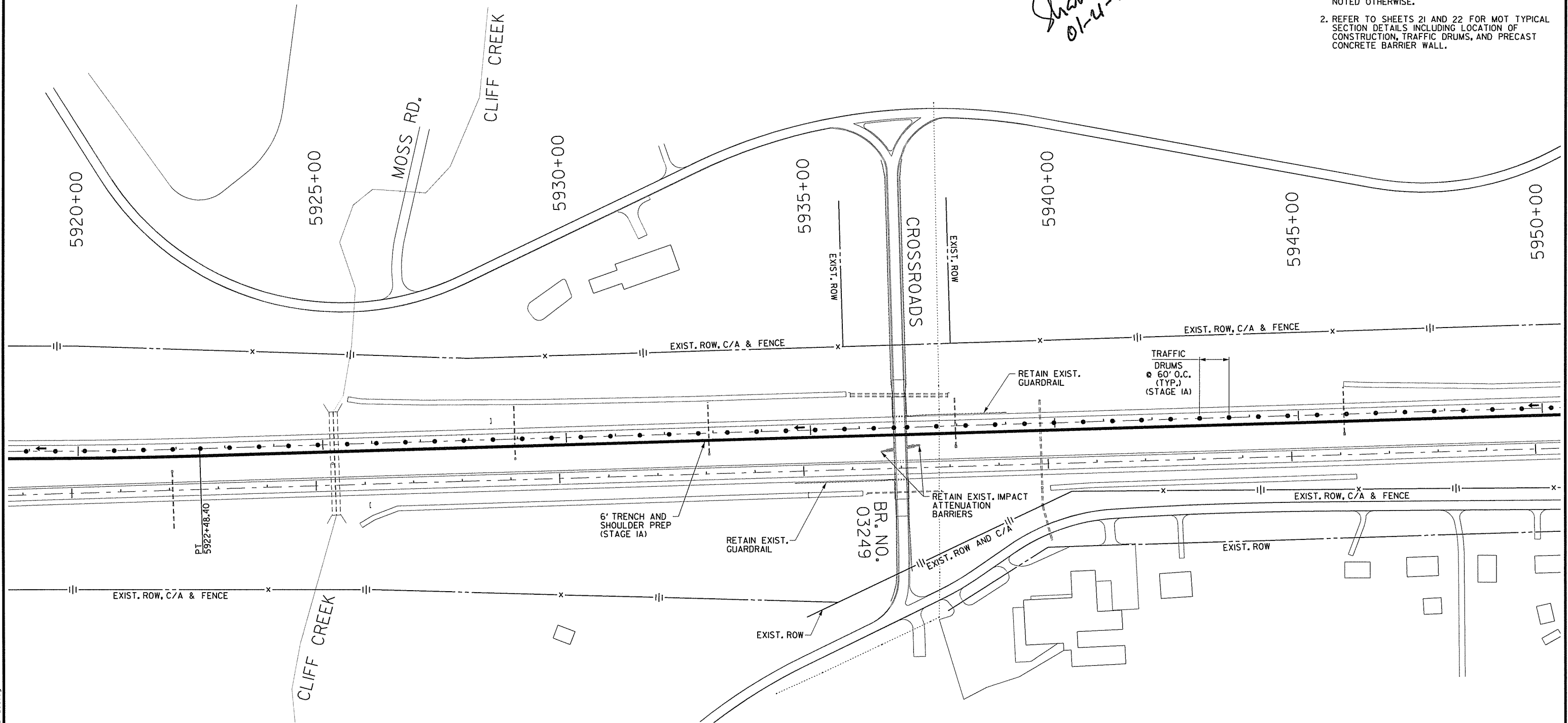
STAGE IA QUANTITIES
TRAFFIC DRUMS = 348 EACH
FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.

STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRIAR AZAD
No. 12448
01-11-2015

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
				JOB NO.	BB0602		31	114

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE I
- TRAFFIC IN NORMAL PATTERN ON I-30.
 - PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE IA
- PLACE TEMPORARY OVERHEAD PLAQUES ON EXISTING SIGNS AT WB EXIT RAMP TO US 70 AND PLACE EXIT-ONLY DOTTED WHITE PAVEMENT MARKINGS.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 AND IN US 70 GORE AREA TO PLACE TRAFFIC IN THE OUTSIDE LANE TO PROTECT WORKERS IN MEDIAN.
 - PERFORM TRENCH AND SHOULDER PREP OF INSIDE 6' SHOULDER FROM STA. 5819+50.00 TO STA. 244+12.50.
 - PERFORM RUMBLE STRIP REMOVAL OF INSIDE 4' PAVED SHOULDER FROM STA. 5808+65.00 TO STA. 5819+50.00. ALSO PERFORM TRENCH AND SHOULDER PREP OF ADJACENT 2' UNPAVED INSIDE SHOULDER IN THIS AREA.
 - REMOVE EXISTING GUARDRAIL AND CONCRETE PIER PROTECTION INSIDE WB MAIN LANES AT BRIDGE NO. 03248 AND FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER.

LEGEND

- UNDER CONSTRUCTION
- STAGE IA TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGES I & IA

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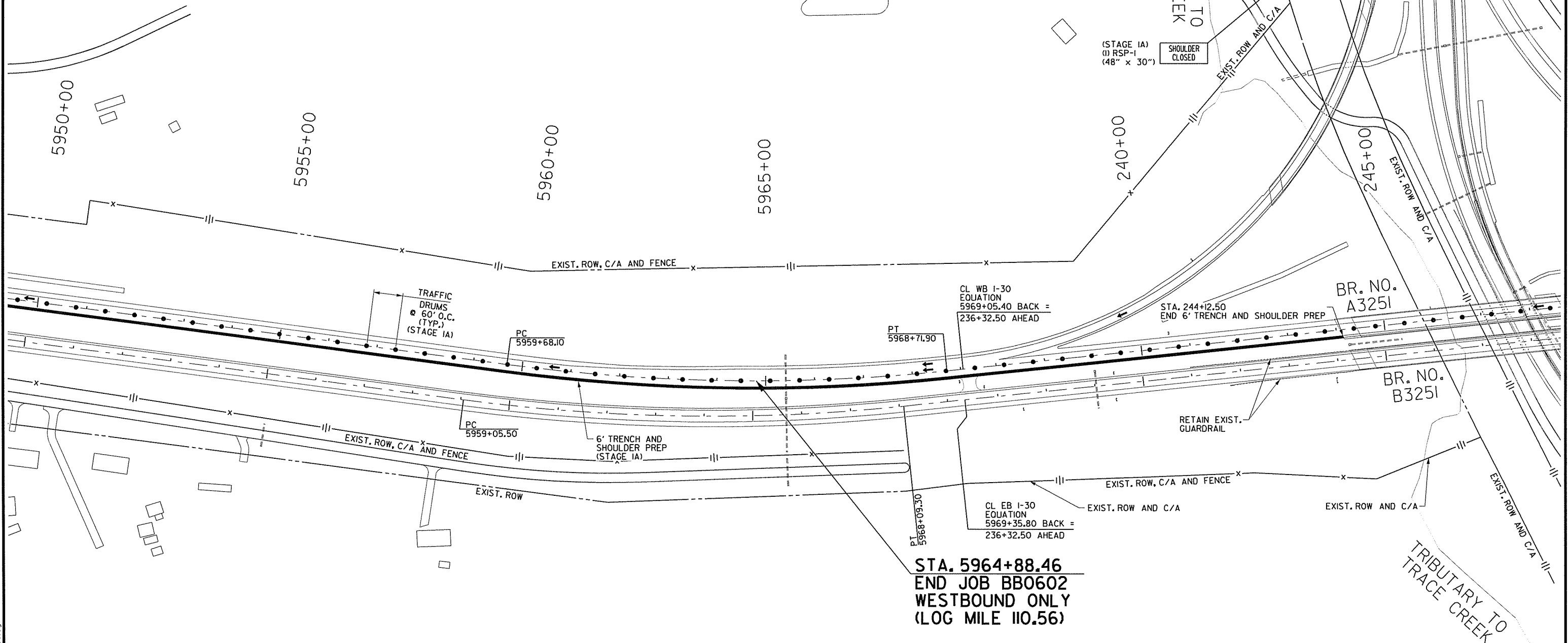
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STAGE IA QUANTITIES
TRAFFIC DRUMS = 348 EACH
FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.

STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRIAR AZAD
No. 2304
01-21-2015

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
						JOB NO.	BB0602	32
						② MAINTENANCE OF TRAFFIC DETAILS		

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STA. 5964+88.46
END JOB BB0602
WESTBOUND ONLY
(LOG MILE 110.56)

- STAGE I
- TRAFFIC IN NORMAL PATTERN ON I-30.
 - PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE IA
- PLACE TEMPORARY OVERHEAD PLAQUES ON EXISTING SIGNS AT WB EXIT RAMP TO US 70 AND PLACE EXIT-ONLY DOTTED WHITE PAVEMENT MARKINGS.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 AND IN US TO GORE AREA TO PLACE TRAFFIC IN THE OUTSIDE LANE TO PROTECT WORKERS IN MEDIAN.
 - PERFORM TRENCH AND SHOULDER PREP OF INSIDE 6' SHOULDER FROM STA. 5819+50.00 TO STA. 244+12.50.
 - PERFORM RUMBLE STRIP REMOVAL OF INSIDE 4' PAVED SHOULDER FROM STA. 5808+65.00 TO STA. 5819+50.00. ALSO PERFORM TRENCH AND SHOULDER PREP OF ADJACENT 2' UNPAVED INSIDE SHOULDER IN THIS AREA.
 - REMOVE EXISTING GUARDRAIL AND CONCRETE PIER PROTECTION INSIDE WB MAIN LANES AT BRIDGE NO. 03248 AND FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER.

LEGEND

- UNDER CONSTRUCTION
- STAGE IA TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGES I & IA

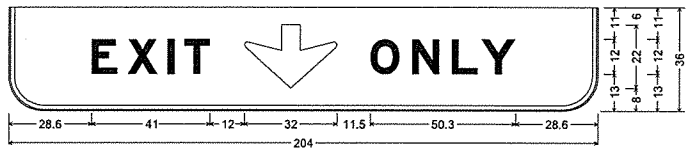
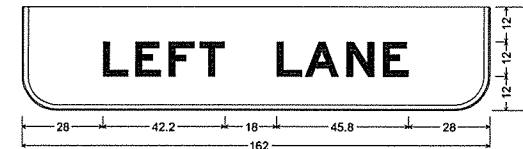
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1/21/2015

STAGE IA QUANTITIES
TRAFFIC DRUMS = 348 EACH
FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.

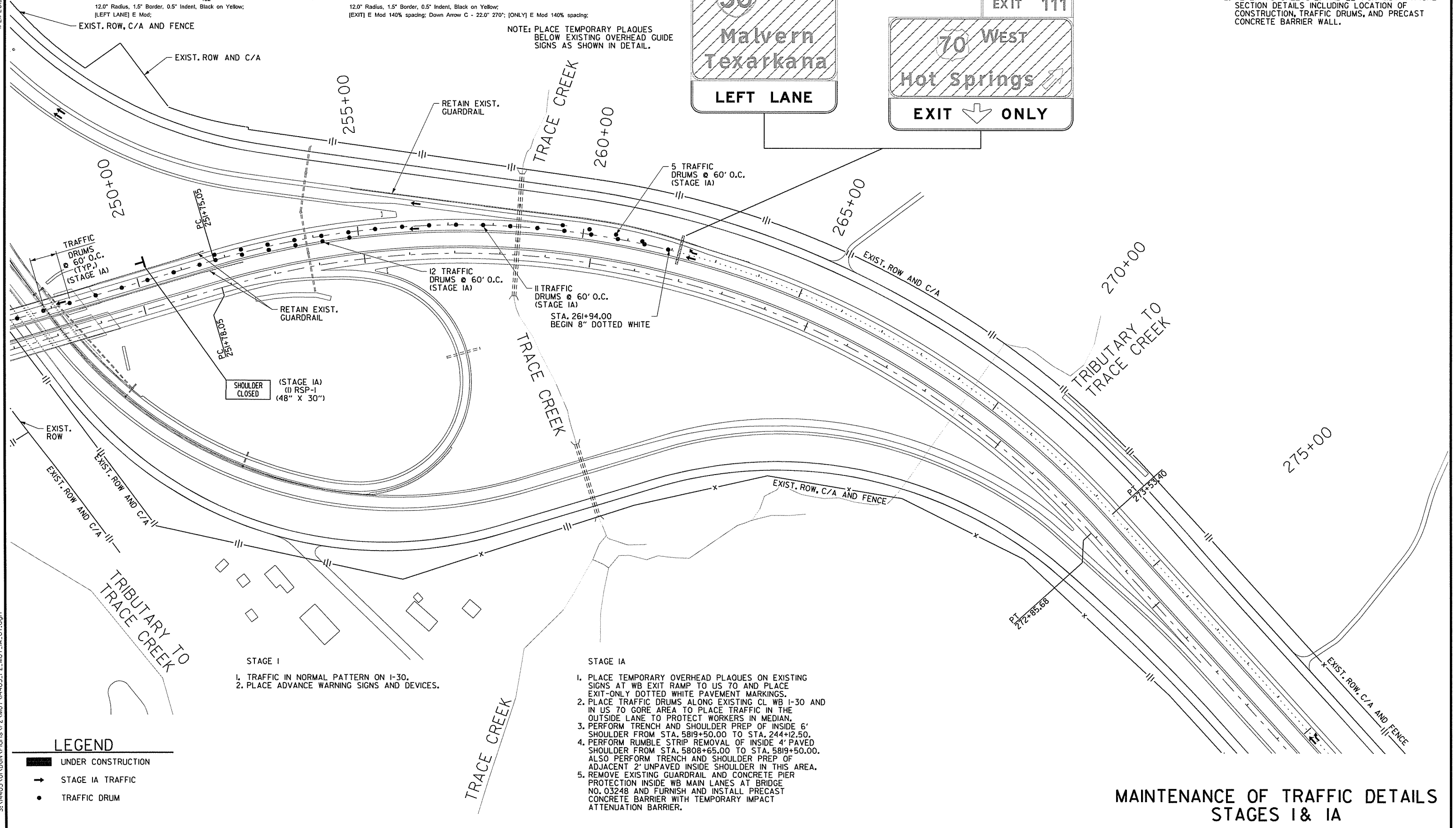
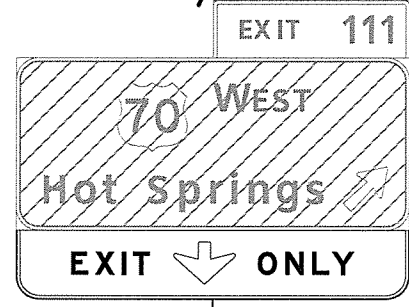
STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
No. 12404
CARRIAR
6/1-2-2015

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
						JOB NO.	BB0602	33
						MAINTENANCE OF TRAFFIC DETAILS		

NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
2. REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



NOTE: PLACE TEMPORARY PLAQUES BELOW EXISTING OVERHEAD GUIDE SIGNS AS SHOWN IN DETAIL.



SHOULDER (STAGE IA)
(1) RSP-1
(48" X 30")
CLOSED

12 TRAFFIC DRUMS @ 60' O.C. (STAGE IA)
STA. 261+94.00
BEGIN 8" DOTTED WHITE

5 TRAFFIC DRUMS @ 60' O.C. (STAGE IA)

RETAIN EXIST. GUARDRAIL

EXIST. ROW AND C/A

EXIST. ROW, C/A AND FENCE

TRAFFIC DRUMS @ 60' O.C. (TYP.) (STAGE IA)

PC 251+15.05

PC 501+15.05

TRIBUTARY TO TRACE CREEK

TRACE CREEK

TRACE CREEK

TRIBUTARY TO TRACE CREEK

275+00

270+00

265+00

260+00

255+00

250+00

EXIST. ROW

EXIST. ROW AND C/A

EXIST. ROW AND C/A

EXIST. ROW, C/A AND FENCE

EXIST. ROW, C/A AND FENCE

STAGE I
1. TRAFFIC IN NORMAL PATTERN ON I-30.
2. PLACE ADVANCE WARNING SIGNS AND DEVICES.

STAGE IA
1. PLACE TEMPORARY OVERHEAD PLAQUES ON EXISTING SIGNS AT WB EXIT RAMP TO US 70 AND PLACE EXIT-ONLY DOTTED WHITE PAVEMENT MARKINGS.
2. PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 AND IN US 70 GORE AREA TO PLACE TRAFFIC IN THE OUTSIDE LANE TO PROTECT WORKERS IN MEDIAN.
3. PERFORM TRENCH AND SHOULDER PREP OF INSIDE 6' SHOULDER FROM STA. 5819+50.00 TO STA. 244+12.50.
4. PERFORM RUMBLE STRIP REMOVAL OF INSIDE 4' PAVED SHOULDER FROM STA. 5808+65.00 TO STA. 5819+50.00. ALSO PERFORM TRENCH AND SHOULDER PREP OF ADJACENT 2' UNPAVED INSIDE SHOULDER IN THIS AREA.
5. REMOVE EXISTING GUARDRAIL AND CONCRETE PIER PROTECTION INSIDE WB MAIN LANES AT BRIDGE NO. 03248 AND FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER.

LEGEND

- UNDER CONSTRUCTION
- STAGE IA TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS STAGES I & IA

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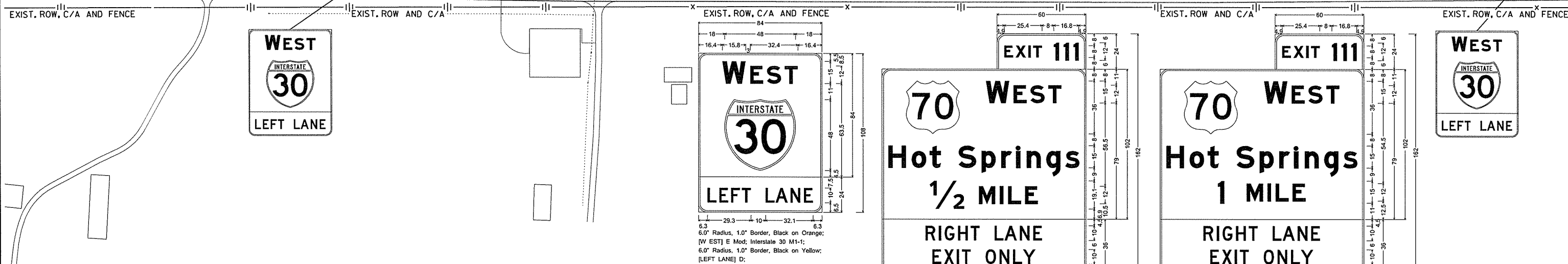
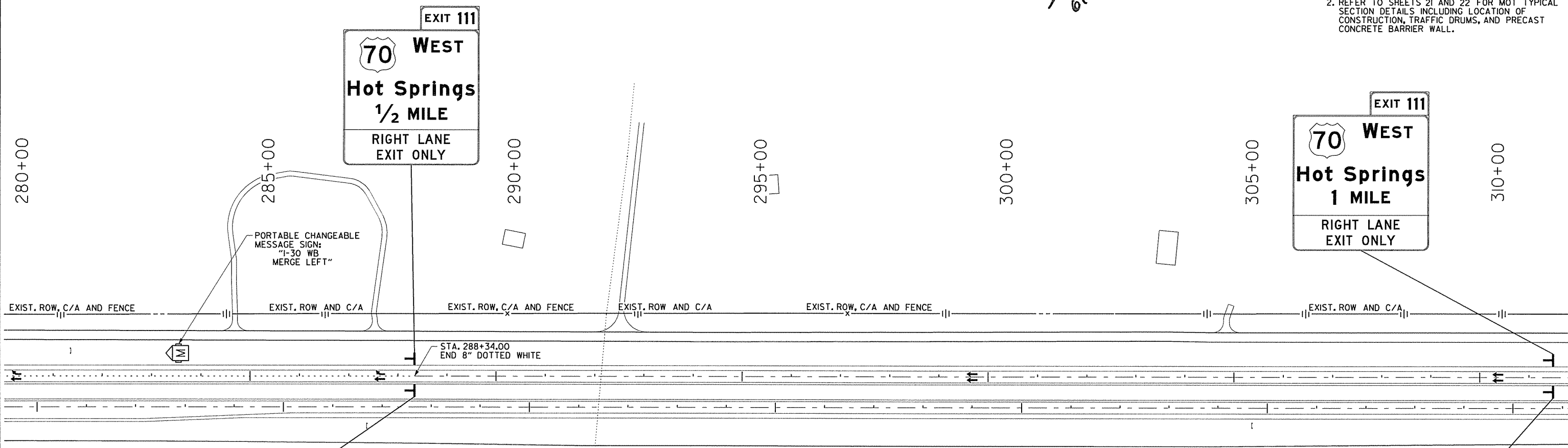
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STAGE IA QUANTITIES
 TRAFFIC DRUMS = 348 EACH
 FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 120 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 19460 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 660 LIN. FT.



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
						JOB NO.	BB0602	34
						② MAINTENANCE OF TRAFFIC DETAILS		

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE I
- TRAFFIC IN NORMAL PATTERN ON I-30.
 - PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE IA
- PLACE TEMPORARY OVERHEAD PLAQUES ON EXISTING SIGNS AT WB EXIT RAMP TO US 70 AND PLACE EXIT-ONLY DOTTED WHITE PAVEMENT MARKINGS.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 AND IN US 70 GORE AREA TO PLACE TRAFFIC IN THE OUTSIDE LANE TO PROTECT WORKERS IN MEDIAN.
 - PERFORM TRENCH AND SHOULDER PREP OF INSIDE 6' SHOULDER FROM STA. 5819+50.00 TO STA. 244+12.50.
 - PERFORM RUMBLE STRIP REMOVAL OF INSIDE 4' PAVED SHOULDER FROM STA. 5808+65.00 TO STA. 5819+50.00. ALSO PERFORM TRENCH AND SHOULDER PREP OF ADJACENT 2' UNPAVED INSIDE SHOULDER IN THIS AREA.
 - REMOVE EXISTING GUARDRAIL AND CONCRETE PIER PROTECTION INSIDE WB MAIN LANES AT BRIDGE NO. 03248 AND FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER.

LEGEND

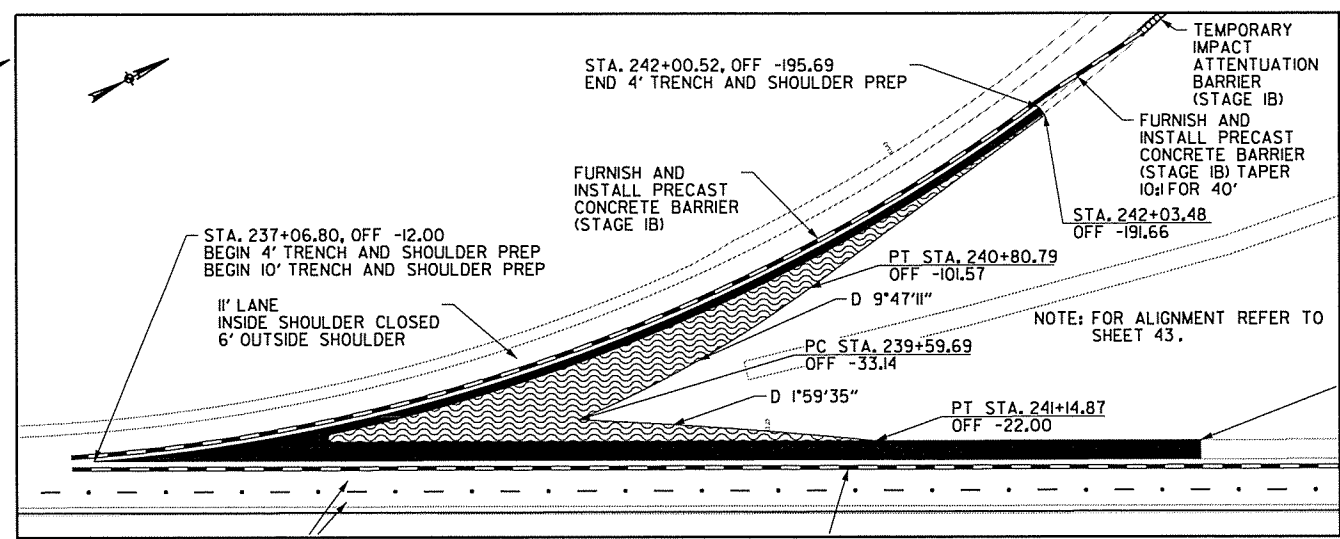
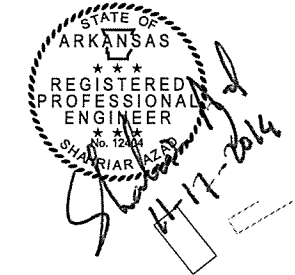
- UNDER CONSTRUCTION
- STAGE IA TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS STAGES I & IA

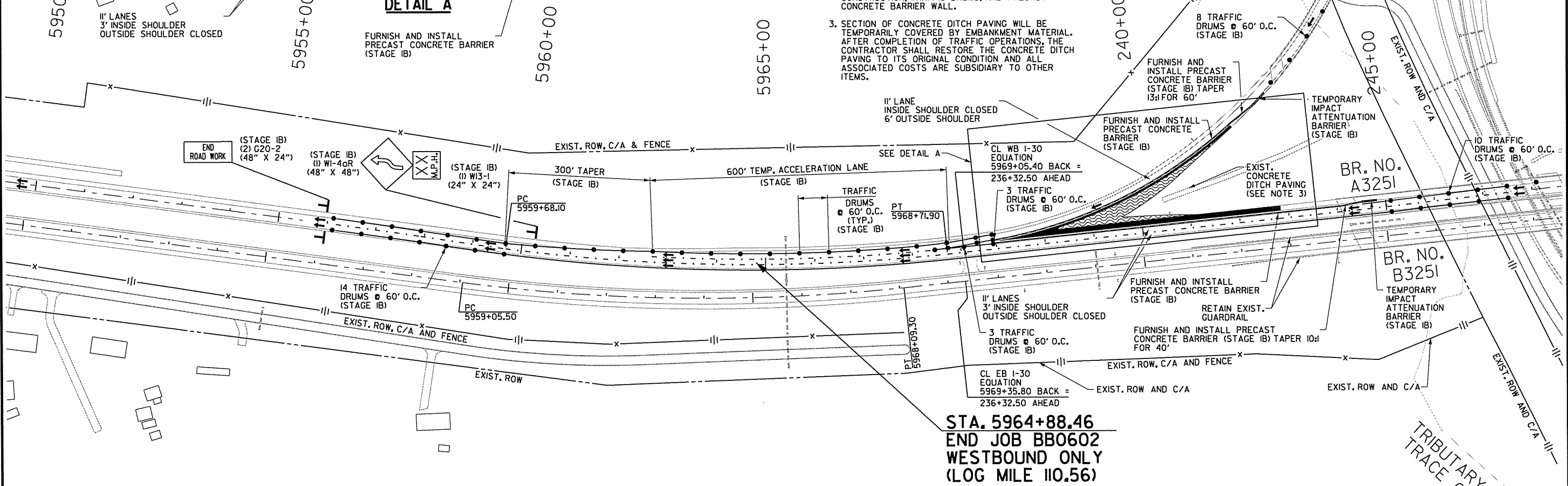
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DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0602							35	114

MAINTENANCE OF TRAFFIC DETAILS



- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.
 3. SECTION OF CONCRETE DITCH PAVING WILL BE TEMPORARILY COVERED BY EMBANKMENT MATERIAL. AFTER COMPLETION OF TRAFFIC OPERATIONS, THE CONTRACTOR SHALL RESTORE THE CONCRETE DITCH PAVING TO ITS ORIGINAL CONDITION AND ALL ASSOCIATED COSTS ARE SUBSIDIARY TO OTHER ITEMS.



STAGE IB QUANTITIES
 TRAFFIC DRUMS = 84 EACH
 FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 4776 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4974 LIN. FT.

LEGEND

- UNDER CONSTRUCTION
- CONSTRUCT TEMPORARY PAVEMENT
- STAGE IB TRAFFIC
- TRAFFIC DRUM

- STAGE IB
1. PLACE ADVANCE WARNING SIGNS AND DEVICES.
 2. FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER OUTSIDE OF WB I-30 CENTERLINE NEAR US 70 ENTRANCE RAMP TO PRODUCE TWO 11' TRAVEL LANES. THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 3' OF THE 6' SHOULDER.
 3. FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER INSIDE OF CENTERLINE OF ENTRANCE RAMP FROM US 70 SUCH THAT THE RAMP WILL USE 11' OF THE EXISTING RAMP LANE TO PRODUCE AN 11' RAMP LANE.
 4. PERFORM TRENCH AND SHOULDER PREP OF INSIDE 4' SHOULDER ON ENTRANCE RAMP FROM US 70.
 5. PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 10' MAIN LANE SHOULDER FROM STA. 237+06.80 TO STA. 242+84.39.
 6. CONSTRUCT TEMPORARY PAVEMENT IN GORE BETWEEN US 70 ENTRANCE RAMP AND WB I-30 MAIN LANES.

MAINTENANCE OF TRAFFIC DETAILS
STAGE IB

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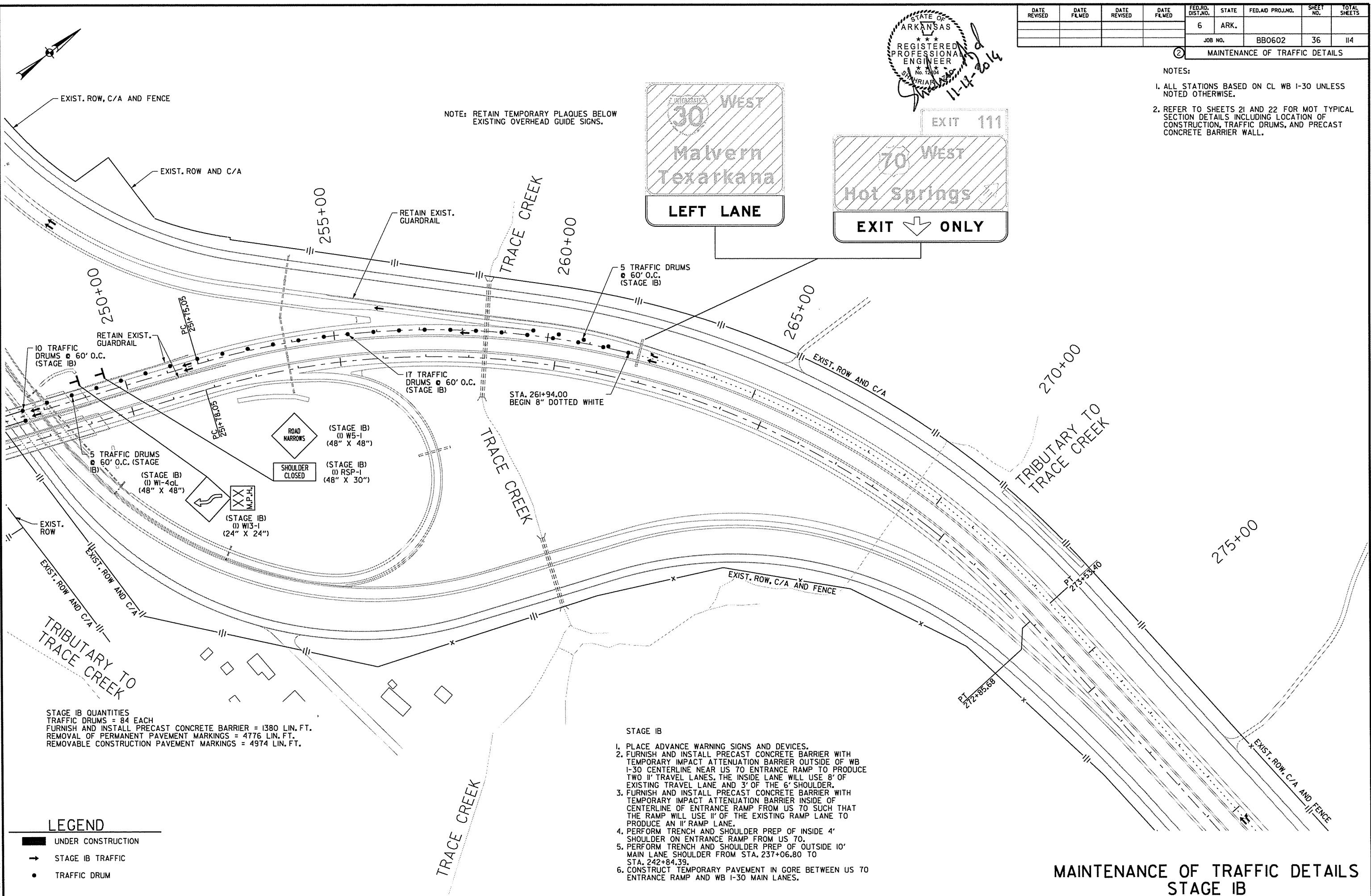
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DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		36	114
② MAINTENANCE OF TRAFFIC DETAILS								

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.

NOTE: RETAIN TEMPORARY PLAQUES BELOW EXISTING OVERHEAD GUIDE SIGNS.



STAGE IB QUANTITIES
 TRAFFIC DRUMS = 84 EACH
 FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 4776 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4974 LIN. FT.

LEGEND

- UNDER CONSTRUCTION
- STAGE IB TRAFFIC
- TRAFFIC DRUM

- STAGE IB
- PLACE ADVANCE WARNING SIGNS AND DEVICES.
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER OUTSIDE OF WB I-30 CENTERLINE NEAR US 70 ENTRANCE RAMP TO PRODUCE TWO 11' TRAVEL LANES. THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 3' OF THE 6' SHOULDER.
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER INSIDE OF CENTERLINE OF ENTRANCE RAMP FROM US 70 SUCH THAT THE RAMP WILL USE 11' OF THE EXISTING RAMP LANE TO PRODUCE AN 11' RAMP LANE.
 - PERFORM TRENCH AND SHOULDER PREP OF INSIDE 4' SHOULDER ON ENTRANCE RAMP FROM US 70.
 - PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 10' MAIN LANE SHOULDER FROM STA. 237+06.80 TO STA. 242+84.39.
 - CONSTRUCT TEMPORARY PAVEMENT IN GORE BETWEEN US 70 ENTRANCE RAMP AND WB I-30 MAIN LANES.

**MAINTENANCE OF TRAFFIC DETAILS
STAGE IB**

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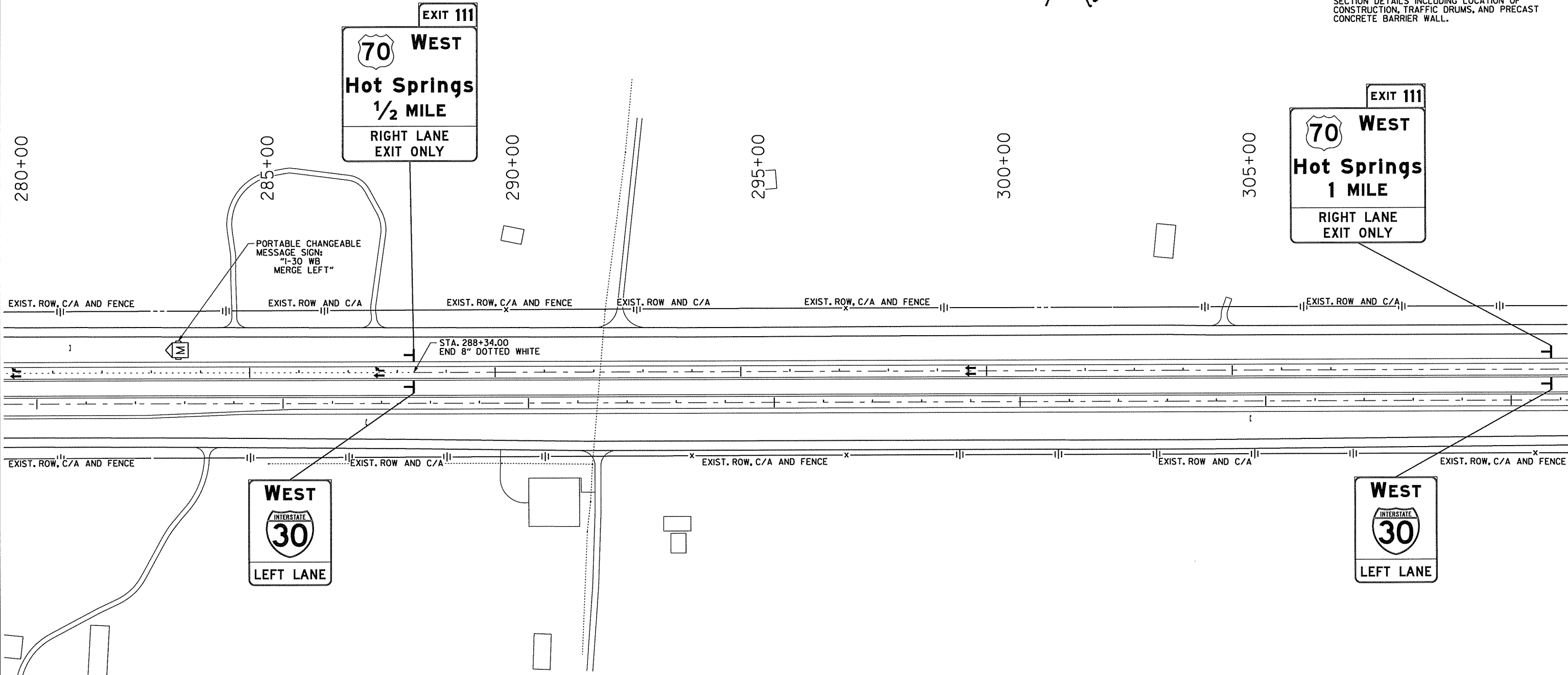


STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
No. 12404
SHARIAR ALD
12-6-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. AID DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602		37	114

MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 1B QUANTITIES
 TRAFFIC DRUMS = 84 EACH
 FURNISH AND INSTALL PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 4776 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4974 LIN. FT.

LEGEND

	UNDER CONSTRUCTION
	STAGE 1B TRAFFIC
	TRAFFIC DRUM

- STAGE 1B
- PLACE ADVANCE WARNING SIGNS AND DEVICES.
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER OUTSIDE OF WB I-30 CENTERLINE NEAR US 70 ENTRANCE RAMP TO PRODUCE TWO 11' TRAVEL LANES. THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 3' OF THE 6' SHOULDER.
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH TEMPORARY IMPACT ATTENUATION BARRIER INSIDE OF CENTERLINE OF ENTRANCE RAMP FROM US 70 SUCH THAT THE RAMP WILL USE 11' OF THE EXISTING RAMP LANE TO PRODUCE AN 11' RAMP LANE.
 - PERFORM TRENCH AND SHOULDER PREP OF INSIDE 4' SHOULDER ON ENTRANCE RAMP FROM US 70.
 - PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 10' MAIN LANE SHOULDER FROM STA. 237+06.80 TO STA. 242+84.39.
 - CONSTRUCT TEMPORARY PAVEMENT IN GORE BETWEEN US 70 ENTRANCE RAMP AND WB I-30 MAIN LANES.

MAINTENANCE OF TRAFFIC DETAILS
STAGE 1B

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STAGE 2A QUANTITIES
TRAFFIC DRUMS = 100 EACH
FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.

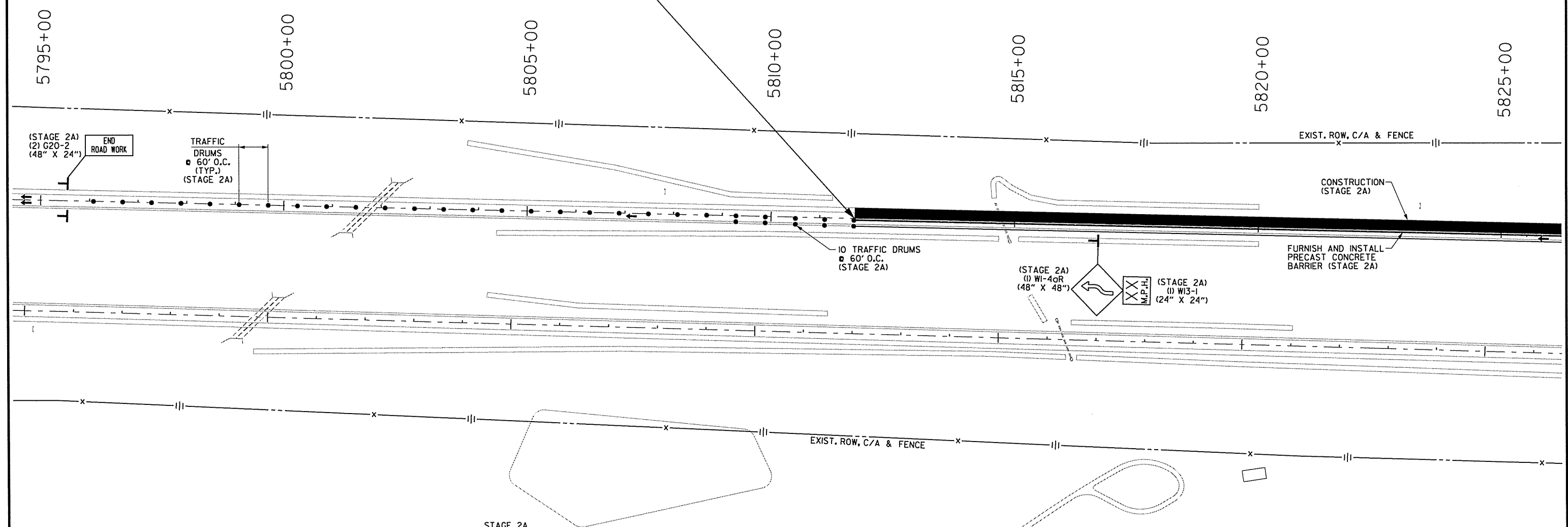
STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRIR AZAD
No. 12404
11-17-2014

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		38	114

MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR NOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.

STA. 5811+71.49
BEGIN JOB BB0602
WESTBOUND ONLY
(LOG MILE 107.64)



STAGE 2A
1. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 2A
- FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE. (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
 - CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
 - CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE RAMP FROM US 70.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2A TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGE 2 & 2A

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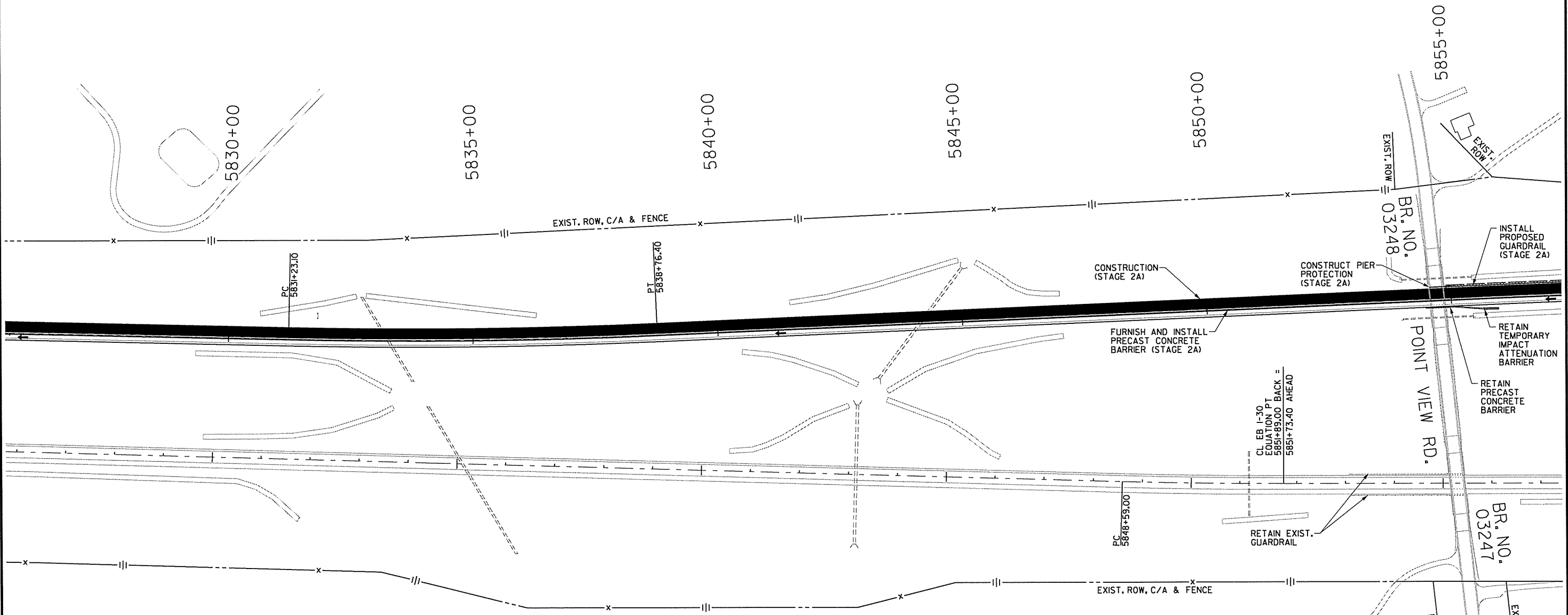
STAGE 2A QUANTITIES
 TRAFFIC DRUMS = 100 EACH
 FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
 RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
 CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRAR AZAD
 No. 2404
 11-17-2014

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		BB0602	39	114

② MAINTENANCE OF TRAFFIC DETAILS

NOTES:
 1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 2A
 1. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 2A
- FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE. (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
 - CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
 - CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE RAMP FROM US 70.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2A TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2 & 2A

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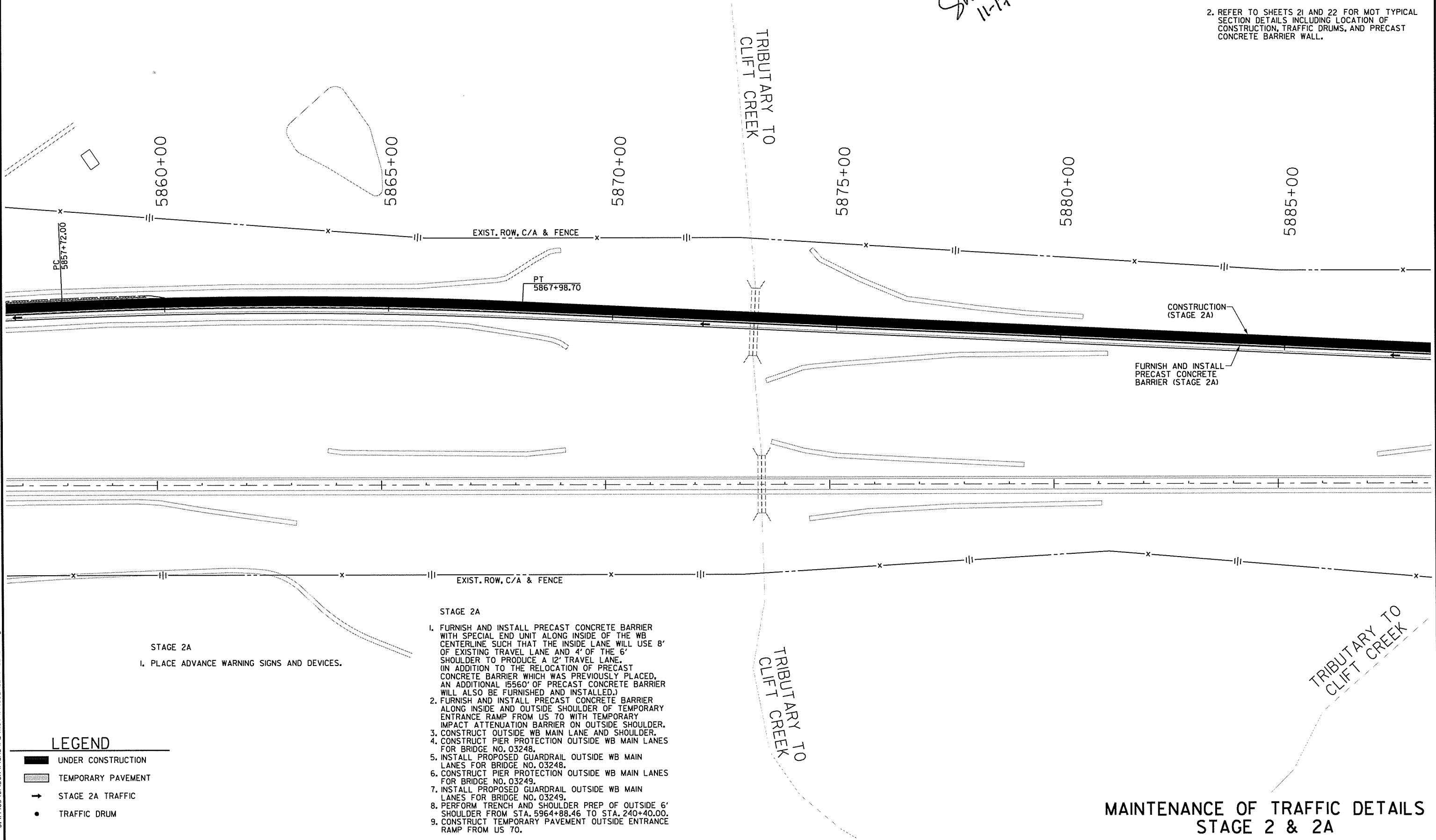
STAGE 2A QUANTITIES
 TRAFFIC DRUMS = 100 EACH
 FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
 RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
 CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.



DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		40	114

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 2A
 1. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 2A
1. FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE. (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
 2. FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
 3. CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
 4. CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 5. INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 6. CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 7. INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 8. PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
 9. CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE RAMP FROM US 70.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2A TRAFFIC
- TRAFFIC DRUM

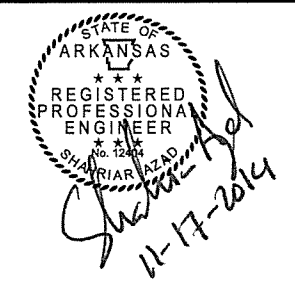
MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2 & 2A

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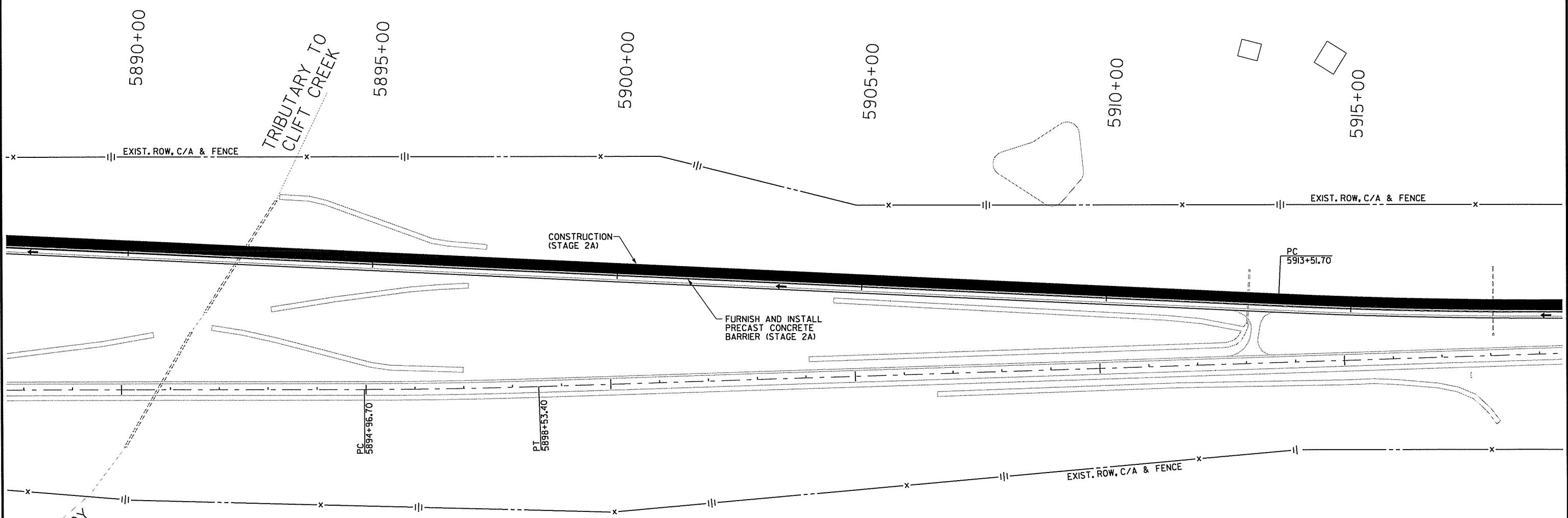
STAGE 2A QUANTITIES
 TRAFFIC DRUMS = 100 EACH
 FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
 RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
 CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.



DATE REVISED	DATE FLMED	DATE REVISED	DATE FLMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602		41	114

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 2A

- STAGE 2A
- PLACE ADVANCE WARNING SIGNS AND DEVICES.

- FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE. (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
- FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
- CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
- CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
- INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
- CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
- INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
- PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
- CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE RAMP FROM US 70.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2A TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2 & 2A

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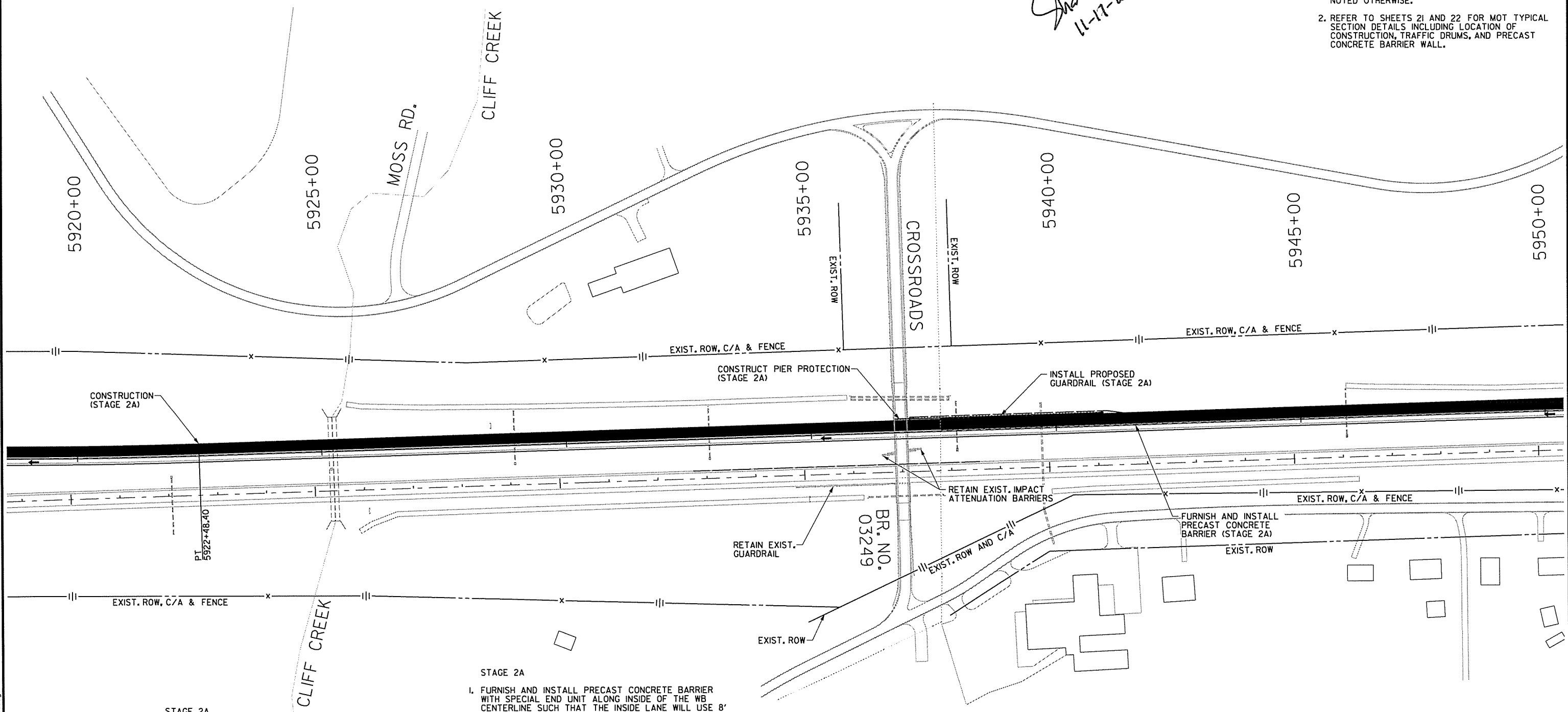
STAGE 2A QUANTITIES
 TRAFFIC DRUMS = 100 EACH
 FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
 RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
 CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.



DATE REVISED	DATE FLMED	DATE REVISED	DATE FLMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		42	114

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 2A
 1. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 2A
- FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE. (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
 - CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
 - CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE RAMP FROM US 70.

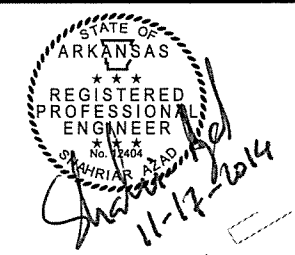
LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2A TRAFFIC
- TRAFFIC DRUM

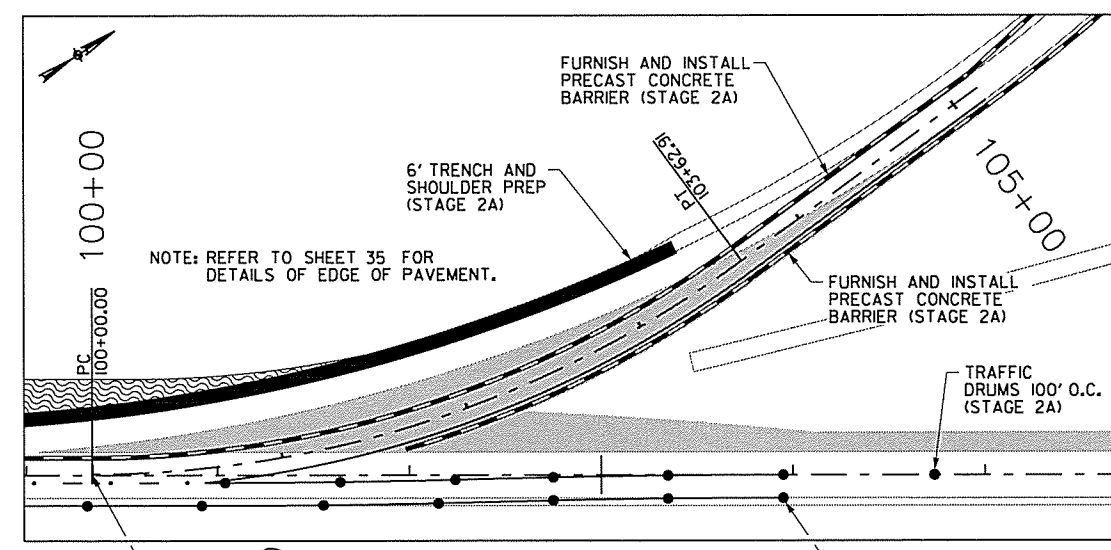
MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2 & 2A

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DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		43	114

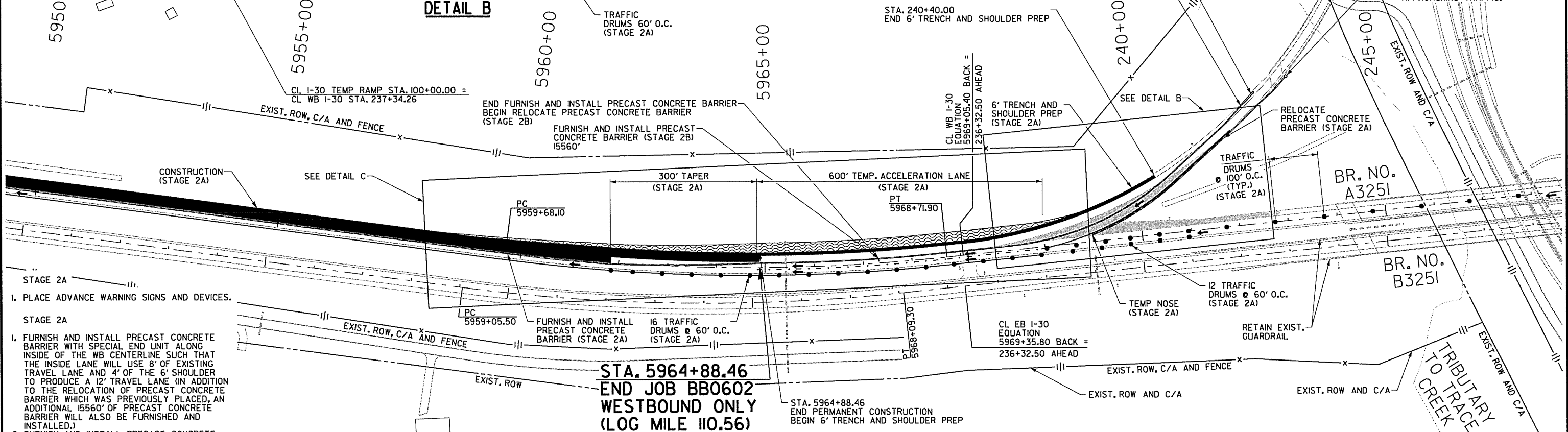


MAINTENANCE OF TRAFFIC DETAILS



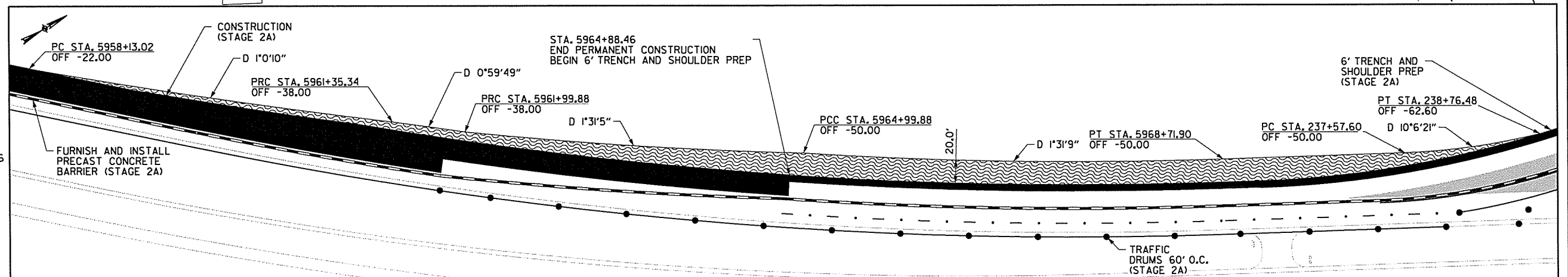
STAGE 2A QUANTITIES
 TRAFFIC DRUMS = 100 EACH
 FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
 RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
 CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.

- NOTES:**
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 2A**
- PLACE ADVANCE WARNING SIGNS AND DEVICES.
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
 - CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
 - CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE RAMP FROM US 70.

STA. 5964+88.46
END JOB BB0602
WESTBOUND ONLY
(LOG MILE 110.56)



LEGEND

- UNDER CONSTRUCTION
- CONSTRUCT TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT
- STAGE 1B TRAFFIC
- TRAFFIC DRUM

DETAIL C

MAINTENANCE OF TRAFFIC DETAILS
STAGE 2 & 2A

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11/7/2014

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STAGE 2A QUANTITIES
 TRAFFIC DRUMS = 100 EACH
 FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
 RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
 CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.

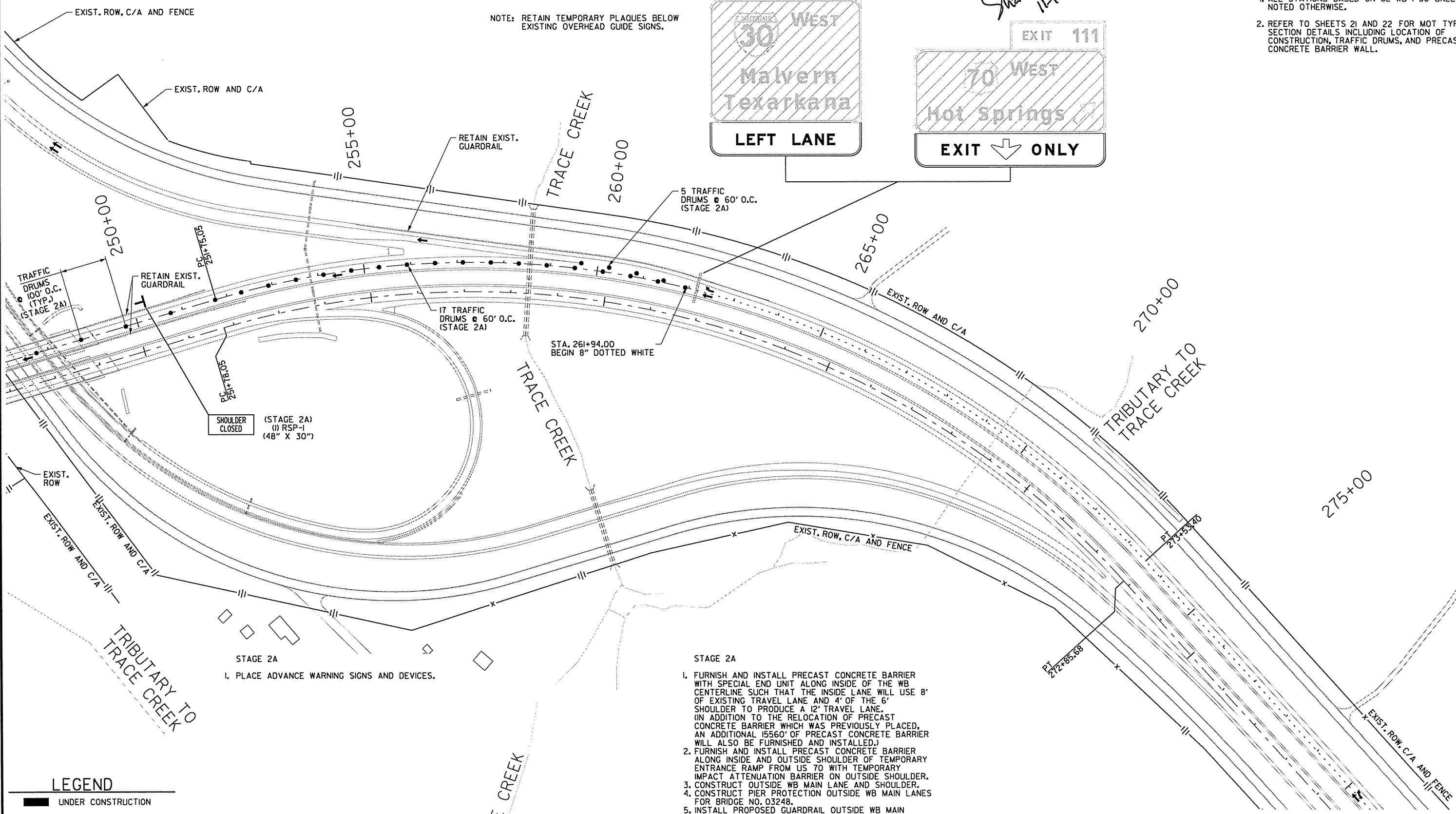
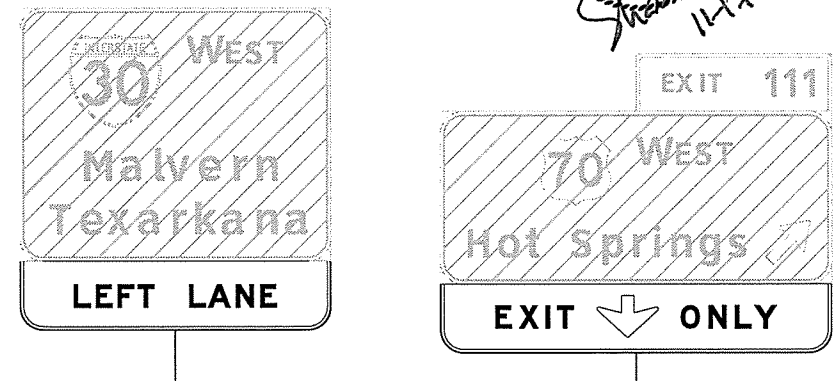


DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		44	114

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO SHEETS 21 AND 22 FOR NOT TYPICAL SECTION DETAILS, INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.

NOTE: RETAIN TEMPORARY PLAQUES BELOW EXISTING OVERHEAD GUIDE SIGNS.



SHOULDER CLOSED (STAGE 2A)
 (1) RSP-1
 (48" X 30")

STA. 261+94.00
 BEGIN 8" DOTTED WHITE

LEGEND

- UNDER CONSTRUCTION
- ▨ TEMPORARY PAVEMENT
- STAGE 1B TRAFFIC
- TRAFFIC DRUM

STAGE 2A
 1. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 2A
1. FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE. (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
 2. FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
 3. CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
 4. CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 5. INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 6. CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 7. INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 8. PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
 9. CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE

MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2 & 2A

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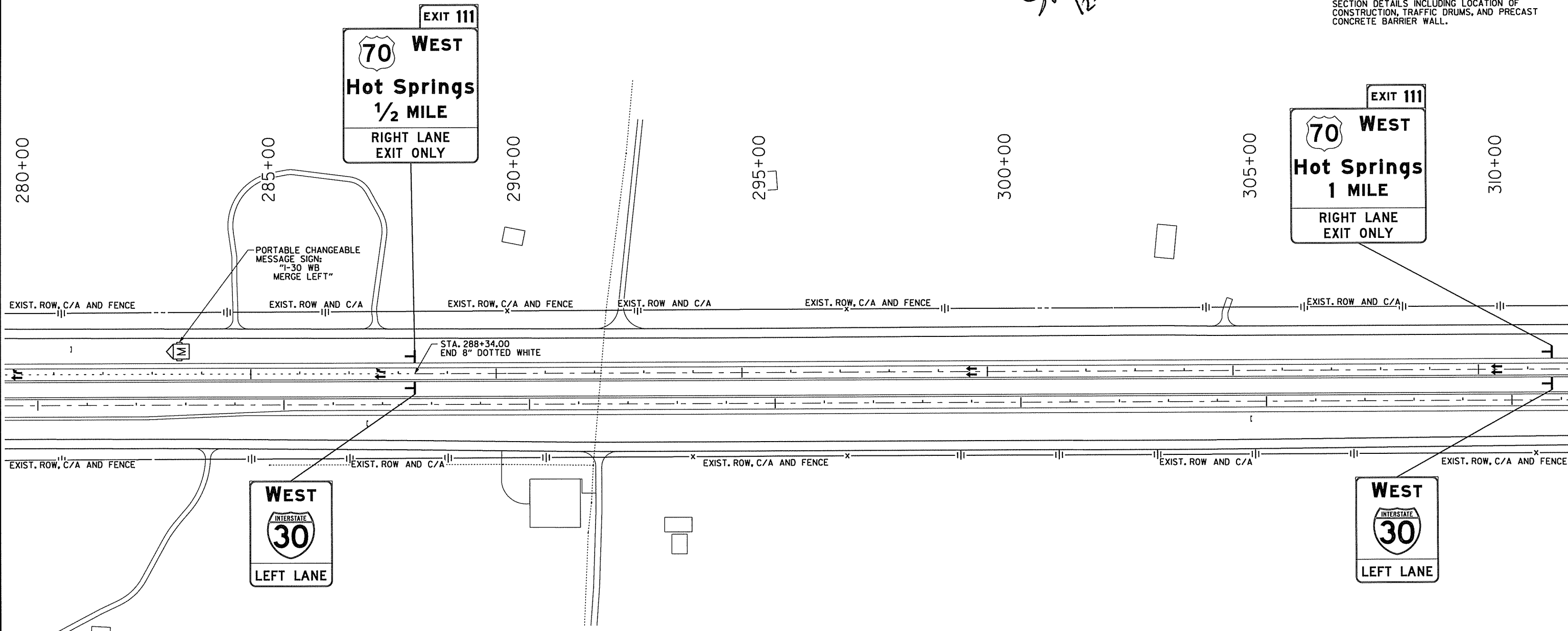
STAGE 2A QUANTITIES
 TRAFFIC DRUMS = 100 EACH
 FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER = 15560 LIN. FT.
 RELOCATING PRECAST CONCRETE BARRIER = 1380 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 882 LIN. FT.
 CONSTRUCTION PAVEMENT MARKINGS = 30633 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 4590 LIN. FT.

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 0404
 SHAHRIAR AZAD
 12-6-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO.	BB0602
							SHEET NO.	45
							TOTAL SHEETS	114

MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 2A
 1. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 2A
- FURNISH AND INSTALL PRECAST CONCRETE BARRIER WITH SPECIAL END UNIT ALONG INSIDE OF THE WB CENTERLINE SUCH THAT THE INSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 6' SHOULDER TO PRODUCE A 12' TRAVEL LANE. (IN ADDITION TO THE RELOCATION OF PRECAST CONCRETE BARRIER WHICH WAS PREVIOUSLY PLACED, AN ADDITIONAL 15560' OF PRECAST CONCRETE BARRIER WILL ALSO BE FURNISHED AND INSTALLED.)
 - FURNISH AND INSTALL PRECAST CONCRETE BARRIER ALONG INSIDE AND OUTSIDE SHOULDER OF TEMPORARY ENTRANCE RAMP FROM US 70 WITH TEMPORARY IMPACT ATTENUATION BARRIER ON OUTSIDE SHOULDER.
 - CONSTRUCT OUTSIDE WB MAIN LANE AND SHOULDER.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - CONSTRUCT PIER PROTECTION OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - INSTALL PROPOSED GUARDRAIL OUTSIDE WB MAIN LANES FOR BRIDGE NO. 03249.
 - PERFORM TRENCH AND SHOULDER PREP OF OUTSIDE 6' SHOULDER FROM STA. 5964+88.46 TO STA. 240+40.00.
 - CONSTRUCT TEMPORARY PAVEMENT OUTSIDE ENTRANCE RAMP FROM US 70.

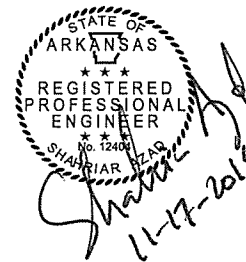
LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2A TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2 & 2A

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11/7/2014

STAGE 2B
TRAFFIC DRUMS = 100 EACH
RELOCATING PRECAST CONCRETE BARRIER = 17060 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 1434 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 35154 LIN. FT.

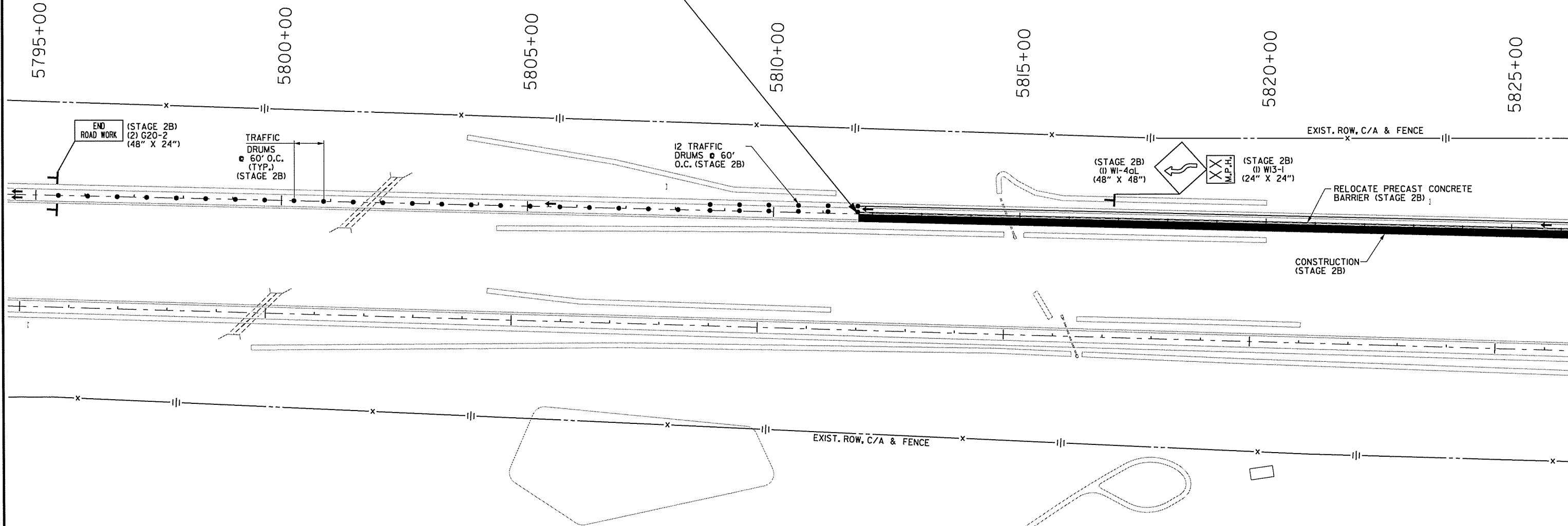


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	46	114	

MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.

STA. 5811+71.49
BEGIN JOB BB0602
WESTBOUND ONLY
(LOG MILE 107.64)



- STAGE 2B
- RELOCATE PRECAST CONCRETE BARRIER ALONG OUTSIDE OF THE WB CENTERLINE SUCH THAT THE OUTSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 10' SHOULDER TO PRODUCE A 12' TRAVEL LANE.
 - CONSTRUCT INSIDE WB MAIN LANE AND SHOULDER.
 - INSTALL WB MAIN LANE CENTER SKIP AND INSIDE SHOULDER PERMANENT PAVEMENT MARKINGS.
 - CONSTRUCT PIER PROTECTION INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03249.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGE 2B

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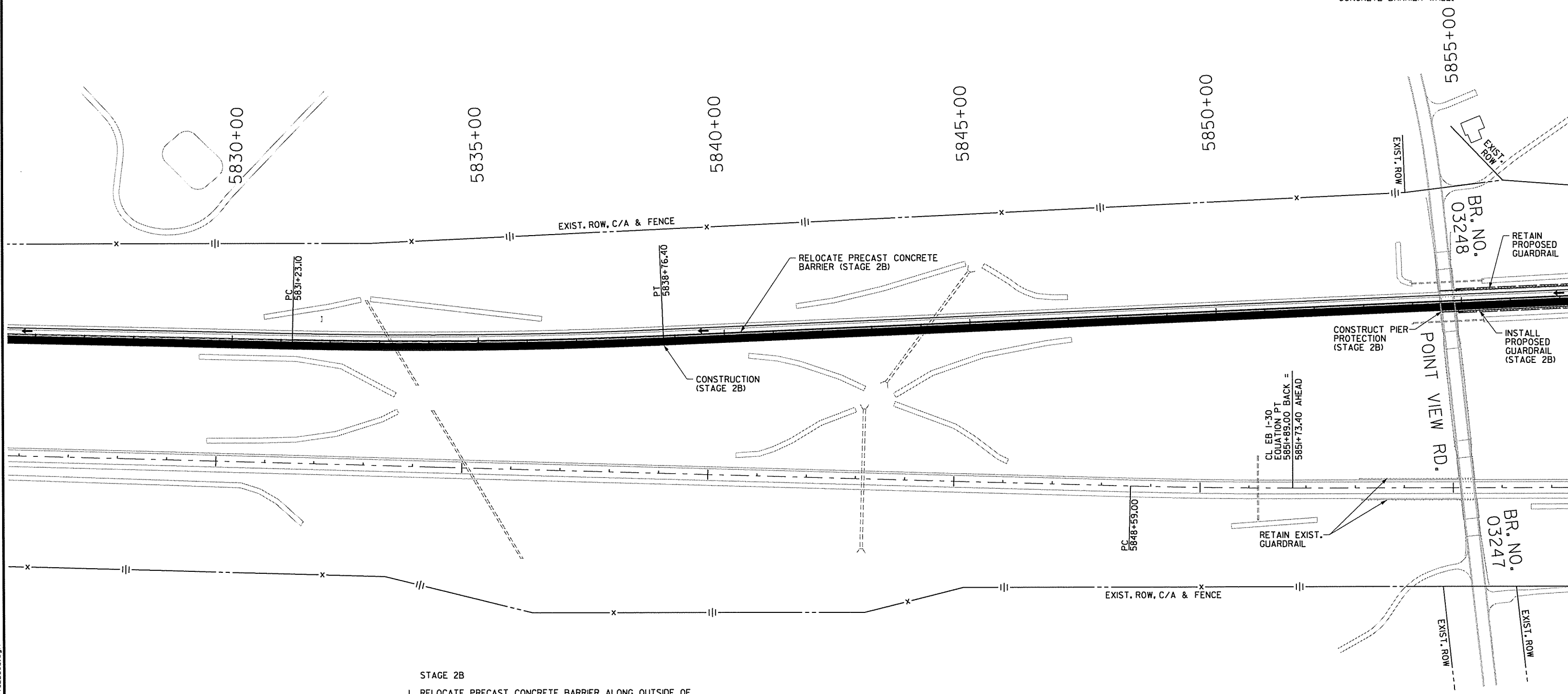
STAGE 2B
 TRAFFIC DRUMS = 100 EACH
 RELOCATING PRECAST CONCRETE BARRIER = 17060 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 1434 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 35154 LIN. FT.

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 2247
 SAHRIAH A. AD
 11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	47	114	

MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 2B
- RELOCATE PRECAST CONCRETE BARRIER ALONG OUTSIDE OF THE WB CENTERLINE SUCH THAT THE OUTSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 10' SHOULDER TO PRODUCE A 12' TRAVEL LANE.
 - CONSTRUCT INSIDE WB MAIN LANE AND SHOULDER.
 - INSTALL WB MAIN LANE CENTER SKIP AND INSIDE SHOULDER PERMANENT PAVEMENT MARKINGS.
 - CONSTRUCT PIER PROTECTION INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03249.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGE 2B

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11/7/2014

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STAGE 2B
 TRAFFIC DRUMS = 100 EACH
 RELOCATING PRECAST CONCRETE BARRIER = 17060 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 1434 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 35154 LIN. FT.

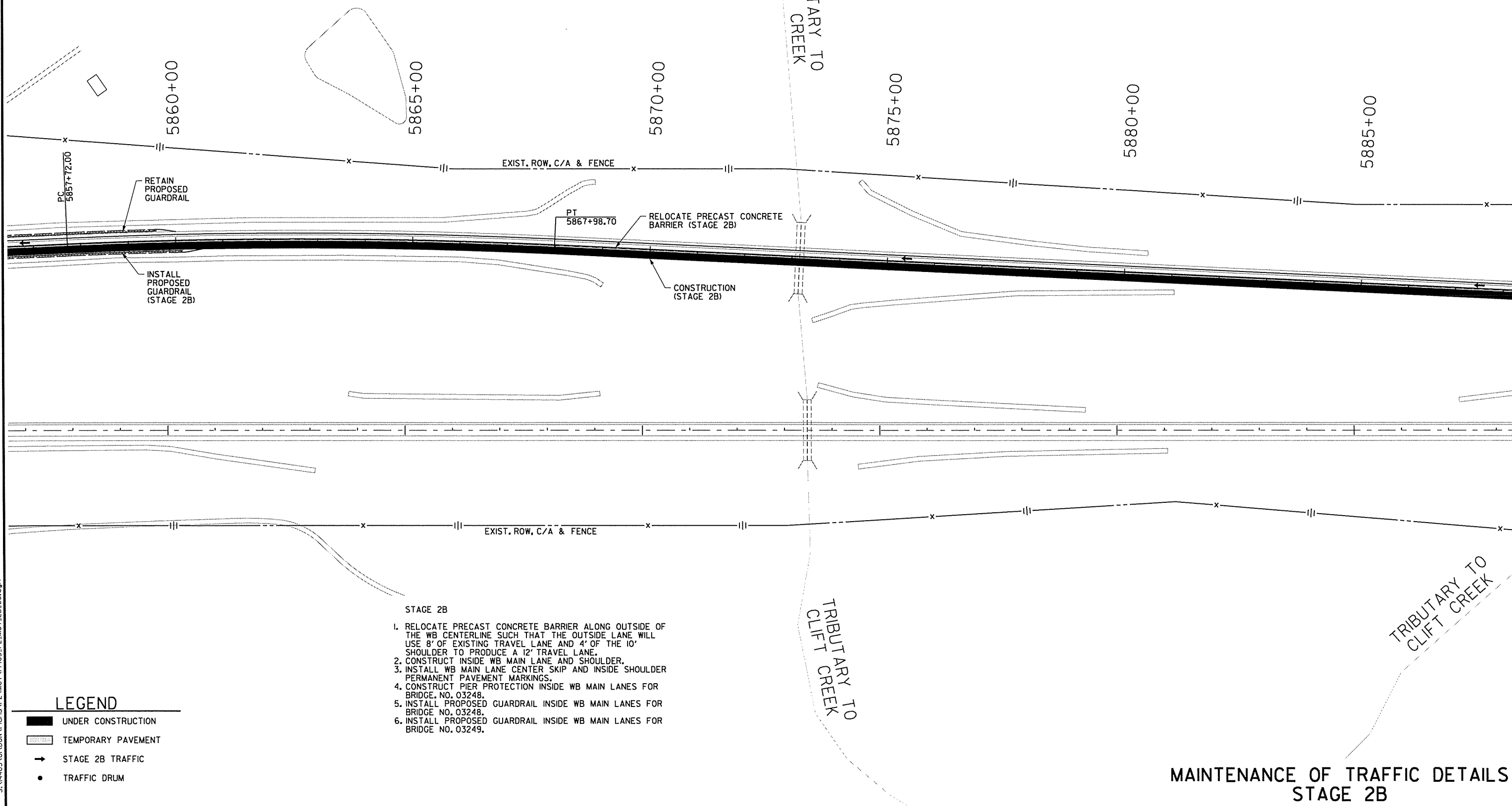
STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 22004
 SHARIAN AZAD
 11-12-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.						BB0602	48	114

② MAINTENANCE OF TRAFFIC DETAILS

NOTES:

1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
2. REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 2B
1. RELOCATE PRECAST CONCRETE BARRIER ALONG OUTSIDE OF THE WB CENTERLINE SUCH THAT THE OUTSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 10' SHOULDER TO PRODUCE A 12' TRAVEL LANE.
 2. CONSTRUCT INSIDE WB MAIN LANE AND SHOULDER.
 3. INSTALL WB MAIN LANE CENTER SKIP AND INSIDE SHOULDER PERMANENT PAVEMENT MARKINGS.
 4. CONSTRUCT PIER PROTECTION INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 5. INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 6. INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03249.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2B

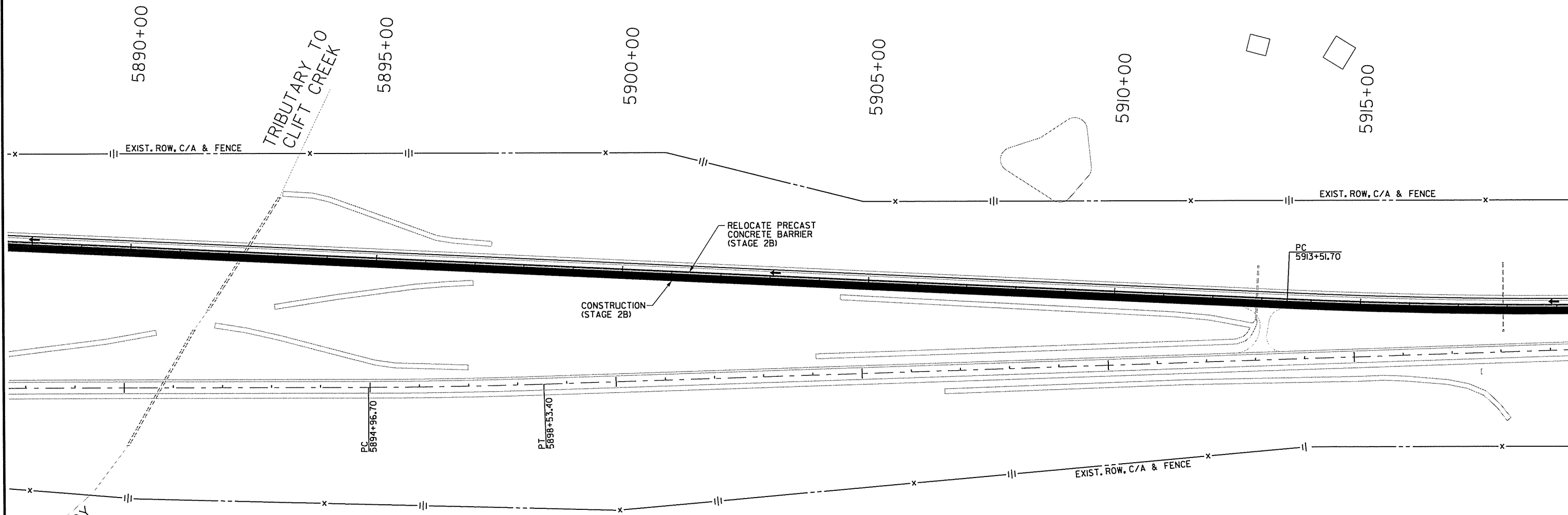
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STAGE 2B
TRAFFIC DRUMS = 100 EACH
RELOCATING PRECAST CONCRETE BARRIER = 17060 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 1434 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 35154 LIN. FT.

STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRIAR AZIZ
11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602	49	114	
MAINTENANCE OF TRAFFIC DETAILS								

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 2B
- RELOCATE PRECAST CONCRETE BARRIER ALONG OUTSIDE OF THE WB CENTERLINE SUCH THAT THE OUTSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 10' SHOULDER TO PRODUCE A 12' TRAVEL LANE.
 - CONSTRUCT INSIDE WB MAIN LANE AND SHOULDER.
 - INSTALL WB MAIN LANE CENTER SKIP AND INSIDE SHOULDER PERMANENT PAVEMENT MARKINGS.
 - CONSTRUCT PIER PROTECTION INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03249.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGE 2B

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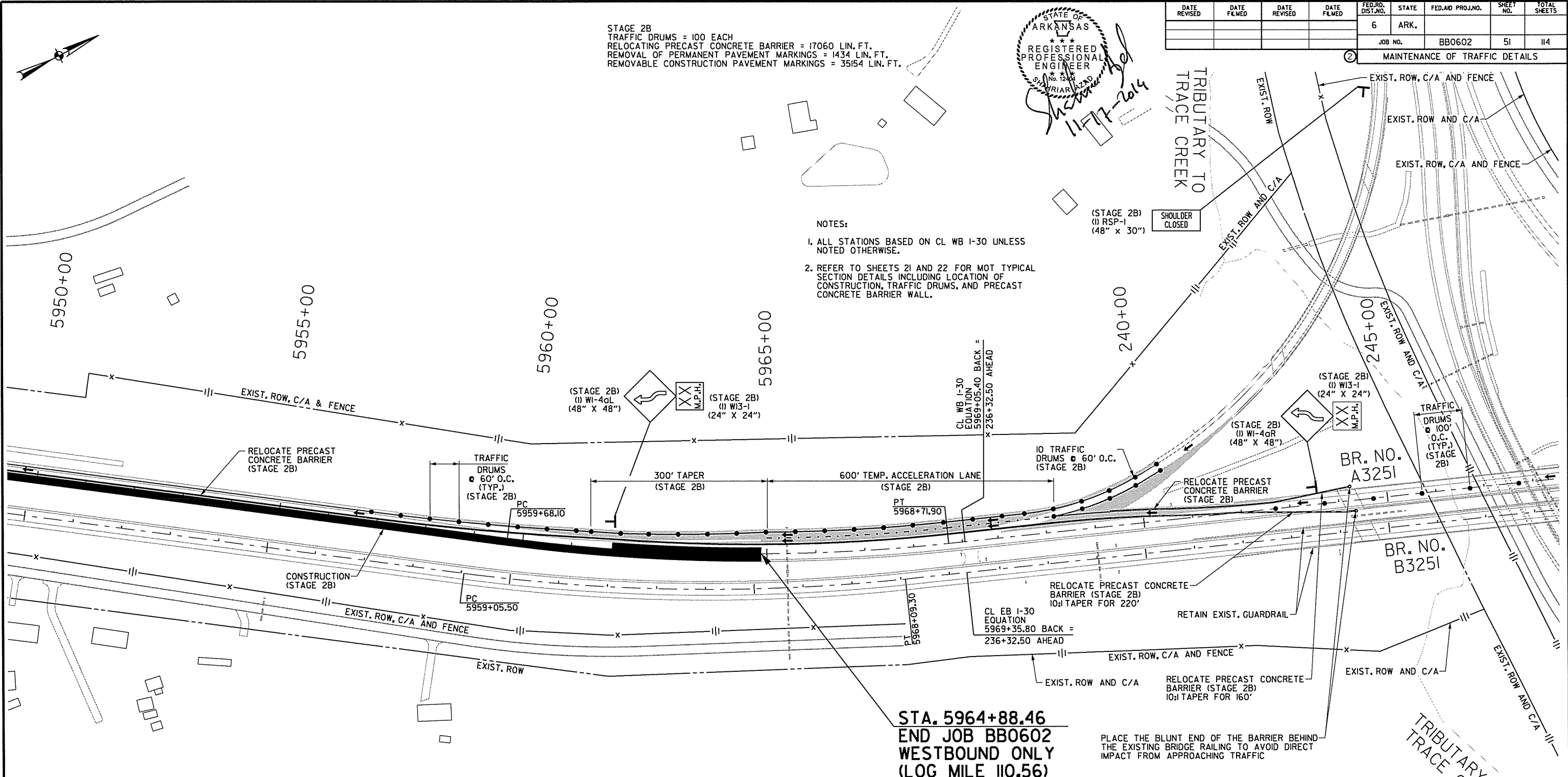
STAGE 2B
 TRAFFIC DRUMS = 100 EACH
 RELOCATING PRECAST CONCRETE BARRIER = 17060 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 1434 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 35154 LIN. FT.

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 12482
 SHARIAR AZAD
 11/17/2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		51	114
				JOB NO.		BB0602		

MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR NOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 2B
- RELOCATE PRECAST CONCRETE BARRIER ALONG OUTSIDE OF THE WB CENTERLINE SUCH THAT THE OUTSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 10' SHOULDER TO PRODUCE A 12' TRAVEL LANE.
 - CONSTRUCT INSIDE WB MAIN LANE AND SHOULDER.
 - INSTALL WB MAIN LANE CENTER SKIP AND INSIDE SHOULDER PERMANENT PAVEMENT MARKINGS.
 - CONSTRUCT PIER PROTECTION INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03249.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
 STAGE 2B

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STAGE 2B
 TRAFFIC DRUMS = 100 EACH
 RELOCATING PRECAST CONCRETE BARRIER = 17060 LIN. FT.
 REMOVAL OF PERMANENT PAVEMENT MARKINGS = 1434 LIN. FT.
 REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 35154 LIN. FT.

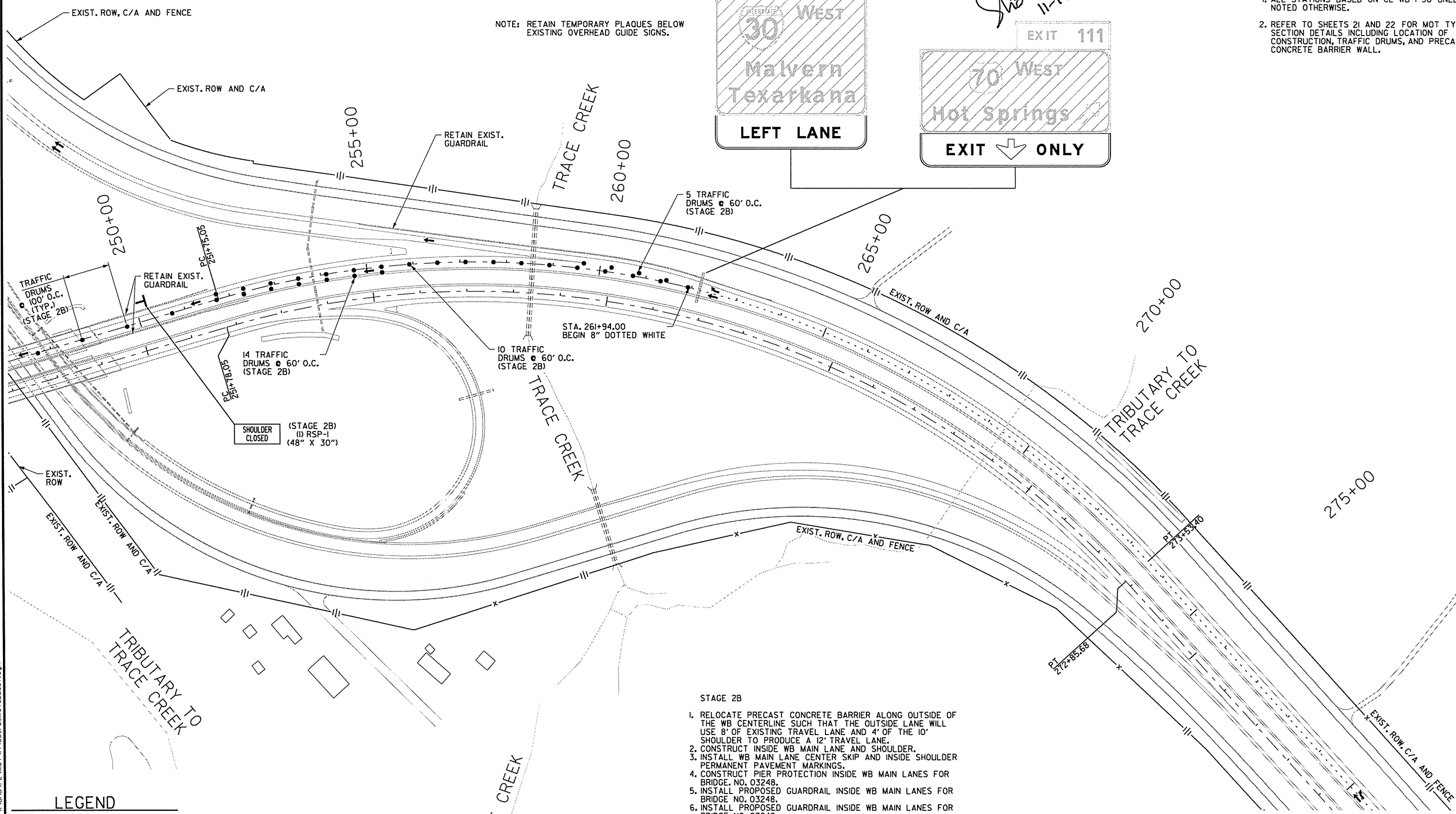
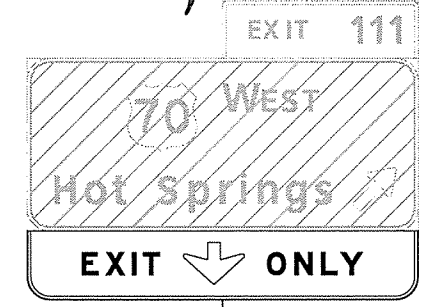


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0602	52 of 114

MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.

NOTE: RETAIN TEMPORARY PLAQUES BELOW EXISTING OVERHEAD GUIDE SIGNS.



LEGEND

- UNDER CONSTRUCTION
- STAGE 2B TRAFFIC
- TRAFFIC DRUM

- STAGE 2B
- RELOCATE PRECAST CONCRETE BARRIER ALONG OUTSIDE OF THE WB CENTERLINE SUCH THAT THE OUTSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 10' SHOULDER TO PRODUCE A 12' TRAVEL LANE.
 - CONSTRUCT INSIDE WB MAIN LANE AND SHOULDER.
 - INSTALL WB MAIN LANE CENTER SKIP AND INSIDE SHOULDER PERMANENT PAVEMENT MARKINGS.
 - CONSTRUCT PIER PROTECTION INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03249.

MAINTENANCE OF TRAFFIC DETAILS
STAGE 2B

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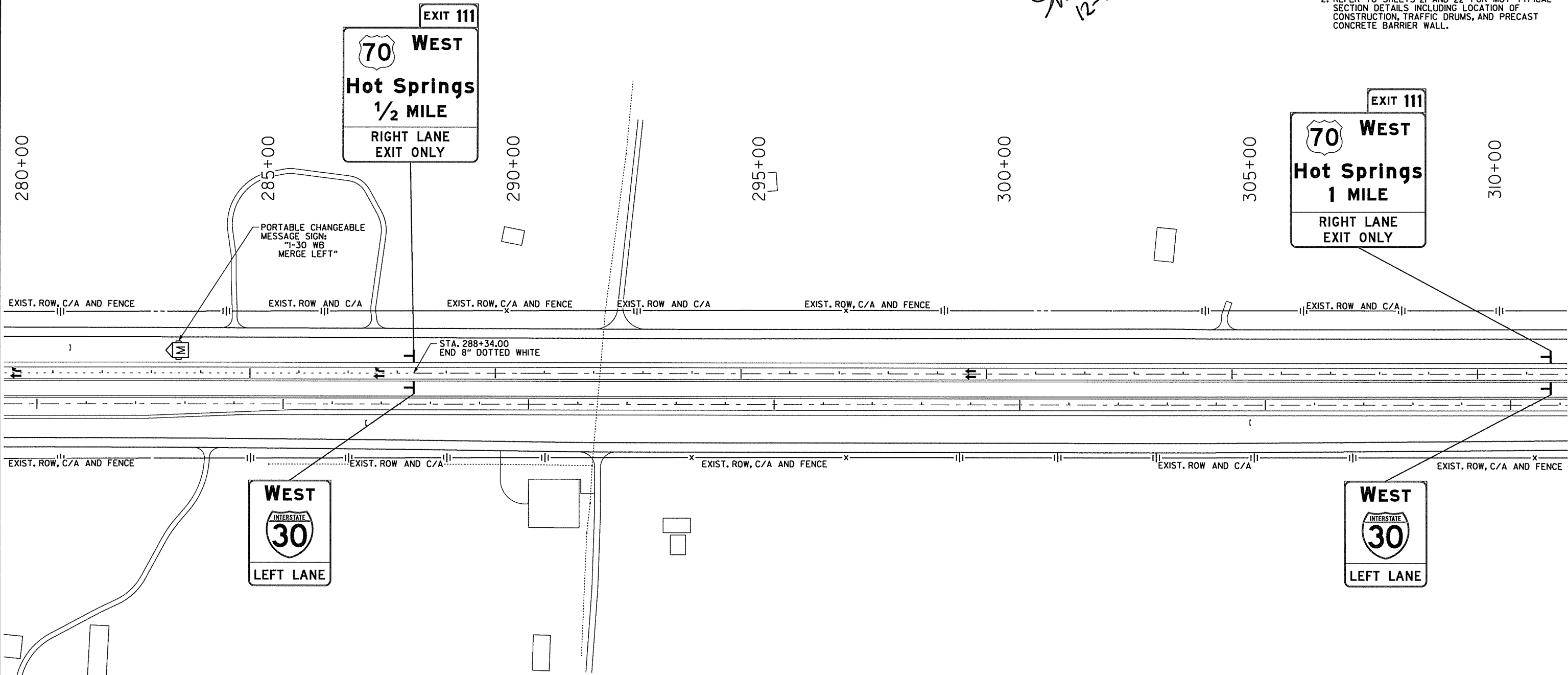
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STAGE 2B
TRAFFIC DRUMS = 100 EACH
RELOCATING PRECAST CONCRETE BARRIER = 17060 LIN. FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 1434 LIN. FT.
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS = 35154 LIN. FT.

STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRIAR AZAD
No. 12804
12-6-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							BB0602	53	114
MAINTENANCE OF TRAFFIC DETAILS									

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 2B
- RELOCATE PRECAST CONCRETE BARRIER ALONG OUTSIDE OF THE WB CENTERLINE SUCH THAT THE OUTSIDE LANE WILL USE 8' OF EXISTING TRAVEL LANE AND 4' OF THE 10' SHOULDER TO PRODUCE A 12' TRAVEL LANE.
 - CONSTRUCT INSIDE WB MAIN LANE AND SHOULDER.
 - INSTALL WB MAIN LANE CENTER SKIP AND INSIDE SHOULDER PERMANENT PAVEMENT MARKINGS.
 - CONSTRUCT PIER PROTECTION INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03248.
 - INSTALL PROPOSED GUARDRAIL INSIDE WB MAIN LANES FOR BRIDGE NO. 03249.

LEGEND

- UNDER CONSTRUCTION
- TEMPORARY PAVEMENT
- STAGE 2B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS STAGE 2B

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11/7/2014

STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH

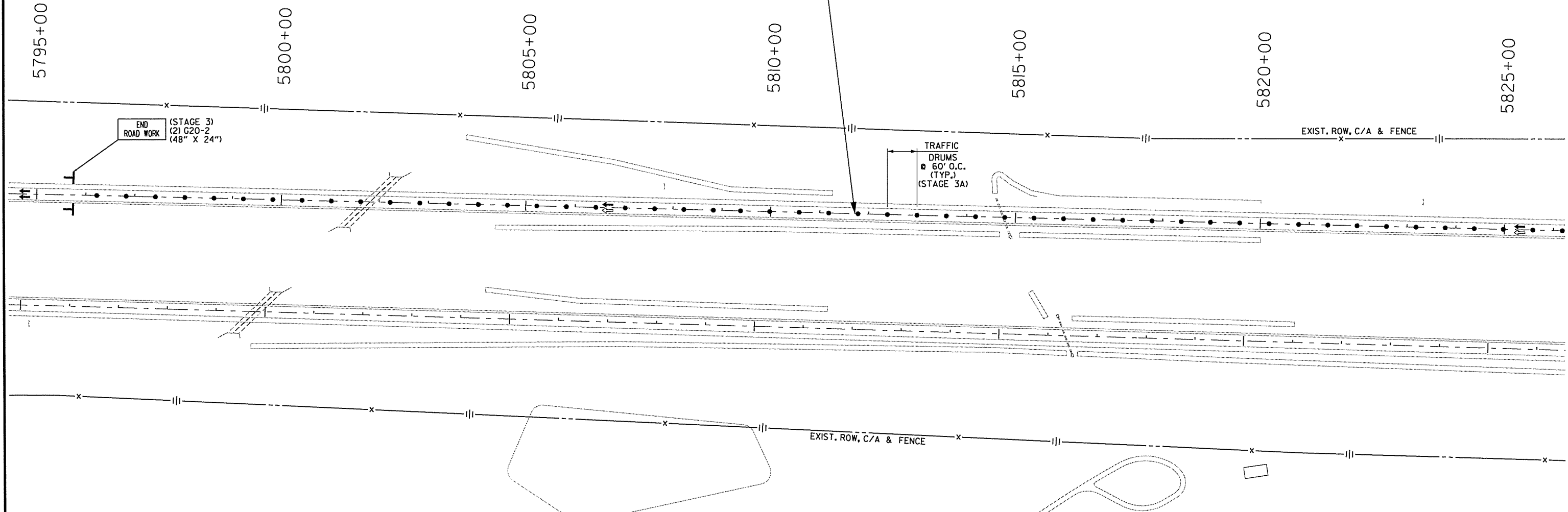


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	54	114	

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.

STA. 5811+71.49
BEGIN JOB BB0602
WESTBOUND ONLY
(LOG MILE 107.64)



STAGE 3
 I. PLACE ADVANCE WARNING SIGNS AND DEVICES.

STAGE 3A

- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
- PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
- USE TRAFFIC DRUMS TO PROTECT WORKERS.
- CONSTRUCT MEDIAN CROSSINGS.
- CONSTRUCT WIRE ROPE SAFETY FENCE AND CONCRETE DITCH PAVING.
- INSTALL INSIDE WB MAIN LANE RUMBLE STRIPS.
- EB LANE CLOSURES AND WORK ALONG INSIDE EB SHOULDERS SHALL ONLY BE PERFORMED DURING NIGHTTIME HOURS.
- PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
- PLACE TRAFFIC DRUMS ALONG EXISTING CL EB I-30 IN AREA OF CONSTRUCTION.
- INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
- REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

STAGE 3B

- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
- PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
- USE TRAFFIC DRUMS TO PROTECT WORKERS.
- INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
- INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
- INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US TO EXIT RAMP.
- REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
- REMOVE TEMPORARY PAVEMENT IN GORE AT US TO ENTRANCE RAMP USED IN STAGE 2.
- RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

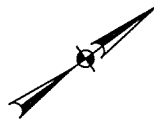
LEGEND

- STAGE 3A TRAFFIC
- ⇄ STAGE 3B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGE 3

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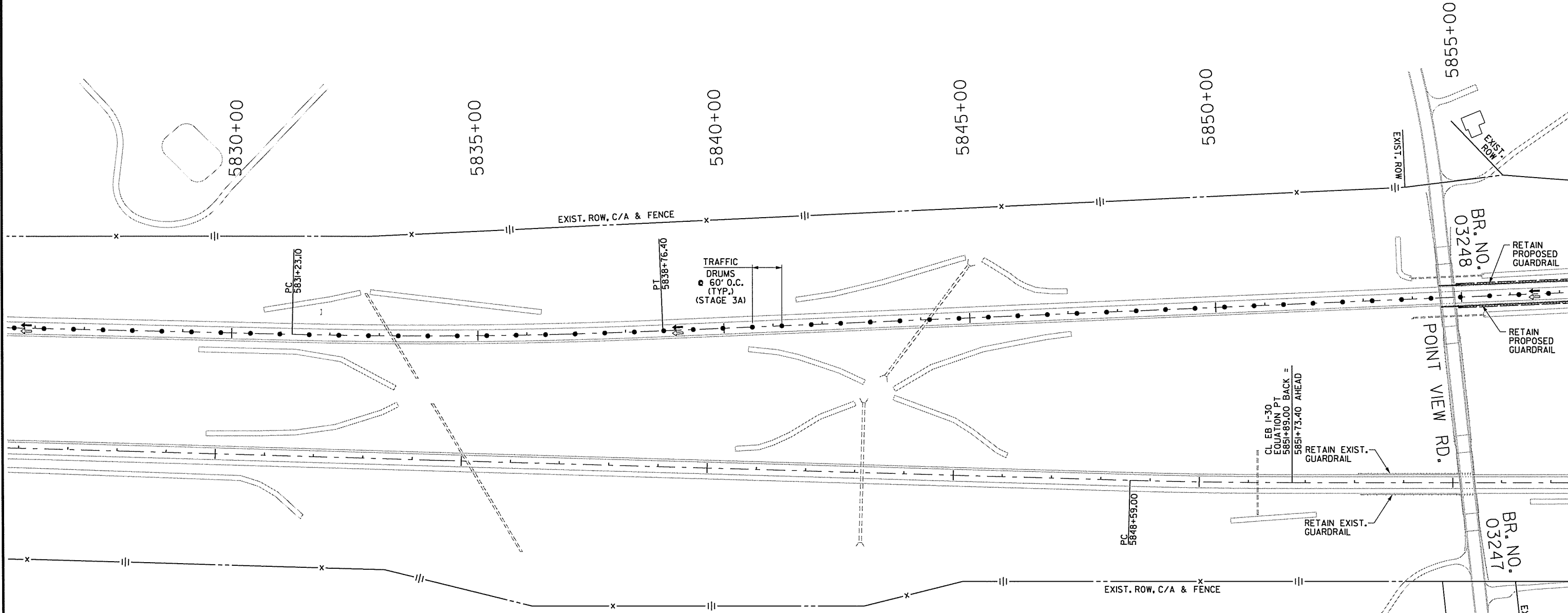
STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	55	114	
MAINTENANCE OF TRAFFIC DETAILS								

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 3**
- PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 3A**
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - CONSTRUCT MEDIAN CROSSINGS.
 - CONSTRUCT WIRE ROPE SAFETY FENCE AND CONCRETE DITCH PAVING.
 - INSTALL INSIDE WB MAIN LANE RUMBLE STRIPS.
 - EB LANE CLOSURES AND WORK ALONG INSIDE EB SHOULDERS SHALL ONLY BE PERFORMED DURING NIGHTTIME HOURS.
 - PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL EB I-30 IN AREA OF CONSTRUCTION.
 - INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
 - REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

- STAGE 3B**
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
 - INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
 - INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US 70 EXIT RAMP.
 - REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
 - REMOVE TEMPORARY PAVEMENT IN GORE AT US 70 ENTRANCE RAMP USED IN STAGE 2.
 - RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

LEGEND

- STAGE 3A TRAFFIC
- ⇨ STAGE 3B TRAFFIC
- TRAFFIC DRUM

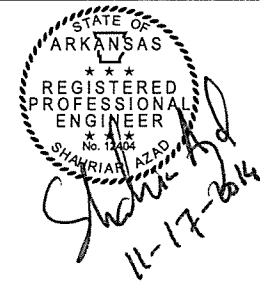
**MAINTENANCE OF TRAFFIC DETAILS
STAGE 3**

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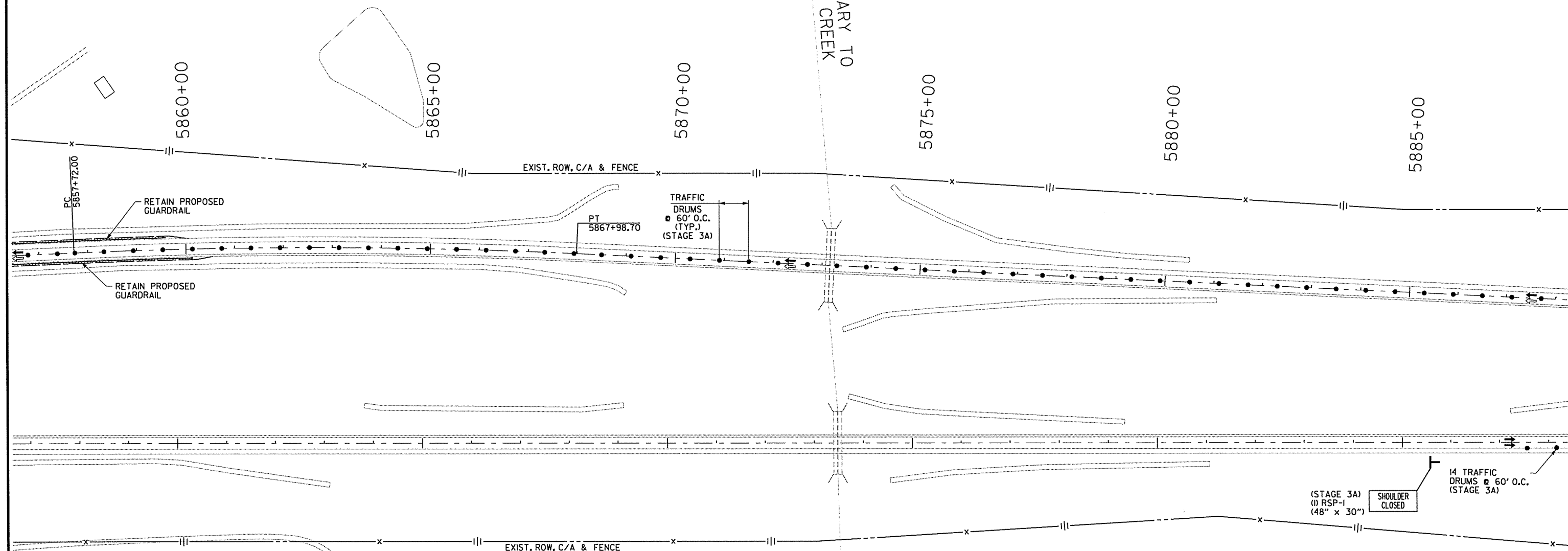
STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH



DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	56	114	
② MAINTENANCE OF TRAFFIC DETAILS								

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR NOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 3
I. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 3A**
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - CONSTRUCT MEDIAN CROSSINGS.
 - CONSTRUCT WIRE ROPE SAFETY FENCE AND CONCRETE DITCH PAVING.
 - INSTALL INSIDE WB MAIN LANE RUMBLE STRIPS.
 - EB LANE CLOSURES AND WORK ALONG INSIDE EB SHOULDERS SHALL ONLY BE PERFORMED DURING NIGHTTIME HOURS.
 - PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL EB I-30 IN AREA OF CONSTRUCTION.
 - INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
 - REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

- STAGE 3B**
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
 - INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
 - INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US TO EXIT RAMP.
 - REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
 - REMOVE TEMPORARY PAVEMENT IN GORE AT US TO ENTRANCE RAMP USED IN STAGE 2.
 - RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

LEGEND

→	STAGE 3A TRAFFIC
⇨	STAGE 3B TRAFFIC
•	TRAFFIC DRUM

**MAINTENANCE OF TRAFFIC DETAILS
STAGE 3**

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STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

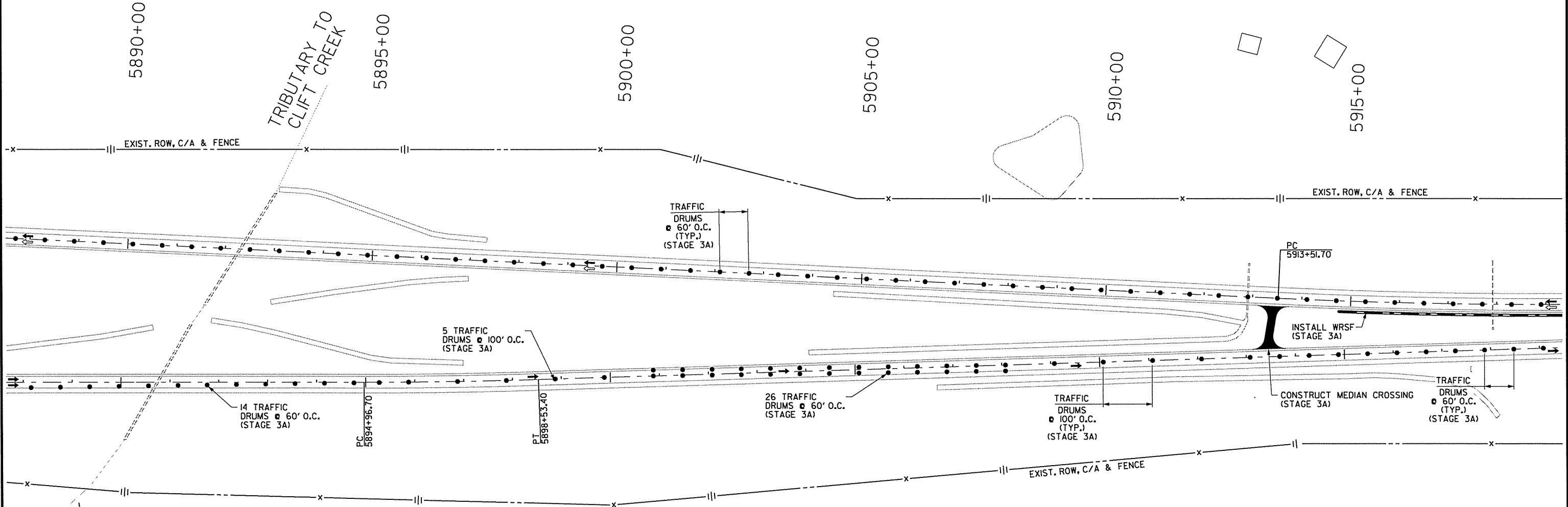
STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH

STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
SHAHRAR ALI
11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		57	114

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 3
- PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 3A
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - CONSTRUCT MEDIAN CROSSINGS.
 - CONSTRUCT WIRE ROPE SAFETY FENCE AND CONCRETE DITCH PAVING.
 - INSTALL INSIDE WB MAIN LANE RUMBLE STRIPS.
 - EB LANE CLOSURES AND WORK ALONG INSIDE EB SHOULDERS SHALL ONLY BE PERFORMED DURING NIGHTTIME HOURS.
 - PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL EB I-30 IN AREA OF CONSTRUCTION.
 - INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
 - REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

- STAGE 3B
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
 - INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
 - INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US 70 EXIT RAMP.
 - REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
 - REMOVE TEMPORARY PAVEMENT IN GORE AT US TO ENTRANCE RAMP USED IN STAGE 2.
 - RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

LEGEND

- STAGE 3A TRAFFIC
- ⇨ STAGE 3B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGE 3

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STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

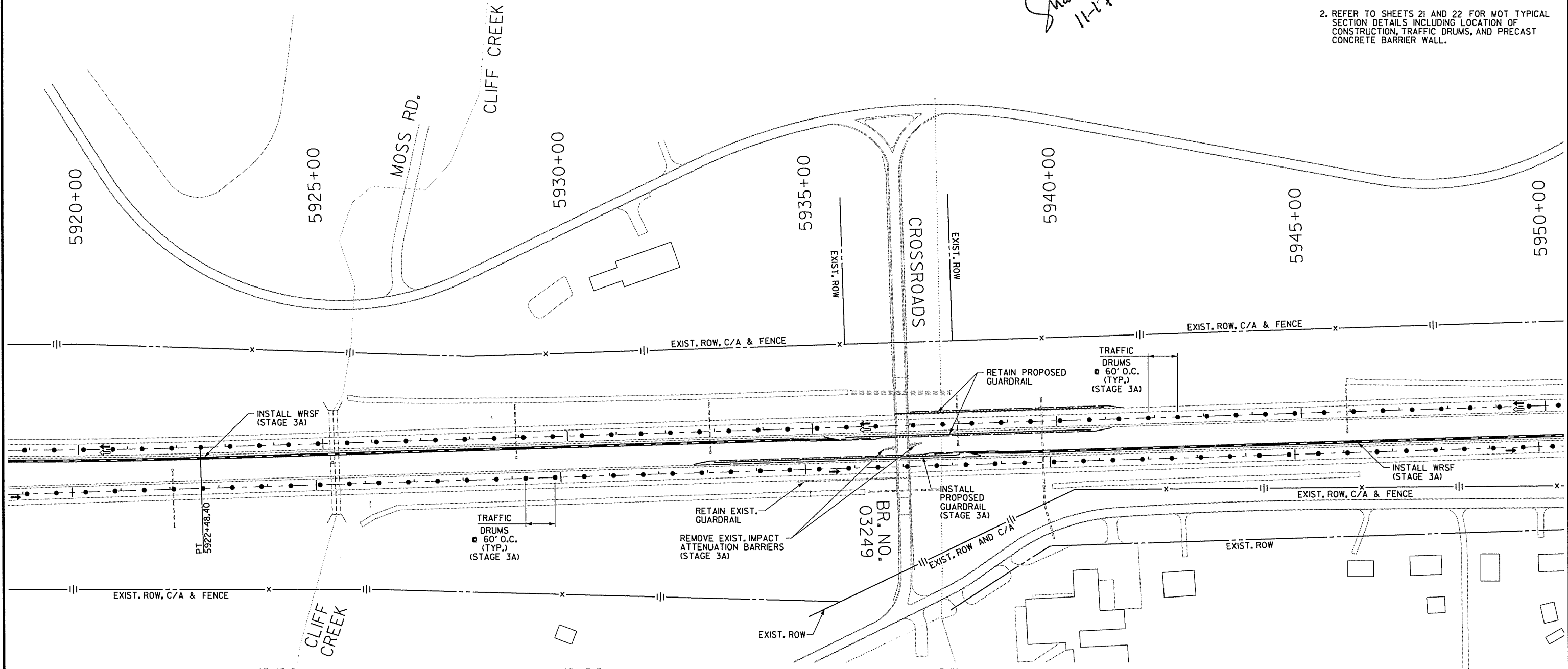
STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 12240
 SHARIK AZAD
 11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	58	114	

② MAINTENANCE OF TRAFFIC DETAILS

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO SHEETS 21 AND 22 FOR NOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 3
1. PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 3A
1. PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 2. PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 3. USE TRAFFIC DRUMS TO PROTECT WORKERS.
 4. CONSTRUCT MEDIAN CROSSINGS.
 5. CONSTRUCT WIRE ROPE SAFETY FENCE AND CONCRETE DITCH PAVING.
 6. INSTALL INSIDE WB MAIN LANE RUMBLE STRIPS.
 7. EB LANE CLOSURES AND WORK ALONG INSIDE EB SHOULDERS SHALL ONLY BE PERFORMED DURING NIGHTTIME HOURS.
 8. PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 9. PLACE TRAFFIC DRUMS ALONG EXISTING CL EB I-30 IN AREA OF CONSTRUCTION.
 10. INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
 11. REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

- STAGE 3B
1. PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
 2. PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 3. USE TRAFFIC DRUMS TO PROTECT WORKERS.
 4. INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
 5. INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
 6. INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US 70 EXIT RAMP.
 7. REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
 8. REMOVE TEMPORARY PAVEMENT IN GORE AT US 70 ENTRANCE RAMP USED IN STAGE 2.
 9. RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

LEGEND

- STAGE 3A TRAFFIC
- ⇄ STAGE 3B TRAFFIC
- TRAFFIC DRUM

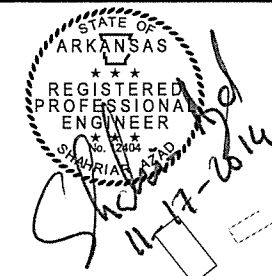
MAINTENANCE OF TRAFFIC DETAILS
STAGE 3

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STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH

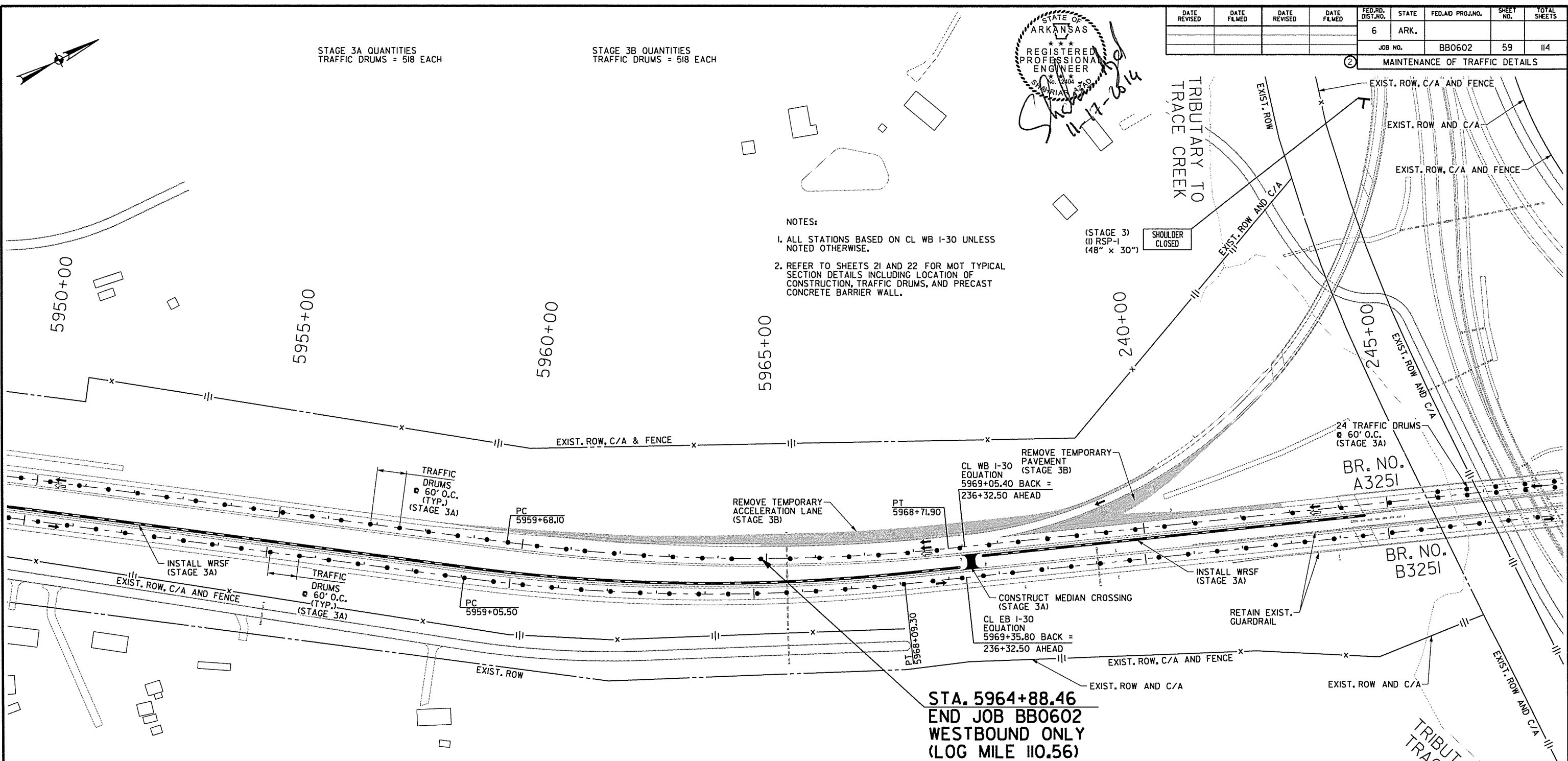


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602	59	114	

MAINTENANCE OF TRAFFIC DETAILS

NOTES:

- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
- REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 3
- PLACE ADVANCE WARNING SIGNS AND DEVICES.

- STAGE 3A
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - CONSTRUCT MEDIAN CROSSINGS.
 - CONSTRUCT WIRE ROPE SAFETY FENCE AND CONCRETE DITCH PAVING.
 - INSTALL INSIDE WB MAIN LANE RUMBLE STRIPS.
 - EB LANE CLOSURES AND WORK ALONG INSIDE EB SHOULDERS SHALL ONLY BE PERFORMED DURING NIGHTTIME HOURS.
 - PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL EB I-30 IN AREA OF CONSTRUCTION.
 - INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
 - REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

- STAGE 3B
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
 - INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
 - INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US TO EXIT RAMP.
 - REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
 - REMOVE TEMPORARY PAVEMENT IN GORE AT US TO ENTRANCE RAMP USED IN STAGE 2.
 - RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

LEGEND

- STAGE 3A TRAFFIC
- ⇄ STAGE 3B TRAFFIC
- TRAFFIC DRUM

MAINTENANCE OF TRAFFIC DETAILS
STAGE 3

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STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

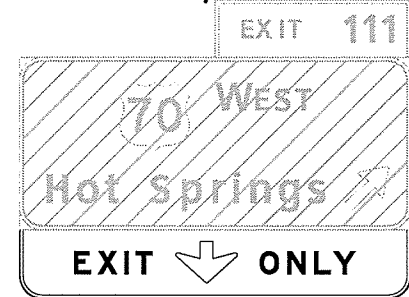
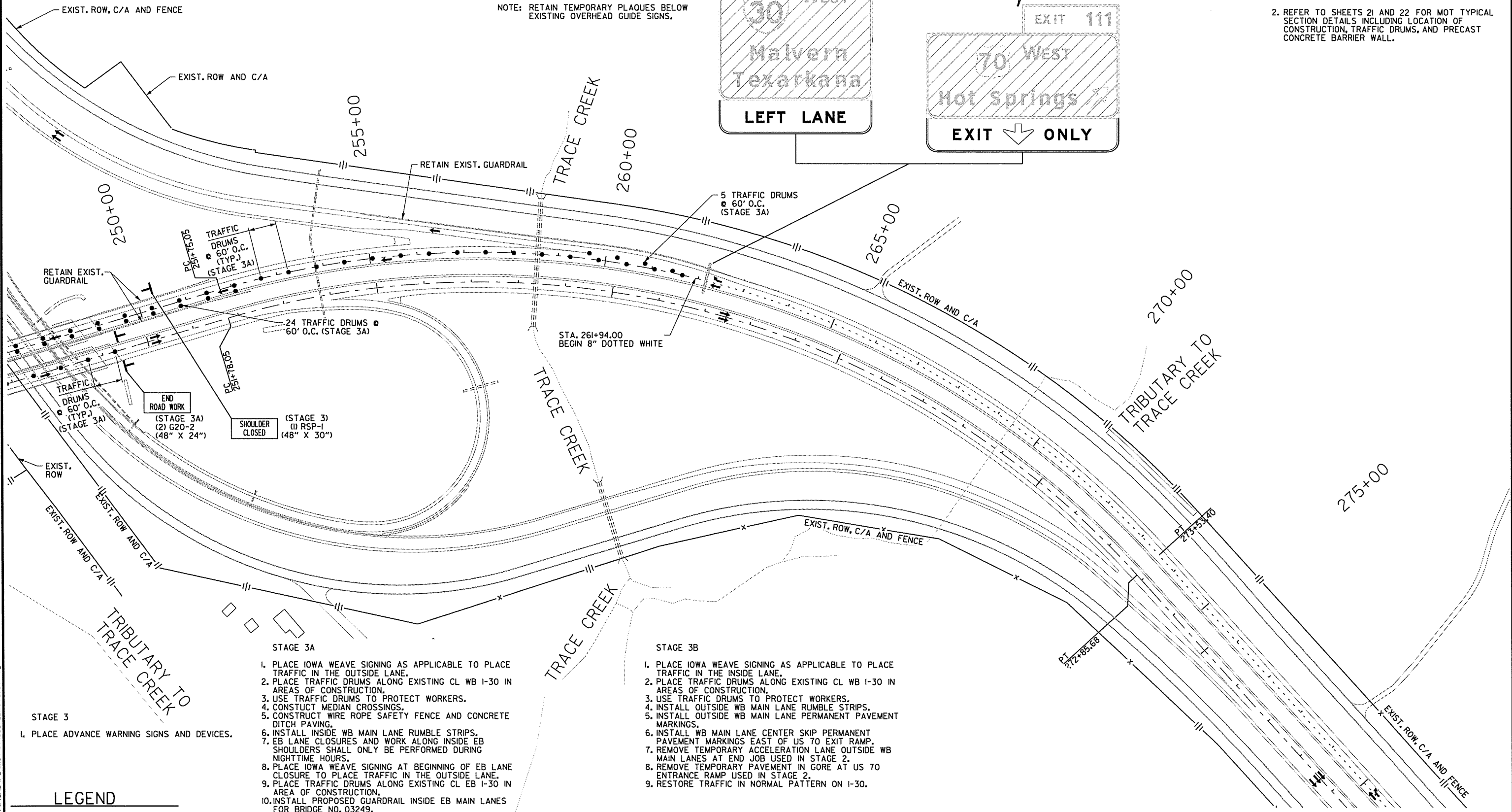
STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH

NOTE: RETAIN TEMPORARY PLAQUES BELOW
EXISTING OVERHEAD GUIDE SIGNS.

STATE OF ARKANSAS
REGISTERED PROFESSIONAL ENGINEER
STAHMUR, A. D.
11-17-2014

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602		60	114
MAINTENANCE OF TRAFFIC DETAILS								

- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



- STAGE 3
- PLACE ADVANCE WARNING SIGNS AND DEVICES.

LEGEND

- STAGE 3A TRAFFIC
- ⇨ STAGE 3B TRAFFIC
- TRAFFIC DRUM

- STAGE 3A
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
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 - PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
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 - INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
 - REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

- STAGE 3B
- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
 - PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
 - USE TRAFFIC DRUMS TO PROTECT WORKERS.
 - INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
 - INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
 - INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US TO EXIT RAMP.
 - REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
 - REMOVE TEMPORARY PAVEMENT IN GORE AT US TO ENTRANCE RAMP USED IN STAGE 2.
 - RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

**MAINTENANCE OF TRAFFIC DETAILS
STAGE 3**

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STAGE 3A QUANTITIES
TRAFFIC DRUMS = 518 EACH

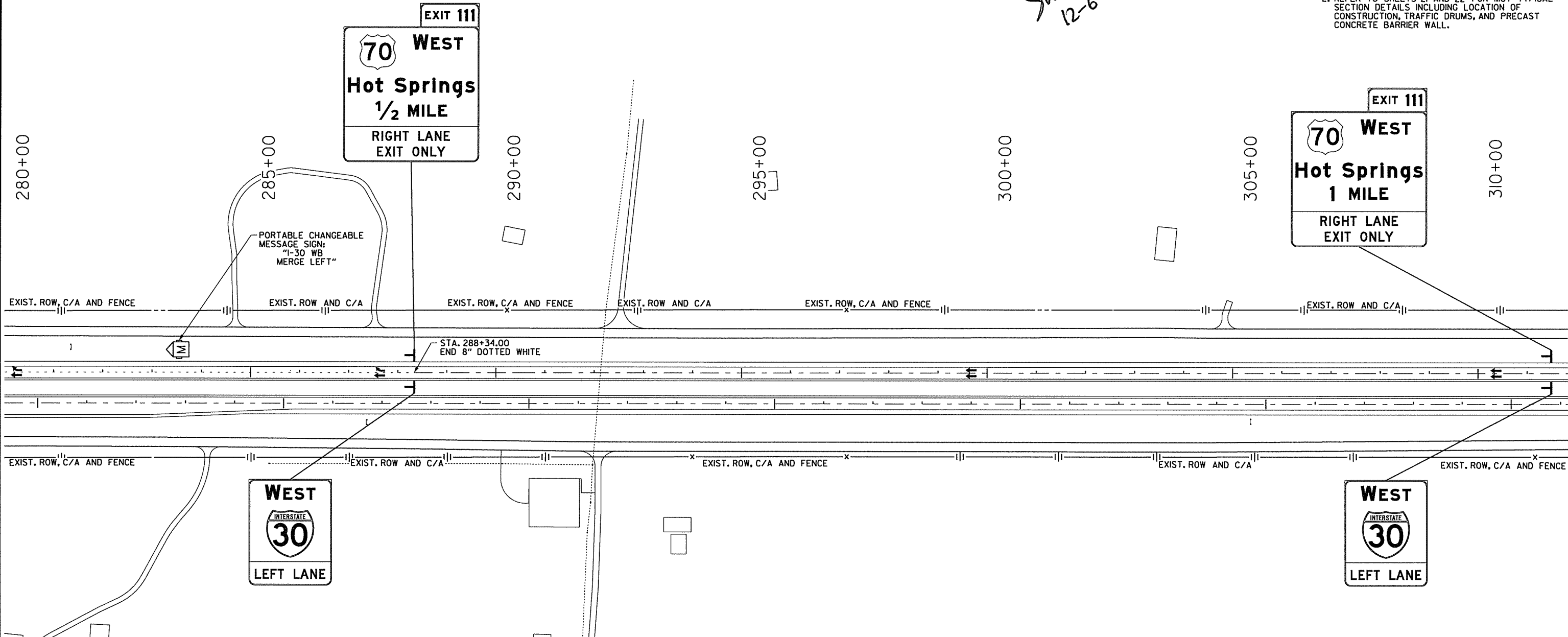
STAGE 3B QUANTITIES
TRAFFIC DRUMS = 518 EACH



Shahriar Azad
12-6-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		61	114
				JOB NO.		BBO602		
				② MAINTENANCE OF TRAFFIC DETAILS				

NOTES:
 1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO SHEETS 21 AND 22 FOR MOT TYPICAL SECTION DETAILS INCLUDING LOCATION OF CONSTRUCTION, TRAFFIC DRUMS, AND PRECAST CONCRETE BARRIER WALL.



STAGE 3
I. PLACE ADVANCE WARNING SIGNS AND DEVICES.

STAGE 3A

- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
- PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
- USE TRAFFIC DRUMS TO PROTECT WORKERS.
- CONSTRUCT MEDIAN CROSSINGS.
- CONSTRUCT WIRE ROPE SAFETY FENCE AND CONCRETE DITCH PAVING.
- INSTALL INSIDE WB MAIN LANE RUMBLE STRIPS.
- EB LANE CLOSURES AND WORK ALONG INSIDE EB SHOULDERS SHALL ONLY BE PERFORMED DURING NIGHTTIME HOURS.
- PLACE IOWA WEAVE SIGNING AT BEGINNING OF EB LANE CLOSURE TO PLACE TRAFFIC IN THE OUTSIDE LANE.
- PLACE TRAFFIC DRUMS ALONG EXISTING CL EB I-30 IN AREA OF CONSTRUCTION.
- INSTALL PROPOSED GUARDRAIL INSIDE EB MAIN LANES FOR BRIDGE NO. 03249.
- REMOVE EXISTING IMPACT ATTENUATION BARRIERS (AS SHOWN ON PLANS) AT BRIDGE NO. 03249.

STAGE 3B

- PLACE IOWA WEAVE SIGNING AS APPLICABLE TO PLACE TRAFFIC IN THE INSIDE LANE.
- PLACE TRAFFIC DRUMS ALONG EXISTING CL WB I-30 IN AREAS OF CONSTRUCTION.
- USE TRAFFIC DRUMS TO PROTECT WORKERS.
- INSTALL OUTSIDE WB MAIN LANE RUMBLE STRIPS.
- INSTALL OUTSIDE WB MAIN LANE PERMANENT PAVEMENT MARKINGS.
- INSTALL WB MAIN LANE CENTER SKIP PERMANENT PAVEMENT MARKINGS EAST OF US 70 EXIT RAMP.
- REMOVE TEMPORARY ACCELERATION LANE OUTSIDE WB MAIN LANES AT END JOB USED IN STAGE 2.
- REMOVE TEMPORARY PAVEMENT IN GORE AT US 70 ENTRANCE RAMP USED IN STAGE 2.
- RESTORE TRAFFIC IN NORMAL PATTERN ON I-30.

LEGEND

- STAGE 3A TRAFFIC
- ⇄ STAGE 3B TRAFFIC
- TRAFFIC DRUM

**MAINTENANCE OF TRAFFIC DETAILS
STAGE 3**

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5795+00

5800+00

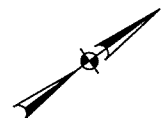
5805+00

5810+00

5815+00

5820+00

5825+00



DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	62	114	

PVMT. MARK. STA. 5795+00 TO 5857+00

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO PLAN SHEETS FOR DETAILS OF PROPOSED ACCELERATION LANES AND TAPERS.
 3. REFER TO STANDARD DRAWING PM-2 FOR DETAILS AT RAMP.

STA. 5808+65.00
BEGIN 4" SKIP WHITE &
TYPE II (WHITE/RED)
R.P.M. @ 80' O.C.

STA. 5808+65.00
BEGIN CONCRETE PAVEMENT MARKINGS

STA. 5811+71.49
BEGIN JOB BB0602
WESTBOUND ONLY
(LOG MILE 107.64)

- 4" SOLID WHITE
- 4" SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 80' O.C.
- 4" SOLID YELLOW

CL WB I-30

CL EB I-30

5830+00

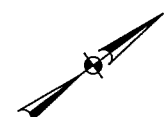
5835+00

5840+00

5845+00

5850+00

00+5885



I-30 WB

LT 4" SOLID WHITE FROM STA. 5808+65.00 TO STA. 5969+05.40 = 16041 LF
RT 4" SOLID YELLOW FROM STA. 5808+65.00 TO STA. 5969+05.40 = 16041 LF
CL LANES 4" CONTRAST SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 80' O.C. FROM STA. 5808+69.55 TO STA. 5958+06.94 = 3736 LF

4" SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 80' O.C.

4" SOLID YELLOW

4" SOLID WHITE

PC 5831+23.10

PT 5838+76.40

24'

CL WB I-30

CL EB I-30

PC 00+5885

CL EB I-30
EQUATION PT
5851+89.00 BACK =
5851+73.40 AHEAD

VIEW RD.
POINT

BR. NO. 03248

BR. NO. 03247

- NOTES:
1. R.P.M. - RAISED PAVEMENT MARKERS.
 2. SEE STANDARD DRAWING PM-2 FOR DETAILS.

PERMANENT PAVEMENT MARKING DETAIL

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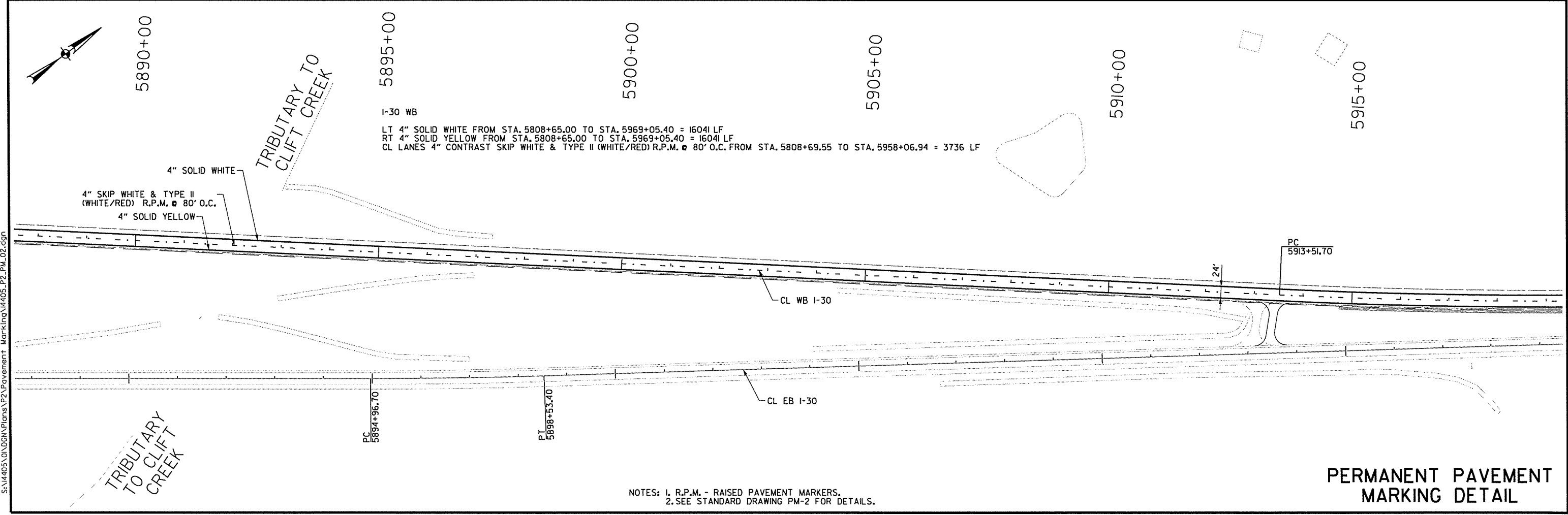
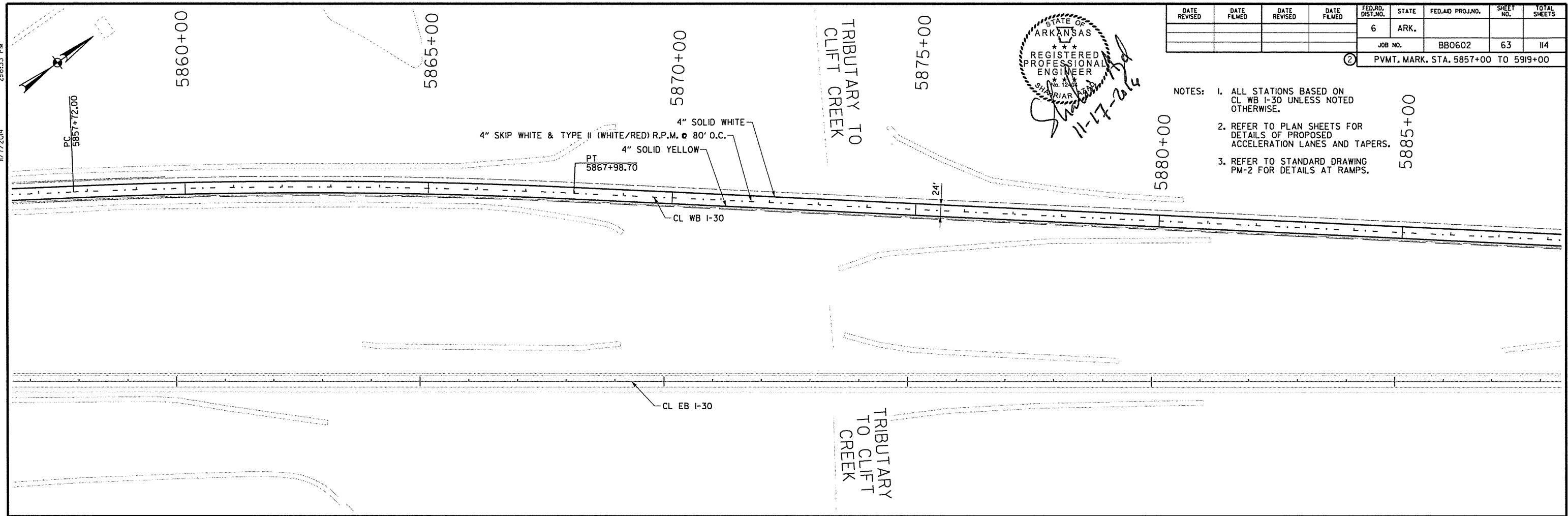
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DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		63	114

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 No. 12488
 SHARIAN
 11-17-2014

NOTES: 1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO PLAN SHEETS FOR DETAILS OF PROPOSED ACCELERATION LANES AND TAPERS.
 3. REFER TO STANDARD DRAWING PM-2 FOR DETAILS AT RAMPS.

PVMT. MARK. STA. 5857+00 TO 5919+00



I-30 WB
 LT 4" SOLID WHITE FROM STA. 5808+65.00 TO STA. 5969+05.40 = 1604I LF
 RT 4" SOLID YELLOW FROM STA. 5808+65.00 TO STA. 5969+05.40 = 1604I LF
 CL LANES 4" CONTRAST SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 80' O.C. FROM STA. 5808+69.55 TO STA. 5958+06.94 = 3736 LF

NOTES: 1. R.P.M. - RAISED PAVEMENT MARKERS.
 2. SEE STANDARD DRAWING PM-2 FOR DETAILS.

PERMANENT PAVEMENT MARKING DETAIL

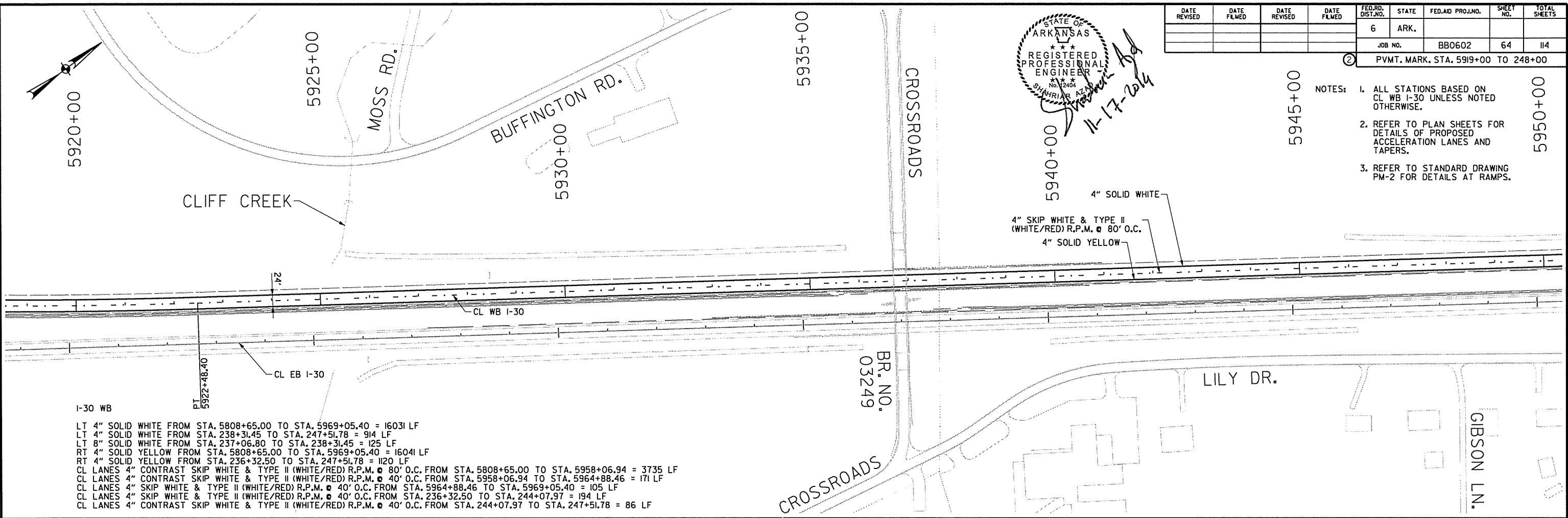
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DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	64	114	



PVMT. MARK. STA. 5919+00 TO 248+00

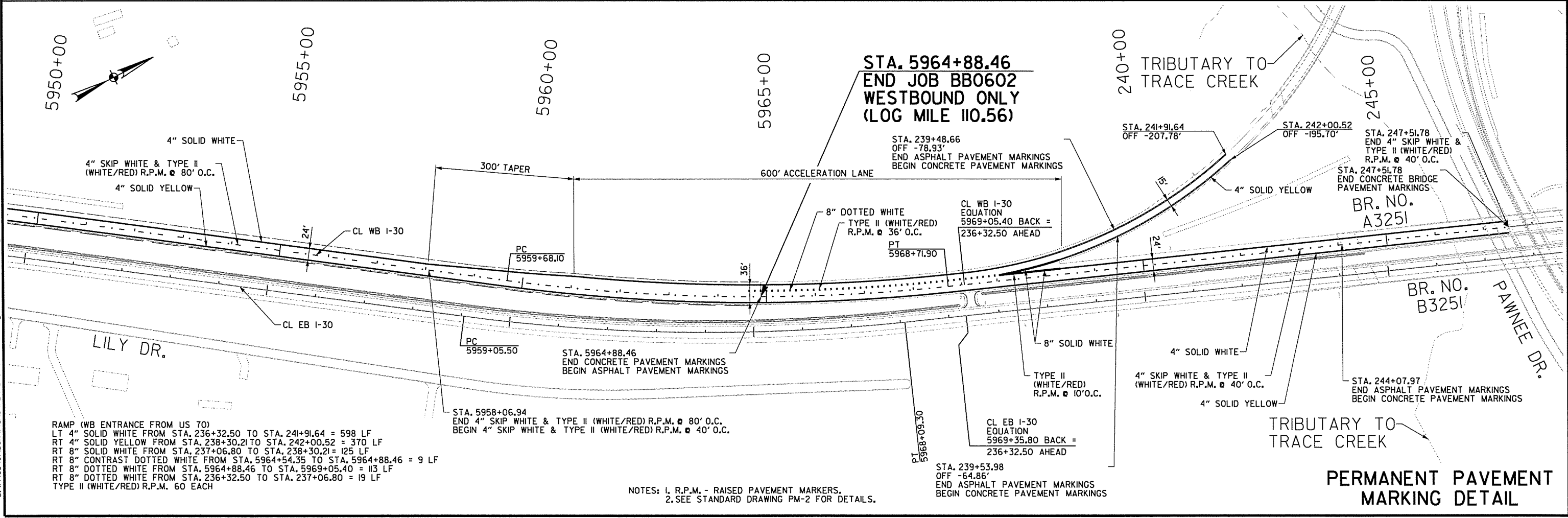
- NOTES:
- ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 - REFER TO PLAN SHEETS FOR DETAILS OF PROPOSED ACCELERATION LANES AND TAPERS.
 - REFER TO STANDARD DRAWING PM-2 FOR DETAILS AT RAMPS.



I-30 WB
PT 5922+48.40

CL WB I-30
CL EB I-30

LT 4" SOLID WHITE FROM STA. 5808+65.00 TO STA. 5969+05.40 = 16031 LF
 LT 4" SOLID WHITE FROM STA. 238+31.45 TO STA. 247+51.78 = 914 LF
 LT 8" SOLID WHITE FROM STA. 237+06.80 TO STA. 238+31.45 = 125 LF
 RT 4" SOLID YELLOW FROM STA. 5808+65.00 TO STA. 5969+05.40 = 16041 LF
 RT 4" SOLID YELLOW FROM STA. 236+32.50 TO STA. 247+51.78 = 1120 LF
 CL LANES 4" CONTRAST SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 80' O.C. FROM STA. 5808+65.00 TO STA. 5958+06.94 = 3735 LF
 CL LANES 4" CONTRAST SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 40' O.C. FROM STA. 5958+06.94 TO STA. 5964+88.46 = 171 LF
 CL LANES 4" SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 40' O.C. FROM STA. 5964+88.46 TO STA. 5969+05.40 = 105 LF
 CL LANES 4" SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 40' O.C. FROM STA. 236+32.50 TO STA. 244+07.97 = 194 LF
 CL LANES 4" CONTRAST SKIP WHITE & TYPE II (WHITE/RED) R.P.M. @ 40' O.C. FROM STA. 244+07.97 TO STA. 247+51.78 = 86 LF



STA. 5964+88.46
END JOB BB0602
WESTBOUND ONLY
(LOG MILE 110.56)

RAMP (WB ENTRANCE FROM US 70)
 LT 4" SOLID WHITE FROM STA. 236+32.50 TO STA. 241+91.64 = 598 LF
 RT 4" SOLID YELLOW FROM STA. 238+30.21 TO STA. 242+00.52 = 370 LF
 RT 8" SOLID WHITE FROM STA. 237+06.80 TO STA. 238+30.21 = 125 LF
 RT 8" CONTRAST DOTTED WHITE FROM STA. 5964+54.35 TO STA. 5964+88.46 = 9 LF
 RT 8" DOTTED WHITE FROM STA. 5964+88.46 TO STA. 5969+05.40 = 113 LF
 RT 8" DOTTED WHITE FROM STA. 236+32.50 TO STA. 237+06.80 = 19 LF
 TYPE II (WHITE/RED) R.P.M. 60 EACH

NOTES: 1. R.P.M. - RAISED PAVEMENT MARKERS.
 2. SEE STANDARD DRAWING PM-2 FOR DETAILS.

PERMANENT PAVEMENT MARKING DETAIL

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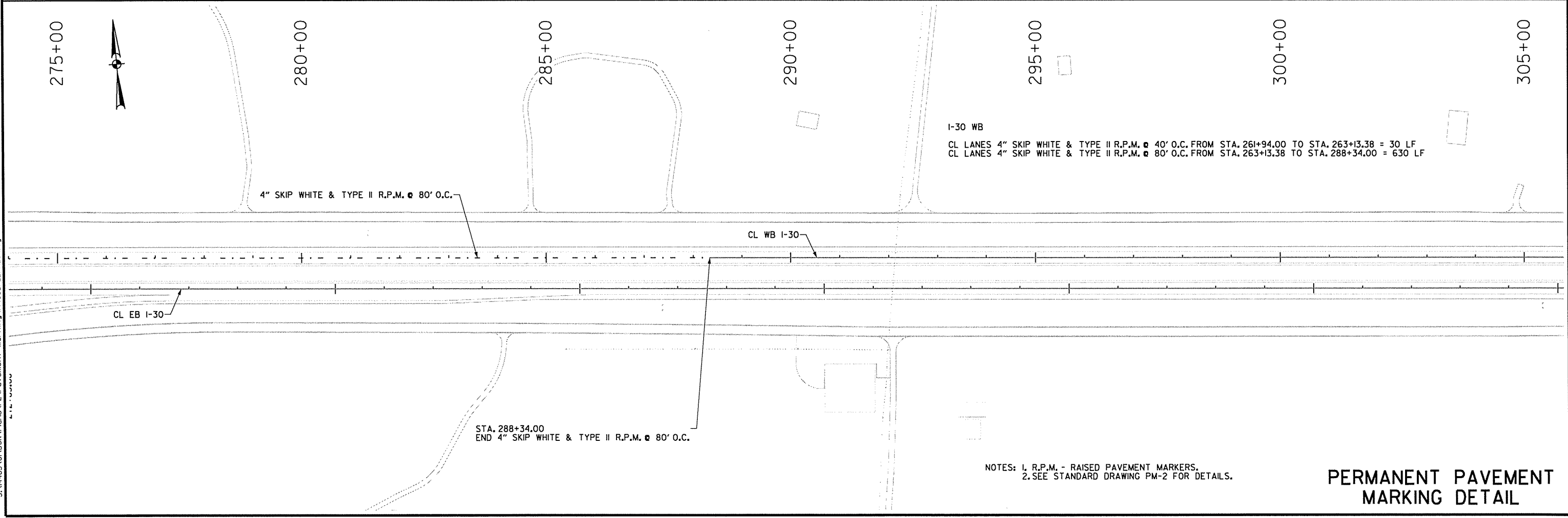
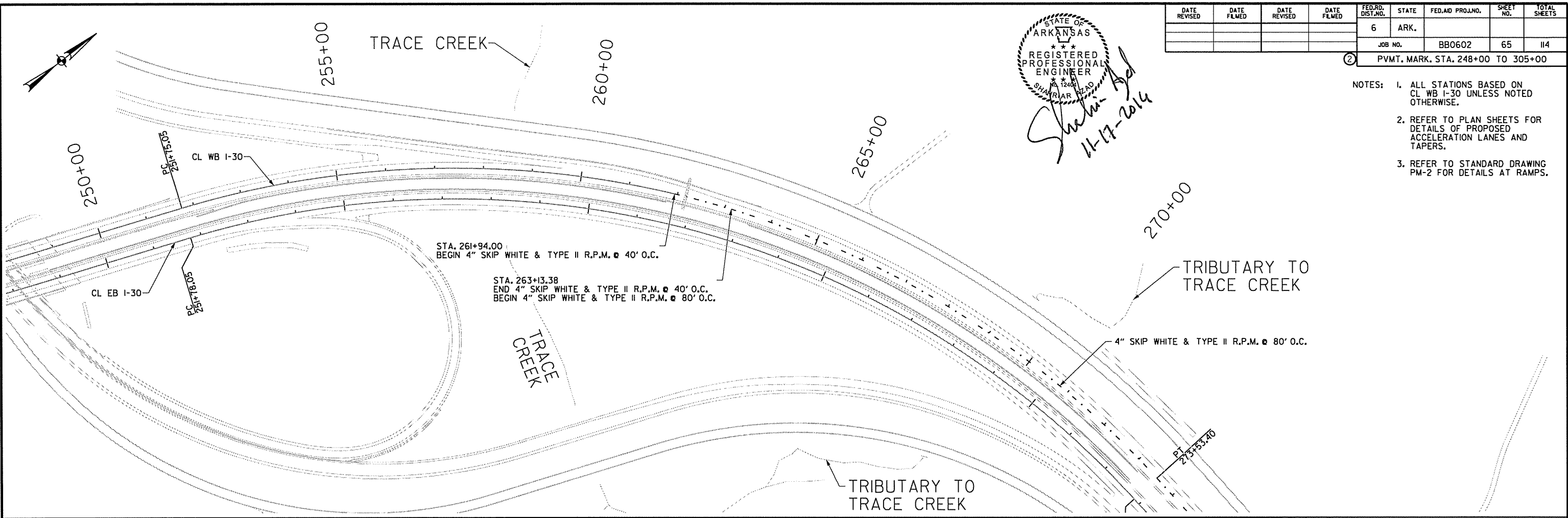
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DATE REVISED	DATE FLMED	DATE REVISED	DATE FLMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	65	114	
				PVMT. MARK. STA. 248+00 TO 305+00				

STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHARIAR AZAD
 11-17-2014

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. REFER TO PLAN SHEETS FOR DETAILS OF PROPOSED ACCELERATION LANES AND TAPERS.
 3. REFER TO STANDARD DRAWING PM-2 FOR DETAILS AT RAMPS.



I-30 WB
 CL LANES 4" SKIP WHITE & TYPE II R.P.M. @ 40' O.C. FROM STA. 261+94.00 TO STA. 263+13.38 = 30 LF
 CL LANES 4" SKIP WHITE & TYPE II R.P.M. @ 80' O.C. FROM STA. 263+13.38 TO STA. 288+34.00 = 630 LF

- NOTES:
1. R.P.M. - RAISED PAVEMENT MARKERS.
 2. SEE STANDARD DRAWING PM-2 FOR DETAILS.

PERMANENT PAVEMENT MARKING DETAIL

12/29/12 PM

12/6/2014

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		66	114
				JOB NO.	BB0602			

② QUANTITIES

ADVANCE WARNING SIGNS & DEVICES - STAGE I							
SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE I	STAGE IA	STAGE IB	TOTAL SIGNS REQUIRED	
						LIN. FT. - EACH	NO.
G20-1	ROAD WORK NEXT XX MILES	(60" X 24")	2	2	2	2	20.00
G20-2	END ROAD WORK	(48" X 24")	3	3	3	3	24.00
R2-1	SPEED LIMIT (60 MPH)	(48" X 60")	3	3	3	3	60.00
R2-1	SPEED LIMIT (70 MPH)	(48" X 60")	2	2	2	2	40.00
R2-2	SPEED LIMIT (TRUCKS 65 MPH)	(48" X 48")	2	2	2	2	32.00
R2-5A	REDUCED SPEED AHEAD	(48" X 60")	3	3	3	3	60.00
R4-1	DO NOT PASS	(48" X 60")	2	2	2	2	40.00
R55-1	FINES DOUBLE	(36" X 60")	3	3	3	3	45.00
RSP-1	SHOULDER CLOSED	(48" X 30")	2	2	2	2	20.00
WI-4AL	REVERSE CURVE LEFT	(48" X 48")	0	0	1	1	16.00
WI-4AR	REVERSE CURVE RIGHT	(48" X 48")	0	0	1	1	16.00
WI-6	LARGE ARROW	(60" X 30")	3	3	6	6	75.00
W5-1	ROAD NARROWS	(48" X 48")	0	0	2	2	32.00
WI3-1	XX M.P.H.	(24" X 24")	0	0	2	2	8.00
W20-1	ROAD WORK (1 MILE)	(48" X 48")	3	3	3	3	48.00
W20-1	ROAD WORK (1/2 MILE)	(48" X 48")	3	3	3	3	48.00
W20-1	ROAD WORK (1500 FT)	(48" X 48")	3	3	3	3	48.00
W20-1	ROAD WORK (AHEAD)	(48" X 48")	2	2	2	2	32.00
SPECIAL	RUMBLE STRIPS AHEAD	(48" X 48")	2	2	2	2	32.00
SPECIAL	LEFT LANE ONLY	(162" X 36")	1	1	1	1	40.50
SPECIAL	EXIT ONLY	(204" X 36")	1	1	1	1	51.00
SPECIAL	I-30 WB	(36" X 30")	4	4	4	4	30.00
SPECIAL	I-30 WB LEFT LANE	(84" X 108")	2	2	2	2	126.00
SPECIAL	US 70 EXIT RIGHT LANE (1/2 MILE)	(138" X 162")	1	1	1	1	155.25
SPECIAL	US 70 EXIT RIGHT LANE (1 MILE)	(138" X 162")	1	1	1	1	155.25
						TOTAL	1254.00

	TOTAL	UNITS
ADVANCE WARNING ARROW PANEL	0	0 EACH
PORTABLE CHANGEABLE MESSAGE SIGN	4	4 EACH
VERTICAL PANELS	100	100 EACH
TRAFFIC DRUMS	0	348 EACH
FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER	0	120 1380 1500 LIN. FT.
RELOCATING PRECAST CONCRETE BARRIER	0	0 0 LIN. FT.
TEMPORARY IMPACT ATTENUATION BARRIER	0	1 2 EACH
TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)	0	1 2 EACH

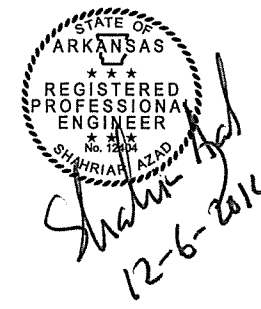
NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

NOTE: THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

QUANTITIES

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						BBO602	67	114
②								QUANTITIES

ADVANCE WARNING SIGNS & DEVICES - STAGE 2							
SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE 2	STAGE 2A	STAGE 2B	TOTAL SIGNS REQUIRED	
			LIN. FT. - EACH			NO.	SO. FT.
G20-1	ROAD WORK NEXT XX MILES	(60" X 24")	2	2	2	2	20.00
G20-2	END ROAD WORK	(48" X 24")	3	3	3	3	24.00
R2-1	SPEED LIMIT (60 MPH)	(48" X 60")	3	3	3	3	60.00
R2-1	SPEED LIMIT (70 MPH)	(48" X 60")	2	2	2	2	40.00
R2-2	SPEED LIMIT (TRUCKS 65 MPH)	(48" X 48")	2	2	2	2	32.00
R2-5A	REDUCED SPEED AHEAD	(48" X 60")	3	3	3	3	60.00
R4-1	DO NOT PASS	(48" X 60")	2	2	2	2	40.00
R55-1	FINES DOUBLE	(36" X 60")	3	3	3	3	45.00
RSP-1	SHOULDER CLOSED	(48" X 30")	2	2	2	2	20.00
WI-4AL	REVERSE CURVE LEFT	(48" X 48")	0	0	2	2	32.00
WI-4AR	REVERSE CURVE RIGHT	(48" X 48")	1	1	1	1	16.00
WI-6	LARGE ARROW	(60" X 30")	6	6	6	6	75.00
WI3-1	XX M.P.H.	(24" X 24")	1	1	3	3	12.00
W20-1	ROAD WORK (1 MILE)	(48" X 48")	3	3	3	3	48.00
W20-1	ROAD WORK (1/2 MILE)	(48" X 48")	3	3	3	3	48.00
W20-1	ROAD WORK (1500 FT)	(48" X 48")	3	3	3	3	48.00
W20-1	ROAD WORK (AHEAD)	(48" X 48")	2	2	2	2	32.00
SPECIAL	RUMBLE STRIPS AHEAD	(48" X 48")	2	2	2	2	32.00
SPECIAL	LEFT LANE ONLY	(162" X 36")	1	1	1	1	40.50
SPECIAL	EXIT ONLY	(204" X 36")	1	1	1	1	51.00
SPECIAL	I-30 WB	(36" X 30")	4	4	4	4	30.00
SPECIAL	I-30 WB LEFT LANE	(84" X 108")	2	2	2	2	126.00
SPECIAL	US 70 EXIT RIGHT LANE (1/2 MILE)	(138" X 162")	1	1	1	1	155.25
SPECIAL	US 70 EXIT RIGHT LANE (1 MILE)	(138" X 162")	1	1	1	1	155.25
TOTAL							1242.00
						TOTAL	UNITS
ADVANCE WARNING ARROW PANEL			0	0	0	0	EACH
PORTABLE CHANGEABLE MESSAGE SIGN			4	4	4	4	EACH
VERTICAL PANELS			100	100	100	100	EACH
TRAFFIC DRUMS			0	100	100	100	EACH
FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER			0	15560	0	15560	LIN. FT.
RELOCATING PRECAST CONCRETE BARRIER			0	1380	17060	18440	LIN. FT.
TEMPORARY IMPACT ATTENUATION BARRIER			2	2	0	2	EACH
TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)			2	2	0	2	EACH

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

NOTE: THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

QUANTITIES

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ADVANCE WARNING SIGNS & DEVICES - STAGE 3

SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE 3	STAGE 3A	STAGE 3B	TOTAL SIGNS REQUIRED	
			LIN. FT. - EACH			NO.	SQ. FT.
G20-1	ROAD WORK NEXT XX MILES	(60" X 24")	3	3	2	3	30.00
G20-2	END ROAD WORK	(48" X 24")	5	5	3	5	40.00
R2-1	SPEED LIMIT (60 MPH)	(48" X 60")	5	5	3	5	100.00
R2-1	SPEED LIMIT (70 MPH)	(48" X 60")	4	4	2	4	80.00
R2-2	SPEED LIMIT (TRUCKS 65 MPH)	(48" X 48")	4	4	2	4	64.00
R2-5A	REDUCED SPEED AHEAD	(48" X 60")	5	5	3	5	100.00
R4-1	DO NOT PASS	(48" X 60")	4	4	2	4	80.00
R55-1	FINES DOUBLE	(36" X 60")	5	5	3	5	75.00
RSP-1	SHOULDER CLOSED	(48" X 30")	3	3	2	3	30.00
W1-6	LARGE ARROW	(60" X 30")	9	9	3	9	113.00
W4-2	RIGHT LANE CLOSED	(48" X 48")	2	2	0	2	32.00
W20-1	ROAD WORK (1 MILE)	(48" X 48")	5	5	3	5	80.00
W20-1	ROAD WORK (1/2 MILE)	(48" X 48")	5	5	3	5	80.00
W20-1	ROAD WORK (1500 FT)	(48" X 48")	5	5	3	5	80.00
W20-1	ROAD WORK (AHEAD)	(48" X 48")	2	2	1	2	32.00
W20-5	RIGHT LANE CLOSED (1500 FT)	(48" X 48")	2	2	0	2	32.00
W20-5	RIGHT LANE CLOSED (1/2 MILE)	(48" X 48")	2	2	0	2	32.00
W20-5	RIGHT LANE CLOSED (1 MILE)	(48" X 48")	2	2	0	2	32.00
SPECIAL	MERGE NOW	(48" X 48")	1	1	0	1	16.00
SPECIAL	RUMBLE STRIPS AHEAD	(48" X 48")	4	4	2	4	64.00
SPECIAL	LEFT LANE ONLY	(162" X 36")	1	1	1	1	41.00
SPECIAL	EXIT ONLY	(204" X 36")	1	1	1	1	51.00
SPECIAL	1-30 WB	(36" X 30")	4	4	4	4	30.00
SPECIAL	1-30 WB LEFT LANE	(84" X 108")	2	2	2	2	126.00
SPECIAL	US 70 EXIT RIGHT LANE (1/2 MILE)	(138" X 162")	1	1	1	1	155.25
SPECIAL	US 70 EXIT RIGHT LANE (1 MILE)	(138" X 162")	1	1	1	1	155.25
						TOTAL	1750.50

	TOTAL	UNITS
ADVANCE WARNING ARROW PANEL	1	1 EACH
PORTABLE CHANGEABLE MESSAGE SIGN	4	4 EACH
VERTICAL PANELS	0	0 EACH
TRAFFIC DRUMS	518	518 EACH
FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER	0	0 LIN. FT.
RELOCATING PRECAST CONCRETE BARRIER	0	0 LIN. FT.
TEMPORARY IMPACT ATTENUATION BARRIER	0	0 EACH
TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)	0	0 EACH

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

NOTE: THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

CONSTRUCTION AND PERMANENT PAVEMENT MARKINGS (ALL STAGES)

DESCRIPTION	UNITS	STAGE 1	STAGE 1A	STAGE 1B	STAGE 2	STAGE 2A	STAGE 2B	STAGE 3	STAGE 3A	STAGE 3B	FINAL	TOTAL REQUIRED
REMOVAL OF PERMANENT PAVEMENT MARKINGS	LIN. FT.		19460	4776		882	1434					26552
CONSTRUCTION PAVEMENT MARKINGS	LIN. FT.					30633						30633
REMOVABLE CONSTRUCTION PAVEMENT MARKINGS	LIN. FT.		660	4974		4590	35154					45378
RAISED PAVEMENT MARKERS TYPE II	EACH										331	331
* HIGH PERFORMANCE CONTRAST PAVEMENT MARKINGS WHITE (4")	LIN. FT.										3992	3992
* HIGH PERFORMANCE PAVEMENT MARKING (SKIP LINE) WHITE (4")	LIN. FT.										957	957
* HIGH PERFORMANCE PAVEMENT MARKING WHITE (4")	LIN. FT.										17527	17527
* HIGH PERFORMANCE PAVEMENT MARKING WHITE (8")	LIN. FT.										354	354
* HIGH PERFORMANCE PAVEMENT MARKING YELLOW (4")	LIN. FT.										17545	17545
* HIGH PERFORMANCE CONTRAST PAVEMENT MARKING WHITE (8")	LIN. FT.										9	9

* DENOTES ALTERNATE BID ITEMS.



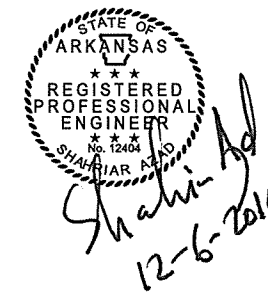
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				6	ARK.			
				JOB NO.	BB0602		68	114

② QUANTITIES

QUANTITIES

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						BBO602	69	114
② QUANTITIES								

ADVANCE WARNING SIGNS & DEVICES - SUMMARY								
SIGN NUMBER	DESCRIPTION	SIGN SIZE	SIGN AREA	STAGE 1	STAGE 2	STAGE 3	TOTAL SIGNS REQUIRED	
				LIN. FT. - EACH			NO.	SQ. FT.
G20-1	ROAD WORK NEXT XX MILES	(60" X 24")	10.0	2	2	3	3	30.00
G20-2	END ROAD WORK	(48" X 24")	8.0	3	3	5	5	40.00
R2-1	SPEED LIMIT (60 MPH)	(48" X 60")	20.0	3	3	5	5	100.00
R2-1	SPEED LIMIT (70 MPH)	(48" X 60")	20.0	2	2	4	4	80.00
R2-2	SPEED LIMIT (TRUCKS 65 MPH)	(48" X 48")	16.0	2	2	4	4	64.00
R2-5A	REDUCED SPEED AHEAD	(48" X 60")	20.0	3	3	5	5	100.00
R4-1	DO NOT PASS	(48" X 60")	20.0	2	2	4	4	80.00
R55-1	FINES DOUBLE	(36" X 60")	15.0	3	3	5	5	75.00
RSP-1	SHOULDER CLOSED	(48" X 30")	10.0	2	2	3	3	30.00
WI-4AL	REVERSE CURVE LEFT	(48" X 48")	16.0	1	2	0	2	32.00
WI-4AR	REVERSE CURVE RIGHT	(48" X 48")	16.0	1	1	0	1	16.00
WI-6	LARGE ARROW	(60" X 30")	12.5	6	6	9	9	112.50
W4-2	RIGHT LANE CLOSED	(48" X 48")	16.0	0	0	2	2	32.00
W5-1	ROAD NARROWS	(48" X 48")	16.0	2	0	0	2	32.00
WI3-1	XX M.P.H.	(24" X 24")	4.0	2	3	0	3	12.00
W20-1	ROAD WORK (1 MILE)	(48" X 48")	16.0	3	3	5	5	80.00
W20-1	ROAD WORK (1/2 MILE)	(48" X 48")	16.0	3	3	5	5	80.00
W20-1	ROAD WORK (1500 FT)	(48" X 48")	16.0	3	3	5	5	80.00
W20-1	ROAD WORK (AHEAD)	(48" X 48")	16.0	2	2	2	2	32.00
W20-5	RIGHT LANE CLOSED (1500 FT)	(48" X 48")	16.0	0	0	2	2	32.00
W20-5	RIGHT LANE CLOSED (1/2 MILE)	(48" X 48")	16.0	0	0	2	2	32.00
W20-5	RIGHT LANE CLOSED (1 MILE)	(48" X 48")	16.0	0	0	2	2	32.00
SPECIAL	MERGE NOW	(48" X 48")	16.0	0	0	1	1	16.00
SPECIAL	RUMBLE STRIPS AHEAD	(48" X 48")	16.0	2	2	4	4	64.00
SPECIAL	LEFT LANE ONLY	(162" X 36")	40.5	1	1	1	1	40.50
SPECIAL	EXIT ONLY	(204" X 36")	51.0	1	1	1	1	51.00
SPECIAL	I-30 WB	(36" X 30")	7.5	4	4	4	4	30.00
SPECIAL	I-30 WB LEFT LANE	(84" X 108")	63.0	2	2	2	2	126.00
SPECIAL	US 70 EXIT RIGHT LANE (1/2 MILE)	(138" X 162")	155.3	1	1	1	1	155.25
SPECIAL	US 70 EXIT RIGHT LANE (1 MILE)	(138" X 162")	155.3	1	1	1	1	155.25
							TOTAL	1841.50

	TOTAL	UNITS
ADVANCE WARNING ARROW PANEL	0	112 DAYS
PORTABLE CHANGEABLE MESSAGE SIGN	4	64 WEEKS
VERTICAL PANELS	100	100 EACH
TRAFFIC DRUMS	348	518 EACH
FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER	1500	17060 LIN. FT.
RELOCATING PRECAST CONCRETE BARRIER	0	18440 LIN. FT.
TEMPORARY IMPACT ATTENUATION BARRIER	2	2 EACH
TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)	2	2 EACH

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

NOTE: THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

QUANTITIES

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EARTHWORK

CL	STATION	STATION	LOCATION/DESCRIPTION	UNCLASSIFIED EXCAVATION	COMPACTED EMBANKMENT	**SELECTED MATERIAL (SM-3)	*** SOIL STABILIZATION
				CU. YD.	CU. YD.	CU. YD.	TON
WB I-30	5811+71.49	5814+68.03	L.M.L. FULL-DEPTH RECON	498	0		
WB I-30	5814+68.03	5815+08.83	L.M.L. FULL-DEPTH RECON	69	32		
WB I-30	5815+08.83	5819+50.00	L.M.L. FULL-DEPTH RECON	740	0		
WB I-30	5819+50.00	5820+01.16	L.M.L. FULL-DEPTH RECON	124	0		
WB I-30	5820+01.16	5829+33.19	L.M.L. FULL-DEPTH RECON	2251	668		
WB I-30	5829+33.19	5831+50.00	L.M.L. FULL-DEPTH RECON	524	0		
WB I-30	5831+50.00	5832+40.74	L.M.L. FULL-DEPTH RECON	220	0		
WB I-30	5832+40.74	5840+47.23	L.M.L. FULL-DEPTH RECON	1948	579		
WB I-30	5840+47.23	5841+22.12	L.M.L. FULL-DEPTH RECON	181	0		
WB I-30	5841+22.12	5844+21.00	L.M.L. FULL-DEPTH RECON	722	215		
WB I-30	5844+21.00	5845+74.69	L.M.L. FULL-DEPTH RECON	318	111		
WB I-30	5845+74.69	5847+00.00	L.M.L. FULL-DEPTH RECON	260	0		
WB I-30	5847+00.00	5847+05.04	L.M.L. FULL-DEPTH RECON	13	0		
WB I-30	5847+05.04	5853+51.00	L.M.L. FULL-DEPTH RECON	1564	480		
WB I-30	5853+51.00	5854+55.28	L.M.L. FULL-DEPTH RECON	253	76		
WB I-30	5854+55.28	5857+00.00	L.M.L. FULL-DEPTH RECON	593	0		
WB I-30	5857+00.00	5863+00.00	L.M.L. FULL-DEPTH RECON	1318	0		
WB I-30	5863+00.00	5865+00.00	L.M.L. FULL-DEPTH RECON	440	0		
WB I-30	5865+00.00	5868+48.52	L.M.L. FULL-DEPTH RECON	763	0		
WB I-30	5868+48.52	5871+00.00	L.M.L. FULL-DEPTH RECON	551	184		
WB I-30	5871+00.00	5872+00.00	L.M.L. FULL-DEPTH RECON	220	71		
WB I-30	5872+00.00	5877+34.34	L.M.L. FULL-DEPTH RECON	1174	377		
WB I-30	5877+34.34	5878+00.00	L.M.L. FULL-DEPTH RECON	145	0		
WB I-30	5878+00.00	5881+00.00	L.M.L. FULL-DEPTH RECON	659	0		
WB I-30	5881+00.00	5881+06.95	L.M.L. FULL-DEPTH RECON	17	0		
WB I-30	5881+06.95	5887+00.00	L.M.L. FULL-DEPTH RECON	1436	429		
WB I-30	5887+00.00	5895+43.38	L.M.L. FULL-DEPTH RECON	2042	610		
WB I-30	5895+43.38	5896+00.00	L.M.L. FULL-DEPTH RECON	138	0		
WB I-30	5896+00.00	5896+98.34	L.M.L. FULL-DEPTH RECON	238	0		
WB I-30	5896+98.34	5902+00.00	L.M.L. FULL-DEPTH RECON	1214	372		
WB I-30	5902+00.00	5904+44.14	L.M.L. FULL-DEPTH RECON	591	181		
WB I-30	5904+44.14	5905+00.00	L.M.L. FULL-DEPTH RECON	136	0		
WB I-30	5905+00.00	5911+00.00	L.M.L. FULL-DEPTH RECON	1328	0		
WB I-30	5911+00.00	5912+92.25	L.M.L. FULL-DEPTH RECON	426	0		
WB I-30	5912+92.25	5918+50.00	L.M.L. FULL-DEPTH RECON	1234	422		
WB I-30	5918+50.00	5924+50.00	L.M.L. FULL-DEPTH RECON	1456	314		
WB I-30	5924+50.00	5928+50.00	L.M.L. FULL-DEPTH RECON	971	210		
WB I-30	5928+50.00	5934+50.00	L.M.L. FULL-DEPTH RECON	1452	310		
WB I-30	5934+50.00	5946+00.00	L.M.L. FULL-DEPTH RECON	2782	594		
WB I-30	5946+00.00	5952+00.00	L.M.L. FULL-DEPTH RECON	1332	323		
WB I-30	5952+00.00	5954+00.00	L.M.L. FULL-DEPTH RECON	444	108		
WB I-30	5954+00.00	5960+00.00	L.M.L. FULL-DEPTH RECON	1328	319		
WB I-30	5960+00.00	5961+00.00	L.M.L. FULL-DEPTH RECON	222	49		
WB I-30	5961+00.00	5964+00.00	L.M.L. FULL-DEPTH RECON - TAPER	951	174		
WB I-30	5964+00.00	5964+88.46	L.M.L. FULL-DEPTH RECON - ACCEL LANE	304	60		
WB I-30	5913+38		MEDIAN CROSSING	18	654		
WB I-30	236+43		MEDIAN CROSSING	26	43		
	ENTIRE PROJECT		GUARDRAIL WIDENING LOCATION	2	625		
*	ENTIRE PROJECT		AS DIRECTED BY ENGINEER			50	50
WB I-30	5958+13.02	5969+05.40	MOT - L.M.L. OUTSIDE- ACCEL & TAPER	2133	1182		
WB I-30	236+32.50	238+76.48	MOT - L.M.L. OUTSIDE- ACCEL LANE	368	197		
WB I-30	238+11.34	242+02.89	MOT - US 70 ENTRANCE RAMP GORE AREA	1387	952		
WB I-30	239+59.69	241+14.82	MOT - L.M.L. OUTSIDE	151	37		
TOTALS				39675	10958	50	50

NOTE: EARTHWORK QUANTITIES SHOWN ABOVE SHALL BE PAID FOR AS PLAN QUANTITY.

* QUANTITY ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECIFICATIONS.

** IF ONLY SMALL, ISOLATED AREAS OF UNSTABLE SOIL ARE ENCOUNTERED IN THE EXPOSED SUBGRADE, UNDERCUT TO THE DEPTH OF STABLE MATERIAL FOR A MAXIMUM OF 2 FEET.

*** IF LARGER, MORE WIDESPREAD AREAS OF INSTABILITY ARE ENCOUNTERED, STABILIZE THE EXISTING SOIL TO A DEPTH OF 16 INCHES WITH PORTLAND CEMENT. IT IS ESTIMATED THAT APPROXIMATELY 9% PORTLAND CEMENT (BY DRY WEIGHT OF SOIL) WOULD BE NEEDED TO STABILIZE THE SOIL.



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		70	114

QUANTITIES

BORROW

CL	STATION	STATION	LOCATION	LENGTH LIN. FT.	OUTSIDE		INSIDE		TOTAL
					SO. FT.	CU. YD.	SO. FT.	CU. YD.	CU. YD.
WB I-30	5811+71.49	5961+00.00	L.M.L. FULL-DEPTH RECON	14928.5	1.76	974	2.10	1162	2136
WB I-30	5961+00.00	5964+00.00	L.M.L. FULL-DEPTH RECON - TAPER	300.0	1.85	21	2.10	24	45
WB I-30	5964+00.00	5964+88.46	L.M.L. FULL-DEPTH RECON - ACCEL LANE	88.5	1.85	7	2.10	7	14
TOTALS						1002		1193	2195

GUARDRAIL

CL	STATION	STATION	LOCATION	GUARDRAIL (TYPE A)	TERMINAL ANCHOR POSTS (TYPE 1)	GUARDRAIL TERMINAL (TYPE 2)	THRE BEAM GUARDRAIL TERMINAL
				LIN. FT.	EACH	EACH	EACH
WB I-30	5854+90.00	5859+58.75	LT. - L.M.L.	400		1	1
WB I-30	5854+93.00	5860+11.75	RT. - L.M.L.	450		1	1
EB I-30	5933+14.00	5938+14.00	LT. - R.M.L.	450	1	1	
WB I-30	5935+67.00	5940+67.00	RT. - L.M.L.	450	1	1	
WB I-30	5937+02.00	5940+95.75	LT. - L.M.L.	325		1	1
TOTALS				2075	2	5	3

NOTE: ADDITIONAL SURFACE COURSE AND AGGREGATE BASE COURSE DUE TO WIDENING FOR GUARDRAIL ARE QUANTIFIED UNDER BASE AND SURFACING TABULATED QUANTITIES.

QUANTITIES

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602		71	114
QUANTITIES								

CONCRETE DITCH PAVING (WRSF)

CL	STATION	STATION	LOCATION	LENGTH	WIDTH	CONCRETE DITCH PAVING (TYPE B)	SOLID SODDING	WATER
				LIN. FT.	LIN. FT.	SO. YD.	SO. YD.	M. GAL.
WB I-30	5914+76	5935+58	RT. - L.M.L.	2082	4'-0"	925.34	925.34	11.7
EB I-30	5938+25	5969+23	LT. - R.M.L.	3098	4'-0"	1376.89	1376.89	17.4
WB I-30	236+68	244+51	RT. - L.M.L.	783	4'-0"	348.00	348.00	4.4
TOTALS						2650.23	2650.23	33.5

WATER = 12.6 GAL. / SQ. YD. OF SOLID SODDING

CLEARING AND GRUBBING

CL	STATION	STATION	CLEARING	GRUBBING	DESCRIPTION
WB I-30	5958+13.02	5965+05.40	7	7	TEMP. RAMP, ACCEL. LANE & TAPER IN AREA WITH EXISTING TREES - US 70 WB ENTRANCE RAMP
WB I-30	236+32.50	242+02.89	6	6	TEMP. RAMP & ACCEL LANE IN AREA WITH EXISTING TREES - US 70 WB ENTRANCE RAMP
TOTALS			13	13	

CONCRETE BARRIER WALL (PIER PROTECTION TYPE A)

CL	STATION	STATION	LOCATION	LENGTH
				LIN. FT.
WB I-30	5854+55.00	5854+90.00	LT. - L.M.L.	35
WB I-30	5854+58.00	5854+93.00	RT. - L.M.L.	35
WB I-30	5936+71.00	5937+02.00	LT. - L.M.L.	31
TOTAL				101

WIRE ROPE SAFETY FENCE (WRSF)

CL	STATION	STATION	LOCATION	LIN. FT.	WRSF MAINTENANCE MATERIALS	WIRE ROPE SAFETY FENCE (POST REPAIR)
					LUMP SUM	EACH
** ENTIRE PROJECT					1.00	25
** AS DIRECTED BY ENGINEER						
WB I-30	5914+76	5936+17	RT. - L.M.L.	2141		
EB I-30	5937+65	5969+23	LT. - R.M.L.	3158		
WB I-30	236+68	244+51	RT. - L.M.L.	783		
TOTALS					6082	25

** QUANTITY IS ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECIFICATIONS.

CULVERT CLEAN OUT

CL	STATION	DESCRIPTION	CULVERT CLEAN OUT
			EACH
WB I-30	5814+75.00	30" X 80' R.C. PIPE CULVERT	1
WB I-30	5833+25.00	48" PIPE CULVERT	1
WB I-30	5844+15.00	5' X 4' BOX CULVERT	1
WB I-30	5855+00.00	UNKNOWN	2
WB I-30	5873+16.00	DOUBLE 8' X 7' BOX CULVERT	1
WB I-30	5892+30.00	48" PIPE CULVERT	1
WB I-30	5912+90.00	30" PIPE CULVERT	1
WB I-30	5917+92.00	18" R.C. PIPE CULVERT	1
WB I-30	5925+19.00	DOUBLE 8' X 6' BOX CULVERT	1
WB I-30	5928+94.00	18" R.C. PIPE CULVERT	1
WB I-30	5932+91.00	18" R.C. PIPE CULVERT	1
WB I-30	5936+83.00	DOUBLE 36" X 200' R.C. PIPE CULVERT	1
WB I-30	5937+97.00	18" X 96' R.C. PIPE CULVERT	1
WB I-30	5939+75.00	30" PIPE CULVERT	1
WB I-30	5945+93.00	18" X 93' R.C. PIPE CULVERT	1
WB I-30	5964+41.00	UNKNOWN	1
TOTAL			17

EROSION CONTROL

LOCATION	PERMANENT EROSION CONTROL					TEMPORARY EROSION CONTROL									
	SEEDING	LIME	MULCH COVER	WATER	SECOND SEEDING APPLICATION	TEMPORARY SEEDING	MULCH COVER	WATER	SAND BAG DITCH CHECKS (E-5)	ROCK DITCH CHECKS (E-6)	DROP INLET SILT FENCE (E-7)	SILT FENCE (E-11)	SEDIMENT BASIN (E-14)	SEDIMENT REMOVAL AND DISPOSAL	OBLITERATION OF SEDIMENT BASIN
	ACRE	TON	ACRE	M. GAL.	ACRE	ACRE	ACRE	M. GAL.	BAGS	CU. YD.	LIN. FT.	LIN. FT.	CU. YD.		
* ENTIRE PROJECT - AS SHOWN ON PLANS															
* ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER															
TOTALS															
	17.00	34	17.00	1734.0	17.00	17.00	17.00	346.8	572	120	238	6628	750	995	750

BASIS OF ESTIMATE: WATER = 20.4 M. G. PER ACRE TEMPORARY SEEDING
 ROCK DITCH CHECKS = 6 CU. YD. PER LOCATION
 SAND BAG DITCH CHECKS = 22 BAGS PER LOCATION
 LIME = 2 TONS / ACRE OF SEEDING
 WATER = 102.0 M.G. / ACRE OF SEEDING
 SEDIMENT BASIN = (5 LOCATIONS APPROX. 150 CU. YD. EACH), W = 20', LENGTH = 40', DEPTH = 5'

TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION OF U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

* QUANTITY IS ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECIFICATIONS.

QUANTITIES



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
2-12-15								
				JOB NO.		BB0602	72	114
				QUANTITIES				

REMOVAL AND DISPOSAL OF ITEMS

CL	STATION	STATION	LOCATION	DESCRIPTION	CONCRETE RIPRAP	GUARDRAIL	IMPACT ATTENUATION BARRIERS	PIER PROTECTION	RUMBLE STRIP	* PIPE UNDERDRAIN	* UNDERDRAIN OUTLET PROTECTORS
					SO. YD.	LIN. FT.	EACH	LIN. FT.	SO. YD.	LIN. FT.	EACH
WB I-30	5808+65.00	5819+50.00	RT. - L.M.L.	RUMBLE STRIP					261		
WB I-30	5811+71.49	5964+88.46	L.M.L.	4" PIPE UNDERDRAIN; IF AND WHERE DIRECTED BY THE ENGINEER						15317	51
WB I-30	5854+55.34	5854+90.34	LT. - L.M.L.	PIER PROTECTION				35			
WB I-30	5854+58.19	5854+93.19	RT. - L.M.L.	PIER PROTECTION				35			
WB I-30	5854+88.34	5856+88.34	LT. - L.M.L.	GUARDRAIL		200					
WB I-30	5854+92.71	5856+92.71	RT. - L.M.L.	GUARDRAIL		200					
EB I-30	5936+75.00		MEDIAN	IMPACT ATTENUATION BARRIER	24		1				
WB I-30	5936+70.82	5937+01.82	LT. - L.M.L.	PIER PROTECTION				31			
WB I-30	5937+10.00		MEDIAN	IMPACT ATTENUATION BARRIER	24		1				
WB I-30	5936+99.84	5938+99.84	LT. - L.M.L.	GUARDRAIL		200					
TOTALS					48	600	2	101	261	15317	51

- NOTES: 1. REMOVAL AND DISPOSAL OF GUARDRAIL TERMINAL (TYPE 2) SHALL BE PAID FOR BY THE LINEAR FOOT AS REMOVAL AND DISPOSAL OF GUARDRAIL.
 2. THE GUARDRAIL QUANTITY SHOWN ABOVE SHALL INCLUDE THE REMOVAL AND DISPOSAL OF ANY TERMINAL ANCHOR POSTS.
 *3. QUANTITIES ARE ESTIMATED, SEE SECTION 104.03 OF THE STD. SPECIFICATIONS. THE REMOVAL OF EXISTING UNDERDRAIN COVERS WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR "REMOVAL AND DISPOSAL OF PIPE UNDERDRAINS."

REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT

CL	STATION	STATION	LOCATION	LENGTH	WIDTH	PORTLAND CEMENT CONCRETE PAVEMENT
				LIN. FT.	FEET	SO. YD.
WB I-30	5811+71.49	5819+50.00	L.M.L. FULL-DEPTH RECONSTRUCTION (38' EXIST. PCC WIDTH)	778.6	38.0	3287.5
WB I-30	5819+50.00	5896+00.00	L.M.L. FULL-DEPTH RECONSTRUCTION (26' EXIST. PCC WIDTH)	7,650.0	26.0	22100.0
WB I-30	5896+00.00	5961+00.00	L.M.L. FULL-DEPTH RECONSTRUCTION (24' EXIST. PCC WIDTH)	6,500.0	24.0	17333.4
WB I-30	5961+00.00	5964+43.21	L.M.L. FULL-DEPTH RECONSTRUCTION - TAPER	343.3	32.3	1231.0
WB I-30	5964+43.21	5964+88.46	L.M.L. FULL-DEPTH RECONSTRUCTION - ACCEL LANE	45.3	39.0	196.3
TOTAL						44148.2

NOTE: THE REMOVAL AND DISPOSAL OF PLOWABLE PAVEMENT MARKERS WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR "REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT."

COLD MILLING ASPHALT PAVEMENT

CL	STATION	STATION	LOCATION	LENGTH	WIDTH	COLD MILLING ASPHALT PAVEMENT
				LIN. FT.	FEET	SO. YD.
WB I-30	5844+21.00	5853+51.00	L.M.L.	930	14	1447
TOTAL						1447

ACHM PATCHING OF EXISTING SHOULDERS

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	50
TOTAL	
	50

NOTE: QUANTITY IS ESTIMATED, SEE SECTION 104.03 OF THE STD. SPECIFICATIONS. FOR ACHM PATCHING OF EXISTING SHOULDERS, THE PG BINDER WILL MATCH THE PG BINDER OF THE SURFACE COURSE USED ON THE LANES.

ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC

DESCRIPTION	TON	TACK COAT
		GALLON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	75	150
TOTALS		
	75	150

NOTE: QUANTITY IS ESTIMATED, SEE SECTION 104.03 OF THE STD. SPECIFICATIONS.
 BASIS OF ESTIMATE: ACHM CONCRETE PATCHING = 25 TON / MILE
 TACK COAT = 50 GAL / MILE

ACHM PATCHING OF EXISTING ROADWAY

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	25
TOTAL	
	25

NOTE: QUANTITY IS ESTIMATED, SEE SECTION 104.03 OF THE STD. SPECIFICATIONS. FOR ACHM PATCHING OF EXISTING ROADWAY, THE PG BINDER WILL MATCH THE PG BINDER OF THE SURFACE COURSE USED ON THE LANES.

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11/19/2014



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		BBO602	74	114
② QUANTITIES								

BASE AND SURFACING - LANES ONLY

CL	STATION	STATION	LOCATION / DESCRIPTION	LENGTH FEET	PORTLAND CEMENT CONCRETE PAVEMENT 13" U.T.		ACHM SURFACE COURSE (3/8")				TACK COAT 0.03 GAL. PER SQ. YD.			PROCESSING CEMENT STABILIZED CRUSHED STONE BASE COURSE (6" COMPACTED DEPTH)		CEMENT IN CEMENT STABILIZED CRUSHED STONE BASE COURSE TON	AGGREGATE IN CEMENT STABILIZED CRUSHED STONE BASE COURSE TON
					AVG. WIDTH	SO. YD.	AVG. WIDTH	SO. YD.	LBS. PER SQ. YD.	PG 64-22	TOTAL WIDTH	SO. YD.	GAL	AVG. WIDTH	SO. YD.		
					FEET	FEET	FEET	FEET	TON	FEET	FEET	FEET	FEET	FEET	FEET		
WB I-30	5811+71.49	5958+03.94	L.M.L. FULL-DEPTH RECON - LANES ONLY	14632.45	26.00	42271.53	26.00	42271.53	110.00	2324.93	26.00	42271.53	1268.15	26.00	42271.53	887.71	13907.34
WB I-30	5958+03.94	5961+03.94	L.M.L. FULL-DEPTH RECON - TAPER - LANES ONLY	300.00	30.00	1000.00	30.00	1000.00	110.00	55.00	30.00	1000.00	30.00	30.00	1000.00	21.00	329.00
WB I-30	5961+03.94	5964+88.46	L.M.L. FULL-DEPTH RECON - ACCEL LANE - LANES ONLY	384.52	36.00	1538.09	36.00	1538.09	110.00	84.59	36.00	1538.09	46.15	36.00	1538.09	32.30	506.04
TOTALS						44809.62		44809.62		2464.52		44809.62	1344.30		44809.62	941.01	14742.38

BASIS OF ESTIMATE: ACHM SURFACE COURSE (3/8") - MINERAL AGGREGATE = 95.1%, ASPHALT BINDER (PG 64-22) = 4.9%
Nmax = 115 (PG 64-22)

BASIS OF ESTIMATE: CEMENT STABILIZED CRUSHED STONE BASE COURSE - AGGREGATE = 94.0%, CEMENT = 6.0%

BASE AND SURFACING - SHOULDER

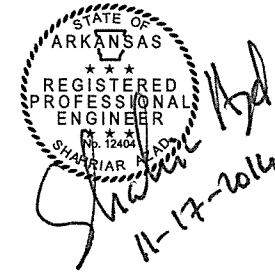
CL	STATION	STATION	LOCATION / DESCRIPTION	LENGTH FEET	PORTLAND CEMENT CONCRETE PAVEMENT 13" U.T.		ACHM SURFACE COURSE (3/8")				TACK COAT 0.03 GAL. PER SQ. YD.			PROCESSING CEMENT STABILIZED CRUSHED STONE BASE COURSE (6" COMPACTED DEPTH)		CEMENT IN CEMENT STABILIZED CRUSHED STONE BASE COURSE TON	AGGREGATE IN CEMENT STABILIZED CRUSHED STONE BASE COURSE TON	AGGREGATE BASE COURSE (CLASS 7)		RUMBLE STRIPS		
					AVG. WIDTH	SO. YD.	AVG. WIDTH	SO. YD.	LBS. PER SQ. YD.	PG 64-22	TOTAL WIDTH	SO. YD.	GAL	AVG. WIDTH	SO. YD.			TON/STA.	TON	ASPHALT SHOULDERS	PORTLAND CEMENT CONCRETE SHOULDERS	
					FEET	FEET	FEET	FEET	TON	FEET	FEET	FEET	FEET	FEET	FEET			TON	TON	LIN. FT.	LIN. FT.	
WB I-30	5808+65.00	5811+71.49	L.M.L. - RT. SHOULDER	306.49																	307	
WB I-30	5811+71.49	5959+03.82	L.M.L. FULL-DEPTH RECON - LT. SHOULDER	14732.33	8.00	13095.41	10.00	16369.26	110.00	900.31	10.00	16369.26	491.08	10.00	16369.26	343.76	5385.49	53.75	7955.00		14733	
WB I-30	5811+71.49	5964+88.46	L.M.L. FULL-DEPTH RECON - RT. SHOULDER	15316.97	4.00	6807.55	6.00	10211.32	110.00	561.63	6.00	10211.32	306.34	6.00	10211.32	214.44	3359.53	80.00	12320.00		15317	
WB I-30	5959+03.82	5961+03.81	L.M.L. FULL-DEPTH RECON - LT. SHOULDER - TAPER	199.99	6.00	133.33	8.00	177.77	110.00	9.78	8.00	177.77	5.34	8.00	177.77	3.74	58.49	45.75	91.50		200	
WB I-30	5961+03.81	5964+88.46	L.M.L. FULL-DEPTH RECON - LT. SHOULDER - ACCEL LANE	384.65	6.00	256.44	8.00	341.92	110.00	18.81	8.00	341.92	10.26	8.00	341.92	7.19	112.50	45.75	183.00		385	
WB I-30	5964+88.46	5969+05.40	L.M.L. - RT. SHOULDER	416.94																	417	
WB I-30	5964+88.46	5969+05.40	L.M.L. - LT. SHOULDER	416.94																	417	
WB I-30	236+32.50	244+12.48	L.M.L. - RT. SHOULDER	779.98																	780	
WB I-30	236+32.50	240+39.37	L.M.L. - LT. SHOULDER	422.36																	423	
WB I-30	238+36.00	242+84.39	L.M.L. - LT. SHOULDER	448.39																	449	
			US 70 ENTRANCE RAMP - RT. SHOULDER	430.66																	431	
TOTALS						20292.73		27100.27		1490.53		27100.27	813.02		27100.27	569.13	8916.01		20549.50		2917	30942

BASIS OF ESTIMATE: ACHM SURFACE COURSE (3/8") - MINERAL AGGREGATE = 95.1%, ASPHALT BINDER (PG 64-22) = 4.9%
Nmax = 115 (PG 64-22)

BASIS OF ESTIMATE: CEMENT STABILIZED CRUSHED STONE BASE COURSE - AGGREGATE = 94.0%, CEMENT = 6.0%

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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		BBO602	75	114
② QUANTITIES								

BASE AND SURFACING - MEDIAN CROSSING

CL	STATION	LOCATION / DESCRIPTION	ACHM SURFACE COURSE (1/2")			AGGREGATE BASE COURSE (CLASS 7)
			SQ. YD.	LBS. PER SQ. YD.	PG 64-22 TON	TON
WB I-30	5913+38	MEDIAN CROSSING	221.26	220.00	24.34	109.27
WB I-30	236+43	MEDIAN CROSSING	68.64	220.00	7.55	34.97
TOTALS			289.90		31.89	144.24

BASIS OF ESTIMATE: ACHM SURFACE COURSE (1/2") - MINERAL AGGREGATE = 95.0%, ASPHALT BINDER (PG 64-22) = 5.0%
Nmax = 115 (PG 64-22)

BASE AND SURFACING - GUARDRAIL WIDENING

CL	STATION	STATION	LOCATION	ACHM SURFACE COURSE (1/2")				AGGREGATE BASE COURSE (CLASS 7)	
				AVG. WIDTH FEET	SQ. YD.	LBS. PER SQ. YD.	PG 64-22 TON	TON / STA.	TON
WB I-30	5854+90.34	5860+01.05	LT. - L.M.L.	VAR.	302.32	220.00	33.26	VAR.	273.23
WB I-30	5854+93.19	5860+54.82	RT. - L.M.L.	VAR.	332.86	220.00	36.62	VAR.	207.49
EB I-30	5932+72.24	5937+43.31	LT. - R.M.L.	VAR.	382.48	220.00	42.08	VAR.	192.49
EB I-30	5937+43.31	5938+58.31	LT. - R.M.L.	VAR.	111.45	220.00	12.26	VAR.	82.62
WB I-30	5935+24.28	5936+39.27	RT. - L.M.L.	VAR.	111.45	220.00	12.26	VAR.	82.62
WB I-30	5936+39.27	5941+10.28	RT. - L.M.L.	VAR.	382.42	220.00	42.07	VAR.	192.49
WB I-30	5937+01.82	5941+38.01	LT. - L.M.L.	VAR.	256.48	220.00	28.22	VAR.	235.61
TOTALS					1879.46		206.77		1266.55

BASIS OF ESTIMATE: ACHM SURFACE COURSE (1/2") - MINERAL AGGREGATE = 95.0%, ASPHALT BINDER (PG 64-22) = 5.0%
Nmax = 115 (PG 64-22)

BASE AND SURFACING - TEMP. RAMP CONNECTIONS & ACCEL. LANES

CL	STATION	STATION	LOCATION	LENGTH FEET	TRENCHING & SHOULDER PREPARATION STATION	ACHM SURFACE COURSE (1/2")				ACHM BINDER COURSE (1")				ACHM BASE COURSE (1 1/2")				TACK COAT 0.03 GAL. PER SQ. YD.		
						AVG. WIDTH FEET	SQ. YD.	LBS. PER SQ. YD.	PG 76-22 TON	AVG. WIDTH FEET	SQ. YD.	LBS. PER SQ. YD.	PG 76-22 TON	AVG. WIDTH FEET	SQ. YD.	LBS. PER SQ. YD.	PG 76-22 TON	TOTAL WIDTH FEET	SQ. YD.	GALLON
STAGE 1 TRENCH & SHOULDER PREP																				
WB I-30	5808+65.00	5819+50.00	INSIDE SHOULDER - LT. MAIN LANES (2')	1085.00	II	2.00	241.12	440.00	53.05	2.00	241.12	330.00	39.79	2.00	241.12	880.00	106.10	10.00	1205.56	36.17
WB I-30	5819+50.00	5969+05.40	INSIDE SHOULDER - LT. MAIN LANES (6')	14955.40	150	6.00	9970.27	440.00	2193.46	6.00	9970.27	330.00	1645.10	6.00	9970.27	880.00	4386.92	30.00	49851.34	1495.55
WB I-30	5964+88.46	5969+05.40	OUTSIDE SHOULDER - LT. MAIN LANES (6')	416.94	5	6.00	277.97	440.00	61.16	6.00	277.97	330.00	45.87	6.00	277.97	880.00	122.31	30.00	1389.81	41.70
WB I-30	236+32.50	244+12.50	INSIDE SHOULDER - LT. MAIN LANES (6')	780.00	8	6.00	520.00	440.00	114.40	6.00	520.00	330.00	85.80	6.00	520.00	880.00	228.80	30.00	2600.00	78.00
WB I-30	237+06.80	238+11.34	OUTSIDE SHOULDER - LT. MAIN LANES (AVG. 6')	104.54	2	6.00	69.70	440.00	15.34	6.00	69.70	330.00	11.51	6.00	69.70	880.00	30.67	30.00	348.47	10.46
WB I-30	238+11.34	242+84.39	OUTSIDE SHOULDER - LT. MAIN LANES (10')	473.05	5	10.00	525.62	440.00	115.64	10.00	525.62	330.00	86.73	10.00	525.62	880.00	231.28	50.00	2628.06	78.85
			INSIDE SHOULDER - WB I-30 ENTRANCE RAMP FROM US 70 (4')	428.82	5	4.00	190.59	440.00	41.93	4.00	190.59	330.00	31.45	4.00	190.59	880.00	83.86	20.00	952.94	28.59
			OUTSIDE SHOULDER - WB I-30 ENTRANCE RAMP FROM US 70 (6')	420.72	5	6.00	280.48	440.00	61.71	6.00	280.48	330.00	46.28	6.00	280.48	880.00	123.42	30.00	1402.40	42.08
STAGE 1 TEMP. RAMP CONNECTIONS																				
WB I-30	238+11.34	239+59.69	SHOULDER WIDENING - US 70 ENTRANCE RAMP GORE AREA (STAGE 1B)	148.35		17.80	293.33	440.00	64.54	18.67	307.75	330.00	50.78	20.05	330.42	880.00	145.39	94.98	1565.53	46.97
WB I-30	239+59.69	241+14.82	SHOULDER WIDENING - OUTSIDE WB MAINLANES (STAGE 1B)	155.13		6.77	116.74	440.00	25.69	7.65	131.82	330.00	21.76	9.02	155.52	880.00	68.43	39.86	687.09	20.62
			SHOULDER WIDENING - INSIDE US 70 ENTRANCE RAMP (STAGE 1B)	286.38		13.40	426.50	440.00	93.83	14.28	454.34	330.00	74.97	15.65	498.09	880.00	219.16	73.02	2323.38	69.71
STAGE 2 TEMP. RAMP CONNECTIONS AND ACCEL. LANES																				
			WB ENTRANCE FROM US 70 ENTRANCE RAMP - ACCEL LANE & TAPER (STAGE 2A)	681.93		9.20	696.79	440.00	153.30	10.07	763.09	330.00	125.91	11.45	867.27	880.00	381.60	51.98	3938.55	118.16
			WB ENTRANCE FROM US 70 ENTRANCE RAMP - ACCEL LANE (STAGE 2A)	399.02		20.50	908.88	440.00	199.96	21.38	947.68	330.00	156.37	22.75	1008.64	880.00	443.81	108.50	4810.41	144.32
			WB ENTRANCE FROM US 70 ENTRANCE RAMP - ACCEL LANE (STAGE 2A)	246.50		11.69	320.25	440.00	70.46	12.57	344.21	330.00	56.80	13.94	381.87	880.00	168.03	64.46	1765.55	52.97
TOTALS					191		14838.24		3264.47		15024.64		2479.12		15317.56		6739.78		75469.09	2264.15

BASIS OF ESTIMATE: ACHM BASE COURSE (1 1/2") - MINERAL AGGREGATE = 96.2%, ASPHALT BINDER (PG 76-22) = 3.8%
ACHM BINDER COURSE (1") - MINERAL AGGREGATE = 95.7%, ASPHALT BINDER (PG 76-22) = 4.3%
ACHM SURFACE COURSE (1/2") - MINERAL AGGREGATE = 95.2%, ASPHALT BINDER (PG 76-22) = 4.8%
Nmax = 205 (PG 76-22)

QUANTITIES

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STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRIAR AZAD
 No. 22404
 11-19-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		BBO602	76	114
QUANTITIES								

BASE AND SURFACING - SUMMARY

LOCATION	AGGREGATE BASE COURSE (CLASS 7)	ACHM SURFACE COURSE (1/2") (PG 64-22)	ACHM SURFACE COURSE (1/2") (PG 76-22)	ACHM SURFACE COURSE (3/8") (PG 64-22)	ACHM BINDER COURSE (1") (PG 76-22)	ACHM BASE COURSE (1-1/2") (PG 76-22)	CEMENT STABILIZED CRUSHED STONE BASE COURSE			P.C.C PAVEMENT (13" U.T.)	RUMBLE STRIPS IN ASPHALT SHOULDERS	RUMBLE STRIPS IN PORTLAND CEMENT CONCRETE SHOULDERS	TACK COAT GALLON	TRENCHING & SHOULDER PREPARATION STA.
							PROCESSING	CEMENT	AGGREGATE					
							TON							
WB I-30 - LANES ONLY				2464.52			44809.62	941.01	14742.38	44809.62			1344.30	
WB I-30 - SHOULDERS	20549.50			1490.53			27100.27	569.13	8916.01	20292.73	2917	30942	813.02	
GUARDRAIL WIDENING	1266.55	206.77												
MEDIAN CROSSING	144.24	31.89												
STAGE CONST. TEMP. RAMP CONNECTIONS			3264.47		2479.12	6739.78							2264.15	191
TOTALS	21960.29	238.66	3264.47	3955.05	2479.12	6739.78	71909.89	1510.14	23658.39	65102.35	2917	30942	4421.47	191

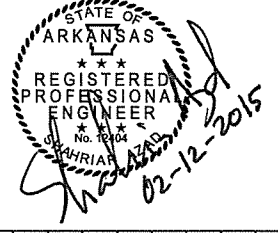
BASIS OF ESTIMATE: ACHM BASE COURSE (1 1/2") - MINERAL AGGREGATE = 96.2%, ASPHALT BINDER (PG 76-22) = 3.8%
 ACHM BINDER COURSE (1") - MINERAL AGGREGATE = 95.7%, ASPHALT BINDER (PG 76-22) = 4.3%
 ACHM SURFACE COURSE (3/8") - MINERAL AGGREGATE = 95.1%, ASPHALT BINDER (PG 64-22) = 4.9%
 ACHM SURFACE COURSE (1/2") - MINERAL AGGREGATE = 95.0%, ASPHALT BINDER (PG 64-22) = 5.0%
 ACHM SURFACE COURSE (1/2") - MINERAL AGGREGATE = 95.2%, ASPHALT BINDER (PG 76-22) = 4.8%
 Nmax = 115 (PG 64-22)
 Nmax = 205 (PG 76-22)

BASIS OF ESTIMATE: CEMENT STABILIZED CRUSHED STONE BASE COURSE - AGGREGATE = 94.0%, CEMENT = 6.0%

NOTE: THE CONTRACTOR, WITH APPROVAL OF THE ENGINEER, WILL BE ALLOWED TO SUBSTITUTE A HIGHER PERFORMANCE GRADE ASPHALT SURFACE COURSE FOR MEDIAN CROSSING CONSTRUCTION AT NO ADDITIONAL COST TO THE DEPARTMENT.

SUMMARY OF QUANTITIES (BOX 1 OF 2)

ITEM NUMBER	ITEM	PEN-B30-0(2)II	BIM-B30-0(2)II	TOTAL	UNIT
201	CLEARING		13	13	STA.
201	GRUBBING		13	13	STA.
202	REMOVAL AND DISPOSAL OF PIER PROTECTION		101	101	LIN. FT.
202	REMOVAL AND DISPOSAL OF CONCRETE RIPRAP		48	48	SO. YD.
SP & 202	REMOVAL AND DISPOSAL OF GUARDRAIL		600	600	LIN. FT.
SP	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIER		2	2	EACH
SP	REMOVAL OF RUMBLE STRIP		261	261	SO. YD.
202	REMOVAL AND DISPOSAL OF PIPE UNDERDRAINS		15317	15317	LIN. FT.
202	REMOVAL AND DISPOSAL OF UNDERDRAIN OUTLET PROTECTORS		51	51	EACH
SP & 202	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT		44148	44148	SO. YD.
210	UNCLASSIFIED EXCAVATION		39675	39675	CU. YD.
210	COMPACTED EMBANKMENT		10958	10958	CU. YD.
SP & 210	BORROW		2195	2195	CU. YD.
SP & 210	SOIL STABILIZATION		50	50	TON
SP & 215	TRENCHING AND SHOULDER PREPARATION		191	191	STA.
302	SELECTED MATERIAL (CLASS SM-3)		50	50	CU. YD.
303	AGGREGATE BASE COURSE (CLASS 7)		21960	21960	TON
308	AGGREGATE IN CEMENT STABILIZED CRUSHED STONE BASE COURSE		23658	23658	TON
308	CEMENT IN CEMENT STABILIZED CRUSHED STONE BASE COURSE		1510	1510	TON
308	PROCESSING CEMENT STABILIZED CRUSHED STONE BASE COURSE		71910	71910	SO. YD.
401	TACK COAT		4571	4571	GAL.
SP & 405	MINERAL AGGREGATE IN ACHM BASE COURSE (1 1/2")		6484	6484	TON
SP & 405	ASPHALT BINDER (PG 76-22) IN ACHM BASE COURSE (1 1/2")		256	256	TON
SP, SS & 406	MINERAL AGGREGATE IN ACHM BINDER COURSE (1")		2372	2372	TON
SP, SS & 406	ASPHALT BINDER (PG 76-22) IN ACHM BINDER COURSE (1")		107	107	TON
SP, SS & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (3/8")		3761	3761	TON
SP, SS & 407	ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (3/8")		194	194	TON
SP, SS & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")		3334	3334	TON
SP, SS & 407	ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (1/2")		12	12	TON
SP, SS & 407	ASPHALT BINDER (PG 76-22) IN ACHM SURFACE COURSE (1/2")		157	157	TON
412	COLD MILLING ASPHALT PAVEMENT		1447	1447	SO. YD.
SP & 414	ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC		75	75	TON
SP	ACHM PATCHING OF EXISTING SHOULDERS		50	50	TON
SP & 415	ACHM PATCHING OF EXISTING ROADWAY		25	25	TON
501	PORTLAND CEMENT CONCRETE PAVEMENT (13" UNIFORM THICKNESS)		65102	65102	SO. YD.
507	REMOVAL AND DISPOSAL OF CONCRETE PAVEMENT FOR PATCHING		2000	2000	SO. YD.
507	PORTLAND CEMENT CONCRETE PAVEMENT PATCHING (8" UNIFORM THICKNESS)		1000	1000	SO. YD.
507	PORTLAND CEMENT CONCRETE PAVEMENT PATCHING (1" UNIFORM THICKNESS)		750	750	SO. YD.
507	PORTLAND CEMENT CONCRETE PAVEMENT PATCHING (12" UNIFORM THICKNESS)		250	250	SO. YD.
601	MOBILIZATION		1.00	1.00	LUMP SUM
SP & 602	FURNISHING FIELD OFFICE		1	1	EACH
SP & 603	TRAFFIC CONTROL SUPERVISOR		1.00	1.00	LUMP SUM
SP & 603	MAINTENANCE OF TRAFFIC		1.00	1.00	LUMP SUM
SS & 604	SIGNS		1842	1842	SO. FT.
SS & 604	TRAFFIC DRUMS		518	518	EACH
604	FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER		17060	17060	LIN. FT.
604	RELOCATING PRECAST CONCRETE BARRIER		18440	18440	LIN. FT.
604	CONSTRUCTION PAVEMENT MARKINGS		30633	30633	LIN. FT.
604	REMOVABLE CONSTRUCTION PAVEMENT MARKINGS		45378	45378	LIN. FT.
604	REMOVAL OF PERMANENT PAVEMENT MARKINGS		26552	26552	LIN. FT.
SP & 604	PORTABLE CHANGEABLE MESSAGE SIGN		64	64	WEEK
604	ADVANCE WARNING ARROW PANEL		112	112	DAY
SS & 604	VERTICAL PANELS		100	100	EACH
SP	TEMPORARY PORTABLE RUMBLE STRIPS		12	12	EACH
SP	PORTABLE CAMERA ASSEMBLY		32	32	WEEK
SP & 605	CONCRETE DITCH PAVING (TYPE B)	2650		2650	SO. YD.
SP	CULVERT CLEAN OUT		17	17	EACH
617	GUARDRAIL (TYPE A)		2075	2075	LIN. FT.
617	TERMINAL ANCHOR POSTS (TYPE 1)		2	2	EACH
617	GUARDRAIL TERMINAL (TYPE 2)		5	5	EACH
617	THREE BEAM GUARDRAIL TERMINAL		3	3	EACH
SP	WIRE ROPE SAFETY FENCE	6082		6082	LIN. FT.
SP	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS	1.00		1.00	LUMP SUM
SP	WIRE ROPE SAFETY FENCE (POST REPAIR)	25		25	EACH
620	LIME		34	34	TON
620	SEEDING		17.00	17.00	ACRE
SS & 620	MULCH COVER		34.00	34.00	ACRE
620	WATER	33.5		33.5	M. GAL.
621	TEMPORARY SEEDING		17.00	17.00	ACRE
621	SILT FENCE		6628	6628	LIN. FT.



DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AD PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-21-15				6	ARK.			
2-12-15								
				JOB NO.	BB0602		77	114
2 SUMMARY OF QUANTITIES								

SUMMARY OF QUANTITIES (BOX 2 OF 2)

ITEM NUMBER	ITEM	PEN-B30-0(2U)	BIM-B30-0(2U)	TOTAL	UNIT
621	SAND BAG DITCH CHECKS		572	572	BAG
621	DROP INLET SILT FENCE		238	238	LIN. FT.
621	SEDIMENT BASIN		750	750	CU. YD.
621	OBLITERATION OF SEDIMENT BASIN		750	750	CU. YD.
621	SEDIMENT REMOVAL AND DISPOSAL		995	995	CU. YD.
621	ROCK DITCH CHECKS		120	120	CU. YD.
623	SECOND SEEDING APPLICATION		17,000	17,000	ACRE
624	SOLID SODDING	2650		2650	SO. YD.
631	CONCRETE BARRIER WALL (PIER PROTECTION TYPE A)		101	101	LIN. FT.
SP & 635	ROADWAY CONSTRUCTION CONTROL		1,000	1,000	LUMP SUM
642	RUMBLE STRIPS IN ASPHALT SHOULDERS		2917	2917	LIN. FT.
642	RUMBLE STRIPS IN PORTLAND CEMENT CONCRETE SHOULDERS		30942	30942	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING WHITE (4") - ALTERNATE NO. 1		17527	17527	LIN. FT.
* SP & 719	HIGH PERFORMANCE MARKING TAPE WHITE (4") - ALTERNATE NO. 2		17527	17527	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING (SKIP LINE) WHITE (4") - ALTERNATE NO. 1		957	957	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING (SKIP LINE) WHITE (4") - ALTERNATE NO. 2		957	957	LIN. FT.
* SP & 719	HIGH PERFORMANCE MARKING TAPE (SKIP LINE) WHITE (4") - ALTERNATE NO. 1		354	354	LIN. FT.
* SP & 719	HIGH PERFORMANCE MARKING TAPE (SKIP LINE) WHITE (4") - ALTERNATE NO. 2		354	354	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING YELLOW (4") - ALTERNATE NO. 1		17545	17545	LIN. FT.
* SP & 719	HIGH PERFORMANCE MARKING TAPE YELLOW (4") - ALTERNATE NO. 2		17545	17545	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC CONTRAST PAVEMENT MARKING WHITE (4") - ALTERNATE NO. 1		3992	3992	LIN. FT.
* SP & 719	HIGH PERFORMANCE CONTRAST MARKING TAPE WHITE (4") - ALTERNATE NO. 2		3992	3992	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC CONTRAST PAVEMENT MARKING WHITE (8") - ALTERNATE NO. 1		9	9	LIN. FT.
* SP & 719	HIGH PERFORMANCE CONTRAST MARKING TAPE WHITE (8") - ALTERNATE NO. 2		9	9	LIN. FT.
721	RAISED PAVEMENT MARKERS (TYPE II)		331	331	EACH
731	TEMPORARY IMPACT ATTENUATION BARRIER		2	2	EACH
731	TEMPORARY IMPACT ATTENUATION BARRIER (REPAIR)		2	2	EACH
SP	AWIS MOBILIZATION		1,000	1,000	LUMP SUM
SP	AWIS OPERATION		4	4	MONTH
SP	DEVICE RELOCATION		11	11	EACH
SP	FURNISH AND INSTALL CLOSED CIRCUIT TELEVISION SYSTEM		4	4	EACH
SP	FURNISH AND INSTALL PUBLIC NOTIFICATION SYSTEM		1	1	EACH
SP	FURNISH AND INSTALL VARIABLE MESSAGE SIGN		4	4	EACH
SP	FURNISH AND INSTALL VEHICLE DETECTION SYSTEM		13	13	EACH

* DENOTES ALTERNATE BID ITEMS.

REVISIONS

DATE	REVISION	SHEET NO.
1-12-15	REVISED MAINTENANCE OF TRAFFIC SPECIAL PROVISION; ADDED ASSESSMENT OF WORKING DAYS - REVISED "CALENDAR DAY" SPECIAL PROVISION.	2 & 78
1-15-15	REVISED MANDATORY USE OF INTERNET BIDDING SPECIAL PROVISION; REVISED GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION SPECIAL PROVISION; ADDED DISADVANTAGED BUSINESS ENTERPRISE BIDDER'S RESPONSIBILITIES SPECIAL PROVISION.	2 & 78
1-21-15	ADDED RUMBLE STRIP REMOVAL DETAIL; REVISED MAINTENANCE OF TRAFFIC NOTES 3 AND 4 FOR STAGE 1A; REPLACED RUMBLE STRIP REMOVAL SPECIAL PROVISION WITH A SPECIAL PROVISION USED FOR CONCRETE SHOULDERS; REVISED UNIT AND QUANTITY FOR RUMBLE STRIP REMOVAL ACCORDING TO NEW SPECIAL PROVISION.	22, 27-34, 72, 77, 78
1-29-15	REVISED "SITE USE (A+C METHOD)" AND "MANDATORY USE OF INTERNET BIDDING" SPECIAL PROVISIONS.	78
2-12-15	ADDED ITEM "REMOVAL AND DISPOSAL OF UNDERDRAIN OUTLET PROTECTORS" TO QUANTITY BOX AND SUMMARY OF QUANTITIES. ADDED NOTE 3 UNDER QUANTITY BOX.	72, 77, & 78



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1-12-15		1-29-15		6	ARK.			
1-15-15		2-12-15						
1-21-15								

2 SUMMARY OF QUANTITIES AND REVISIONS

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11/7/2014

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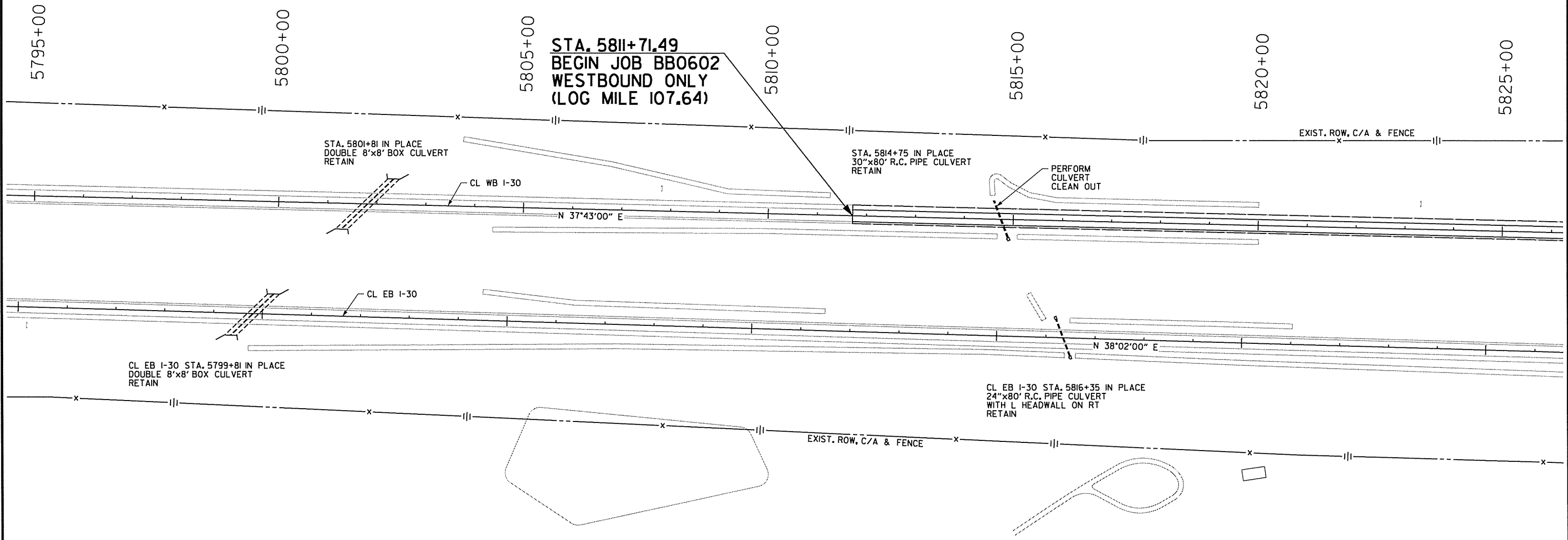


Shariar A. Ad
11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		79	114

② PLAN STA. 5795+00 TO STA. 5826+00

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. RUMBLE STRIPS SHALL BE CONSTRUCTED ON I-30 WB SHOULDERS PER THE DETAILS PROVIDED ON THE SPECIAL DETAIL SHEETS.

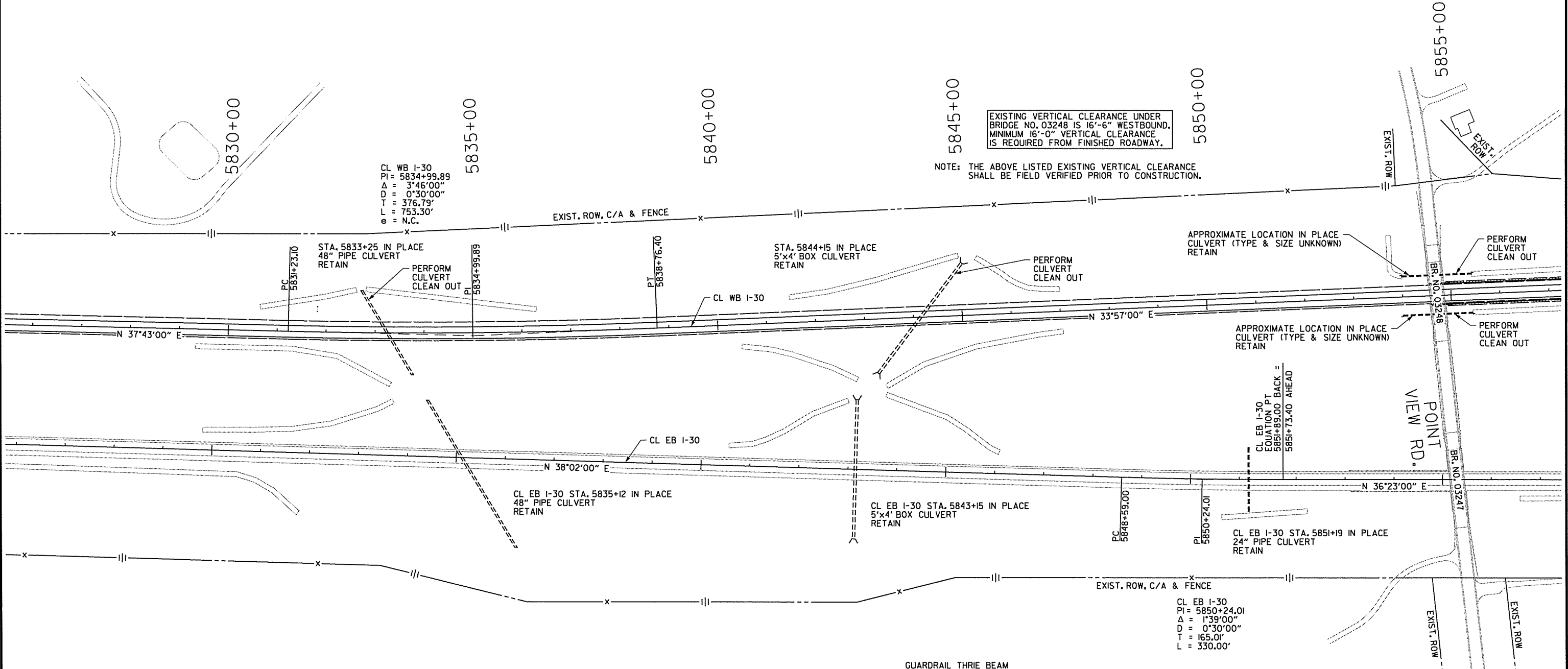


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DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602	80	114	
② PLAN STA. 5826+00 TO STA. 5857+00								

- NOTES: 1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
2. RUMBLE STRIPS SHALL BE CONSTRUCTED ON I-30 WB SHOULDERS PER THE DETAILS PROVIDED ON THE SPECIAL DETAIL SHEETS.



EXISTING VERTICAL CLEARANCE UNDER BRIDGE NO. 03248 IS 16'-6" WESTBOUND. MINIMUM 16'-0" VERTICAL CLEARANCE IS REQUIRED FROM FINISHED ROADWAY.

NOTE: THE ABOVE LISTED EXISTING VERTICAL CLEARANCE SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION.

PROPOSED GUARDRAIL LOCATION	GUARDRAIL (TYPE A)	GUARDRAIL TERMINAL (TYPE 2)	THRE BEAM GUARDRAIL TERMINAL
STA. 5854+90.00 TO STA. 5859+58.75	LT OF LT LANES = 400 LIN. FT.	1 EACH	1 EACH
STA. 5854+93.00 TO STA. 5860+11.75	RT OF LT LANES = 450 LIN. FT.	1 EACH	1 EACH

EXISTING GUARDRAIL LOCATION (REMOVE)	GUARDRAIL
STA. 5854+88.34 TO STA. 5856+88.34	LT OF LT LANES = 200 LIN. FT.
STA. 5854+92.71 TO STA. 5856+92.71	RT OF LT LANES = 200 LIN. FT.

PROPOSED CONCRETE BARRIER WALL (PIER PROTECTION) (TYPE A)	
STA. 5854+55.00 TO STA. 5854+90.00	LT OF LT LANES = 35 LIN. FT.
STA. 5854+58.00 TO STA. 5854+93.00	RT OF LT LANES = 35 LIN. FT.

CONCRETE BARRIER WALL (PIER PROTECTION) (REMOVE)	
STA. 5854+55.34 TO STA. 5854+90.34	LT OF LT LANES = 35 LIN. FT.
STA. 5854+58.19 TO STA. 5854+93.19	RT OF LT LANES = 35 LIN. FT.

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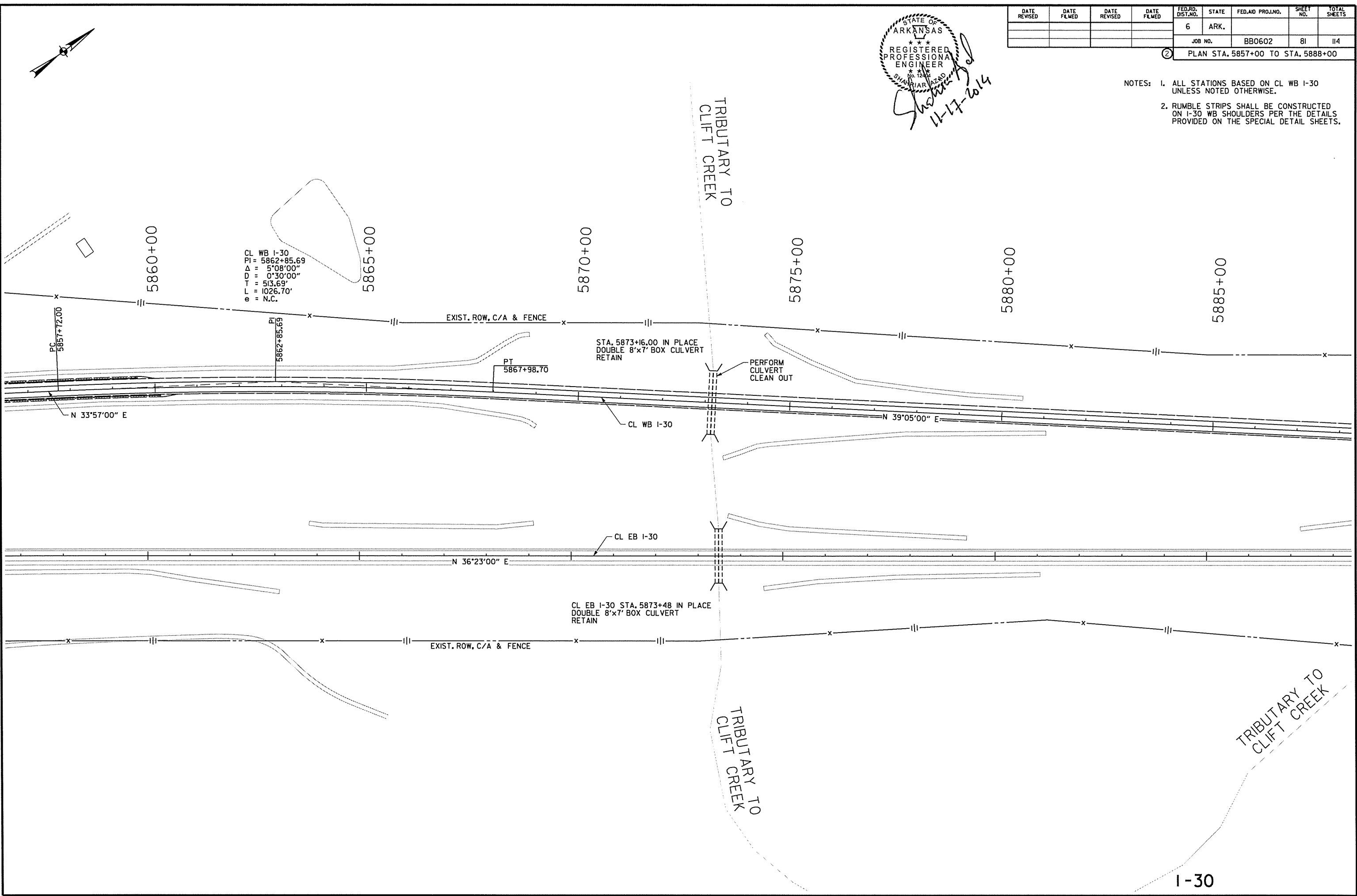
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STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRIAR SAZAD
 No. 12442
 11-17-2014

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		81	114

② PLAN STA. 5857+00 TO STA. 5888+00

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. RUMBLE STRIPS SHALL BE CONSTRUCTED ON I-30 WB SHOULDERS PER THE DETAILS PROVIDED ON THE SPECIAL DETAIL SHEETS.



I-30

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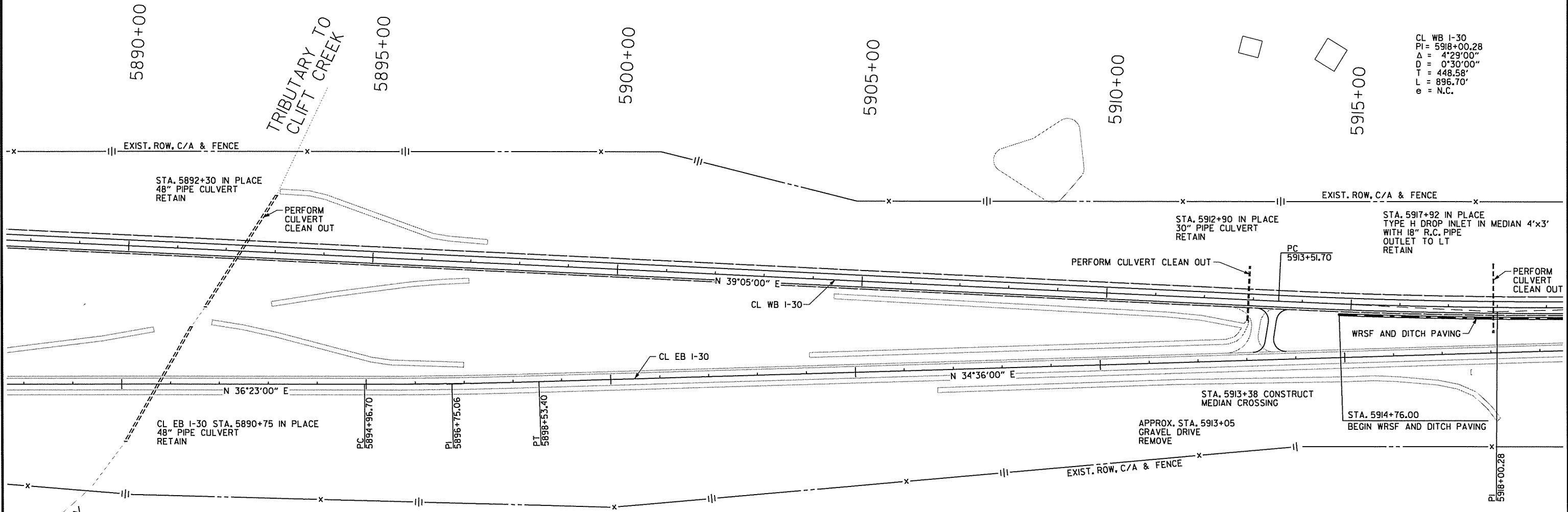
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STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHRAR ALAD
 No. 12404
 11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BBO602		82	114

② PLAN STA. 5888+00 TO STA. 5919+00

- NOTES: 1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. RUMBLE STRIPS SHALL BE CONSTRUCTED ON I-30 WB SHOULDERS PER THE DETAILS PROVIDED ON THE SPECIAL DETAIL SHEETS.



CL WB I-30
 PI = 5918+00.28
 Δ = 4°29'00"
 D = 0°30'00"
 T = 448.58'
 L = 896.70'
 e = N.C.

CL EB I-30
 PI = 5896+75.06
 Δ = 1°47'00"
 D = 0°30'00"
 T = 178.36'
 L = 356.70'

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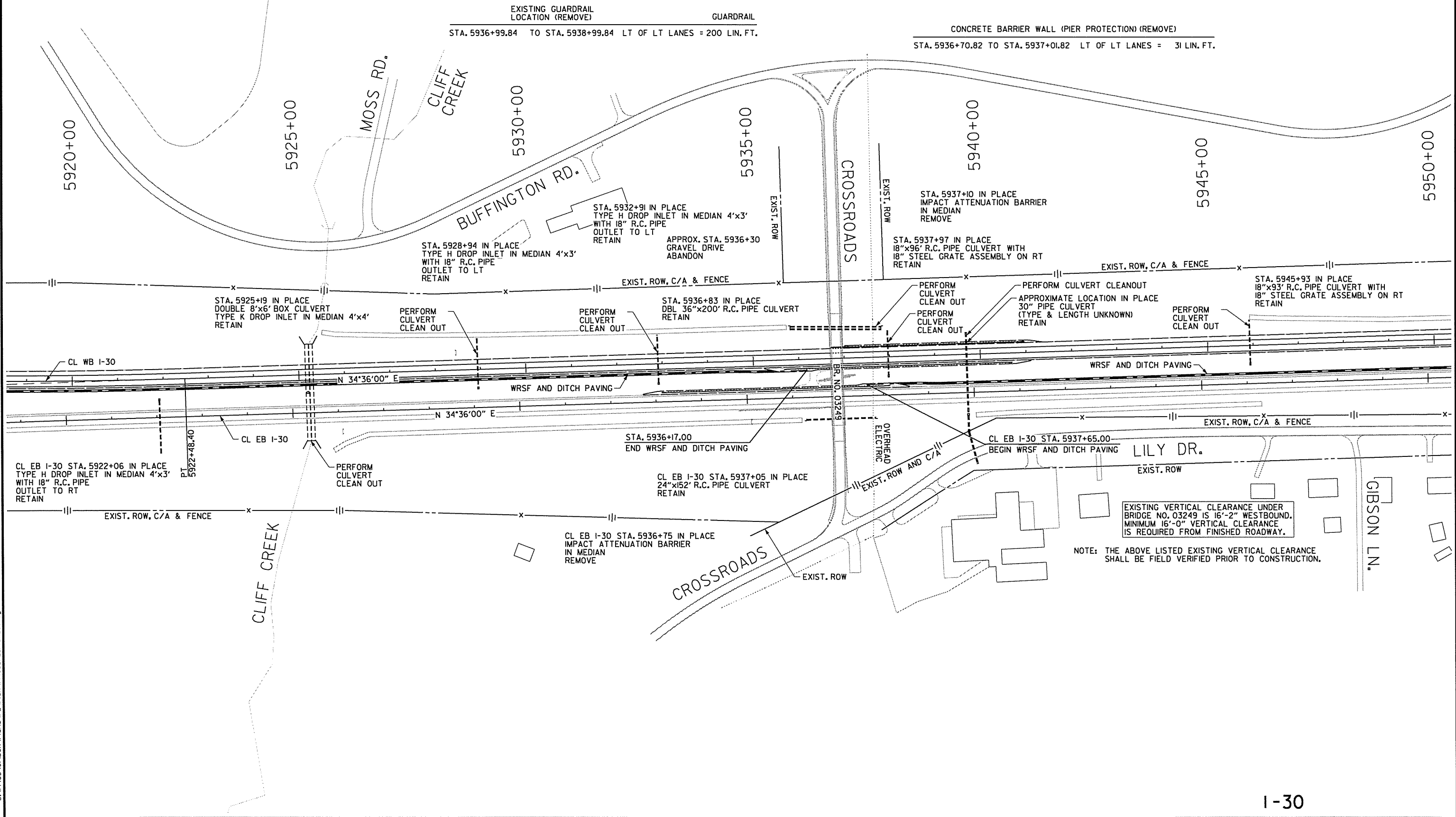
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		83	114
				PLAN STA. 5919+00 TO STA. 5950+00				

- NOTES:
1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
 2. RUMBLE STRIPS SHALL BE CONSTRUCTED ON I-30 WB SHOULDERS PER THE DETAILS PROVIDED ON THE SPECIAL DETAIL SHEETS.

PROPOSED GUARDRAIL LOCATION	GUARDRAIL TERMINAL (TYPE A)	GUARDRAIL TERMINAL (TYPE 2)	THREE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 1)
CL EB I-30 STA. 5933+14.00 TO STA. 5938+14.00	LT OF RT LANES = 450 LIN. FT.	1 EACH	1 EACH	1 EACH
STA. 5935+67.00 TO STA. 5940+67.00	RT OF LT LANES = 450 LIN. FT.	1 EACH	1 EACH	1 EACH
STA. 5937+02.00 TO STA. 5940+95.75	LT OF LT LANES = 325 LIN. FT.	1 EACH	1 EACH	1 EACH

PROPOSED CONCRETE BARRIER WALL (PIER PROTECTION) (TYPE A)
 STA. 5936+71.00 TO STA. 5937+02.00 LT OF LT LANES = 31 LIN. FT.

CONCRETE BARRIER WALL (PIER PROTECTION) (REMOVE)
 STA. 5936+70.82 TO STA. 5937+01.82 LT OF LT LANES = 31 LIN. FT.



EXISTING VERTICAL CLEARANCE UNDER BRIDGE NO. 03249 IS 16'-2" WESTBOUND. MINIMUM 16'-0" VERTICAL CLEARANCE IS REQUIRED FROM FINISHED ROADWAY.

NOTE: THE ABOVE LISTED EXISTING VERTICAL CLEARANCE SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION.

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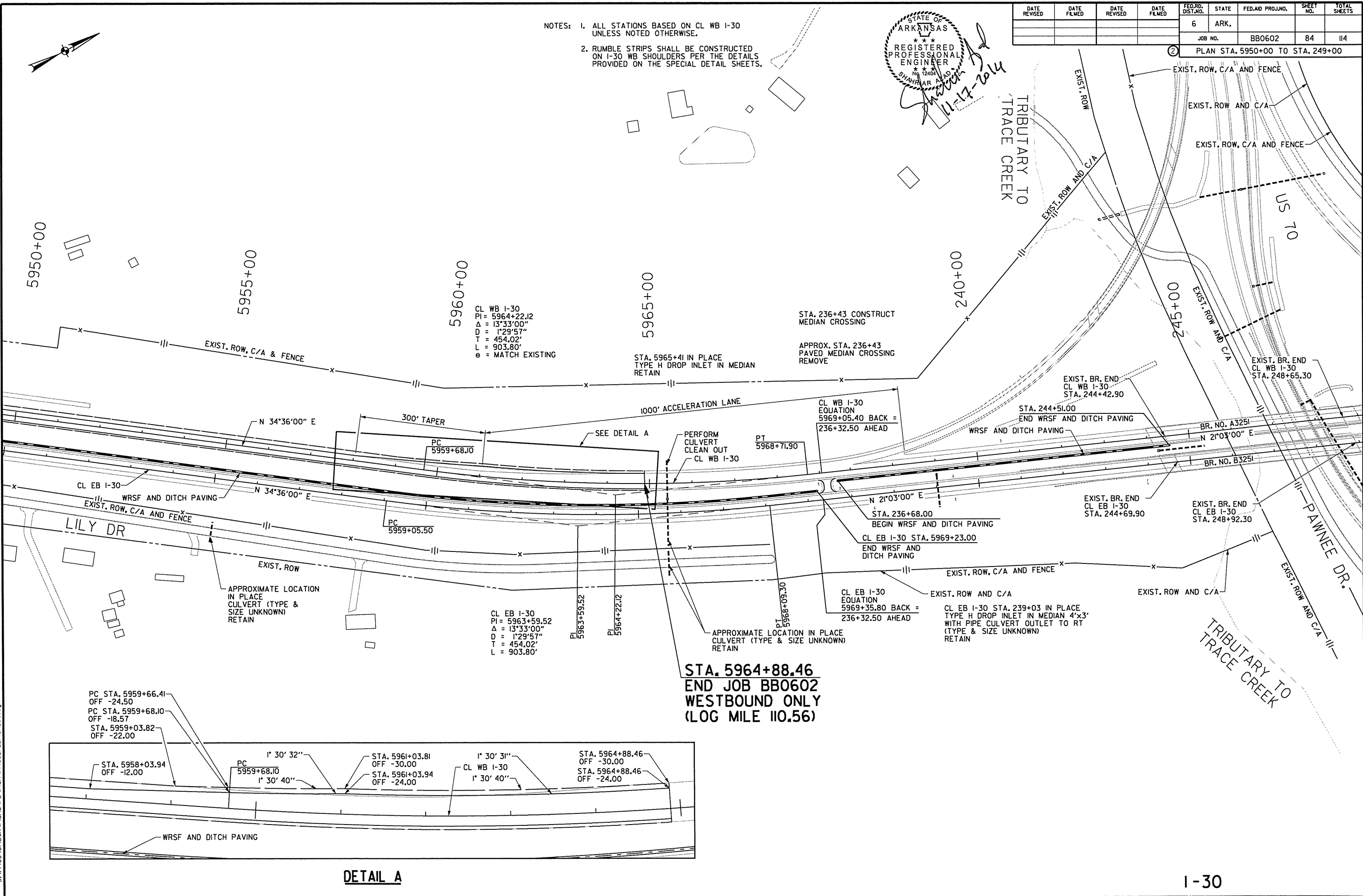
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- NOTES: 1. ALL STATIONS BASED ON CL WB I-30 UNLESS NOTED OTHERWISE.
2. RUMBLE STRIPS SHALL BE CONSTRUCTED ON I-30 WB SHOULDERS PER THE DETAILS PROVIDED ON THE SPECIAL DETAIL SHEETS.

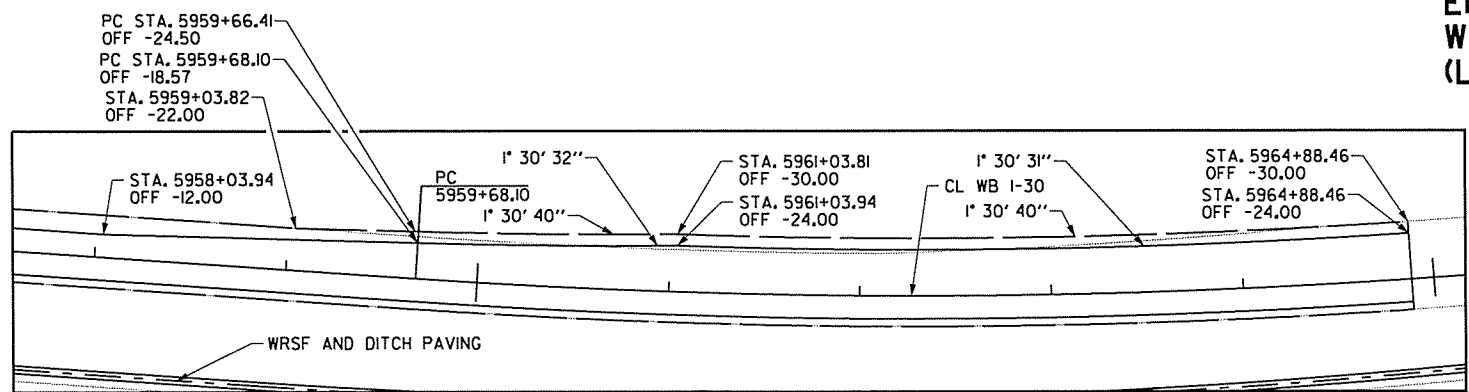
STATE OF ARKANSAS
 REGISTERED PROFESSIONAL ENGINEER
 SHAHAR AD
 No. 12404
 11-17-2014

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0602		84	114
				PLAN STA. 5950+00 TO STA. 249+00				



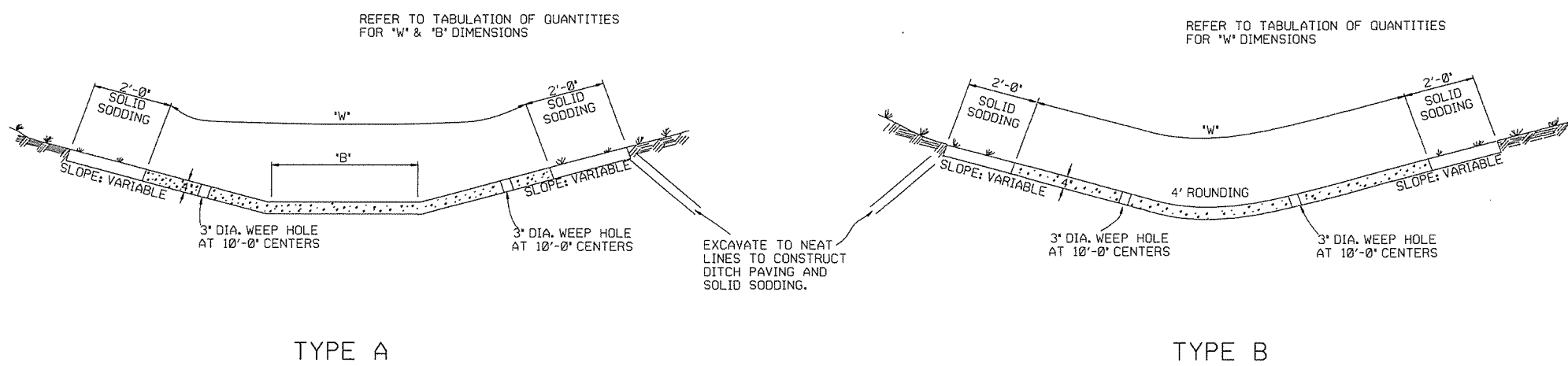
CL WB I-30
 PI = 5964+22.12
 Δ = 13°33'00"
 D = 1'29'57"
 T = 454.02'
 L = 903.80'
 e = MATCH EXISTING

CL EB I-30
 PI = 5963+59.52
 Δ = 13°33'00"
 D = 1'29'57"
 T = 454.02'
 L = 903.80'



DETAIL A

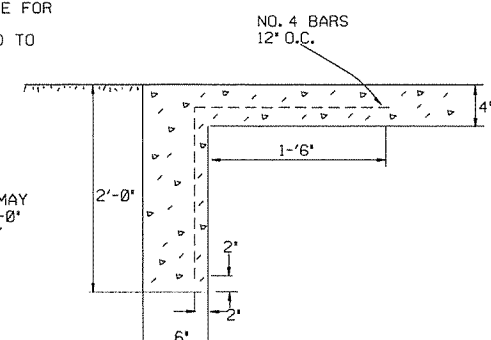
I-30



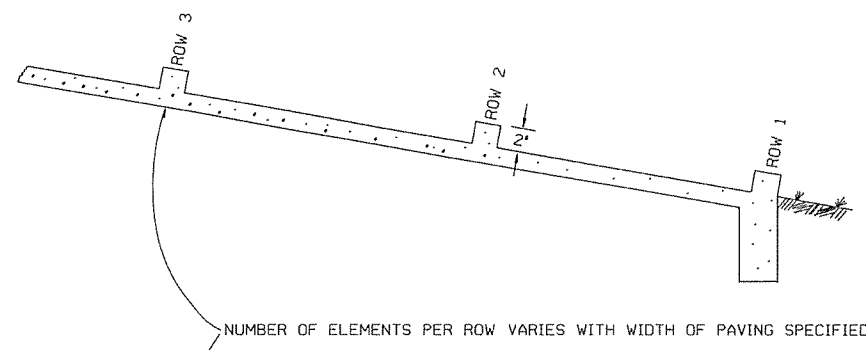
TYPE A

TYPE B

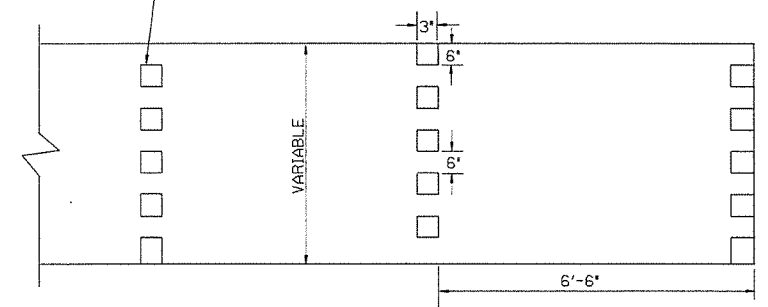
THE STEEL AND ADDITIONAL CONCRETE FOR THE WALLS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR 'CONCRETE DITCH PAVING.'



TOE WALL DETAIL FOR CONCRETE DITCH PAVING



ENERGY DISSIPATORS TO BE USED FOR THE ENTIRE LENGTH OF DITCH WHEN SLOPE OF DITCH PAVING EXCEEDS 7%. THE DISSIPATORS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR CONCRETE DITCH PAVING.



ENERGY DISSIPATORS (NO SCALE)

GENERAL NOTES:

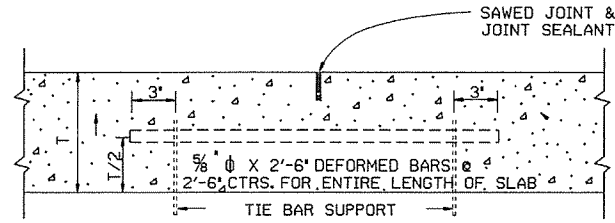
- THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.
- TOE WALLS TO BE CONSTRUCTED FULL WIDTH AT EACH END OF DITCH PAVING, AND POURED MONOLITHICALLY.
- SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.
- 1" WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.

11-17-10	ADDED GENERAL NOTE	
6-2-94	ADDED GENERAL NOTE ABOUT SOLID SODDING	
11-30-8	ELIMINATED MIN. ROWS OF ELEMENTS	111-30-89
7-15-88	REVISED DISSIPATOR NOTE	653-7-15-88
4-3-87	REVISED ENERGY DISSIPATOR	671-4-3-87
1-9-87	MODIFIED NOTE ON ENERGY DISS.	532-1-9-87
11-3-86	ADDED NOTE TO ENERGY DISS.	599-12-1-86
11-1-84	ENERGY DISSIPATOR DETAILS ADDED	508-11-1-84
11-1-84	EXCAVATION DETAILS ADDED	
10-2-72	TYPED A & B REVISED AND REDRAWN	508-10-2-72
DATE	REVISION	DATE FILM'D

ARKANSAS STATE HIGHWAY COMMISSION

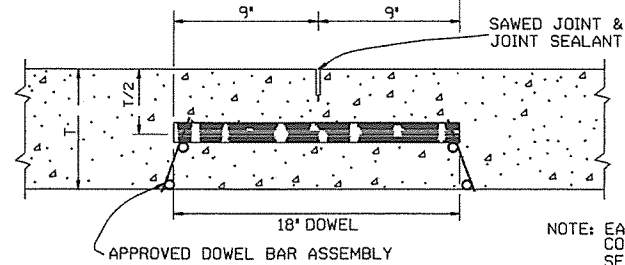
CONCRETE DITCH PAVING

STANDARD DRAWING CDP-1



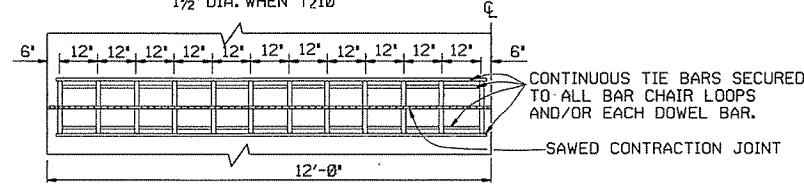
LONGITUDINAL JOINT

NOTE: THE TIE BAR SUPPORT SHOWN ABOVE MAY BE ELIMINATED IF OTHER APPROVED METHODS FOR PLACING AND SUPPORTING THE TIE BARS ARE PROVIDED.
TIE BARS SHALL BE 15' FROM TRANSVERSE JOINTS.



NOTE: EACH DOWEL TO BE COATED ACCORDING TO SECTION 502 OF THE STANDARD SPECIFICATIONS.

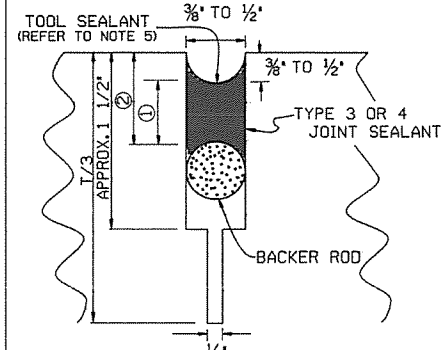
ROUND STEEL BAR DOWEL
1 1/4" DIA. WHEN T < 10'
1 1/2" DIA. WHEN T ≥ 10'



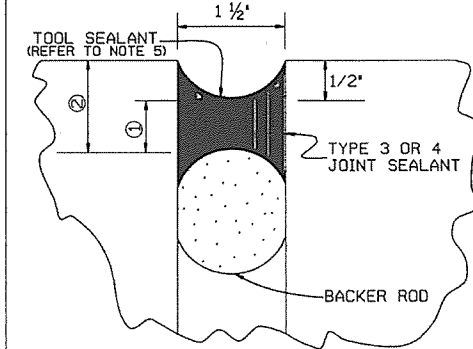
ONE-HALF 24' PAVEMENT
12 DOWELS
PLAN

NOTE: FOR 20' PAVEMENT USE 20 DOWELS @ 12' CTRS. WITH 6' SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR 15' PAVEMENT USE 15 DOWELS @ 12' CTRS. WITH 6' SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR 26' PAVEMENT USE 26 DOWELS @ 12' CTRS. WITH 6' SPACING FROM C.L. AND EDGE OF SLAB TO FIRST BAR. FOR PAVEMENT WIDTHS OTHER THAN THOSE SHOWN ABOVE, USE DOWELS AT 12' CTRS. WITH 6' MAX. SPACING FROM C.L. TO FIRST BAR. DISTANCE FROM EDGE OF SLAB TO FIRST BAR SHALL BE ADJUSTED TO MAINTAIN 12' DOWEL BAR SPACING

CONTRACTION JOINT DETAILS



DETAIL OF SAWED CONTRACTION JOINT



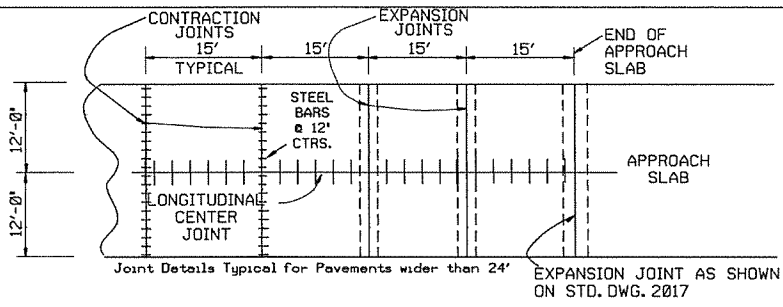
DETAIL OF EXPANSION JOINT

JOINT CONFIGURATION FOR TYPE 3 OR 4 JOINT SEALANT

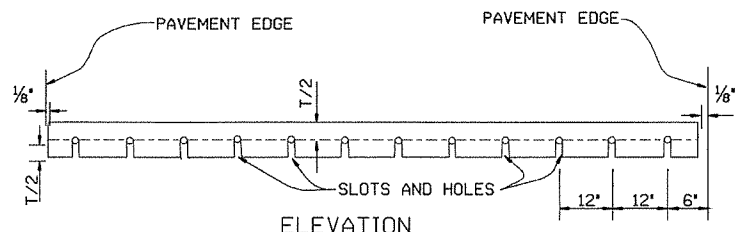
JOINT WIDTH	SEALANT THICKNESS ①	BACKER ROD DIAMETER	BACKER ROD PLACEMENT DEPTH ②
INCHES			
1/4	1/4	3/8	1/2
3/8	1/4	1/2	1/2
1/2	1/4	5/8	1/2
5/8	3/8	3/4	1/2
3/4	3/8	7/8	1/2
1 1/2	3/4	2	1 1/4

JOINT CONFIGURATION FOR TYPE 5 JOINT SEALANT

JOINT WIDTH	SEALANT THICKNESS ①	BACKER ROD DIAMETER	BACKER ROD PLACEMENT DEPTH ②
INCHES			
1/4	1/2	3/8	3/4
3/8	3/4	1/2	1

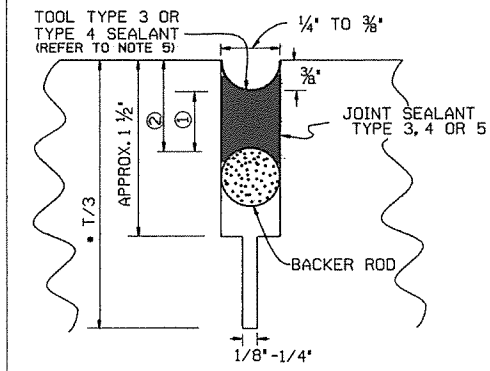


PLAN SHOWING EXPANSION JOINTS AT BRIDGE APPROACH SLABS



ELEVATION

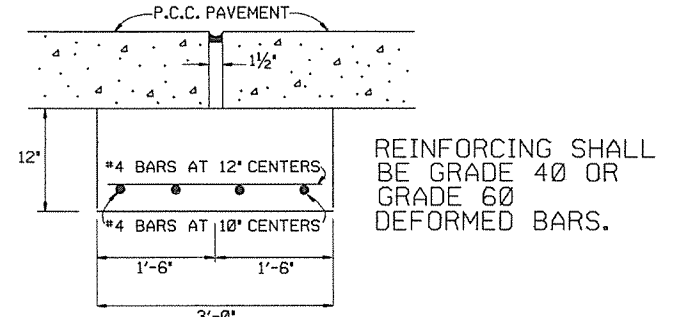
NOTE: ALL DOWEL BARS SHALL CONFORM TO THE DETAILS FOR CONTRACTION JOINTS.



DETAIL OF SAWED LONGITUDINAL JOINT AND LONGITUDINAL CONSTRUCTION JOINT

*NOTE: T/3 SAW CUT NOT REQUIRED FOR LONGITUDINAL CONSTRUCTION JOINT.

DATE	REVISION	DATE FILMED
5-25-06	ADDED GENERAL NOTE 7	
10-9-03	REMOVED TIE BAR COATING & REVISED GENERAL NOTES	
11-16-01	ADDED TOOL SEALANT AND NOTE 5; REVISED NOTE 3	
4-26-96	REVISED CONTRACTION JOINT NOTE	
11-3-94	ADDED NOTE RE: REINF. BARS	
4-1-93	REVISED DOWEL BARS & GEN. NOTES	4-1-93
10-1-92	REVISED DOWEL SPACING	10-1-92
8-15-91	ADDED SPAC FOR CONTR JTS & DEL KEYWAY	
05-24-90	REVISED TIE BAR, DOWEL & JOINT SIZE	
01-25-90	ADDED EXPANSION JOINT	01-25-90
11-30-89	CHANGED T/4+1 TO T/3+1	11-30-89
03-23-89	ALTERED SAWED JOINT & ADDED NOTE 5	03-23-89
07-15-88	REVISED AND REDRAWN	03-07-15-88



DETAIL OF JOINT SUPPORT FOR EXPANSION JOINTS

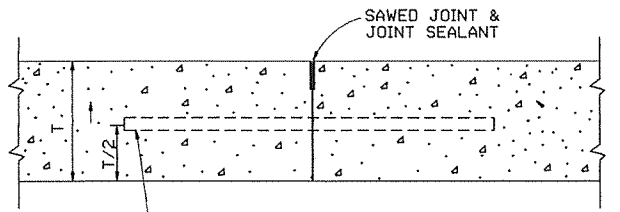
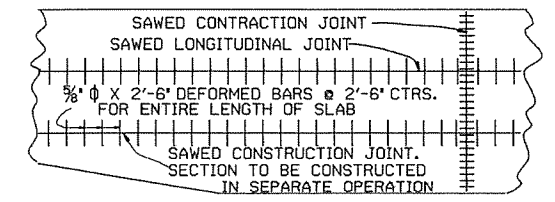
REINFORCING SHALL BE GRADE 40 OR GRADE 60 DEFORMED BARS.

- GENERAL NOTES
- *T' DENOTES THICKNESS OF SLAB.
 - DOWEL BARS SHALL BE PLACED IN ACCORDANCE WITH THE DIMENSIONS SHOWN. A TOLERANCE OF PLUS OR MINUS ONE INCH WILL BE ALLOWED FOR THE VERTICAL AND LATERAL PLACEMENT AND A TOLERANCE OF PLUS OR MINUS 1/4" WILL BE ALLOWED FOR THE TILT AND SKEW. DOWEL BARS SHALL BE FIELD COATED FOR A MINIMUM DISTANCE OF 2' GREATER THAN HALF THE LENGTH OF THE BAR WITH AN APPROVED GREASE AS A BOND BREAKER JUST PRIOR TO PLACEMENT OF CONCRETE.
 - THE EXPANSION JOINT SUPPORT MAY BE CONSTRUCTED WITH CLASS 'A', 'S' OR PAVING CONCRETE. PAYMENT FOR THE JOINT SUPPORT SHALL BE FOR THE CONTRACT UNIT PRICE BID FOR THE CLASS OF CONCRETE SPECIFIED IN THE PLANS. PAYMENT FOR ALL OTHER WORK AND MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE JOINT SUPPORT SHALL BE INCLUDED IN THE PRICE BID FOR THE ABOVE ITEMS.
 - CONTRACTION JOINTS SHALL BE CONSTRUCTED ON 15' CENTERS.
 - TOOLING NOT REQUIRED FOR SELF-LEVELING SILICONE.
 - UNLESS OTHERWISE SPECIFIED IN THE PLANS, CONCRETE SHOULDERS SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN HEREON. CONTRACTION JOINTS SHALL MATCH CONTRACTION JOINTS IN THE LANES.
 - TIE WIRES IN DOWEL BAR ASSEMBLIES SHALL NOT BE CUT PRIOR TO PLACEMENT OF PAVING CONCRETE.

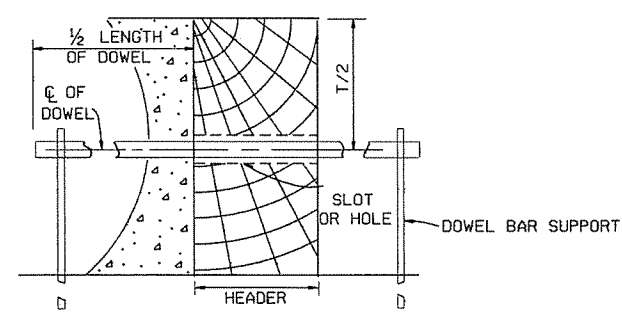
ARKANSAS STATE HIGHWAY COMMISSION

TRANSVERSE & LONGITUDINAL JOINTS FOR CONCRETE PAVEMENT (NON-REINFORCED)

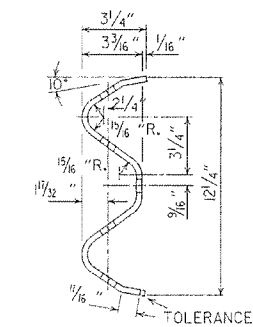
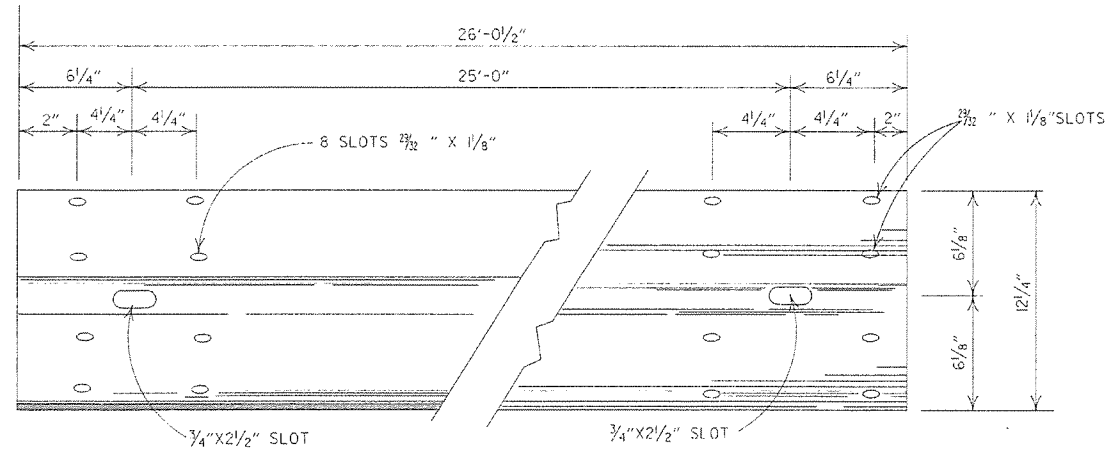
STANDARD DRAWING CPTJ - 6A



NOTE: TIE BARS SHALL BE 15' FROM TRANSVERSE JOINTS. LONGITUDINAL CONSTRUCTION JOINT

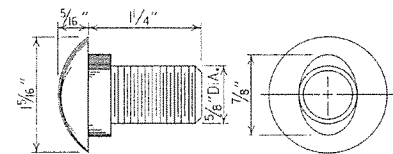


SECTION
TRANSVERSE CONSTRUCTION JOINT

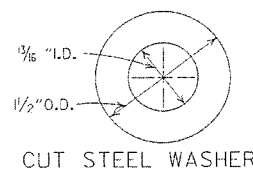


DETAILS OF W-BEAM GUARD RAIL

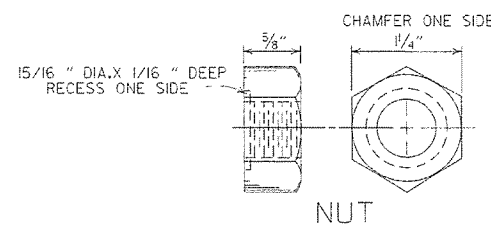
RAIL SECTION OF CLOSELY SIMILAR DIMENSIONS AND COMPARABLE STRENGTH MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.



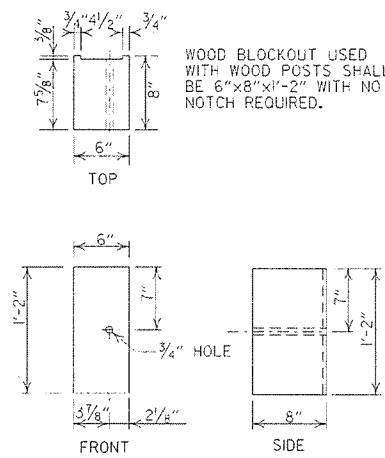
SPLICE BOLT
POST BOLT - SAME EXCEPT LENGTH



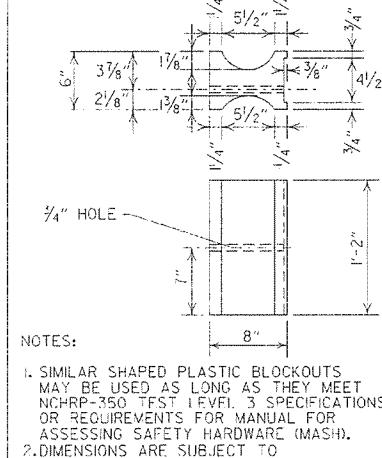
CUT STEEL WASHER



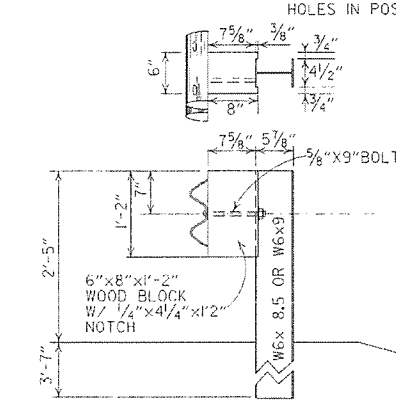
NUT



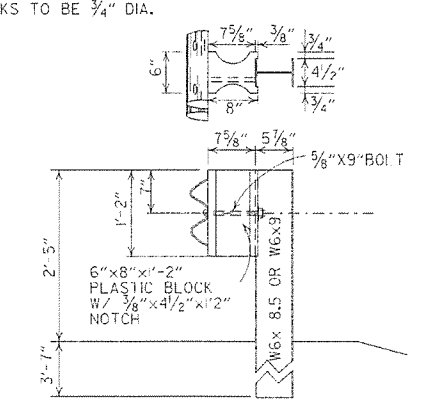
WOOD BLOCKOUT (W-BEAM)



PLASTIC BLOCKOUT (W-BEAM)

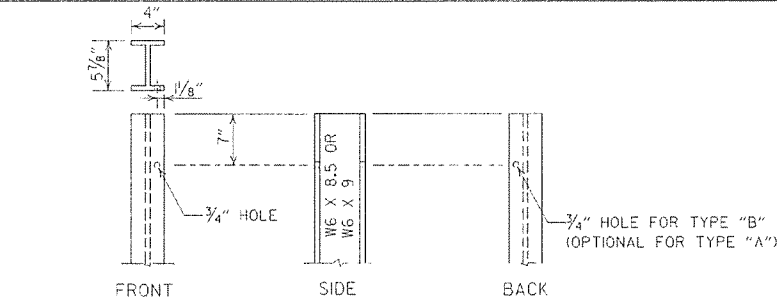


WOOD BLOCKOUT CONNECTIONS

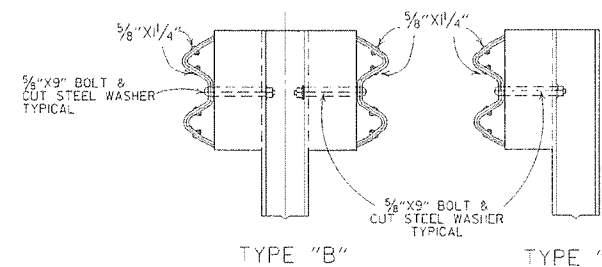


PLASTIC BLOCKOUT CONNECTIONS

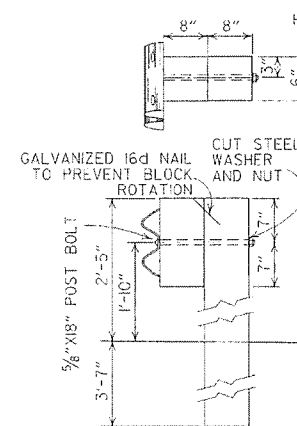
DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



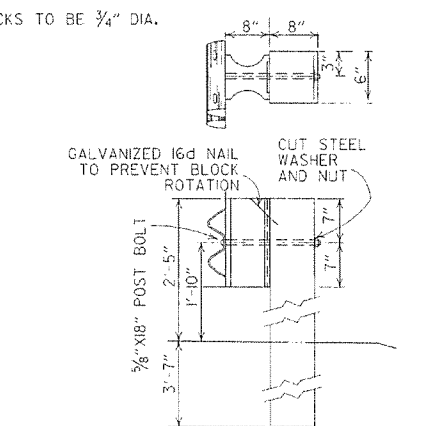
STEEL POST



DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



WOOD BLOCKOUT CONNECTIONS



PLASTIC BLOCKOUT CONNECTIONS

DETAILS OF WOOD LINE POST CONNECTIONS (W-BEAM)

-GENERAL NOTES-

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.

WHERE W-BEAM GUARD RAIL CONTINUES, THE INTERMEDIATE SECTIONS SHALL HAVE A POST SPACING OF 6'-3" UNLESS OTHERWISE NOTED.

W-BEAM GUARD RAIL REPRESENTING INTERMEDIATE SECTIONS WILL BE MEASURED ALONG THE ROADWAY FACE FROM CENTERLINE OF POST TO CENTERLINE OF POST.

USE W-BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. FOR EXTENSIONS OR MODIFICATION OF EXISTING GUARD RAIL, W-BEAM GUARD RAIL COMPONENTS OF THE SAME TYPE AS THOSE EXISTING SHALL BE USED.

ANY BACKFILLING UNDER OR AROUND POST SHALL BE DAMP SAND THOROUGHLY TAMPED IN PLACE.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1 1350 f SOUTHERN PINE.

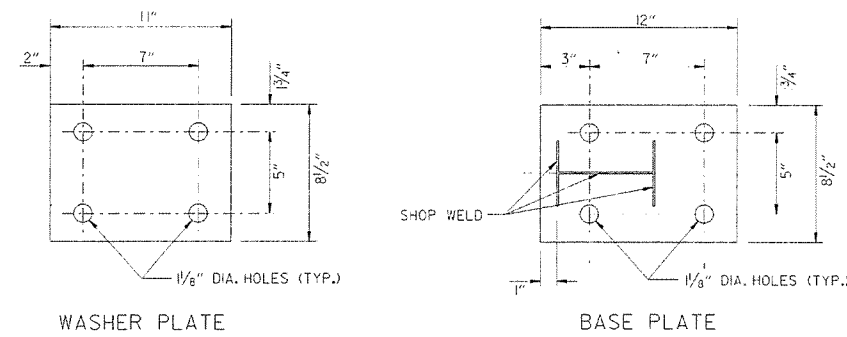
CONTRACTOR SHALL HAVE THE OPTION OF USING WOOD BLOCKOUTS FOR W-BEAM GUARD RAIL OR PLASTIC BLOCKOUTS, AS LONG AS BLOCKOUT USED MEETS NCHRP-350 TEST LEVEL 3 SPECIFICATIONS OR REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) FOR W-BEAM GUARD RAIL.

DATE	REVISION	DATE FILM
7-4-10	RAISED HEIGHT OF GUARD RAIL 1"	
10-15-09	ADDED REFERENCE TO MASH	
4-10-03	REVISED GENERAL NOTES	
8-22-02	REVISED DIMENSION ON WOOD & PLASTIC BLOCKOUT CONNECTIONS & ON STEEL POST	
11-16-01	REVISED WOOD BLOCKOUT & DETAILS OF WOOD LINE POST CONNECTIONS	
3-30-00	REMOVED GUARD RAIL AT BRIDGE ENDS	
1-2-00	ADDED PLASTIC BLOCKOUT	
8-12-98	REV. BLOCKOUTS TO WOOD, DELETED CONC. POST & REV. GENERAL NOTE, DELETED DET. OF GUARD RAIL REPLACE. BEHIND CURB & DET. OF POST PLACE IN SOLID ROCK. 3. ADDED DETAILS OF STEEL LINE POST CONN. REMOVED BACK-UP PLATE, REVISED HOLES IN STEEL POLES	
4-3-97	REMOVED "LAP IN DIRECTION OF TRAFFIC" NOTE & PLACED ARROWS ON WASHERS	
10-18-96	REVISED WOOD POST NOTE	
6-2-94	ADDED ALT. STEEL POST SIZE	
8-5-93	REVISED STEEL POST SIZE	8-5-93
10-1-92	REDRAWN & REVISED	10-1-92
8-15-91	REVISED WASHER NOTE	8-15-91
8-2-90	REV. GEN. NOTE & DETPH OF ANC. POST IN ROCK	8-2-90
7-15-88	REVISED SECTION 3 & GENERAL NOTES	
3-4-88	REV. ANCHOR POST, LEVEL NOTES & POST IN ROCK	780-3-4-88
10-30-87	REVISED WOOD LINE POST DETAIL	546-10-30-87
10-9-87	REDRAWN & REVISED	802-10-9-87

ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

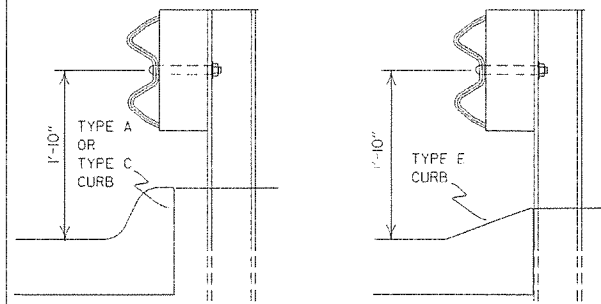
STANDARD DRAWING GR-8



WASHER PLATE

BASE PLATE

Note: Bolts, nuts, washers and plates shall be galvanized in accordance with Section 807 of the Standard Specifications.

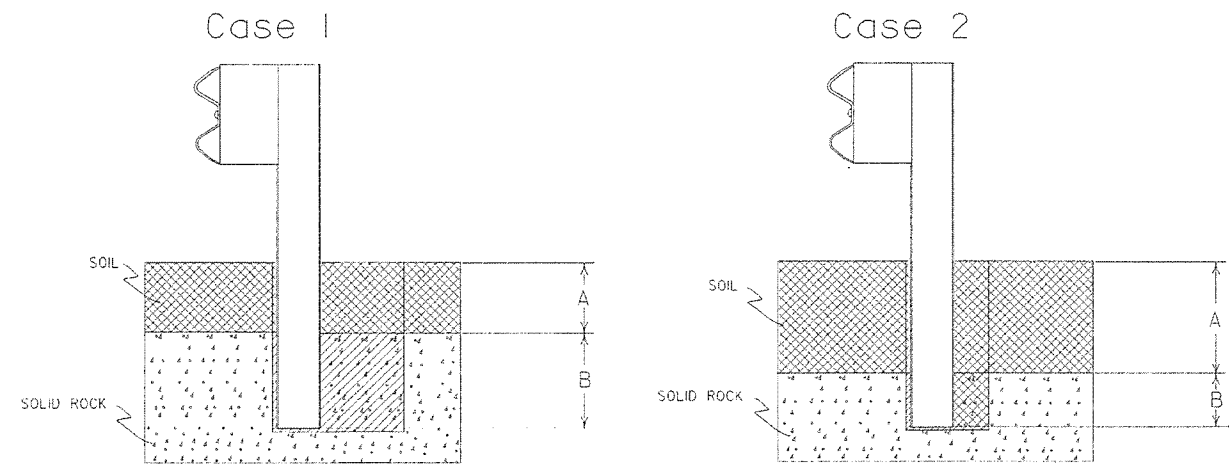


FOR DESIGN SPEEDS OF 50 MPH OR LESS
ALIGN FACE OF GUARD RAIL WITH FACE OF CURB.

FOR DESIGN SPEEDS OF 55 MPH OR MORE
PLACE GUARD RAIL POSTS AGAINST BACK OF CURB.

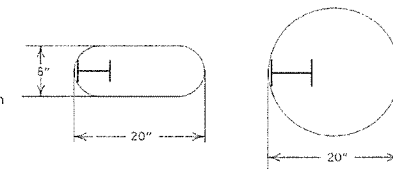
DETAIL OF GUARD RAIL PLACEMENT BEHIND CURB (W-BEAM)

FOR DESIGN SPEEDS OF 50 MPH OR LESS ALL CURB FACES, AS SHOWN ON STD. DRWG. CG-1, MAY BE USED. FOR DESIGN SPEEDS OF 55 MPH OR MORE TYPE "E" CURB FACE SHALL BE USED.



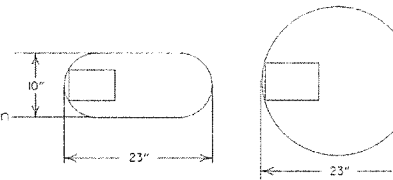
Plan View Steel Posts

Either hole configuration acceptable



Plan View Wood Posts

Either hole configuration acceptable



Notes: For overlying soil depths (A) ranging from 0 to 18", the depth of required drilling (B) is equal to 24".

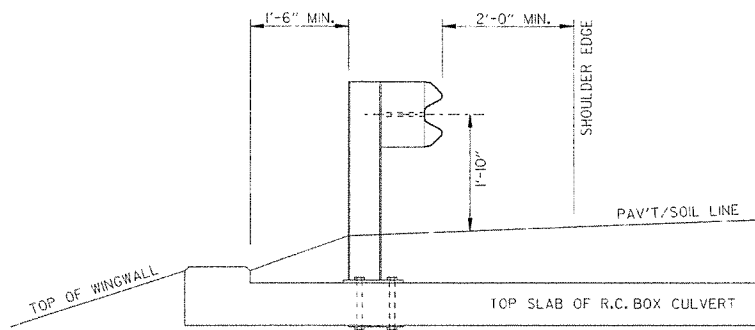
Zone A: Backfill according to Section 617.03(a).

Zone B: Backfill hole in 6" lifts with material meeting the requirements of Section 802.02(c) - Alternate gradation. Compact to 95% maximum dry density per ASTM D-698.

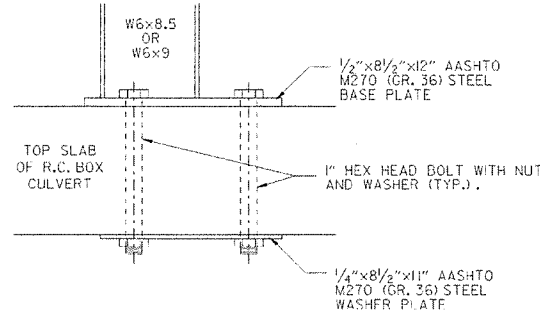
Notes: For overlying soil depths (A) ranging from 18" to 44", the depth of required drilling (B) is equal to either 12" or 44" minus the depth of soil whichever is less.

Zone A & B: Backfill according to Section 617.03(a).

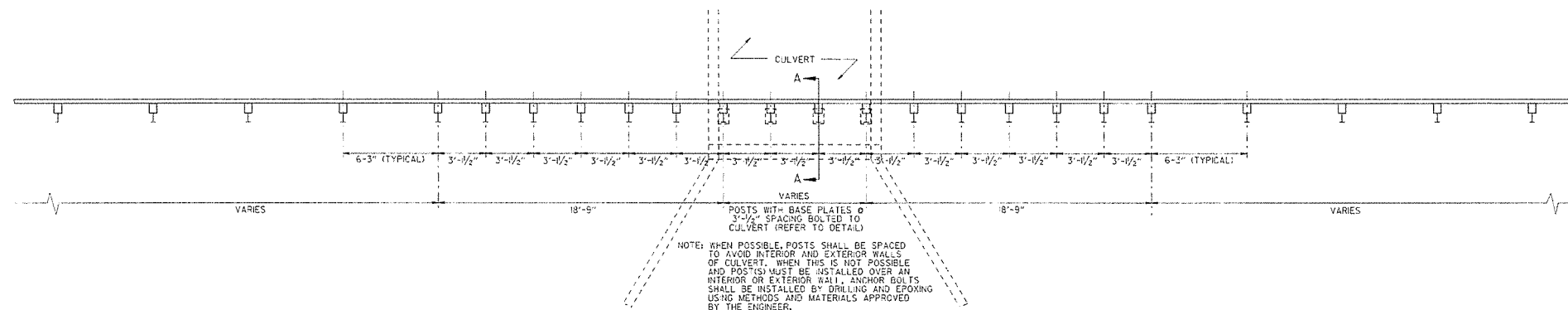
DETAIL OF POST PLACEMENT IN SOLID ROCK (W-BEAM)



SECTION A-A



DETAIL OF CONNECTION



PLAN LAYOUT OF TYPE A GUARD RAIL AT LOW-FILL CULVERTS

NOTE: THIS DETAIL IS TO BE USED ONLY WHEN THE COVER OVER THE CULVERT DOES NOT PERMIT FULL EMBEDMENT OF GUARD RAIL POSTS AS SHOWN ON STD. DRWG. GR-8.

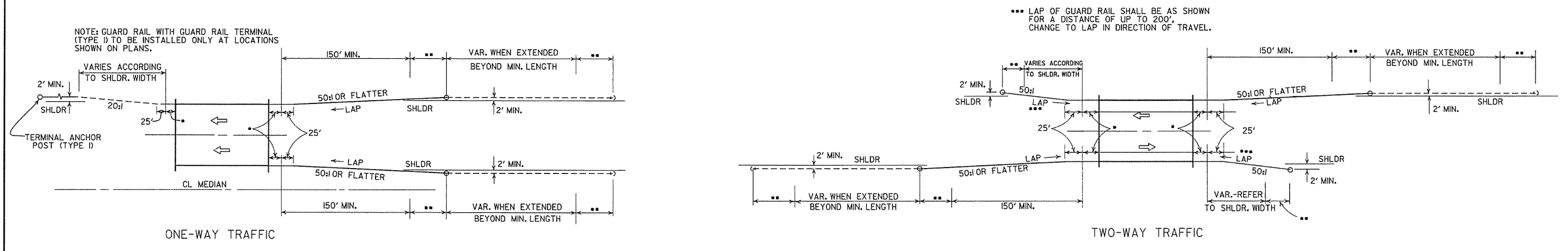
NOTE: WHEN POSSIBLE, POSTS SHALL BE SPACED TO AVOID INTERIOR AND EXTERIOR WALLS OF CULVERT. WHEN THIS IS NOT POSSIBLE AND POST(S) MUST BE INSTALLED OVER AN INTERIOR OR EXTERIOR WALL, ANCHOR BOLTS SHALL BE INSTALLED BY DRILLING AND EPOXYING USING METHODS AND MATERIALS APPROVED BY THE ENGINEER.

7-14-10	RAISED HEIGHT OF GUARD RAIL 1"	
4-12-07	REVISED DETAIL OF GUARD RAIL PLACEMENT BEHIND CURB	
11-10-05	ADDED GUARD RAIL PLACEMENT BEHIND CURB; REVISED DETAIL OF CONNECTION	
11-18-04	REVISED POST PLACEMENT IN ROCK & CULVERT CONNECTION DETAILS, ADDED DETAIL FOR GUARD RAIL PLACEMENT AT LOW-FILL CULVERTS	
3-30-00	REMOVED CONCRETE INSERT ANCHOR	
6-12-98	CHANGED STEEL SPACER BLOCK TO WOOD BLOCKOUT, ADD. DET. OF GUARD RAIL CONNECTION TO R.C. BOX CULVT. DELETED DET. OF STEEL LINE POST CONN. & ADDED DET. OF GUARD RAIL PLACE. BEHIND CURB & DET. OF POST PLACE. IN SOLID ROCK	
4-3-96	PLACED ARROWS AT CUT STEEL WASHERS	4-3-96
10-18-96	REV. ASTM REF. TO AASHTO	
11-22-95	ADDED OPTIONAL HOLES	
6-2-94	REVISED ALTERNATE POST SIZE	
8-5-93	REVISED STEEL POST SIZE	
10-1-92	REDRAWN & REVISED	10-1-92
8-2-90	DEL. WASHER ON ANCHOR ASSEMBLY	8-2-90
7-15-88	CONFORMED TO 1988 SPECS	
3-4-88	REVISED ANCHOR NOTE	
10-30-87	REVISED ANCHOR ASSEMBLY	10-30-87
10-30-87	REVISED PLACEMENT BEHIND CURB	547-10-30-87
10-9-87	REDRAWN & REVISED	803-10-9-87
DATE	REVISION	DATE FILM

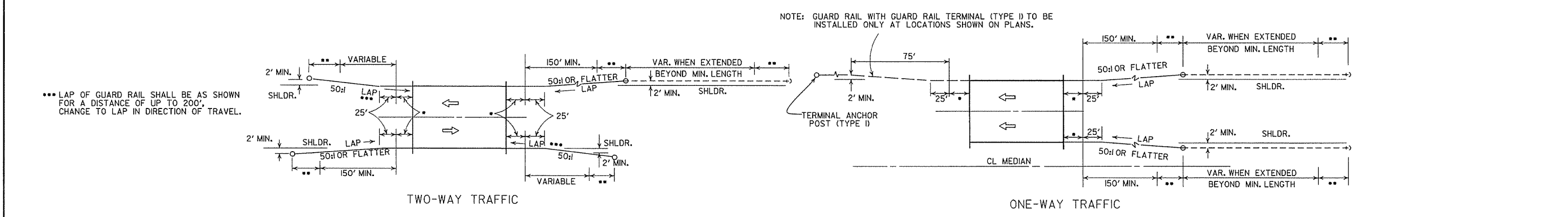
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

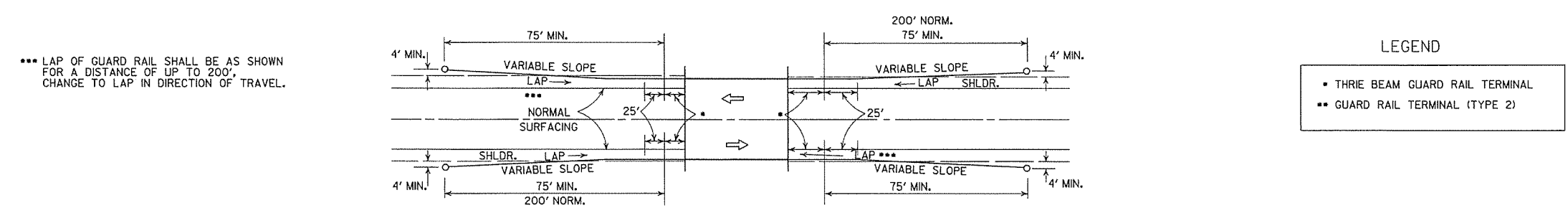
STANDARD DRAWING GR-8A



METHODS OF INSTALLATION OF GUARD RAIL AT LESS THAN FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)

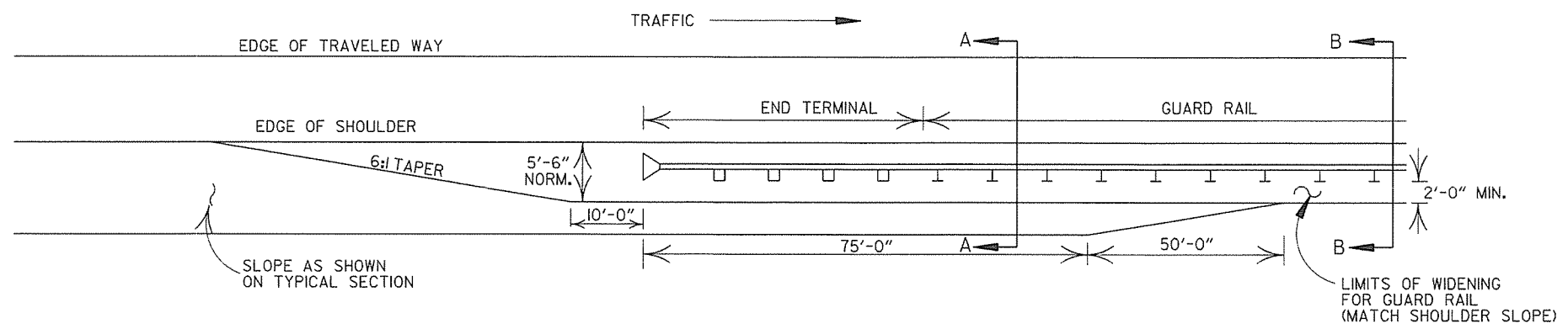


METHOD OF INSTALLATION OF GUARD RAIL AT FULL SHOULDER WIDTH BRIDGES USING GUARD RAIL TERMINAL (TYPE 2)

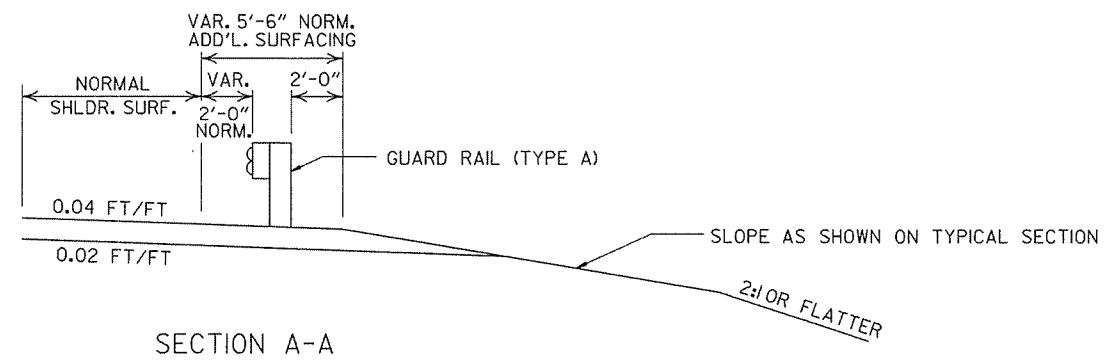


METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERMINAL (TYPE 1) (FULL SHOULDER WIDTH OR LESS BRIDGES)

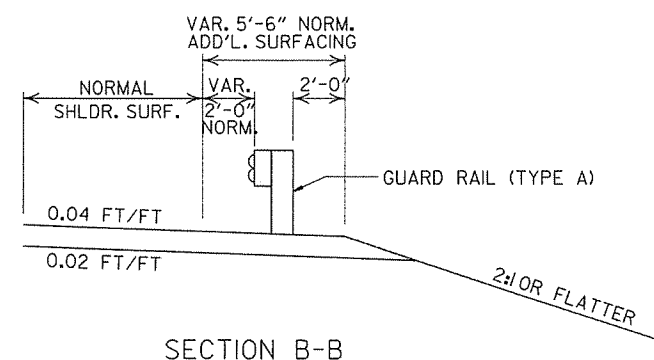
ARKANSAS STATE HIGHWAY COMMISSION		
GUARD RAIL DETAILS		
STANDARD DRAWING GR-9		
4-17-08	REVISED LAYOUTS	
11-10-05	REMOVED GUARD RAIL NOTES AND DETAILS	
11-16-01	DELETED NOTE-METHOD OF INSTALLATION OF GUARD RAIL USING GUARD RAIL TERM. (TY. 1)	
1-12-00	ADDED CONSTRUCTION NOTE	1-12-00
6-26-97	REVISED LAYOUT	
10-1-92	REDRAWN & REVISED	10-1-92
10-9-87	ADDED NOTE	
10-9-87	REDRAWN & REVISED	
DATE	REVISION	DATE FILM



NOTE: NORMAL SECTION TO BE WIDENED APPROX. 5'-6" EACH SIDE TO SUPPORT GUARD RAIL.

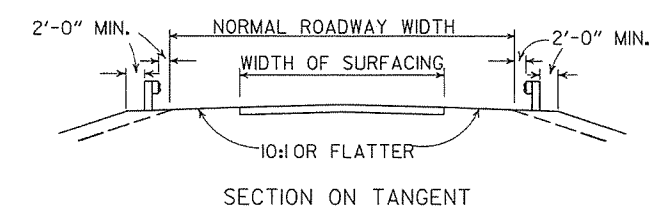


SECTION A-A

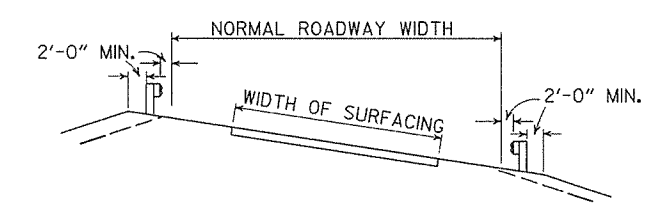


SECTION B-B

DETAILS OF WIDENING FOR GUARD RAIL

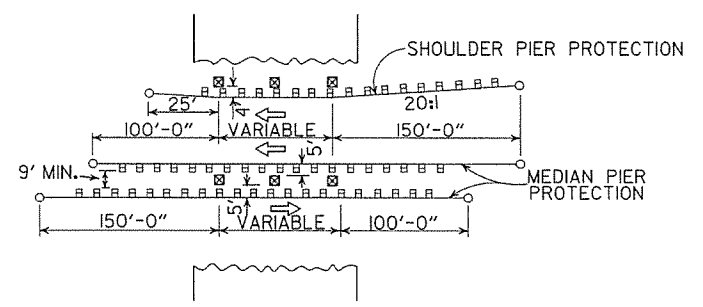


SECTION ON TANGENT



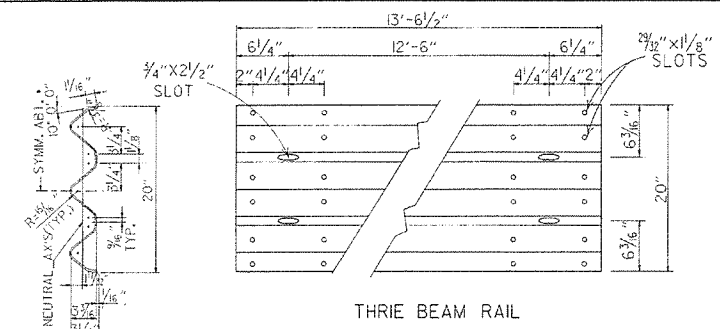
SECTION ON CURVE

DETAILS SHOWING POSITION OF GUARD RAIL ON HIGHWAY



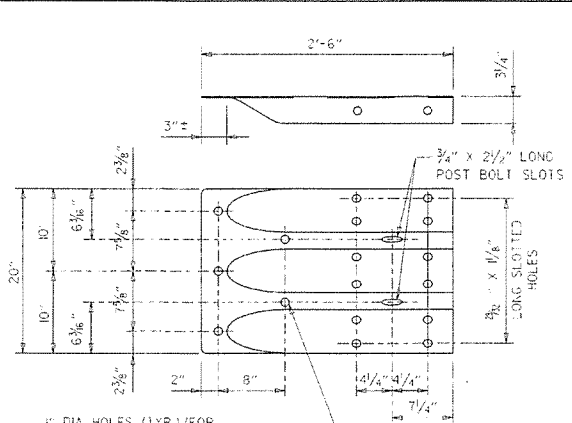
METHOD OF INSTALLATION OF GUARD RAIL AT FIXED OBSTACLE

ARKANSAS STATE HIGHWAY COMMISSION			
GUARD RAIL DETAILS			
STANDARD DRAWING GR-9A			
4-17-08	MINOR REVISION		
11-10-05	DRAWN		
DATE	REVISION	DATE	FILM



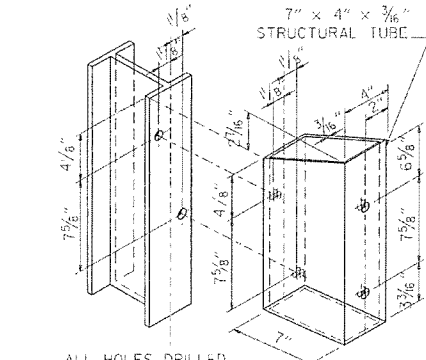
THRIE BEAM RAIL

SECTION THRU THRIE BEAM RAIL



SPECIAL END SHOE

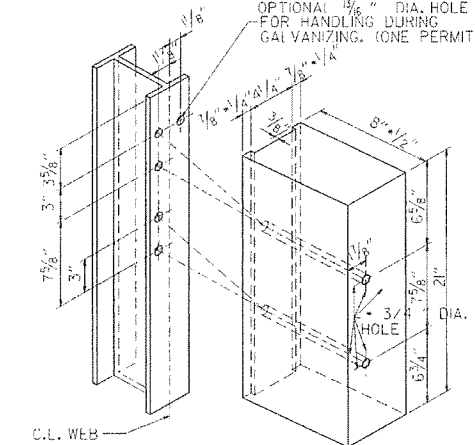
1" DIA. HOLES (TYP.) FOR 7/8" DIA. HIGH STRENGTH BOLTS WITH HEX HEADS, NUTS AND WASHERS



ATTACH BLOCKOUT TO POST USING 5/8" DIA. HEX HEAD BOLTS WITH 1/2" O.D. CUT STEEL WASHERS AND NUT.

ALL HOLES DRILLED OR PUNCHED 1/16" DIA.

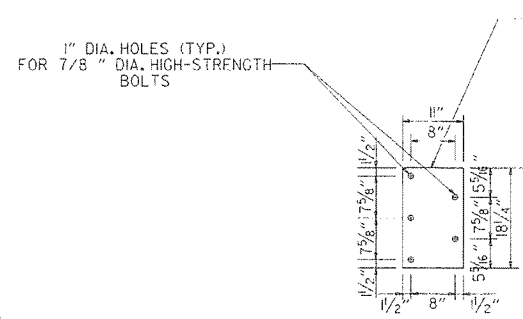
STRUCTURAL STEEL TUBING BLOCKOUT DETAIL



ALL HOLES 1/16" DIAMETER EXCEPT AS NOTED

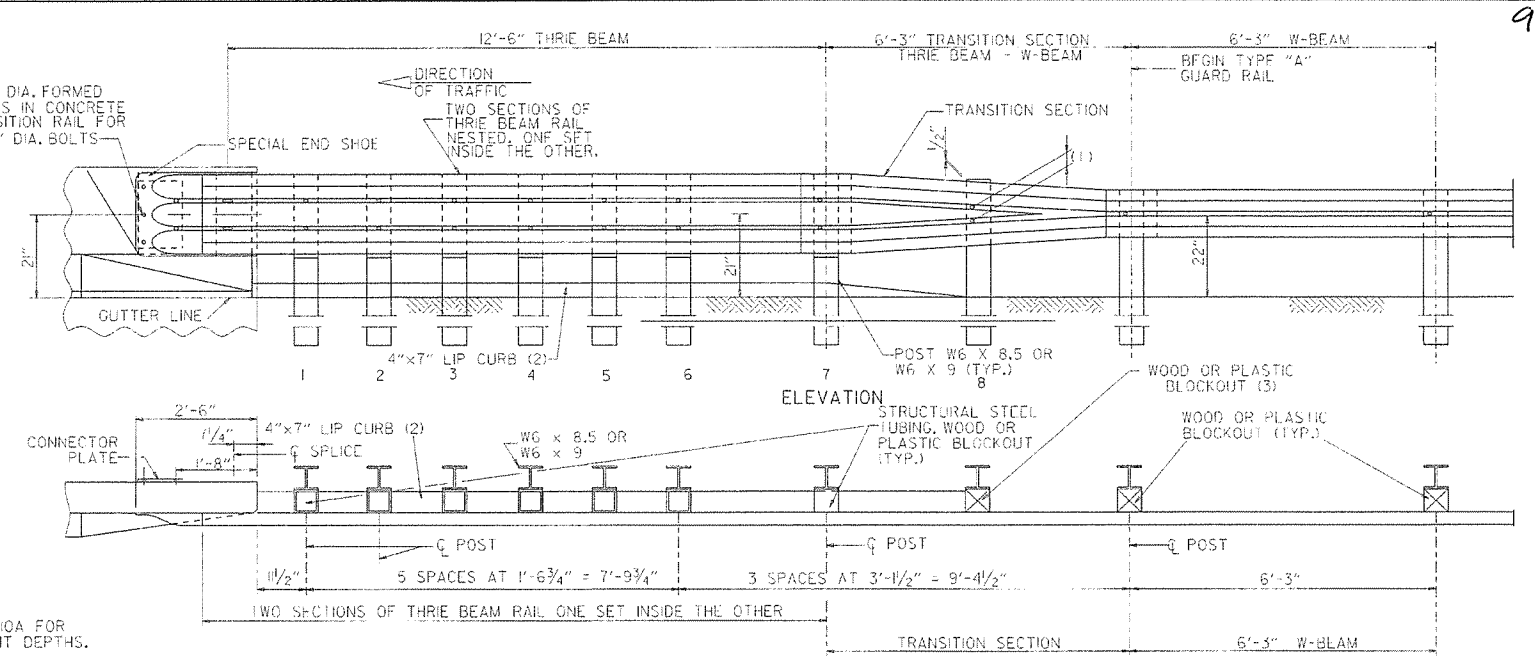
HOLE PUNCHING DETAIL FOR STEEL POST & WOOD OR PLASTIC BLOCKOUTS

NOTE: BLOCKS SHALL BE THE SAME TYPE THROUGHOUT THE PROJECT LIMITS.

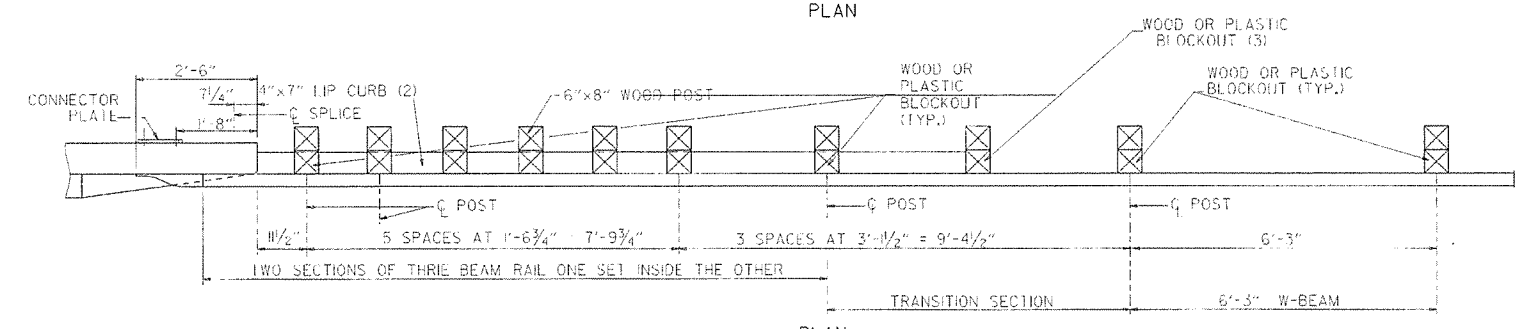


CONNECTOR PLATE

CONNECTOR PLATE SHALL BE AASHTO M270, GR. 36 AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING SHALL CONFORM TO SUBSECTION 807.19 OF THE STANDARD SPECIFICATIONS. CONNECTOR PLATE TO BE BOLTED TO SPECIAL END SHOE USING 1/4" DIA. HIGH STRENGTH BOLTS, WITH THE HEADS PLACED ON THE TRAFFIC FACE. WASHERS SHALL BE USED UNDER THE HEAD AND NUT. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED AND SHALL CONFORM TO SUBSECTION 807.06.



ELEVATION



PLAN

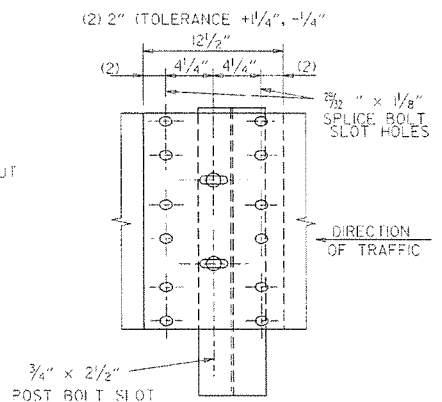
PLAN

- (1) VERIFY BOLT SPACING FROM RAIL TRANSITION PRODUCER.
- (2) REFER TO APPROACH GUTTER DETAILS.
- (3) LENGTH OF BLOCKOUT ON POST 8 TO BE MODIFIED TO FIT RAIL WIDTH.

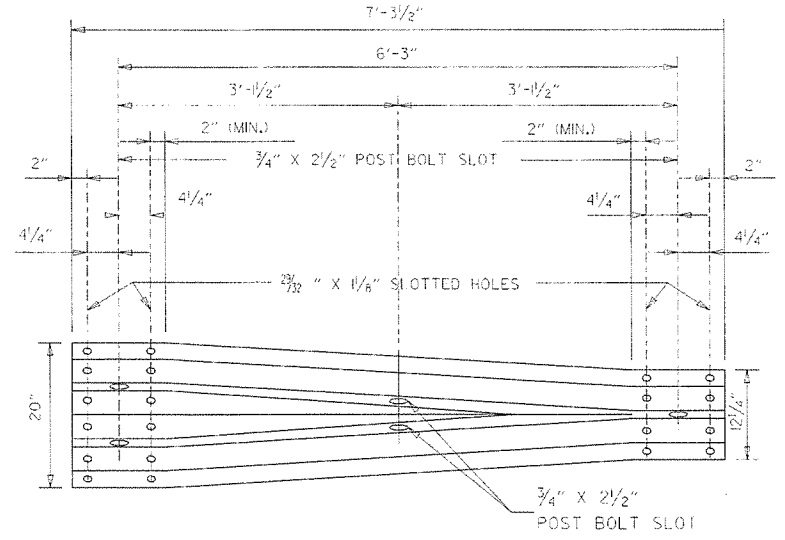
THRIE BEAM GUARD RAIL CONNECTION AT BRIDGE ENDS

GENERAL NOTES:

THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE I. RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION. ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 1/4" BEYOND IT. ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-9 & GR-11. WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7+ (1400 #) OR NO. 1 350 # SOUTHERN PINE. REFER TO STD. DRWG. GR-10A FOR POST DETAILS. USE THRIE BEAM GUARD RAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W BEAM POSTS FOR ENTIRE JOB.



THRIE BEAM RAIL SPLICE AT POST



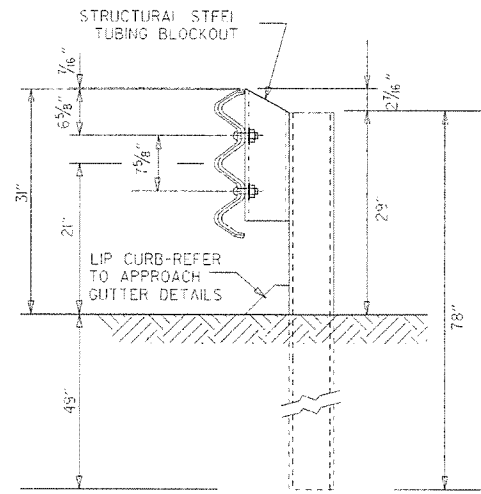
TRANSITION SECTION

DATE	REVISION	DATE	FILM
7-14-10	RAISED HEIGHT OF W-BEAM 1"		
11-29-07	ADDED PLASTIC BLOCKOUTS		
11-10-05	ADDED NOTE FOR ATTACHING STEEL BLOCKOUT		
11-18-04	REVISED GENERAL NOTES		
10-9-03	REVISED GENERAL NOTES		
4-10-03	REVISED GENERAL NOTES		
8-22-02	REVISED NOTE (2)		
6-29-00	MOVED DIMENSION LINES		
5-18-00	ADDED NOTE		
3-30-00	DRAWN & ISSUED		

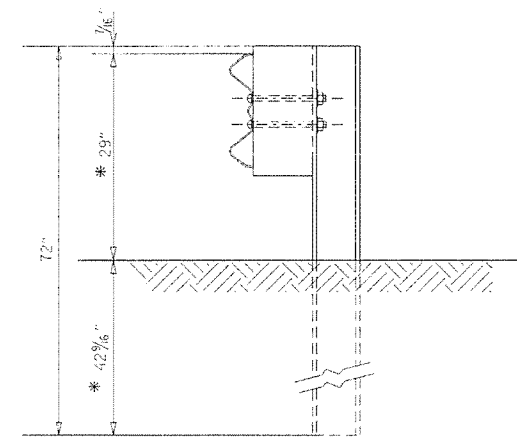
ARKANSAS STATE HIGHWAY COMMISSION

GUARD RAIL DETAILS

STANDARD DRAWING GR-10

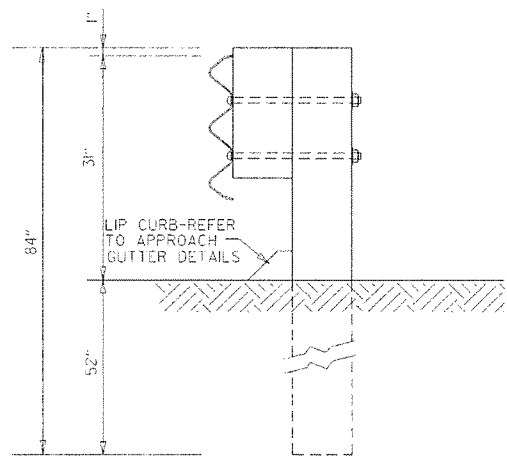


THREE BEAM RAIL WITH STEEL TUBING BLOCKOUT AND STEEL POST
POSTS 1-7

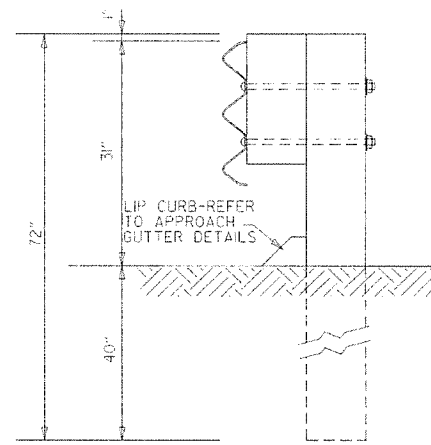


W-BEAM TO THREE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT AND STEEL POST
POST 8

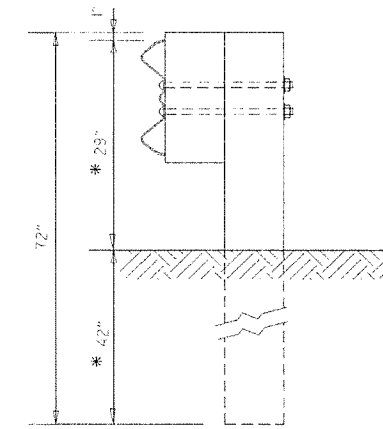
* NOTE:
THESE DIMENSIONS WILL NEED TO BE ADJUSTED IN THE FIELD TO MAKE THE TRANSITION FROM 21" MID POINT OF THREE BEAM TO 22" MID POINT OF W-BEAM.



THREE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUTS & WOOD POSTS
POSTS 1-6



THREE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 7

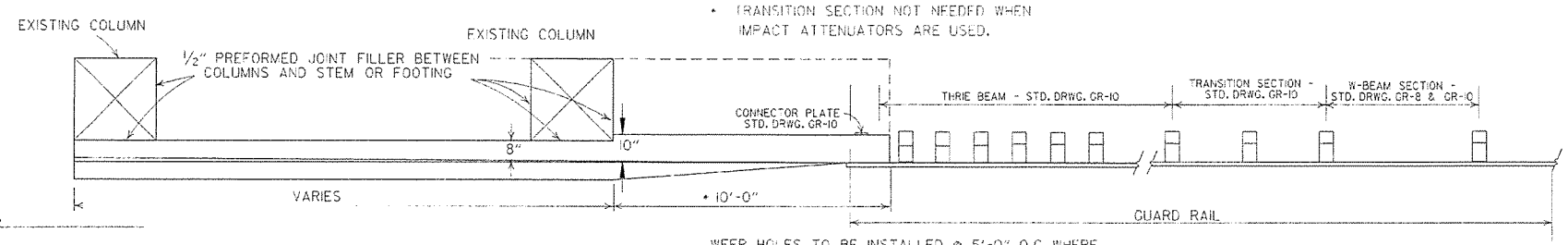


W-BEAM TO THREE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 8

GENERAL NOTES:
RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.
WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7F (1400 F) OR NO. 1 (350 F) SOUTHERN PINE.

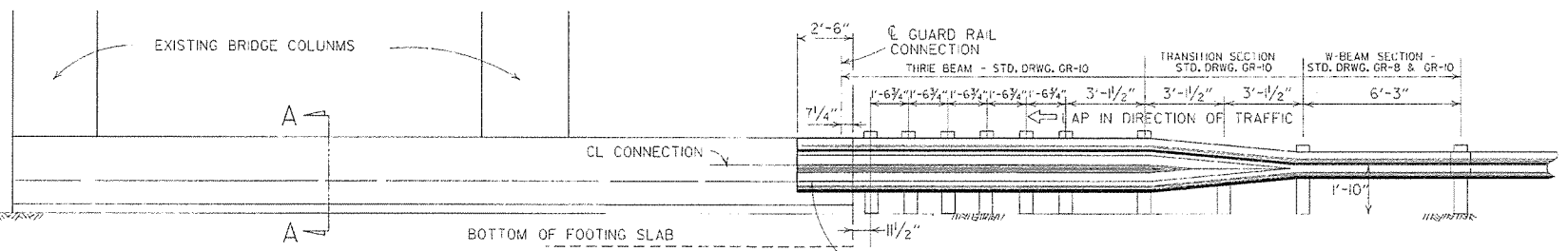
DATE	REVISION	DATE FILED
7-14-10	REVISED POST 8 DIMENSIONS	
11-29-07	ADDED PLASTIC BLOCKOUTS	
8-22-02	REVISED LIP CURB NOTE	
3-30-00	DRAWN & ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION
GUARD RAIL DETAILS
STANDARD DRAWING GR-10A



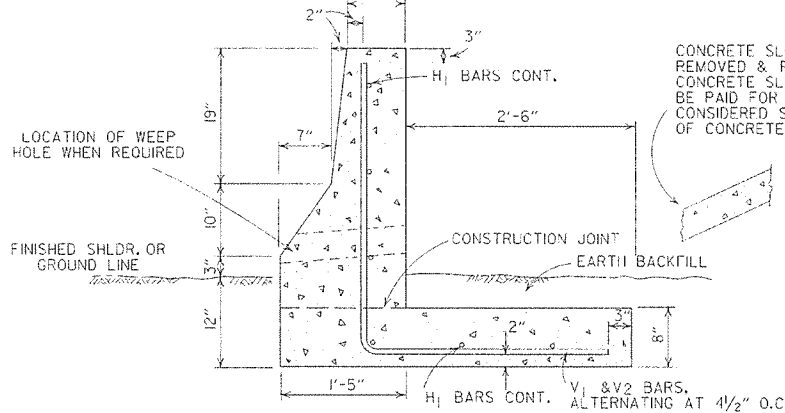
AT LEAST ONE 1/2" JOINT SHALL BE CONSTRUCTED IN THE CONCRETE BARRIER WALL. JOINTS SHALL BE EQUALLY SPACED AT A MAXIMUM OF 25'-0" O.C. FILL JOINT WITH PREFORMED JOINT FILLER.

PLAN OF CONCRETE BARRIER WALL

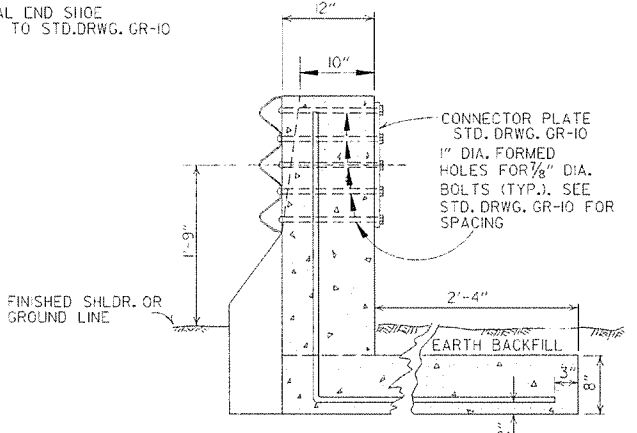


NOTE: ALL EXPOSED EDGES OF THE CONCRETE BARRIER WALL SHALL HAVE A 1/4" CHAMFER.

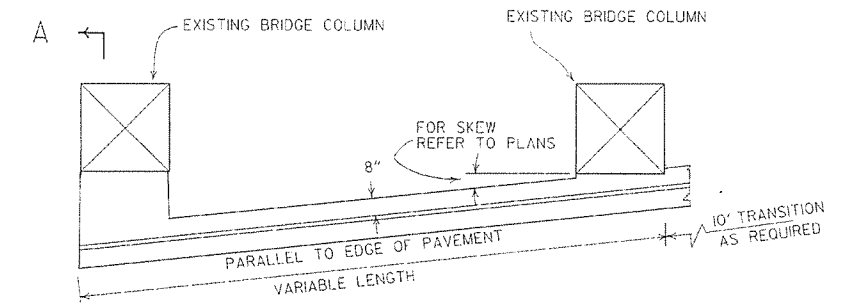
ELEVATION OF CONCRETE BARRIER WALL



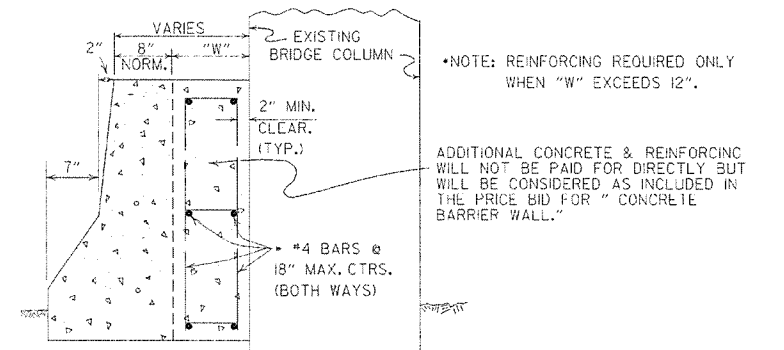
SECTION A-A



SECTION THRU CONNECTION



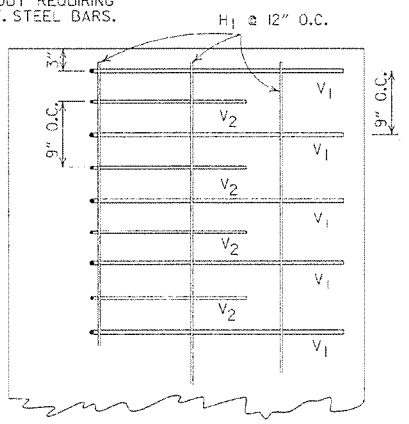
PLAN



SECTION A-A

DETAILS OF CONCRETE BARRIER WALL WHEN PIERS ARE SKEWED TO ROADWAY

IF FOR ANY REASON IT IS NECESSARY TO CONSTRUCT THE FOOTING AT A LOWER ELEVATION THAN IS SHOWN, THE STEM MAY BE LENGTHENED 1'-0" BETWEEN FIN. SHLDR. AND TOP OF FOOTING WITHOUT REQUIRING HEAVIER REINF. STEEL BARS.

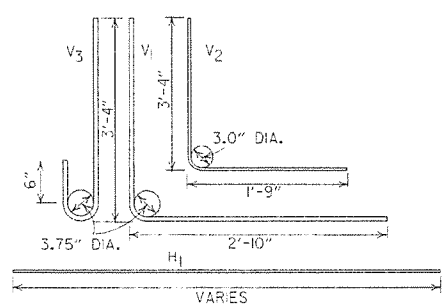


PLAN OF REINFORCING STEEL IN FOOTING

BAR LIST

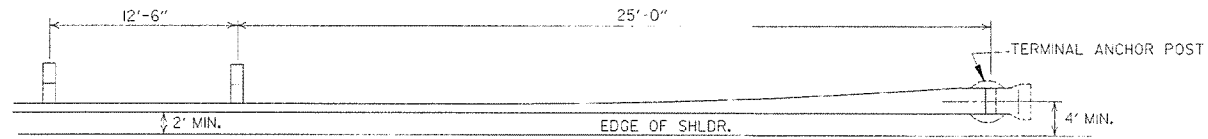
MARK	NO.	SIZE	LENGTH
V1	#5	6'-2 1/4"	
V2	#4	5'-1"	
V3	#5	2'-1 1/8"	
H1	6	4	VAR.

THE V3 BARS SHALL BE USED IN PLACE OF THE V1 & V2 BARS IN FRONT OF PIERS.

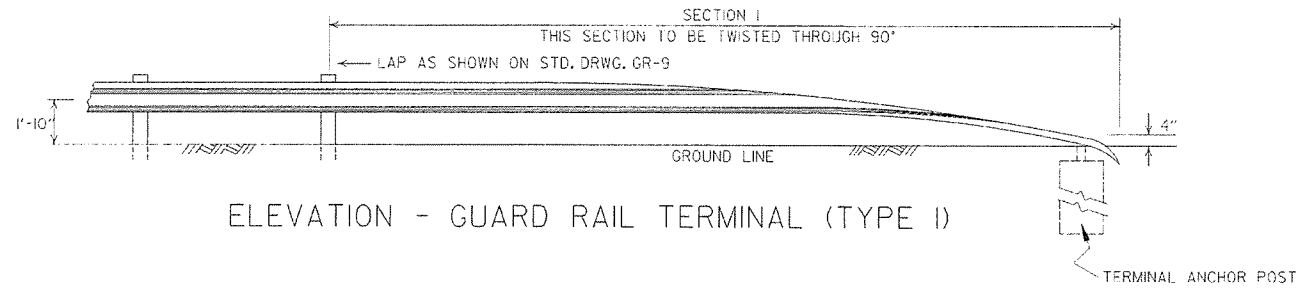


BEND DIAGRAMS

DATE	REVISION	DATE FILM	ARKANSAS STATE HIGHWAY COMMISSION
7-14-10	RAISED HEIGHT OF W-BEAM 1"		CONCRETE BARRIER WALL (PIER PROTECTION TYPE A)
8-22-02	REV. SECTION A A OF DETAILS OF CONCRETE BARRIER WALL		
6-29-00	MOVED DIMENSION LINE		STANDARD DRAWING GR-II
5-18-00	ADDED NOTE		
3-30-00	REVISED TO INCLUDE THREE BEAM		
6-2-94	ADDED TRANSITION SECTION NOTE		
10-1-92	REDRAWN & REVISED	10-1-92	
8-15-91	REVISED DRAWING PLAN CONC. BARR.	8-15-91	
2-16-89	ADDED SKEWED DETAILS	5/94-2-16-89	
7-14-88	CHANGED TITLE		
10-9-87	REDRAWN & REVISED		

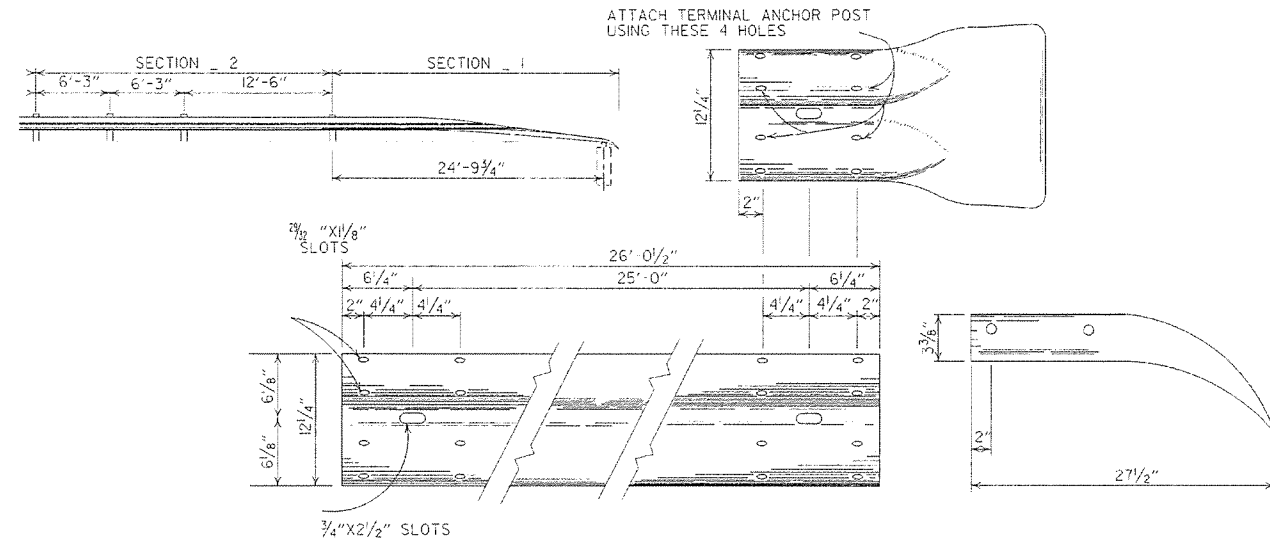


PLAN - GUARD RAIL TERMINAL (TYPE I)



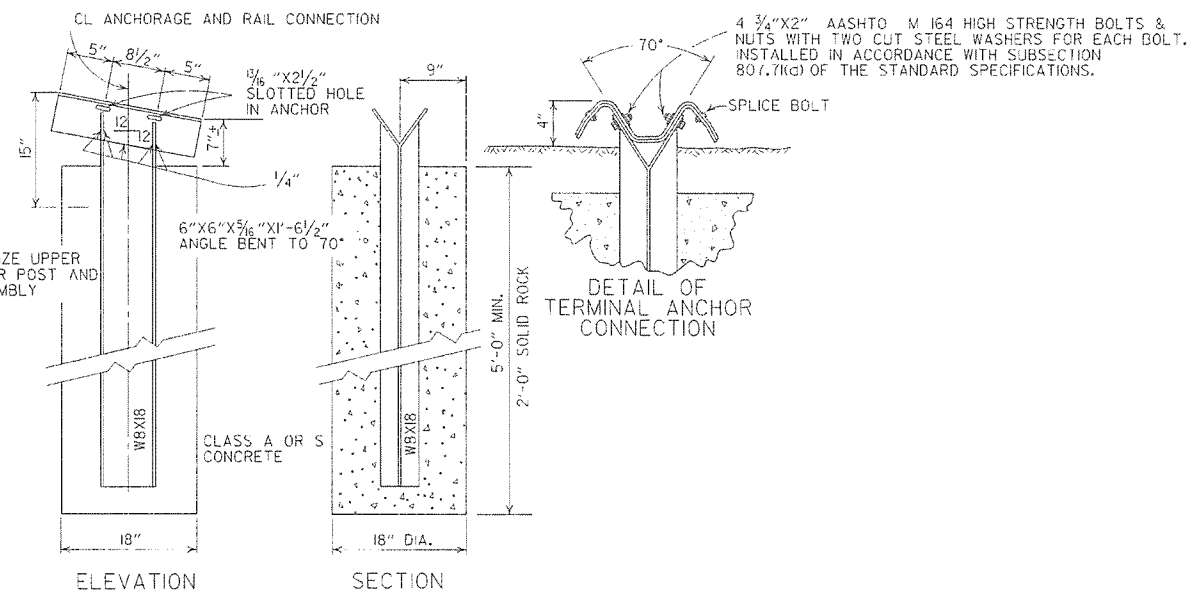
ELEVATION - GUARD RAIL TERMINAL (TYPE I)

NOTE:
SECTIONS 1 AND 2 OF GUARD RAIL TERMINAL
SHALL BE PAID FOR AT THE PRICE BID PER
LINEAR FOOT OF THE TYPE OF GUARD RAIL SPECIFIED.



SECTION 1

TERMINAL SECTION



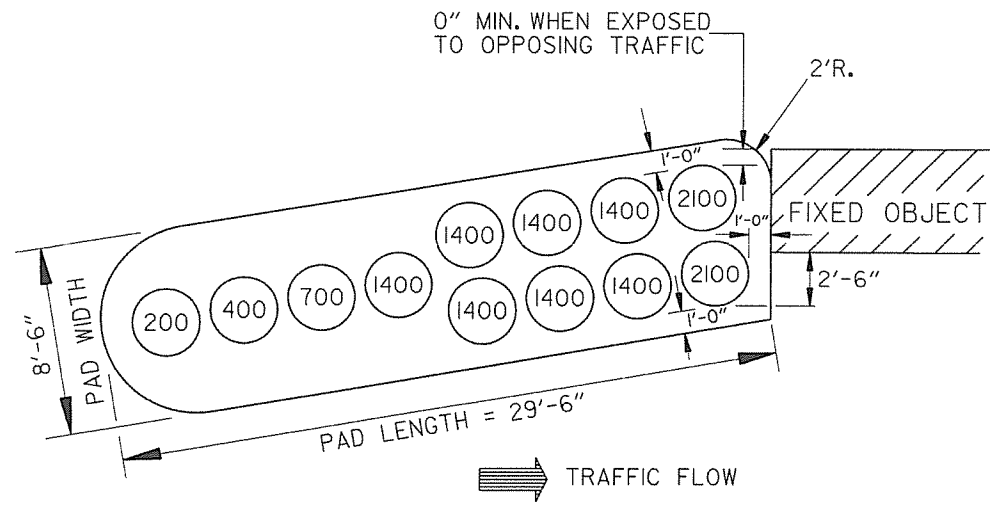
DETAIL OF TERMINAL ANCHOR POST (TYPE I)

NOTE: GALVANIZE UPPER
15" OF ANCHOR POST AND
ANCHOR ASSEMBLY

4 3/4" x 2" AASHTO M 164 HIGH STRENGTH BOLTS &
NUTS WITH TWO CUT STEEL WASHERS FOR EACH BOLT.
INSTALLED IN ACCORDANCE WITH SUBSECTION
807.7(k) OF THE STANDARD SPECIFICATIONS.

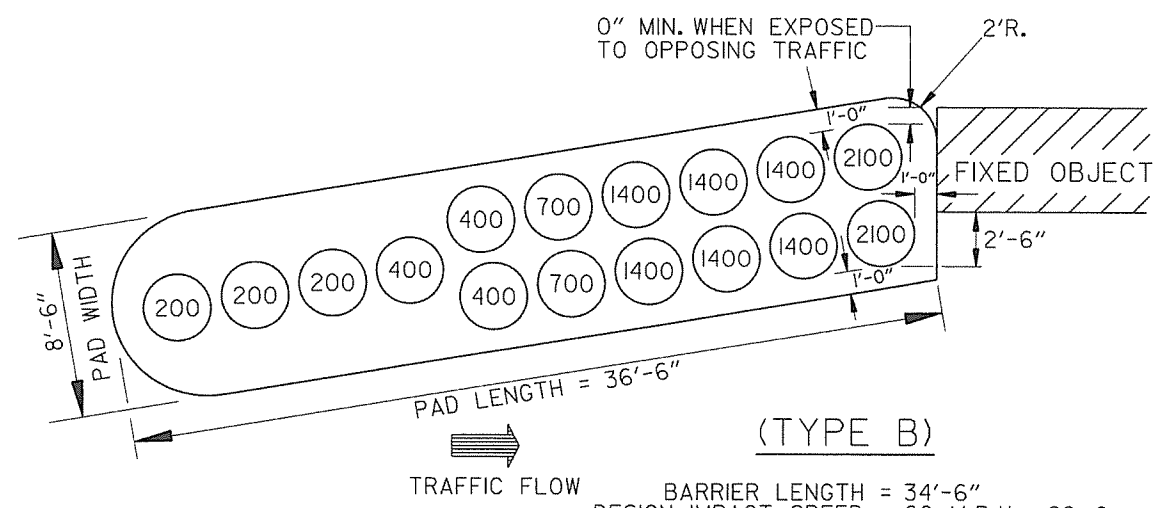
NOTE: RAIL MEMBERS MAY BE BOLTED TO ANGLE AT TERMINAL ANCHOR AND THE TWO
ASSEMBLIES POSITIONED TO PROPER ALIGNMENT PRIOR TO PLACING CONCRETE
AROUND B W F 17 POST IF CONTRACTOR SO DESIRES.

			ARKANSAS STATE HIGHWAY COMMISSION
			GUARD RAIL DETAILS
7-14-10	RAISED HEIGHT OF GUARD RAIL 1"		STANDARD DRAWING GRT-1
6-26-97	REVISED LAP NOTE		
10-18-96	REVISED ASTM REF. TO AASHTO		
11-3-94	DIMENSION TERMINAL DETAIL		
11-11-92	ADDED NOTE FOR PAYMENT	11-11-92	
10-1-92	DRAWN & ISSUED	10-1-92	
	REVISION	DATE	FILM



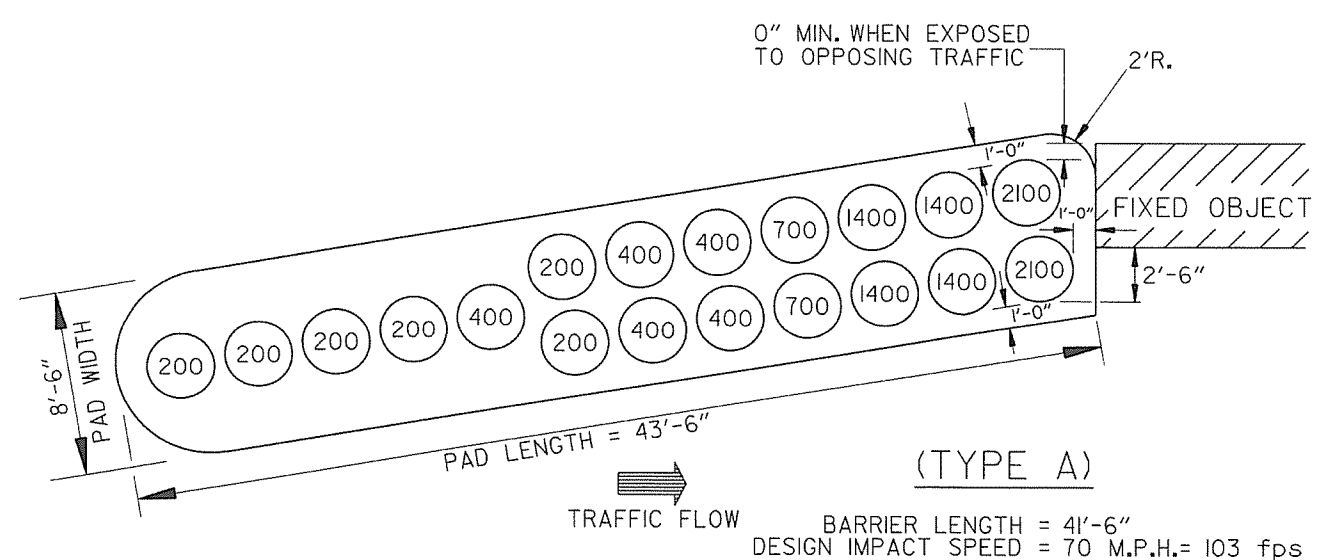
(TYPE C)

BARRIER LENGTH = 27'-6"
 DESIGN IMPACT SPEED = 50 M.P.H. = 73.3 fps



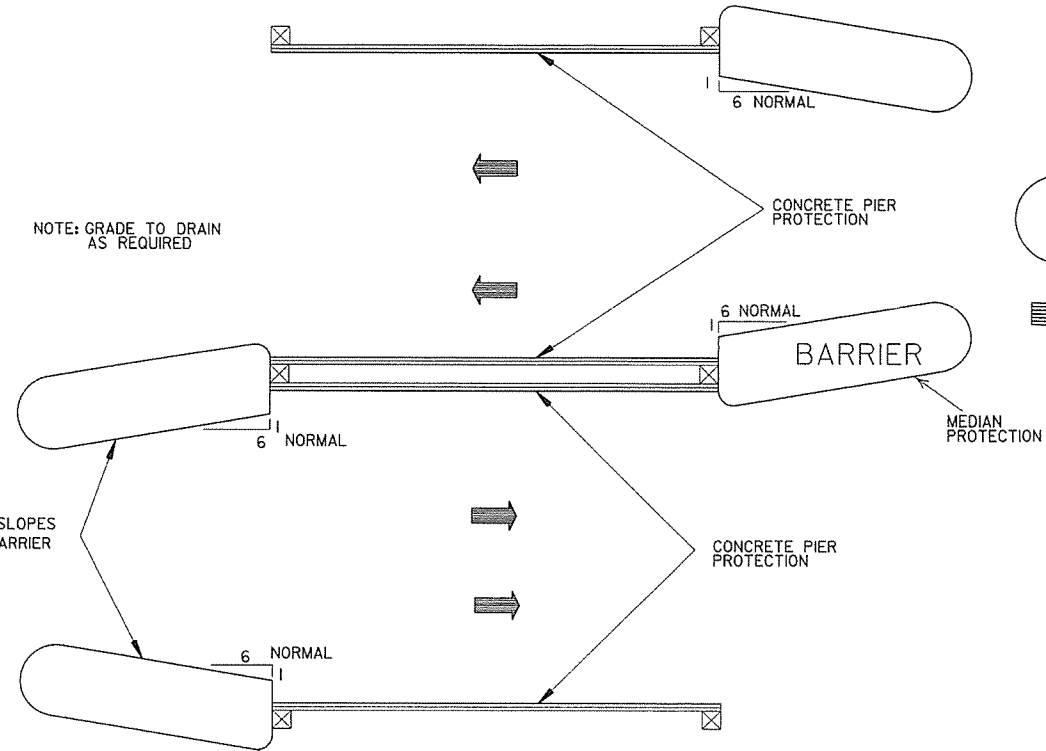
(TYPE B)

BARRIER LENGTH = 34'-6"
 DESIGN IMPACT SPEED = 60 M.P.H. = 88 fps

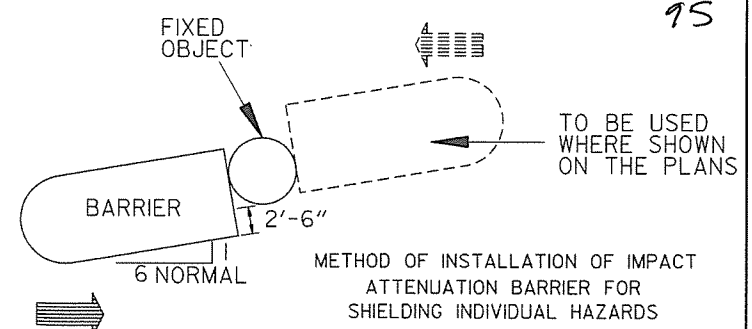


(TYPE A)

BARRIER LENGTH = 41'-6"
 DESIGN IMPACT SPEED = 70 M.P.H. = 103 fps



METHOD OF INSTALLATION OF IMPACT ATTENUATION BARRIER FOR PIER PROTECTION



METHOD OF INSTALLATION OF IMPACT ATTENUATION BARRIER FOR SHIELDING INDIVIDUAL HAZARDS

APPROXIMATE QUANTITIES PER PAD

TYPE	ALTERNATE #1		ALTERNATE #2
	AGGR. BASE COURSE TONS	A.C.H.M. SURFACE COURSE TONS	P.C. CONC. BASE (4" U.T.) SQ. YDS.
A	9.7	4.6	41.6
B	8.1	3.8	34.9
C	6.6	3.1	28.3

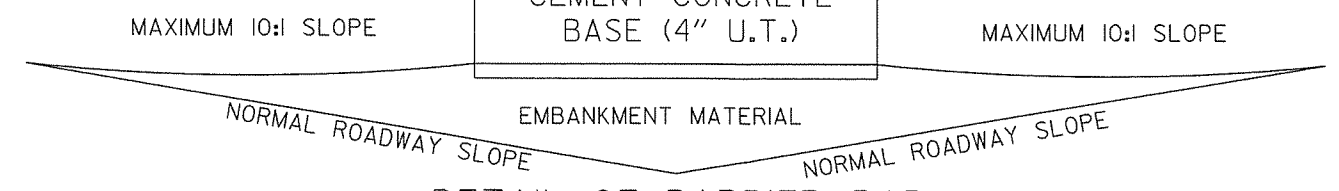
NOTE: APPROXIMATE QUANTITIES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY. PAYMENT TO BE INCLUDED IN UNIT PRICE BID FOR IMPACT ATTENUATION BARRIER.

GENERAL NOTES

1. DIMENSIONS SHOWN ARE TO TOP OF PLASTIC MODULES.
2. SPACING BETWEEN PLASTIC MODULES SHALL NOT EXCEED 6" AT THE TOP.
3. PLASTIC MODULES SHALL MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

ALTERNATE #1
 AVG. 8'-6" A.C.H.M. SURF. COURSE (1/2")
 220 LBS. PER SQ. YD. &
 AGGREGATE BASE COURSE
 (4" COMPACTED DEPTH)

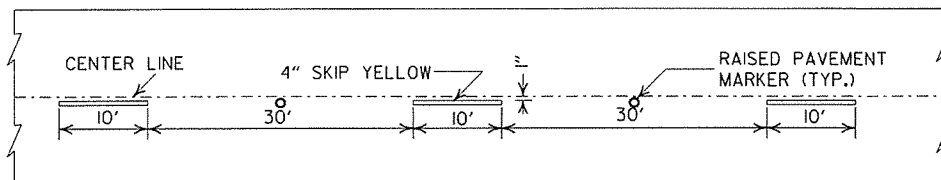
OR ALTERNATE #2
 AVG. 8'-6" PORTLAND CEMENT CONCRETE BASE (4" U.T.)



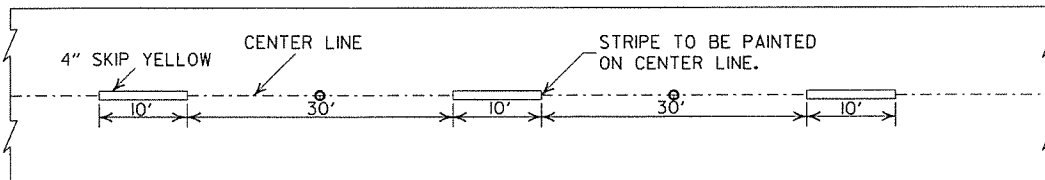
DETAIL OF BARRIER PAD

NOTE: BARRIER PAD TO BE SKEWED TOWARD ONCOMING TRAFFIC
 A MAXIMUM OF 6:1 WITH 6:1 BEING NORMAL

10-15-09	ADDED REFERENCE TO MASH		ARKANSAS STATE HIGHWAY COMMISSION
11-29-07	REVISED TY. A & TY. C ARRAYS		
11-19-98	REVISED FIXED OBJECT		
11-18-98	REV. NOTES & TYPE A MOD. WTS.		
10-18-96	REDRAWN		
7-15-88	CONFORMED TO 1988 SPECS		
7-29-87	REDRAWN		IMPACT ATTENUATION BARRIER
DATE	REVISION	DATE FILMED	
			STANDARD DRAWING IB-1

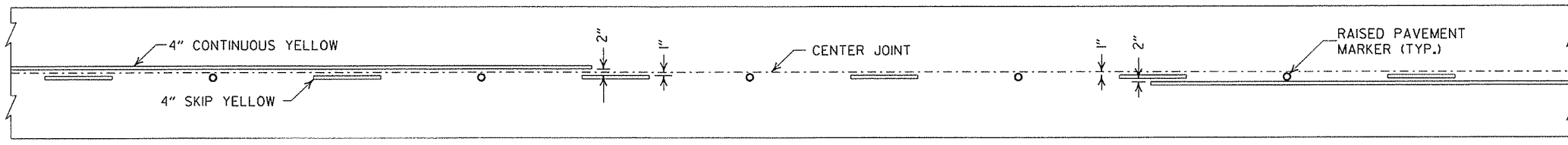


CONCRETE PAVEMENT

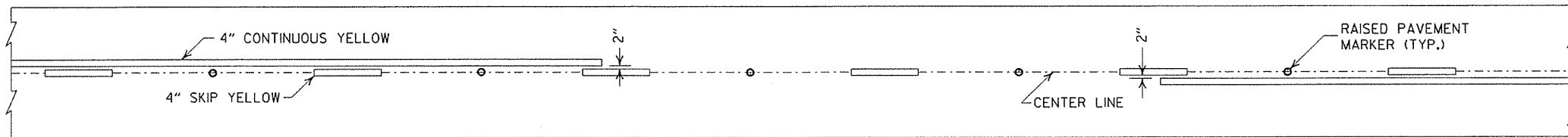


ASPHALT PAVEMENT

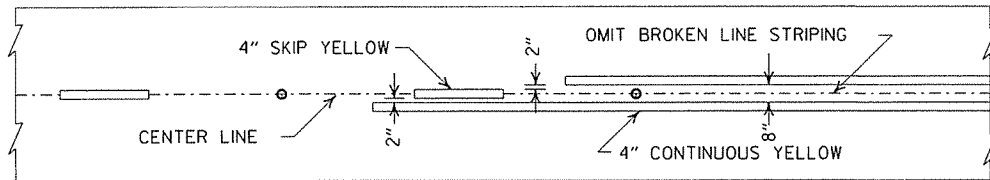
BROKEN LINE STRIPING



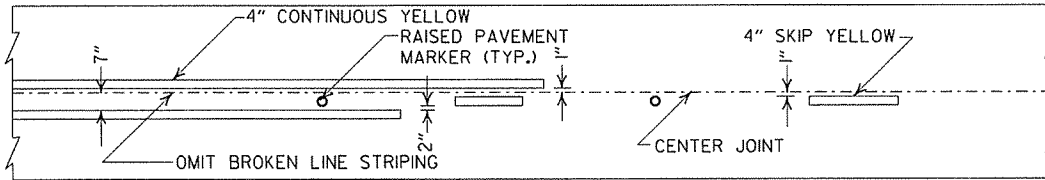
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

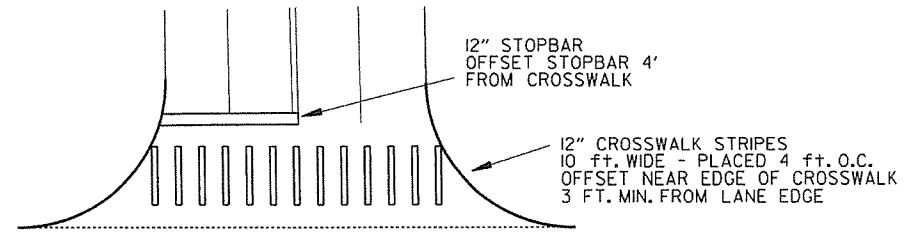


ASPHALT PAVEMENT



CONCRETE PAVEMENT

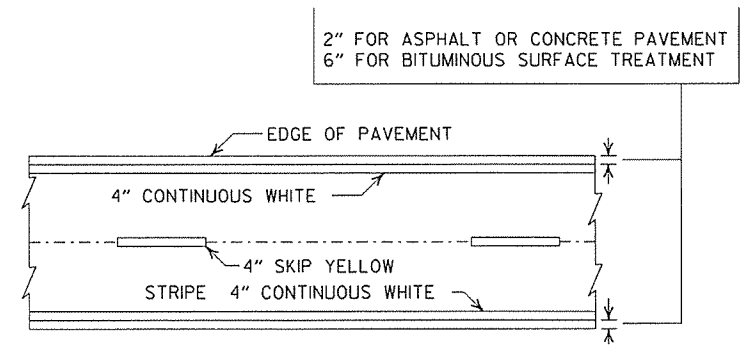
STRIPING AT ADJACENT NO PASSING LANES



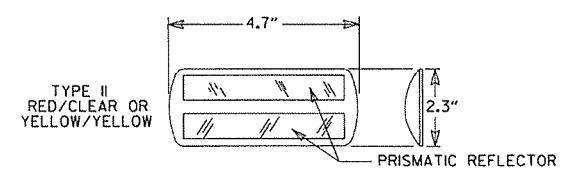
CROSSWALK AND STOPBAR DETAILS

NOTES:

1. ALL LINES SHALL HAVE A WIDTH OF 4 INCHES.
2. THE THICKNESS AND RATE OF PAINT APPLICATION SHALL BE AS SPECIFIED IN SECTION 718 OF THE STANDARD SPECIFICATIONS.
3. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. RAISED PAVEMENT MARKERS SHALL BE CENTERED BETWEEN SKIP LINES ON 40 FEET SPACING UNLESS OTHERWISE SHOWN ON THE PLANS.



PAVEMENT EDGE LINE MARKING



NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND RAISED PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

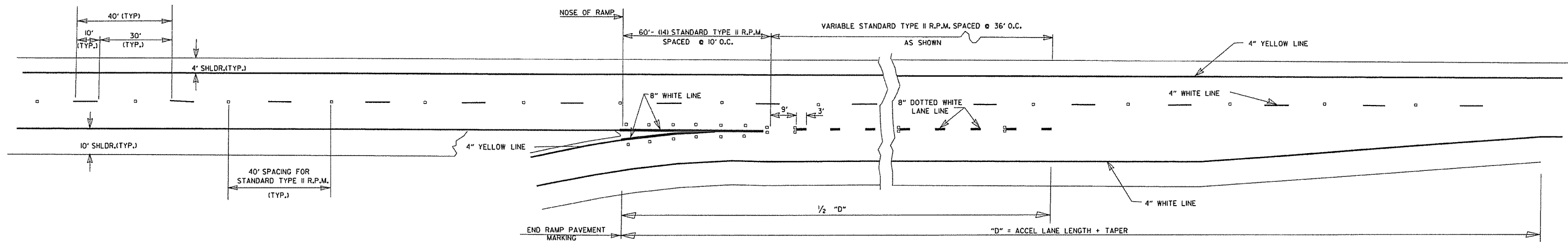
DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80

ARKANSAS STATE HIGHWAY COMMISSION	
PAVEMENT MARKING DETAILS	
STANDARD DRAWING PM-1	

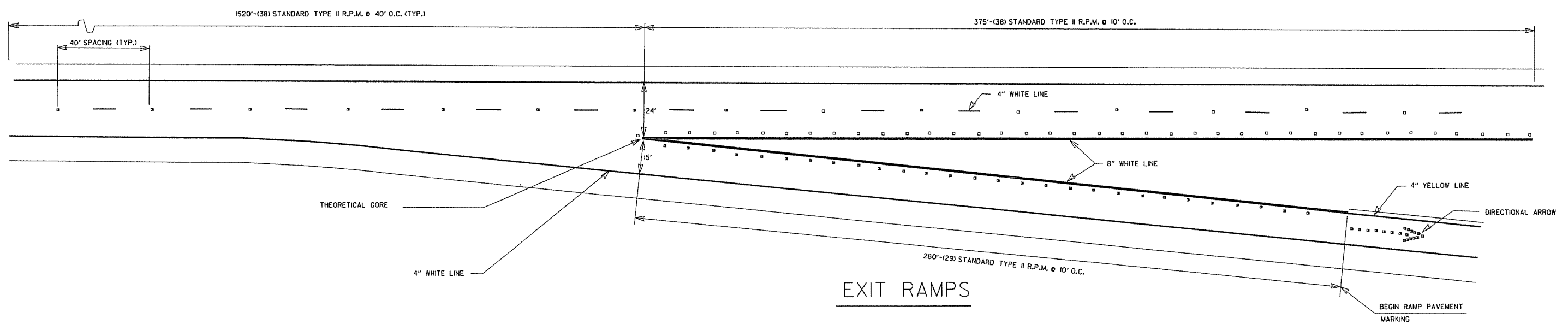
PAVEMENT MARKING QUANTITIES
(BASED ON 700' ACCEL. LANE + 300' TAPER)

ENTRANCE RAMP
8" WHITE = 228 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

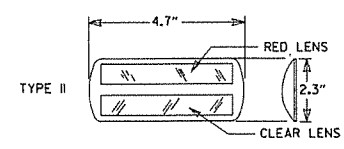
EXIT RAMP
4" WHITE = 280 LIN. FT.
8" WHITE = 655 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH



ENTRANCE RAMPS

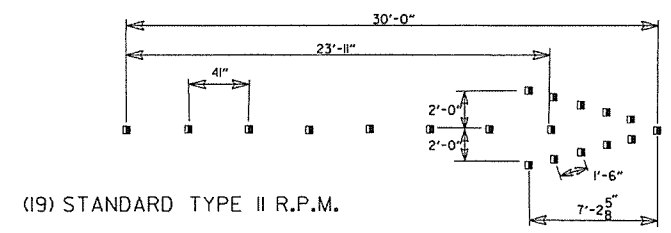


EXIT RAMPS



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



(19) STANDARD TYPE II R.P.M. DIRECTIONAL ARROWS

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER, REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMPS	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION
PERMANENT PAVEMENT MARKING
ON ACCESS CONTROLLED ROADWAYS

STANDARD DRAWING PM-2

SUPERELEVATION TABLE FOR ONE - WAY TRAFFIC

DEGREE OF CURVE	30 MPH			40 MPH			50 MPH			55 MPH			60 MPH			65 MPH			70 MPH		
	Ls (FT)		e	Ls (FT)		e	Ls (FT)		e	Ls (FT)		e	Ls (FT)		e	Ls (FT)		e	Ls (FT)		
	MINIMUM	DESIRABLE		MINIMUM	DESIRABLE		MINIMUM	DESIRABLE		MINIMUM	DESIRABLE		MINIMUM	DESIRABLE		MINIMUM	DESIRABLE		MINIMUM	DESIRABLE	MINIMUM
0° 15'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		
0° 30'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		
0° 45'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		
1° 00'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		
1° 15'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		
1° 30'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		
1° 45'	N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		N.C.		
2° 00'	R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		
2° 15'	R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		R.C.		
2° 30'	0.021		0.034		0.045		0.053		0.058		0.061		0.067		0.069		0.078		0.085		
2° 45'	0.023		0.037		0.049		0.057		0.063		0.067		0.072		0.075		0.081		0.085		
3° 00'	0.025		0.040		0.053		0.061		0.067		0.072		0.077		0.081		0.087		0.091		
3° 15'	0.027		0.043		0.057		0.065		0.072		0.076		0.082		0.086		0.090		0.094		
3° 30'	0.029		0.046		0.061		0.069		0.076		0.080		0.086		0.090		0.095		0.098		
3° 45'	0.031		0.049		0.065		0.072		0.078		0.083		0.088		0.093		0.097		0.099		
4° 00'	0.033		0.051		0.069		0.077		0.083		0.087		0.091		0.095		0.099		1.000		
4° 30'	0.037		0.056		0.074		0.083		0.088		0.092		0.096		0.099		1.000		1.000		
5° 00'	0.040		0.061		0.078		0.087		0.092		0.096		0.099		1.000		1.000		1.000		
5° 30'	0.043		0.066		0.084		0.093		0.098		1.000		1.000		1.000		1.000		1.000		
6° 00'	0.046		0.070		0.088		0.097		1.000		1.000		1.000		1.000		1.000		1.000		
6° 30'	0.050		0.074		0.092		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
7° 00'	0.053		0.078		0.096		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
7° 30'	0.056		0.081		0.099		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
8° 00'	0.058		0.084		1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
8° 30'	0.061		0.087		1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
9° 00'	0.063		0.089		1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
10° 00'	0.068	160	0.094	235	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
11° 00'	0.072	170	0.097	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
12° 00'	0.076	175	0.099	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
13° 00'	0.080	180	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
14° 00'	0.083	190	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
15° 00'	0.086	195	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
16° 00'	0.089	200	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
17° 00'	0.091	200	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
18° 00'	0.093	205	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
19° 00'	0.095	210	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
20° 00'	0.097	215	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
21° 00'	0.098	215	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
22° 00'	0.099	215	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
23° 00'	0.099	215	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		
24° 00'	0.100	220	1.000	250	1.000		1.000		1.000		1.000		1.000		1.000		1.000		1.000		

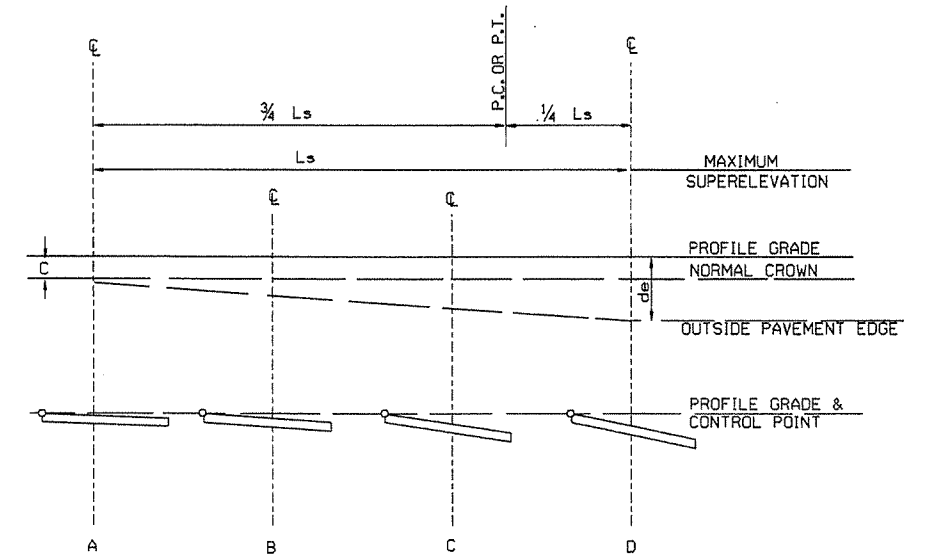
ABBREVIATIONS

- NC - NORMAL CROWN
- RC - REVERSE CROWN, SUPERELEVATION AT NORMAL CROWN SLOPE
- S - SUPERELEVATION
- L - DISTANCE FROM BEGINNING OF SUPERELEVATION TRANSITION TO ANY POINT (FT.)
- d - WIDTH OF PAVEMENT
- e - MAXIMUM RATE OF SUPERELEVATION (FT. PER FT.)
- Ls - LENGTH OF SUPERELEVATION TRANSITION (FT.)
- C - NORMAL CROWN (FT.)

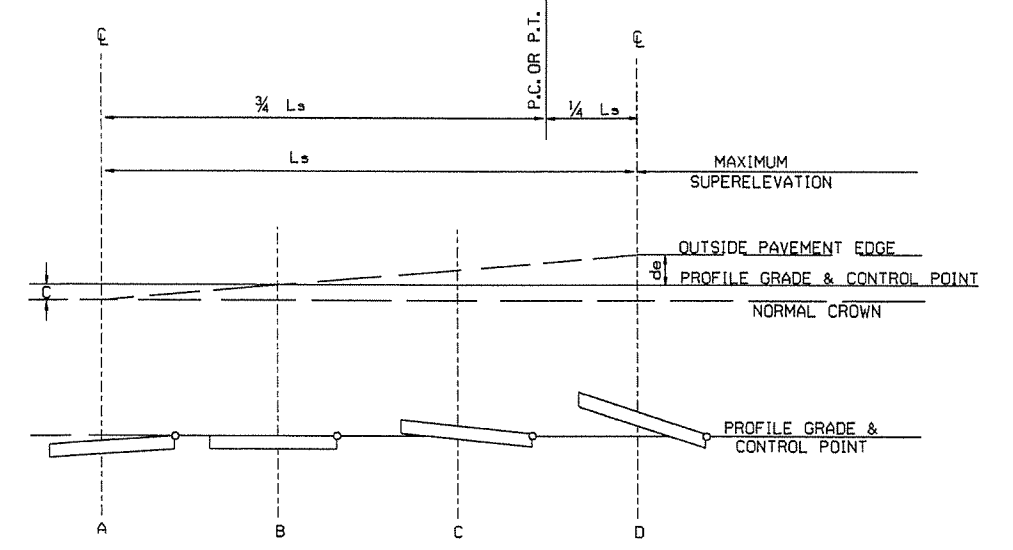
GENERAL NOTES

1. ON PAVEMENT WITH ONE-WAY TRAFFIC, THE SUPERELEVATION SHALL BE REVOLVED ON THE PROFILE GRADE POINT.
2. SUPERELEVATION VALUES SHOWN ON THE CROSS SECTIONS ARE VALUES (+) OR (-) TO BE ADDED OR SUBTRACTED FROM THE POINT OF CONTROL.
3. LENGTHS FOR Ls MAY BE ROUNDED IN MULTIPLES OF 25 FT. OR 50 FT. TO PERMIT SIMPLER CALCULATIONS.
4. MINIMUM Ls VALUES MAY BE USED FOR RAMPS; DESIRABLE VALUES SHALL APPLY TO MAIN LANES.
5. DIVIDED PAVEMENTS WIDER THAN 4 LANES SHALL HAVE ADDITIONAL TRANSITION LENGTHS AS FOLLOWS:

6 LANE DIVIDED-----+20%
8 LANE DIVIDED-----+50%

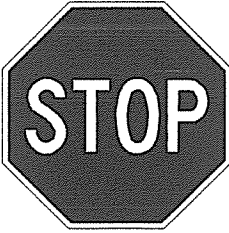
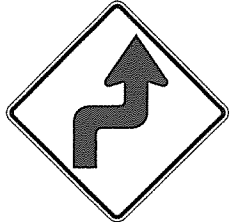


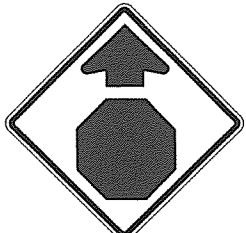

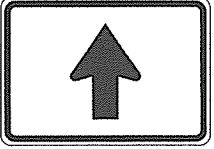
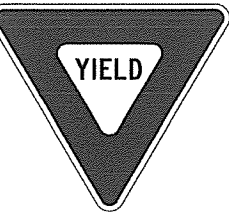

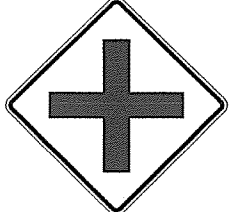

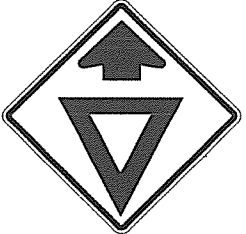

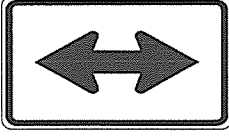
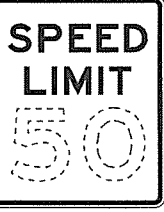
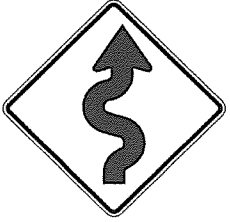
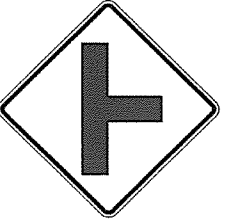



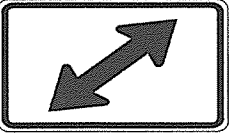
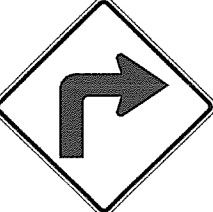
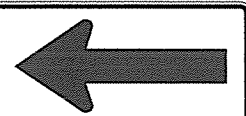
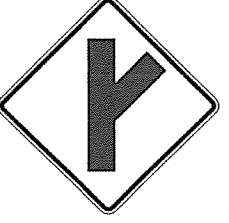

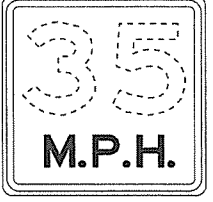
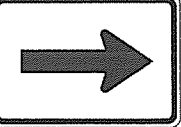
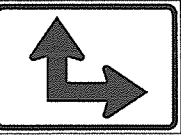
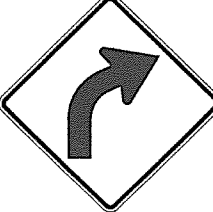
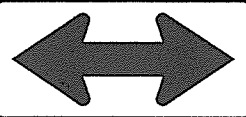
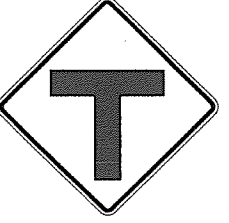
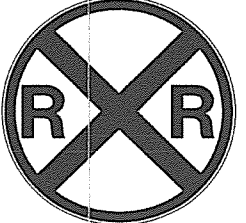
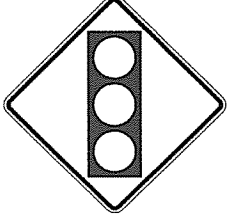
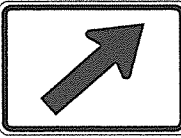
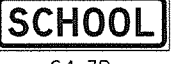

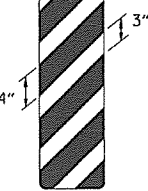


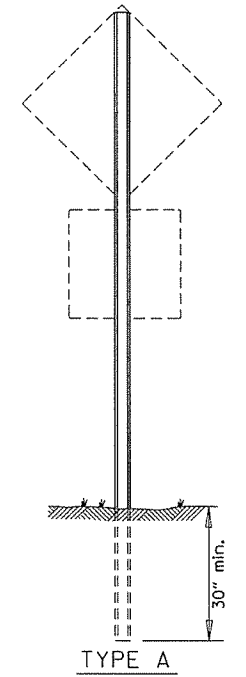
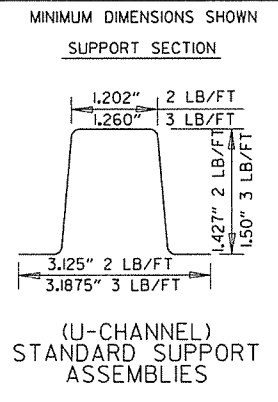
SUPERELEVATION FORMULA = $S = - \frac{L(d_e - C)}{L_s}$



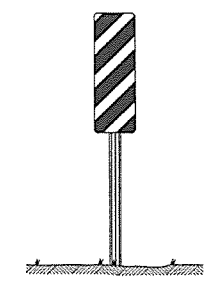
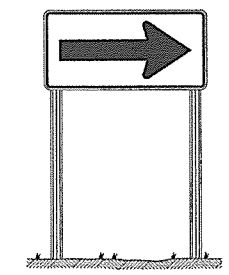
SUPERELEVATION FORMULA = $S = + \frac{L(d_e + C)}{L_s}$

ARKANSAS STATE HIGHWAY COMMISSION		
TABLES AND METHOD OF SUPERELEVATION FOR ONE-WAY TRAFFIC		
STANDARD DRAWING SE-1		
01-09-87	ISSUED	578-1-15-87
DATE	REVISION	DATE FILMED

 RI-1 30"x30"	 W1-3 30"x30" (LT. OR RT.)	 W1-8 18"x24"	 W2-5 30"x30"	 W3-1 36"x36"	 W5-1 36"x36"	 M6-3 21"x15"
 RI-2 36"x36"x36"	 W1-4 30"x30" (LT. OR RT.)	 W2-1 30"x30"	 SI-1 36"x36"	 W3-2 36"x36"	 LASSEN 16 COUNTY County Route Marker MI-6 24"x24"	 M6-4 21"x15"
 R2-1 24"x30"	 W1-5 30"x30" (LT. OR RT.)	 W2-2 30"x30"	 NARROW BRIDGE W5-2 36"x36"	 PAVEMENT ENDS W8-3 36"x36"	 ALL WAY RI-3P 18"x6"	 M6-5 21"x15"
 W1-1 30"x30" (LT. OR RT.)	 W1-6 48"x24"	 W2-3 30"x30" (LT. OR RT.)	 ONE LANE BRIDGE W5-3 36"x36"	 35 M.P.H. W13-1P 18"x18"	 M6-1 21"x15"	 M6-6 21"x15"
 W1-2 30"x30" (LT. OR RT.)	 W1-7 48"x24"	 T W2-4 30"x30"	 R X R W10-1 36" DIAMETER	 W3-3 36"x36"	 M6-2 21"x15"	 SCHOOL S4-3P 24"x8"
					 WHEN CHILDREN ARE PRESENT S4-2P 24"x10"	 OM-3 12"x36" (LT. OR RT.)



NOTE: LENGTH OF SIGN POSTS SHALL BE DETERMINED SO AS TO PROVIDE FOR MINIMUM VERTICAL CLEARANCES AS CALLED FOR IN THE SPECIFICATIONS PLUS A MINIMUM VERTICAL PENETRATION OF 30" IN THE SOIL.

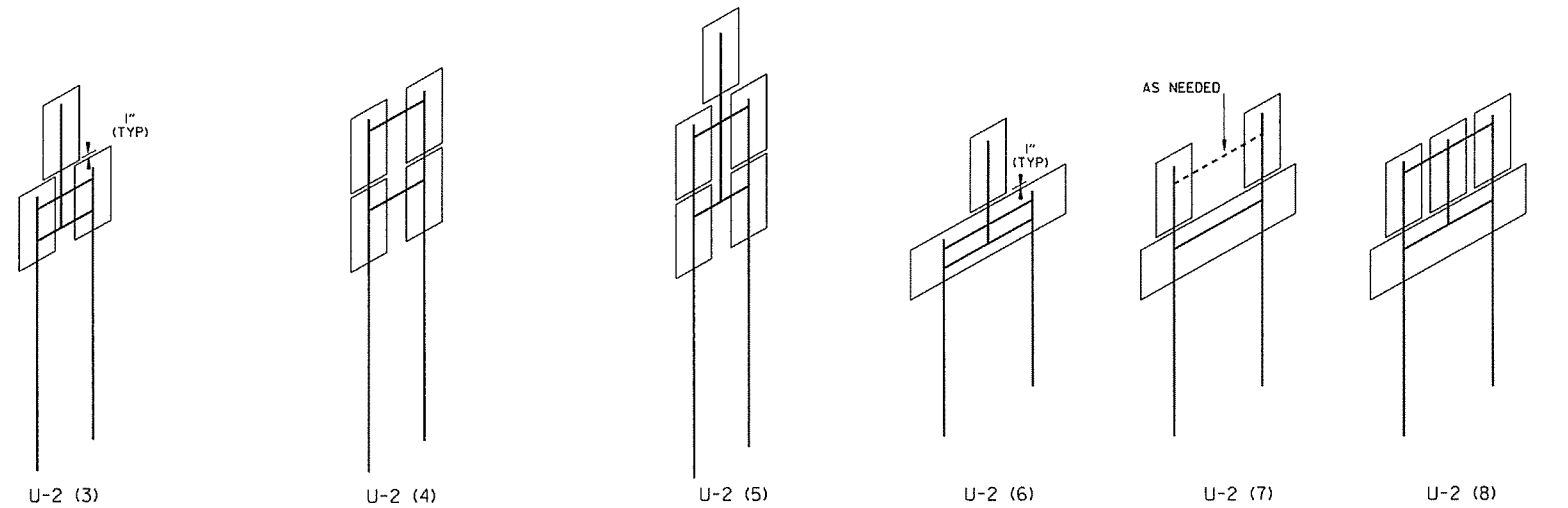
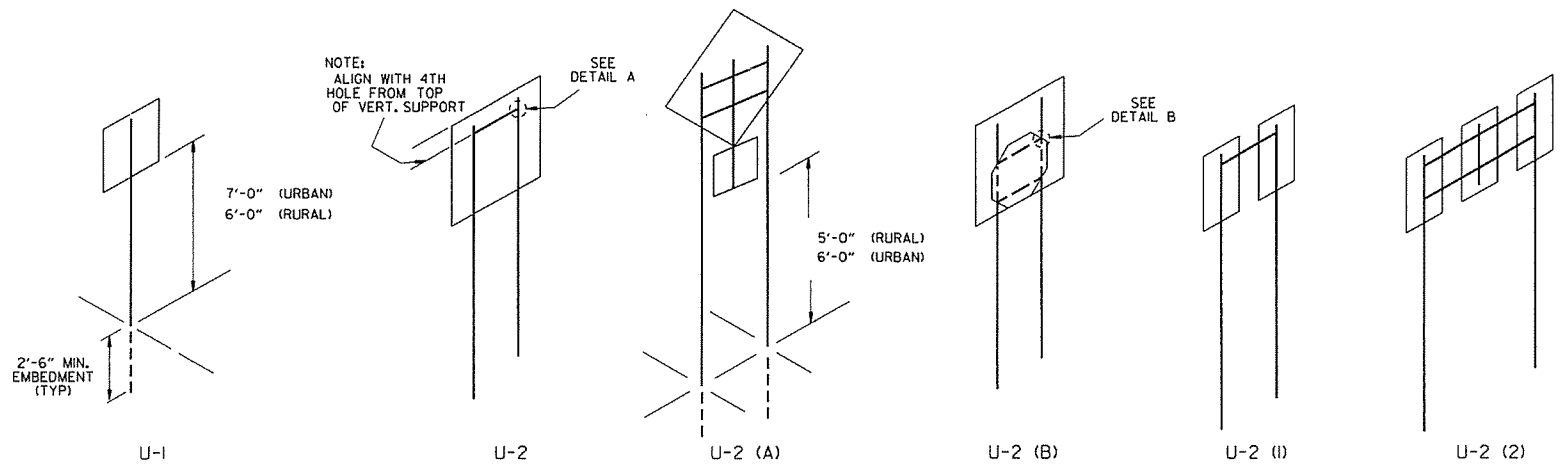


MINIMUM WEIGHT
TYPE A & B = 3 LBS./FT.
TYPE C = 2 LBS./FT.

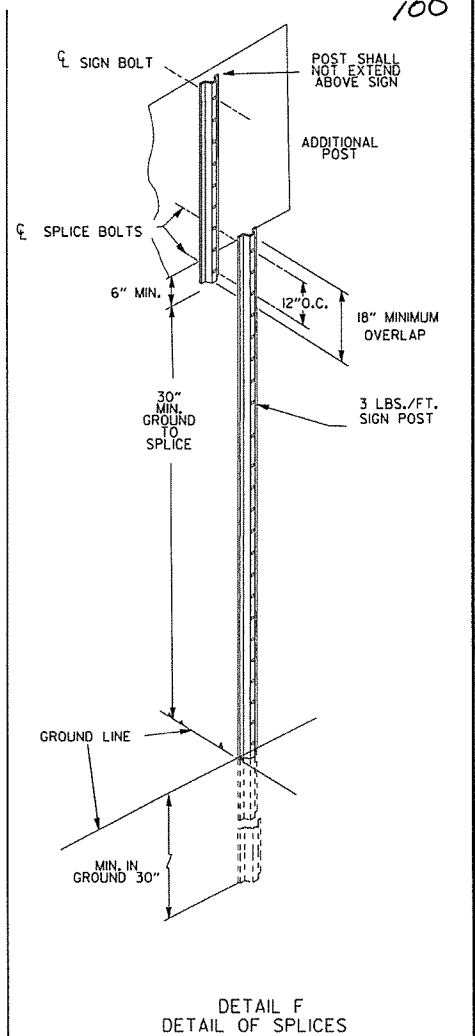
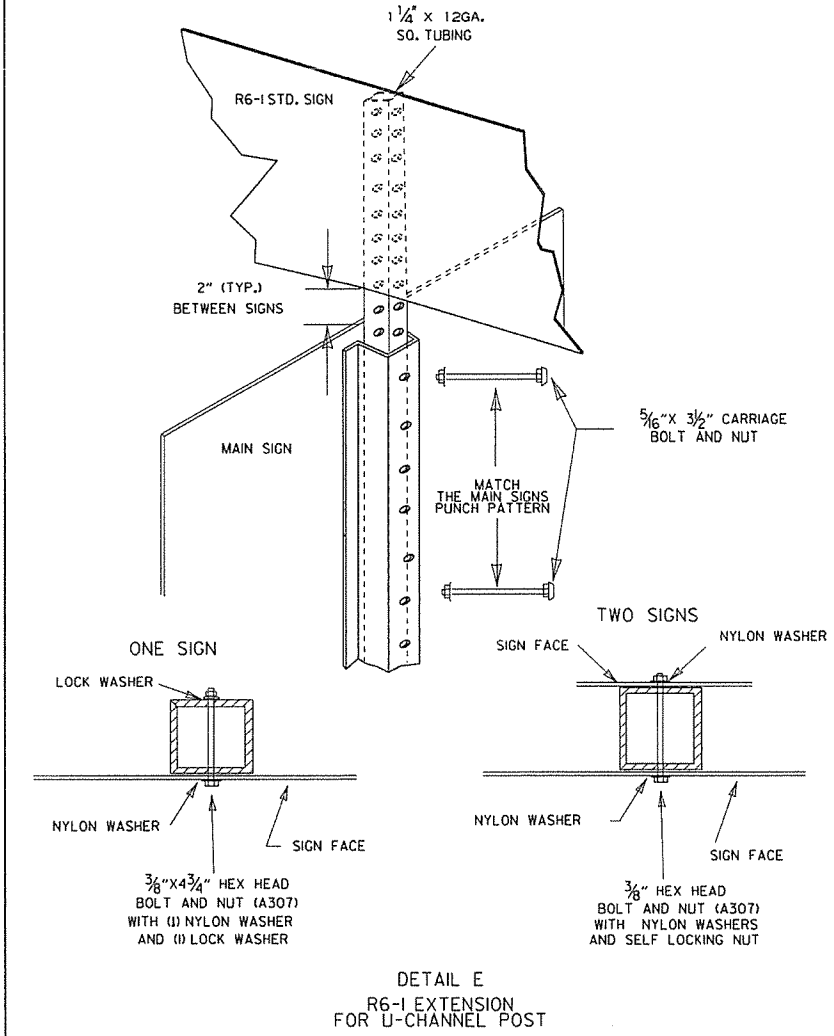
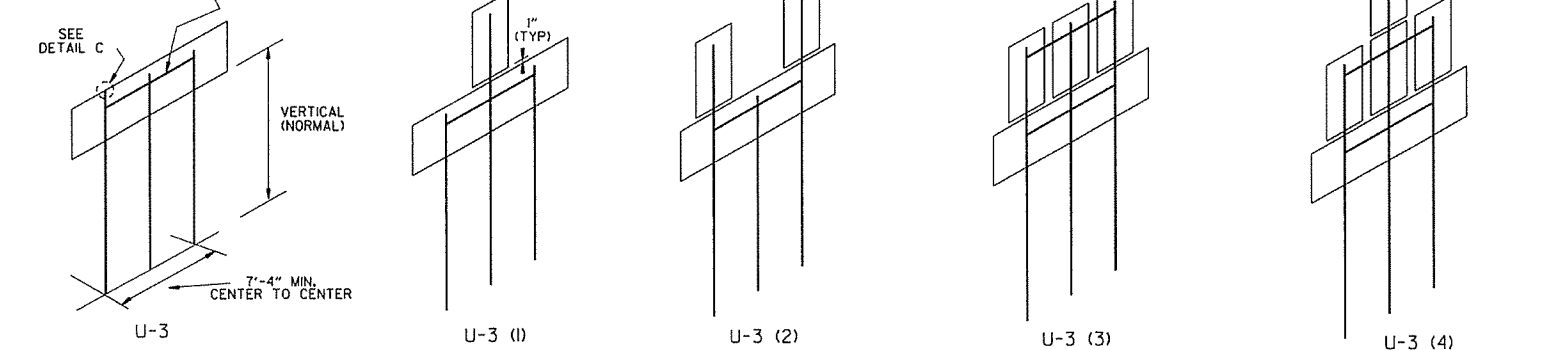
STANDARD HIGHWAY SIGNS

SUPPORT ASSEMBLIES
ARKANSAS STATE HIGHWAY COMMISSION
STANDARD HIGHWAY SIGNS
AND SUPPORT ASSEMBLIES
STANDARD DRAWING SHS-1

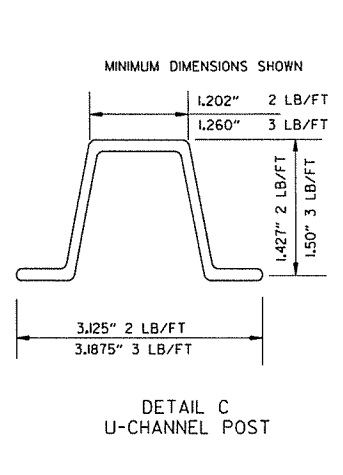
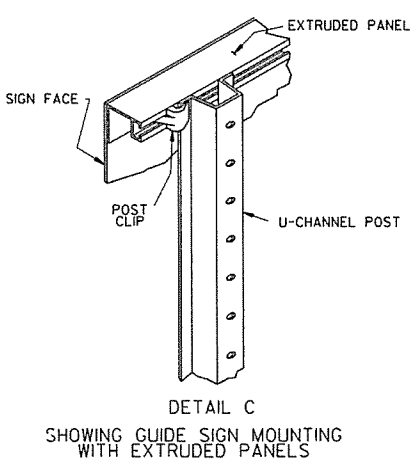
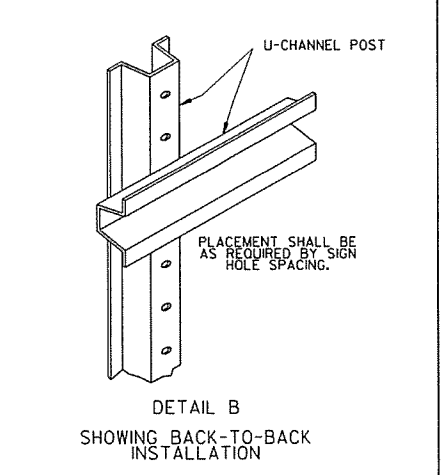
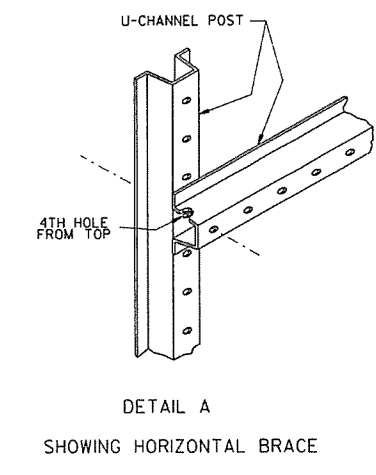
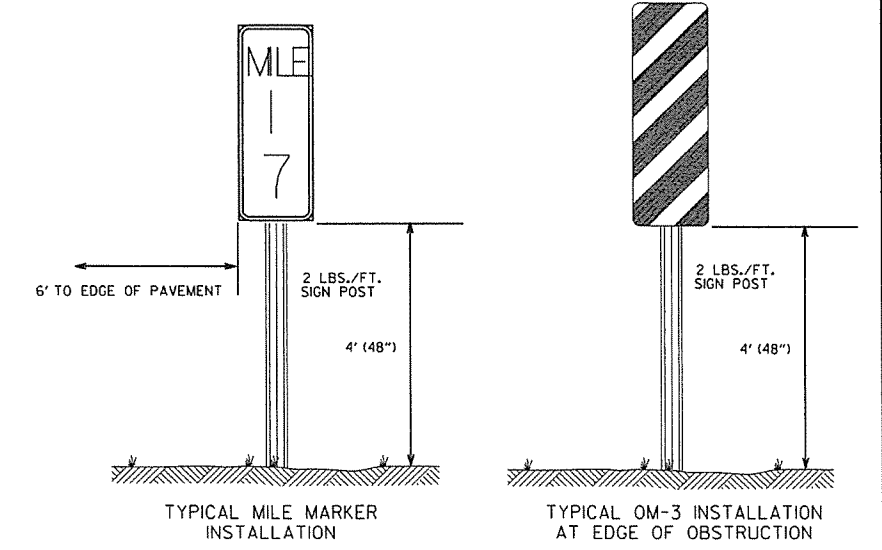
9-12-13	DELETED JOB NO. BLOCK; REVISED RI-3 TO RI-3P	
4-17-08	REVISED SIGN DESIGNATION - W3-1 & W3-2	
4-10-03	REVISED W5-2, W8-3, OM-3; ADDED W1-8	
1-5-81	REDRAWN	960-1-15-81
9-15-78	ADDED W14-3	877-9-15-78
9-2-76	POST WT.	623-9-3-76
5-3-76	STEEL POST WT. FROM 2"-3"; ADDED S4-2 & S4-3	504-5-3-76
8-12-74	REV. HT. TYPE "C" ASSEMBLY	500-8-21-74
12-21-72	ADDED W6-2,3,4,5,6	500-12-21-72
12-1-72	ISSUED	562-12-1-72
DATE	REVISION	DATE FILMED



HORIZONTAL BRACE FOR ALL MULTIPLE POST ASSEM. WITH FLAT SHEET SIGNS

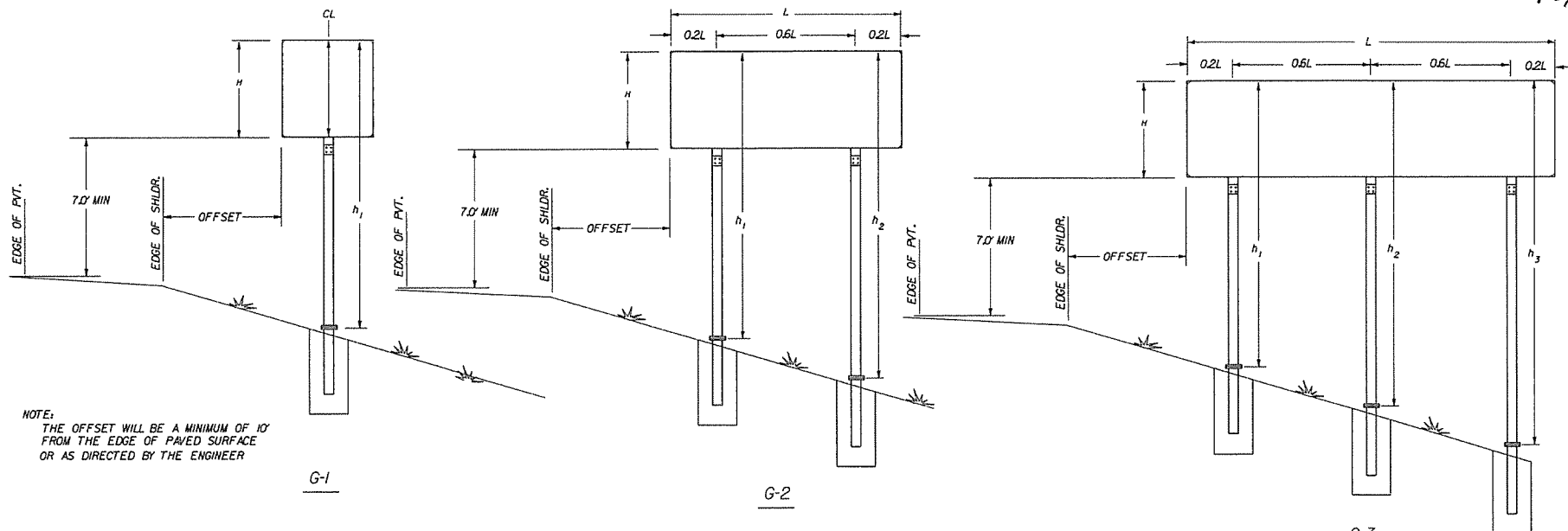


NOTES:
SIGNS AT LEAST 8' IN LENGTH MAY BE INSTALLED ON THREE 3 LB. POST. IN NO CASE SHALL THERE BE MORE THAN TWO 3 LB. POSTS WITHIN A 7' PATH.
SPLICES NECESSARY TO ATTAIN PROPER MOUNTING HEIGHT SHALL BE AS SHOWN IN DETAIL (F).
NORMAL INSTALLATIONS WILL REQUIRE 5/16" DIA. CARRIAGE BOLTS TO MOUNT SIGNS TO POST AND TO ASSEMBLE THE VARIOUS POST SUPPORTS.
ALL SIGN POSTS SHALL BE PLUMB.
THE POST FOR "TYPE U" SUPPORTS SHALL BE HOT DIP GALVANIZED.



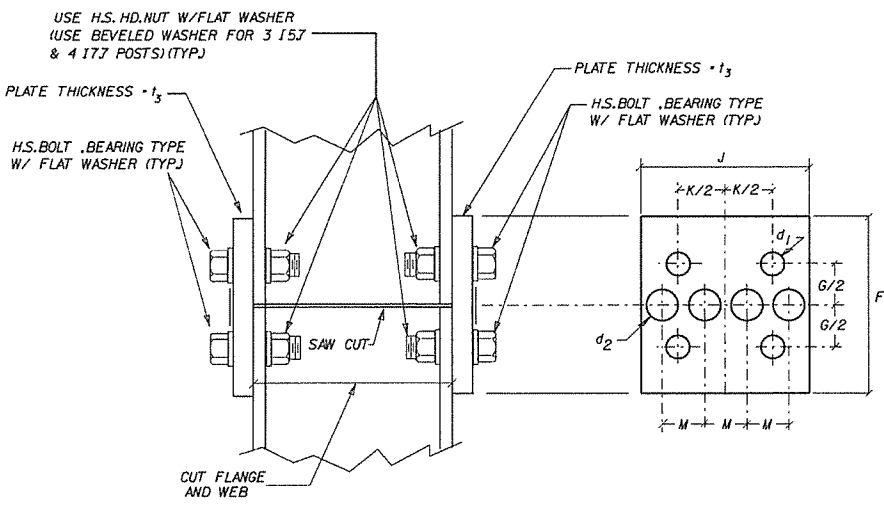
ARKANSAS STATE HIGHWAY COMMISSION		
U-CANNEL POST ASSEMBLIES		
STANDARD DRAWING SHS-2		
DATE	REVISION	FILMED
9-12-13	REVISED U-2(3), U-2(6), U-3(I), DETAIL D; ADDED DETAILS E & F; ADDED TYPICAL MARKERS	
10-9-03	REMOVED ROUND POST & REVISED SPACING	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL	6-8-95
2-2-95	REDRAWN	2-2-95

POST SIZE	BASE CONNECTION DATA												FUSE PLATE DATA										WT. OF EACH FUSE PLATE LBS.
	BOLT SIZE	BOLT TORQUE (INCH/LBS)	A	B	C	D	E	t ₁	t ₂	W	R	F	G	J	K	M	d ₁	d ₂	t ₃	BOLT SIZE			
W 6X9																							
W 6X12	5/8" x 2 3/4"	450-680*	5"	2"	1 1/4"	2 3/4"	1 1/8"	3/4"	1/2"	1/4"	1/2"	4 1/4"	2"	4"	2 1/4"	1"	3/16"	3/4"	1/4"	1/2" x 1 1/2"	1.01		
W 6X15																							
W 8X18																							
W 8X21																							
W 10X22	3/4" x 3 1/2"	150-1050*	6"	2 1/4"	1 3/8"	3 1/2"	1 1/4"	1"	3/4"	3/8"	1/2"	6"	3"	5 1/4"	2 3/4"	1 3/8"	1 1/8"	1 1/2"	3/4" x 2 1/4"	2.26			
W 10X26																							
W 12X26																							



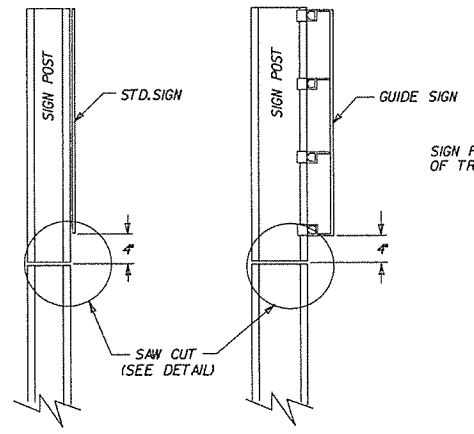
NOTE: THE OFFSET WILL BE A MINIMUM OF 10' FROM THE EDGE OF PAVED SURFACE OR AS DIRECTED BY THE ENGINEER

NOTE: SECTIONS SHOWN ARE FOR INSTALLATION ON THE RIGHT SHOULDER AND IN THE GORE. BOLT HOLES IN BASE PLATE ARE SLOTTED AND BEVELED AS SHOWN. USE H.S. BOLTS WITH HEX HD., HEX NUT AND THREE FLAT WASHERS FOR EACH BOLT. SEE TABLE FOR BOLT DIA. AND TORQUE.
 NOTE: ASSEMBLE SIGN POST TO STUB POST USING THE BOLTS SPEC. IN THE TABLE AND AS SHOWN IN THE ELEVATION DETAILS. THERE SHALL BE THREE FLAT WASHERS ON EACH BOLT LOCATED AS SHOWN IN THE ELEVATIONS. USE A SHIM TO PLUMB THE SIGN POST, THEN TIGHTEN THE BOLTS USING A 12\"/>

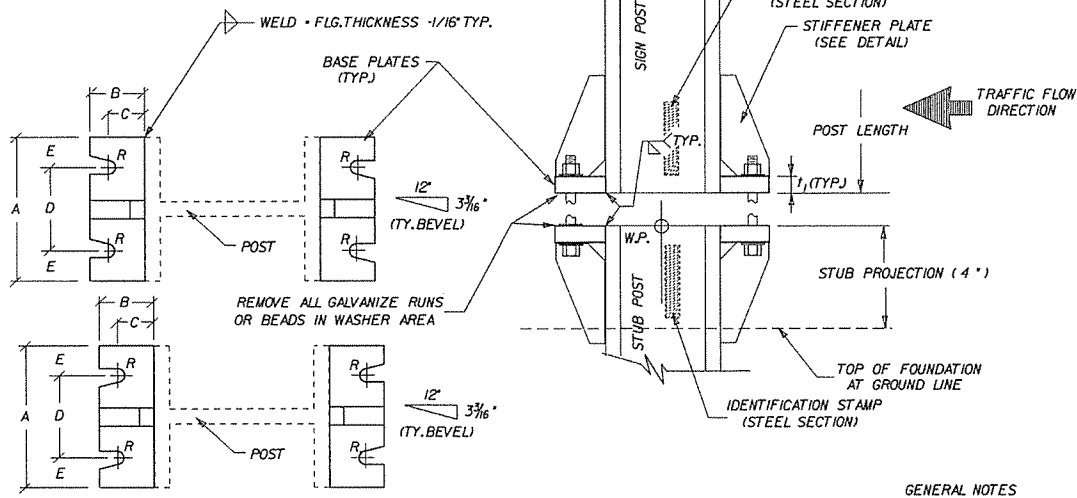


NOTE: USE H.S. HEX HEAD BOLTS, HEX HEAD NUTS AND BEVEL OR FLAT WASHERS (WHERE REQ.) UNDER NUTS. ALL HOLES SHALL BE DRILLED. ALL PLATE CUTS SHALL PREFERABLY BE SAW CUTS. HOWEVER FLAME CUTTING WILL BE PERMITTED PROVIDED ALL EDGES ARE GROUND. METAL PROJECTING BEYOND THE PLANE OF THE PLATE FACE WILL NOT BE PERMITTED. STEEL FUSE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM-A36, ASTM-A441, ASTM-572 GRADE 50, OR ASTM-A588 MAY BE SUBSTITUTED FOR A36 AT THE OPTION OF THE FABRICATOR. STEEL USED SHALL HAVE AN ULTIMATE TENSILE STRENGTH NOT TO EXCEED 80 KSI.

NOTE: BOLT HOLES, USED IN THE MOUNTING OF STANDARD SIGNS SHALL BE LOCATED IN THE FLANGE ADJACENT TO THE NEAR EDGE OF PAVEMENT FOR SINGLE POST ASSEMBLIES AND IN THE OUTSIDE FLANGES FOR MULTIPLE POST ASSEMBLIES.



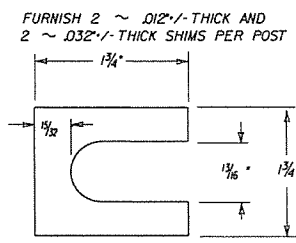
STANDARD SIGNS
GUIDE SIGNS
FUSE PLATE DETAILS



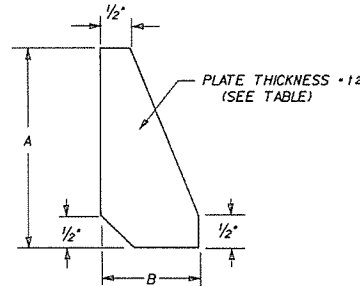
GENERAL NOTES
TIGHTEN THE HIGH STRENGTH BOLTS IN THE BASE CONNECTION ONLY TO THE TORQUE SHOWN. DO NOT OVERTIGHTEN.

BASE PLATES AND STIFFENER PLATES SHALL BE OF THE SAME MATERIAL AS THE PRIMARY SUPPORT POSTS WHICH THEY ARE WELDED.

REFER TO THE PLANS FOR FOOTING DIMENSIONS.
EACH STUB POST AND SIGN POST SHALL HAVE A PERMANENT IDENTIFYING STAMP WHICH SPECIFIES THE STEEL SECTION USED. IF THE CONTRACTOR ELECTS TO SHIP THE STUB POST SEPARATE FROM THE SIGN POST A MATCH MARK SYSTEM WILL BE REQUIRED.



SHIM DETAIL

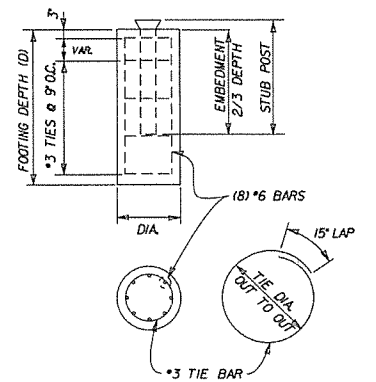


STIFFENER PLATE DETAIL

STEEL SCHEDULE

FOOTING DIAMETER	#3 TIE BARS		POUNDS
	DIAMETER	BAR LENGTH	
INCHES	INCHES	FEET	
18	12	4.39	1.65
24	18	5.96	2.24
30	24	7.53	2.83
36	30	9.1	3.42

FOOTING DEPTH	#6 STRAIGHT BARS		
	BAR LENGTH	REQ'D.	POUNDS
FEET	FEET		
2.50	2.00	8	24.03
3.00	2.50	8	30.04
3.50	3.00	8	36.05
4.00	3.50	8	42.06
4.50	4.00	8	48.06
5.00	4.50	8	54.07
5.50	5.00	8	60.08
6.00	5.50	8	66.09
6.50	6.00	8	72.10
7.00	6.50	8	78.10
7.50	7.00	8	84.11
8.00	7.50	8	90.12

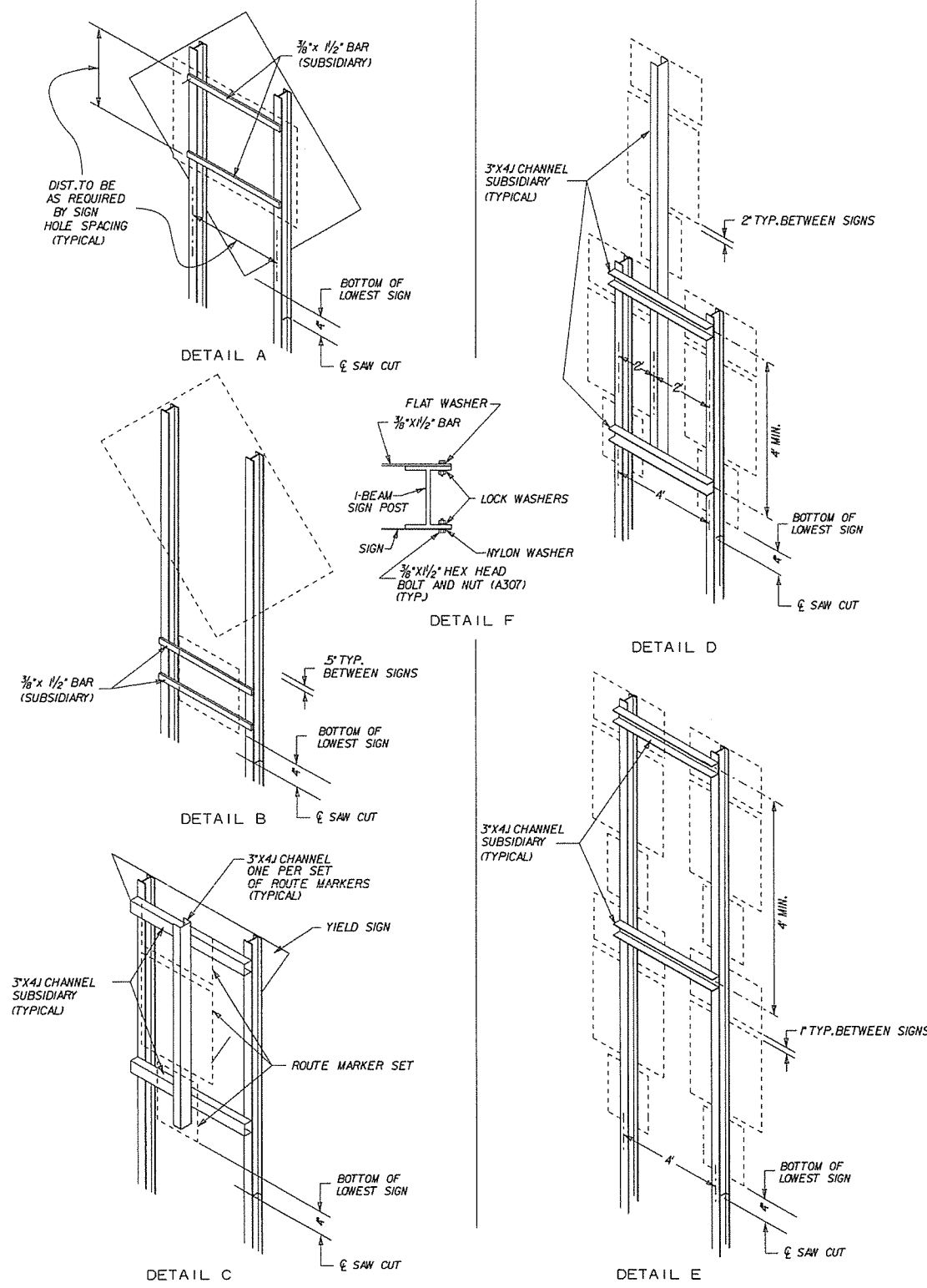


FOOTING QUANTITIES

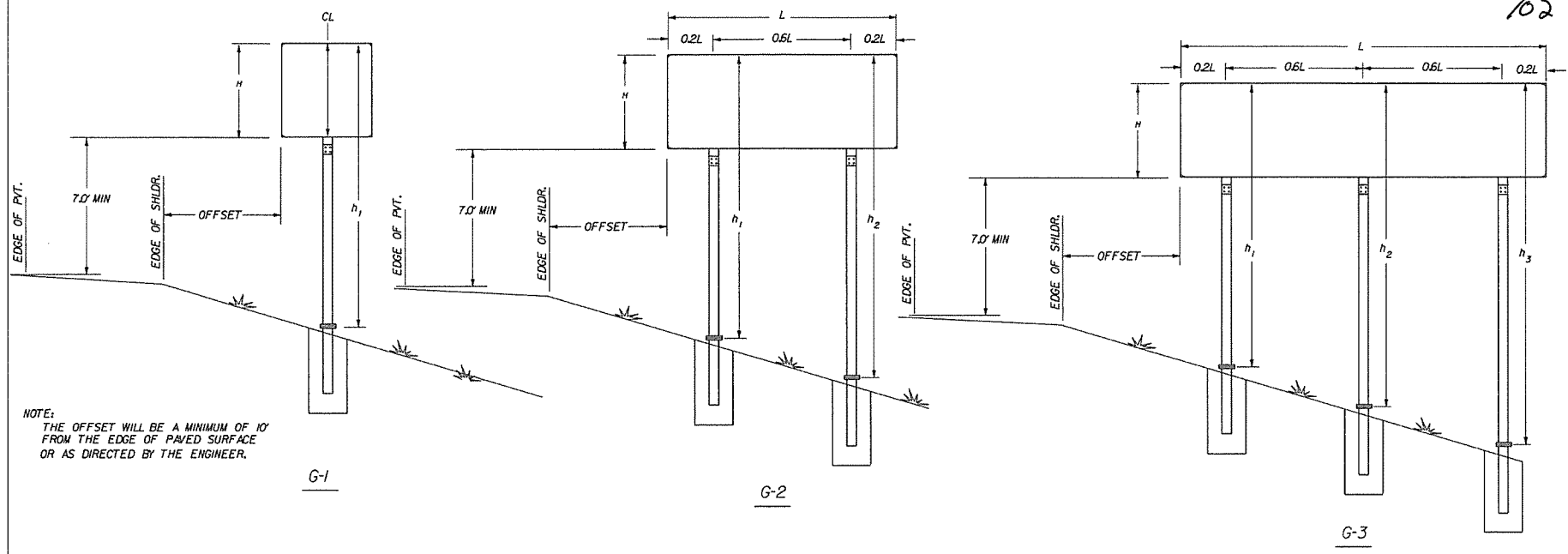
FOOTING DEPTH	NUMBER TIE BARS REQ'D	18\"/>							
		CLASS 5 CONCRETE	REINF STEEL	CLASS 5 CONCRETE	REINF STEEL	CLASS 5 CONCRETE	REINF STEEL	CLASS 5 CONCRETE	REINF STEEL
		CU. YD.	(GRADE 60)	CU. YD.	(GRADE 60)	CU. YD.	(GRADE 60)	CU. YD.	(GRADE 60)
2.50	4	0.16	31						
3.00	4	0.20	37						
3.50	5	0.23	44						
4.00	6	0.26	52	0.47	56				
4.50	6	0.29	58	0.52	62				
5.00	7	0.33	66	0.58	70	0.91	74		
5.50	8			0.64	78	1.00	83		
6.00	8			0.70	84	1.09	89	1.57	93
6.50	9					1.18	98	1.70	103
7.00	10					1.27	106	1.83	112
7.50	10							1.96	118
8.00	11							2.09	128

SIGN POST AND STUB POST

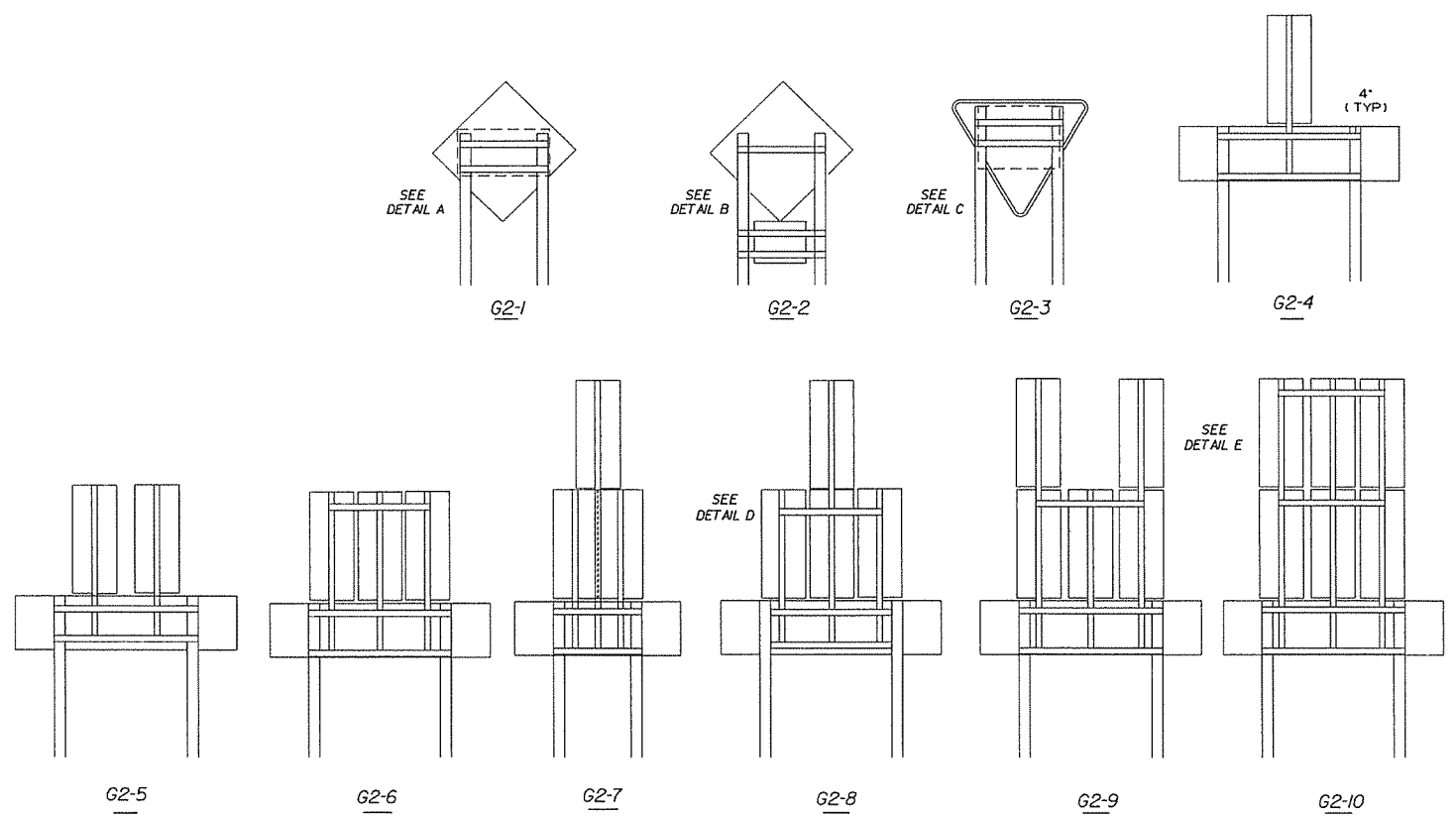
ARKANSAS STATE HIGHWAY COMMISSION		
DETAIL OF BREAKAWAY SIGN SUPPORTS FOR GUIDE SIGNS		
STANDARD DRAWING SHS-3		
9-12-13	ISSUED	
DATE	REVISION	FILED



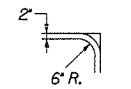
NOTE
 ALL ADDITIONAL MOUNTING HARDWARE, BOLTS, NUTS, CHANNELS AND BAR STRAPS REQUIRED TO MOUNT SECONDARY SIGNS WILL BE CONSIDERED TO BE SUPPLEMENTAL TO THE MAIN SIGN SUPPORT SPECIFIED. PAYMENT WILL BE CONSIDERED SUBSIDIARY TO THE MAIN SUPPORT.
 THE GALVANIZED STEEL CHANNEL AND BAR SUPPORTS MAY BE ASTM A-36.
 REFER TO THE P.C. RUTLEDGE FORMULA ON PAGE 58 OF THE AASHTO PUBLICATION "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS."
 ALL BOLT HOLES SHALL BE 1/8" DIA. UNLESS OTHERWISE SHOWN.



NOTE:
 THE OFFSET WILL BE A MINIMUM OF 10' FROM THE EDGE OF PAVED SURFACE OR AS DIRECTED BY THE ENGINEER.

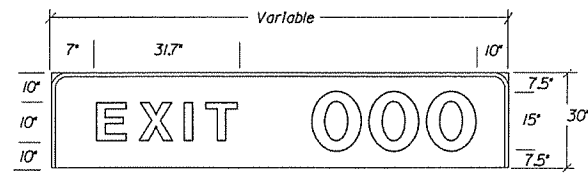


ARKANSAS STATE HIGHWAY COMMISSION		
DETAIL OF BREAKAWAY SIGN SUPPORTS FOR STANDARD SIGNS		
STANDARD DRAWING SHS-4		
9-12-13	ISSUED	
DATE	REVISION	FILMED



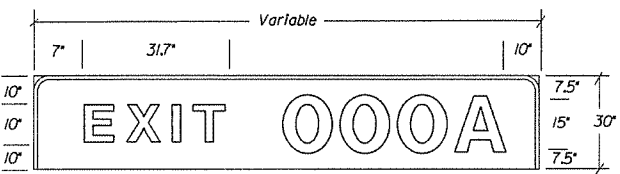
TYPICAL DETAIL

TYPE A



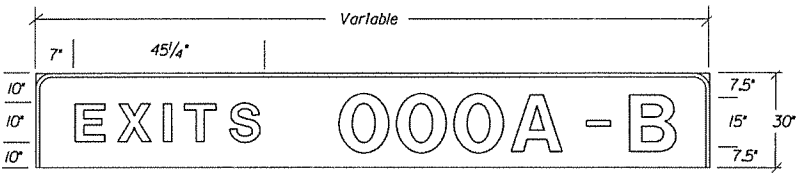
EXIT WITH 1 DIGIT 84"X30"-17.50 SF
 EXIT WITH 2 DIGITS 96"X30"-20.0 SF
 EXIT WITH 3 DIGITS 114"X30"-23.57 SF

TYPE B



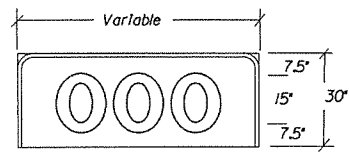
EXIT WITH 1 DIGIT PLUS "A"OR"B" 96"X30"-20.0 SF
 EXIT WITH 2 DIGITS PLUS "A"OR"B" 114"X30"-23.57 SF
 EXIT WITH 3 DIGITS PLUS "A"OR"B" 126"X30"-26.25 SF

TYPE C



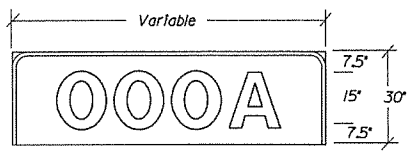
EXITS WITH 1 DIGIT PLUS "A"OR"B" 132"X30"-27.50 SF
 EXITS WITH 2 DIGITS PLUS "A"OR"B" 150"X30"-31.25 SF
 EXITS WITH 3 DIGITS PLUS "A"OR"B" 168"X30"-35.00 SF

TYPE D



1 DIGIT 24"X30"-5.0 SF
 2 DIGITS 42"X30"-8.75 SF
 3 DIGITS 60"X30"-12.50 SF

TYPE E

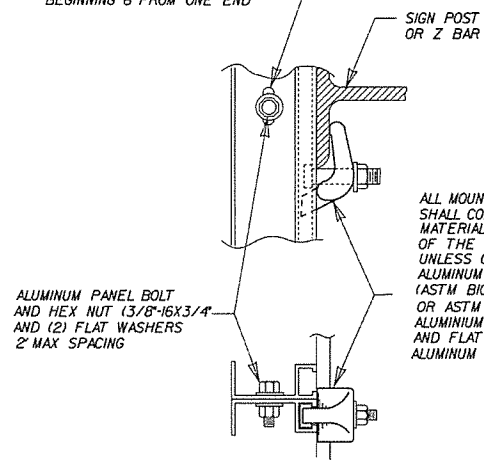


1 DIGIT PLUS "A"OR"B" 42"X30"-8.75 SF
 2 DIGITS PLUS "A"OR"B" 60"X30"-12.50 SF
 3 DIGITS PLUS "A"OR"B" 78"X30"-16.25 SF

EXIT PANEL DETAILS

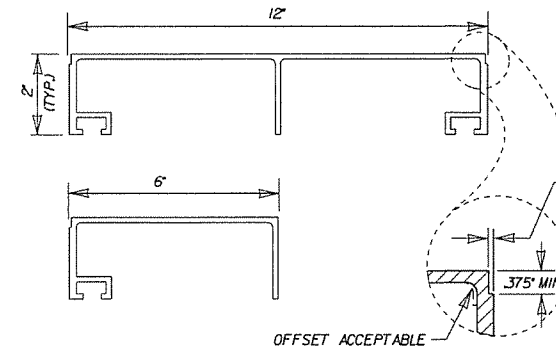
NOTE: EXIT NUMBER PANELS SHALL HAVE WHITE LEGENDS AND BORDERS. THE BACK GROUND COLOR WILL BE AS USE SPECIFIES. SHEETING TYPE WILL BE THE SAME AS THE GUIDE SIGN WHICH THE EXIT PANEL IS ATTACHED OR AS SPECIFIED IN THE PLANS. PAYMENT FOR ALL POST CLIPS, BOLTS, AND ANGLES SHALL BE SUBSIDIARY TO THE ITEM "EXIT NUMBER PANEL".

SLOTTED HOLES (7/16" X 7/8")
 DRILLED OR PUNCHED @ 12" O.C.
 BEGINNING 6" FROM ONE END



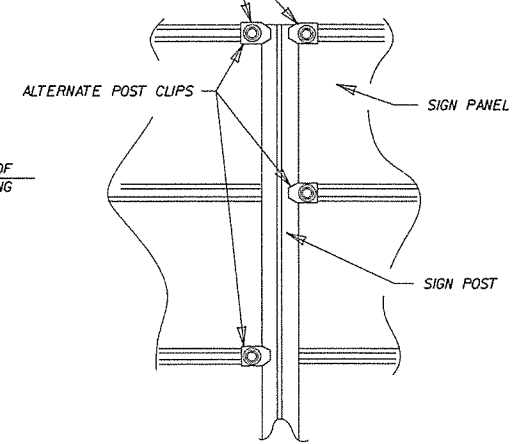
ALL MOUNTING HARDWARE SHALL COMPLY WITH THE MATERIALS SECTION OF 724 OF THE STANDARD SPECIFICATIONS UNLESS OTHERWISE SPECIFIED.
 ALUMINUM POST CLIP (ASTM B108 ALLOY 356-T6) OR ASTM B25 ALLOY 356-T6)
 ALUMINUM POST CLIP BOLT AND FLAT WASHER (3/8"-16X1 1/4")
 ALUMINUM STOP NUT

ALUMINUM PANEL BOLT AND HEX NUT (3/8"-16X3/4") AND (2) FLAT WASHERS 2" MAX SPACING

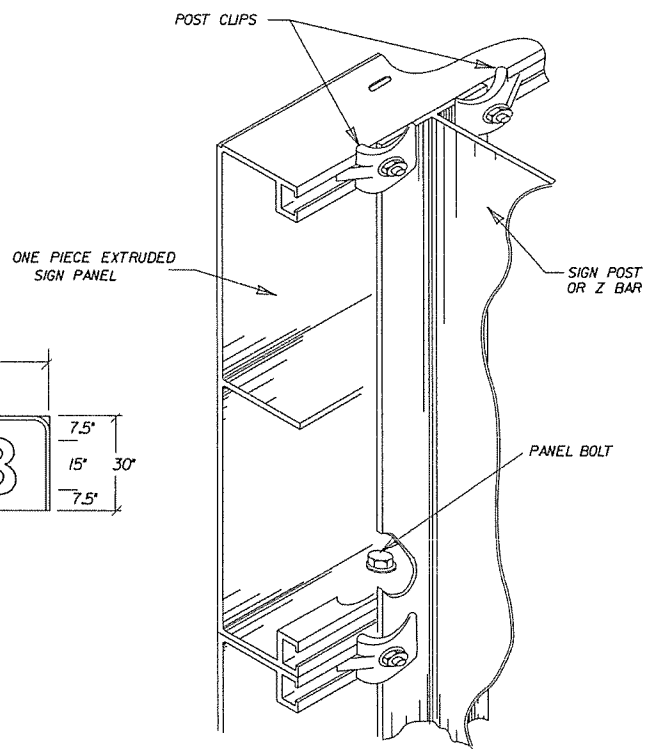


ONE PIECE EXTRUDED SIGN PANELS

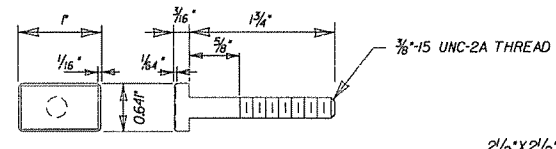
USE DOUBLE POST CLIPS AT TOP AND BOTTOM OF SIGN



POST CLIP PLACEMENT

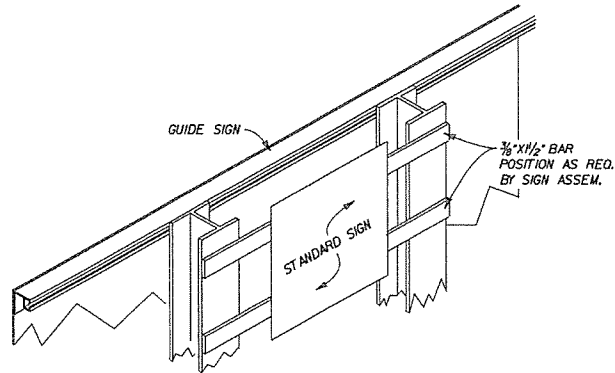
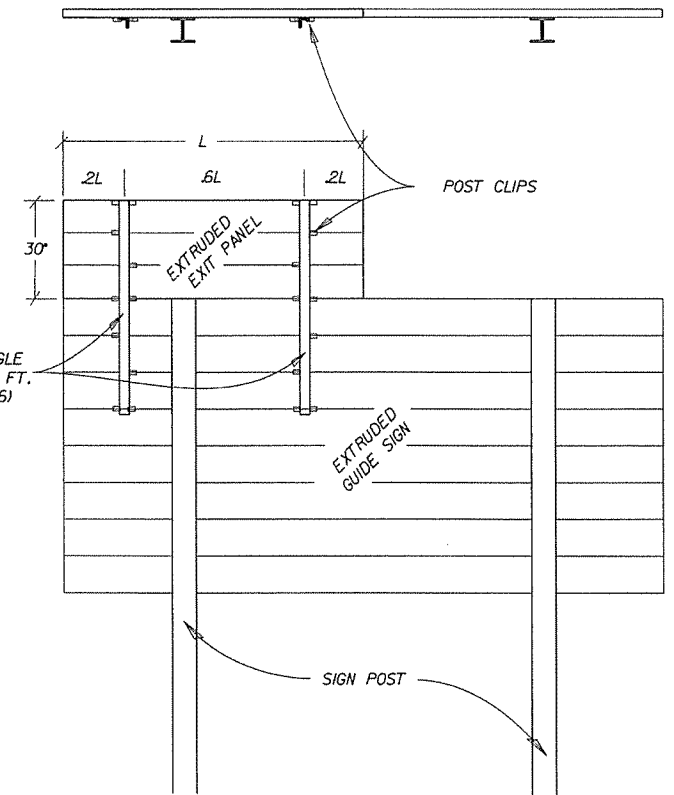


MOUNTING HARDWARE



POST CLIP BOLT

2 1/2" X 2 1/2" X 1/4" ANGLE 5'-8" LONG 1.4" PER FT. (ALUM. ALLOY 6061-T6)

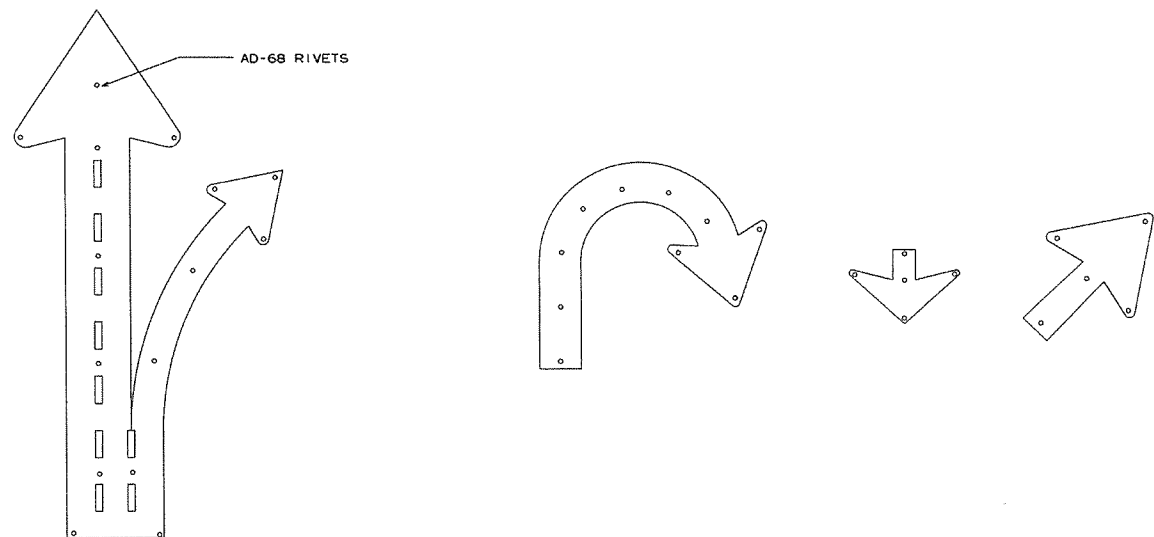
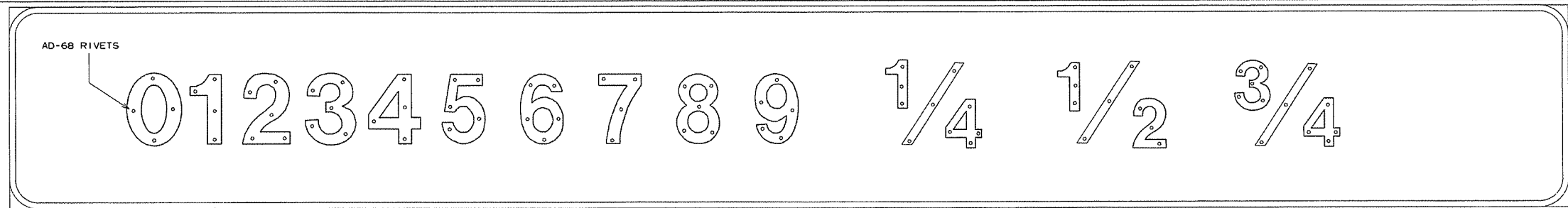
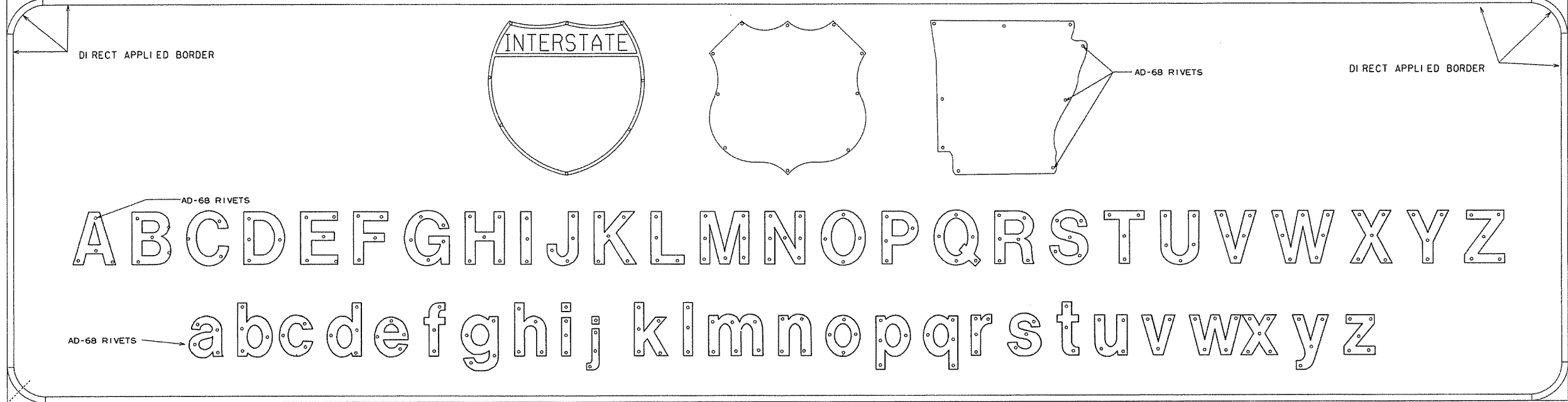


SECONDARY SIGN INSTALLATION ON BACKSIDE OF GUIDE SIGN

		ARKANSAS STATE HIGHWAY COMMISSION	
		DETAILS OF GUIDE SIGN PANELS	
		STANDARD DRAWING SHS-5	
9-12-13	ISSUED	REVISION	FILMED
DATE			

THE CONTRACTOR SHALL DRILL AND POP-RIVET LEGEND, SHIELDS, ARROWS, OR OTHER COPY AS SHOWN.

MOUNTING DETAILS FOR DEMOUNTABLE
LEGEND ON GUIDE SIGNS



NOTES:

LEGEND ON GUIDE SIGNS ON THE MAIN LANES SHALL BE DEMOUNTABLE LEGEND.
LEGEND ON GUIDE SIGNS ON CROSS ROADS AND RAMPS SHALL BE DIRECT APPLIED.
THE DEMOUNTABLE AND DIRECT APPLIED LEGENDS SHALL BE TYPE IX SHEETING.

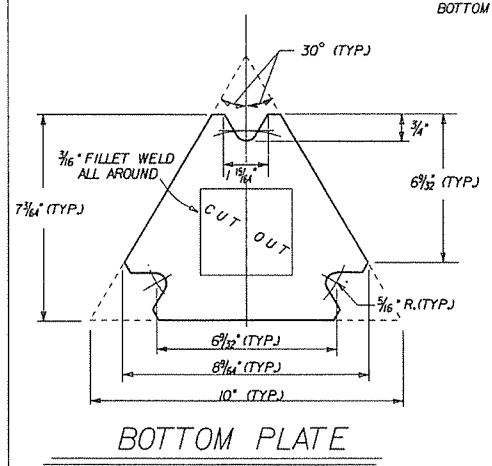
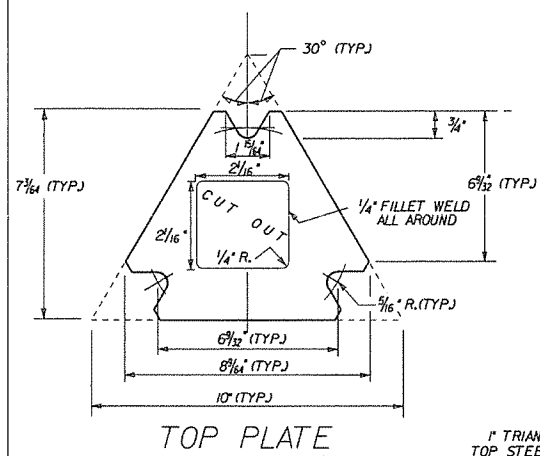
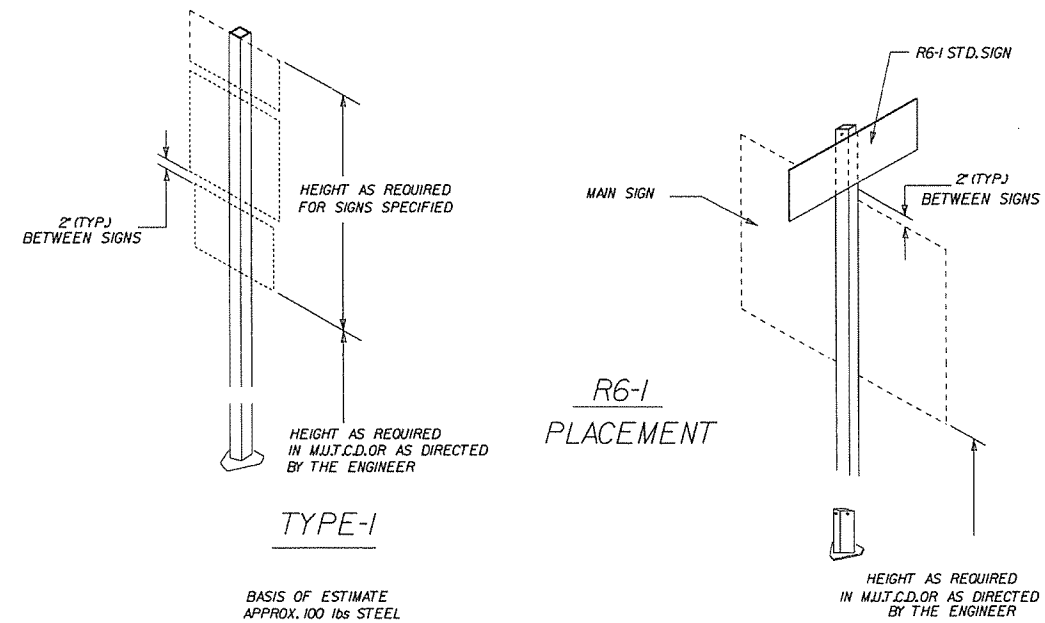
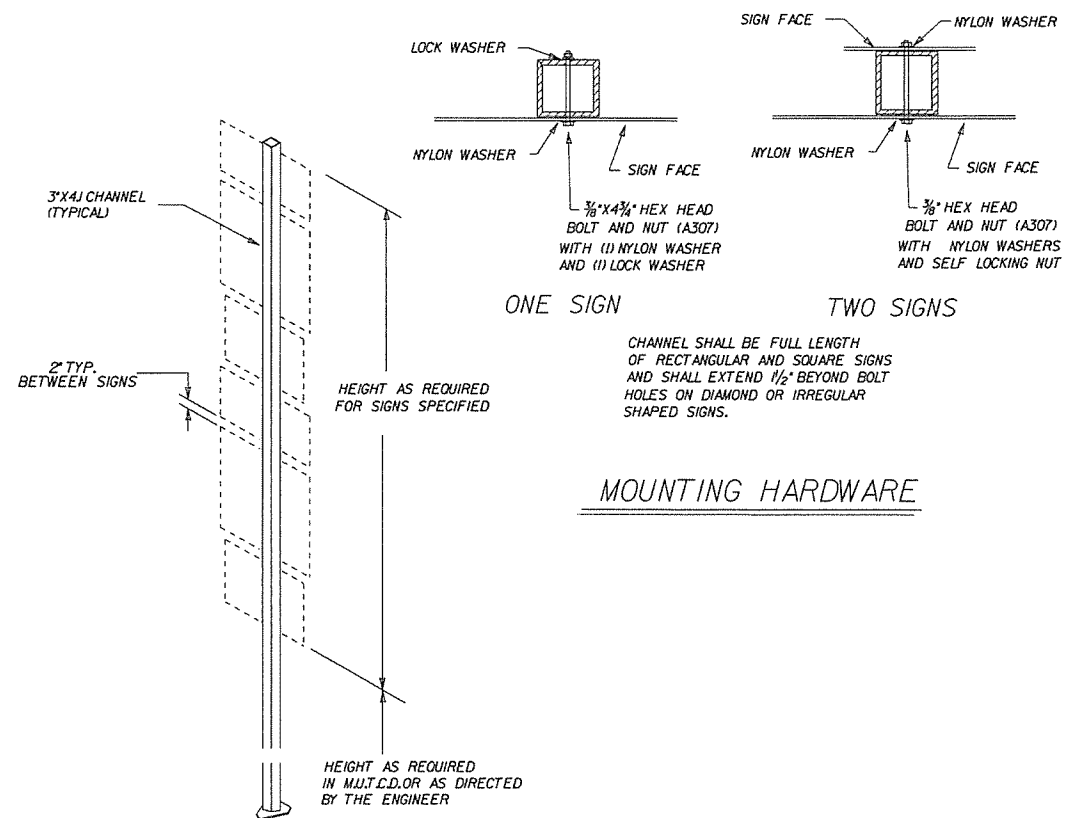
THE BACKGROUND ON ALL GUIDE SIGNS AND STANDARD SIGNS SHALL BE CONSTRUCTED USING TYPE III SHEETING.

TYPE IX SHEETING FOR BORDER, LEGEND, SHIELDS, ARROWS, OR OTHER COPY SHALL BE ORIENTED VERTICALLY AS PER MANUFACTURERS' DATUM MARKS, ORIENTATION MARKS, OR OTHER RECOMMENDATIONS.

SIGN LEGEND, SHIELDS, ARROWS OR OTHER COPY SHALL BE APPLIED WITH RIVETS ONLY.

NO OTHER METHOD OF APPLYING CHARACTERS IS ALLOWED.

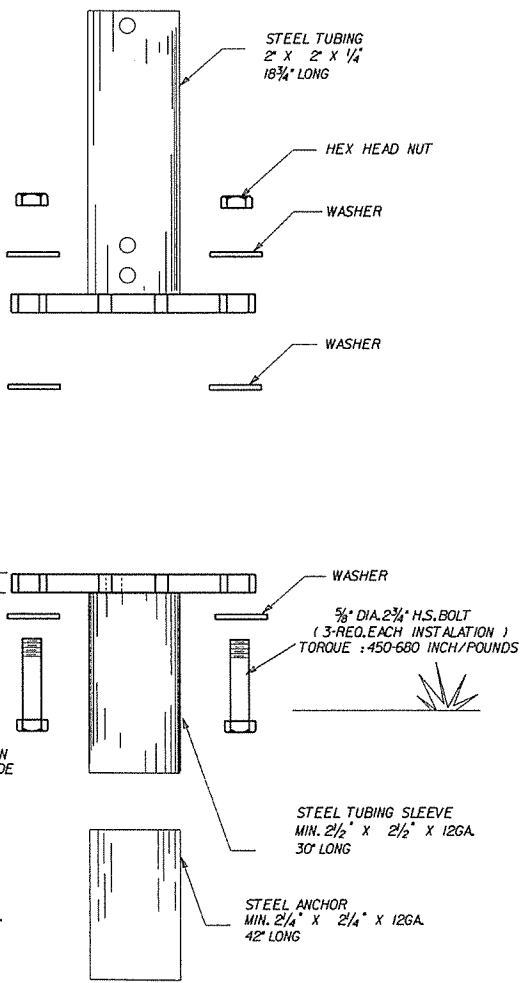
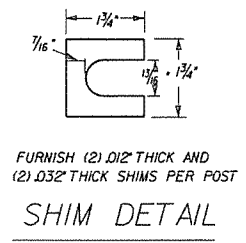
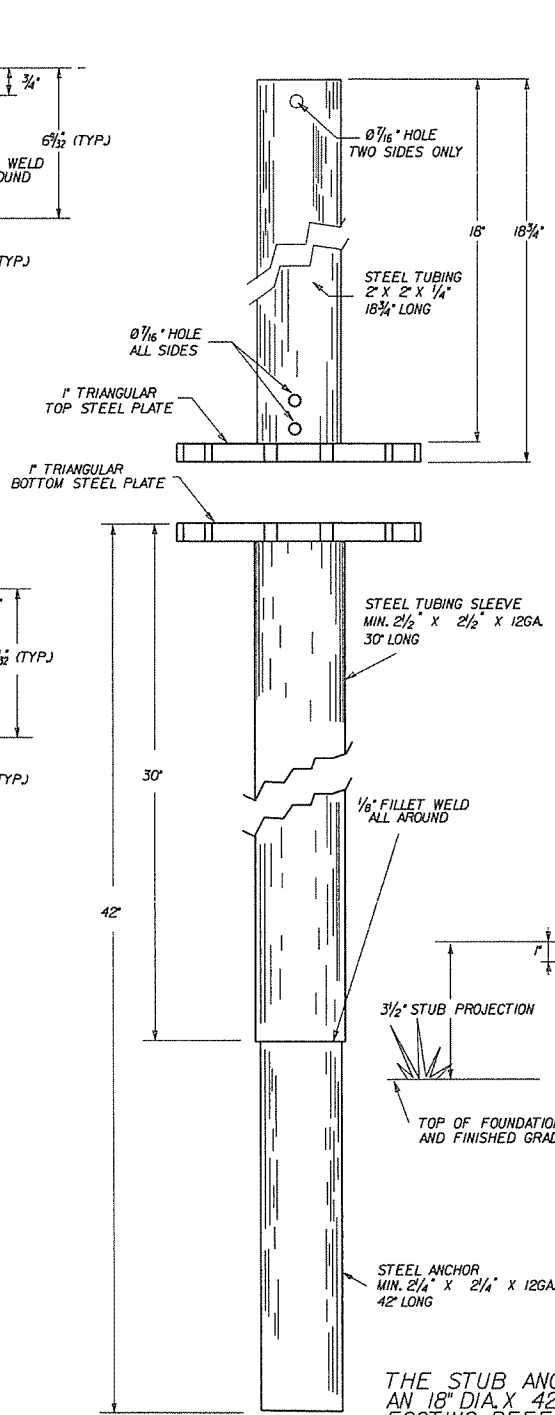
			ARKANSAS STATE HIGHWAY COMMISSION
			MOUNTING DETAILS FOR DEMOUNTABLE LEGEND ON GUIDE SIGNS
			STANDARD DRAWING SHS-6
9-12-13 DATE	ISSUED	REVISION	FILMED



GENERAL NOTES:
 THE TOP PLATE OF TRIANGULAR SUP BASES SHALL HAVE THE SAME EXTERIOR DIMENSIONS AS THE BOTTOM PLATE.


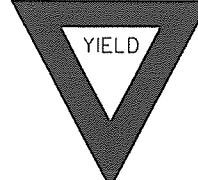

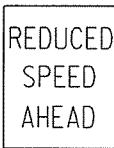

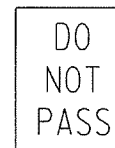

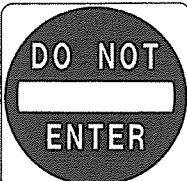

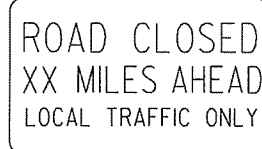
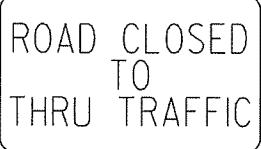
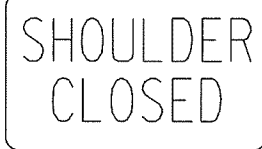
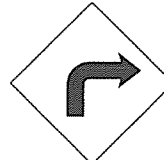
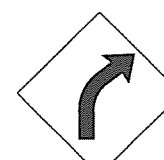
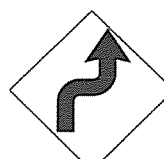
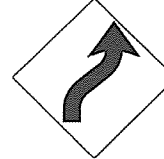
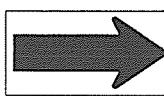
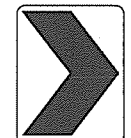
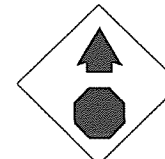
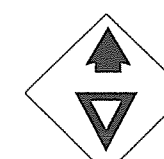
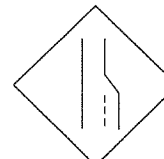

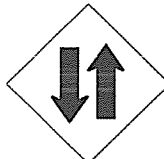

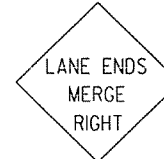








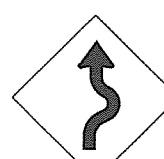
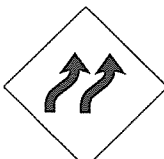

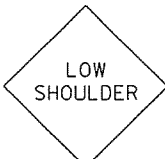
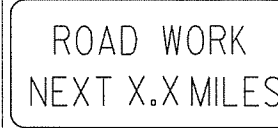
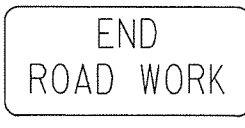
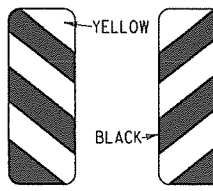
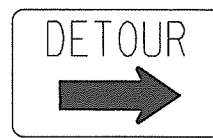


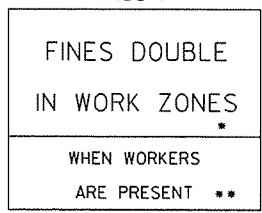
INSIDE DIAMETER OF THE SIGN POST SHALL BE CUT THROUGH THE CENTER OF THE TOP PLATE WITH THE HOLE EDGE BEVELED AS SHOWN. THE BEVEL END SHALL BE TANGENT TO THE BOLT HOLE. ANY MISALIGNMENT SHALL BE REMOVED BY GRINDING. FACE OF BEVEL SHALL BE FINISHED TO A MINIMUM SMOOTHNESS OF 1-500.

OTHER WASH COMPLIANT BREAKAWAY SIGN SUPPORTS THAT HAVE THE SAME TOP PLATE DIMENSIONS AND SUPPORT 2" x 2" SQUARE TUBE SIGN POSTS MAY BE SUBSTITUTED AS APPROVED BY THE ENGINEER.



THE STUB ANCHOR SHALL BE SET IN AN 18" DIA. X 42" DEEP CONCRETE FOOTING. REFER TO STD. DRWG. SHS-3 FOR THE FOOTING DETAILS.

ARKANSAS STATE HIGHWAY COMMISSION		
DETAIL OF OMNI-DIRECTIONAL BREAKAWAY SIGN SUPPORTS		
9-12-13	ISSUED	STANDARD DRAWING SHS-7
DATE	REVISION	FILMED

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>18" 500 FEET W16-2 24"</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>
<p>W8-II</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9.</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>
						<p>R56-1</p>  <p>STD. 18"x18"</p>
						<p>R55-1</p>  <p>36"x60"</p> <p>* USE 6" C LETTERS ** USE 4" D LETTERS</p>

ADVANCE DISTANCES (XXXX)

500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

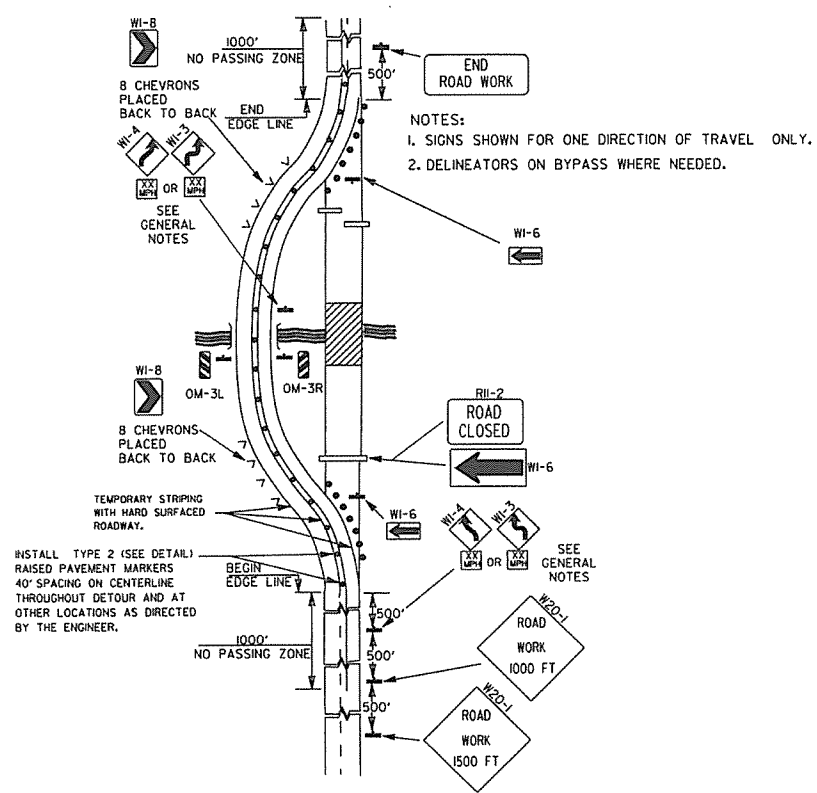
GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.

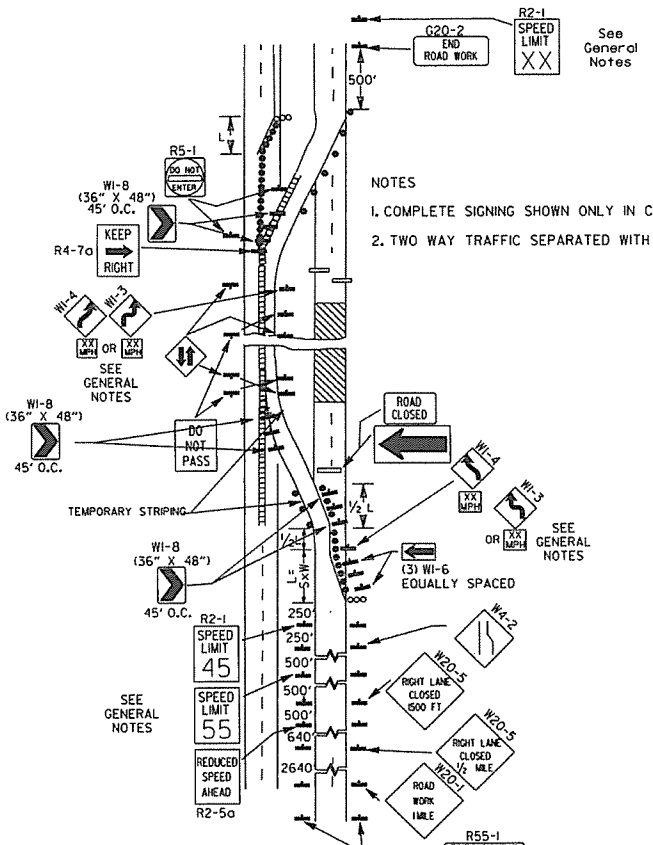
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

* NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

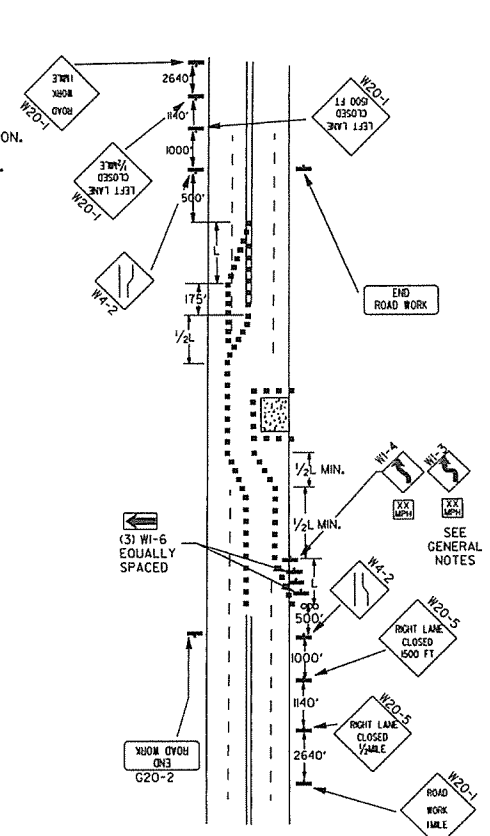
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED



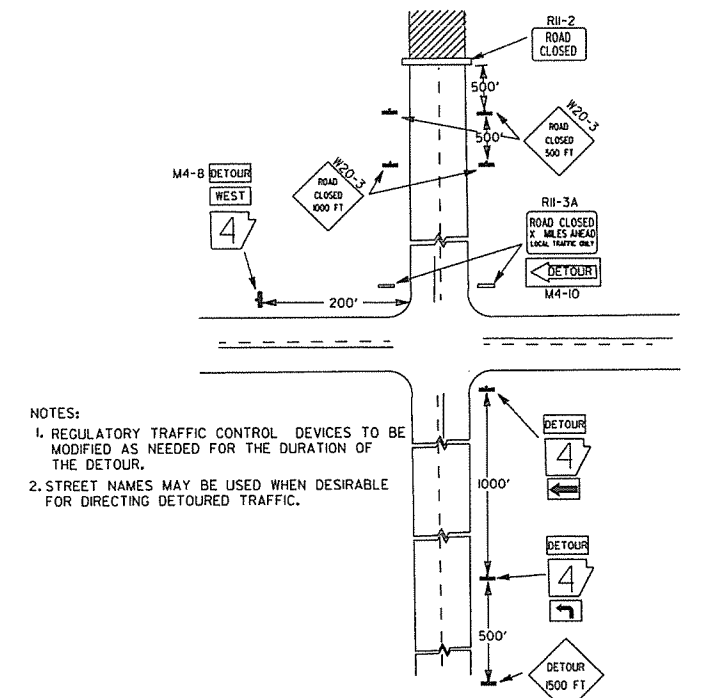
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



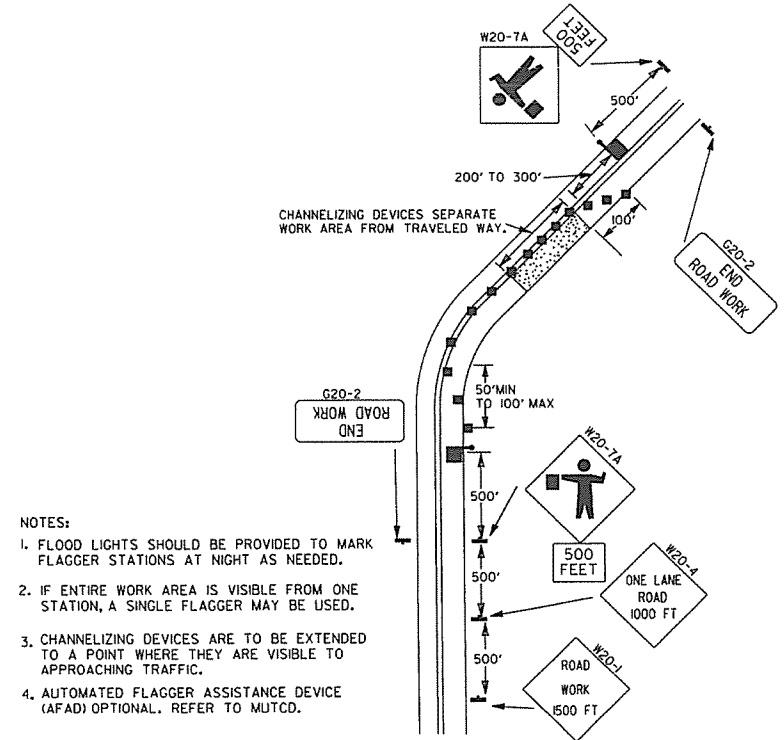
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



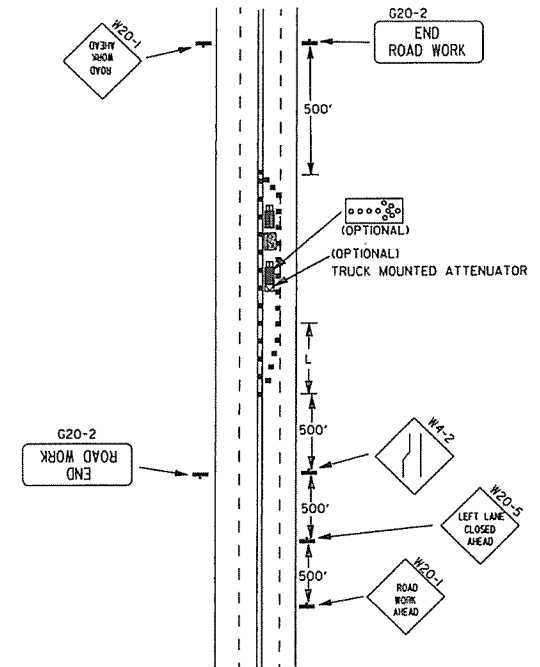
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

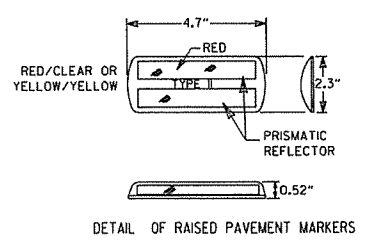


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:
- FLAGGER
 - POSITIVE BARRIER
 - ARROW PANEL (IF REQUIRED)
 - TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



TYPICAL ADVANCE WARNING SIGN PLACEMENT

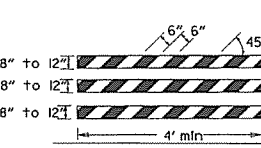
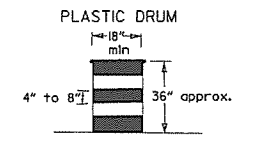
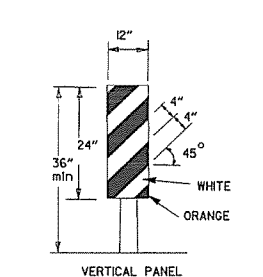
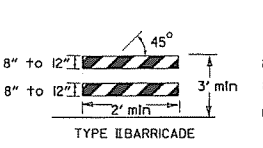
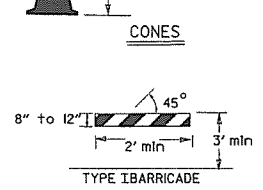
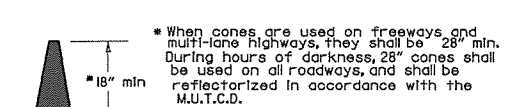
TAPER FORMULAE:
 L= SXW FOR SPEEDS OF 45MPH OR MORE.
 L= $\frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
 WHERE:
 L= MINIMUM LENGTH OF TAPER.
 S= NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
 W= WIDTH OF OFFSET.

- GENERAL NOTES:
1. ADVISORY SPEED POSTED ON WI-3 OR WI-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE WI-4 WHEN SPEED IS GREATER THAN 30MPH AND WI-3 WHEN 30MPH OR LESS.
 2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-(W55) SHALL BE OMITTED AND THE R2-5A SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(IXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-(W65) SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(IXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.

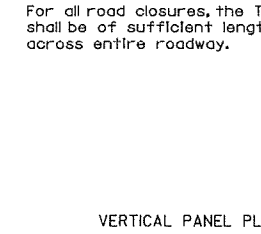
DATE	REVISION	FILED
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON WI-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION
 STANDARD TRAFFIC CONTROLS
 FOR HIGHWAY CONSTRUCTION
 STANDARD DRAWING TC-2

Channelizing devices



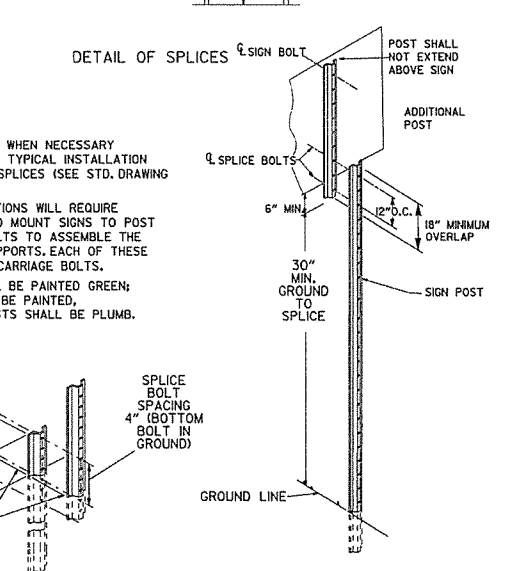
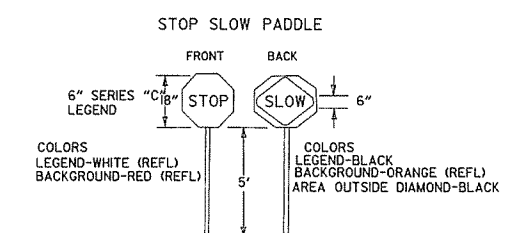
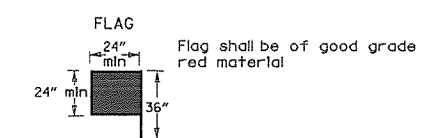
NOTE: For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-lane vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

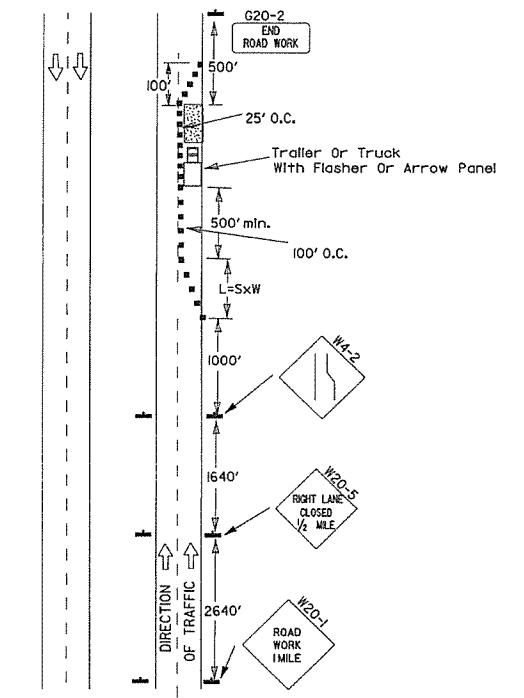
* When shown on the plans concrete barrier will be used.
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



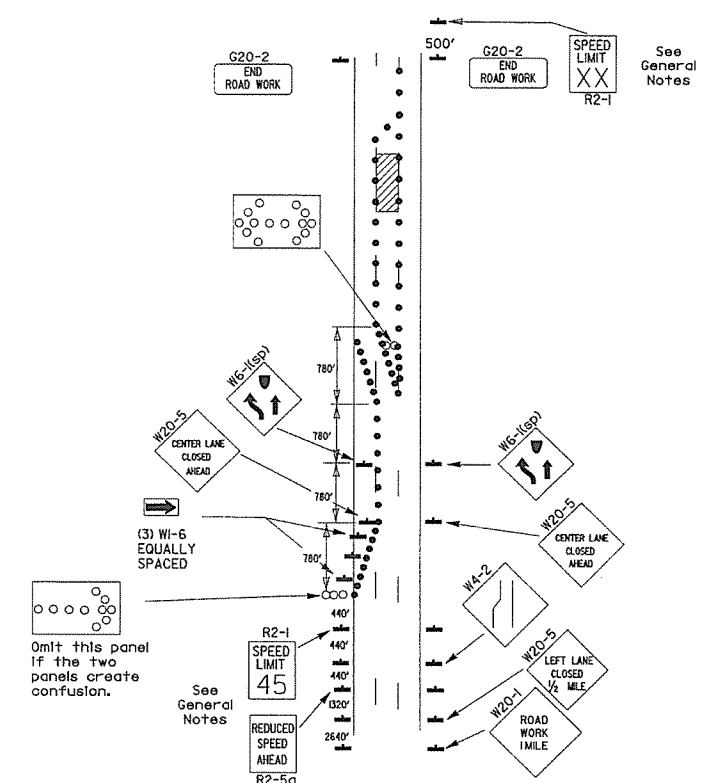
NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2)
NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.
SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.

DATE	REVISION	FILMED
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-3



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

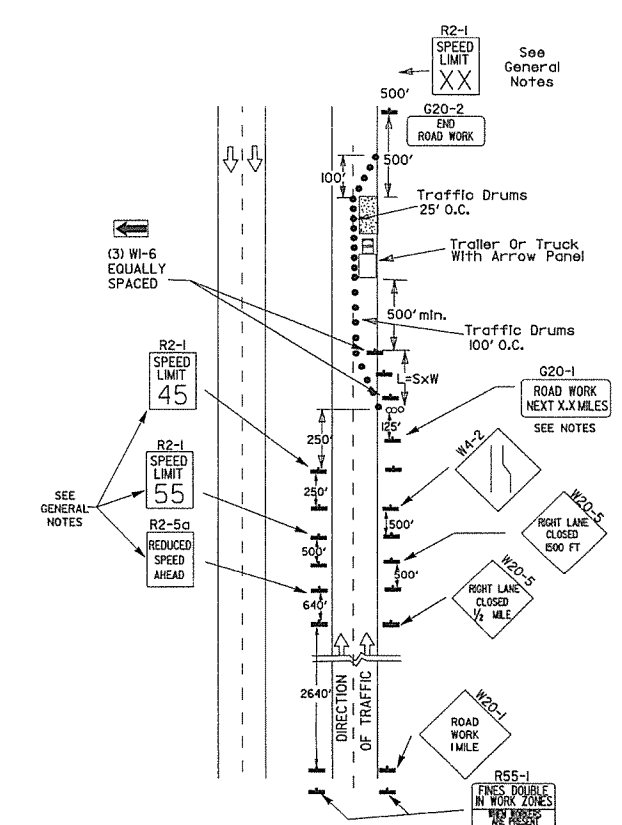


(B) Typical application - 3-lane oneway roadway where center lane is closed.

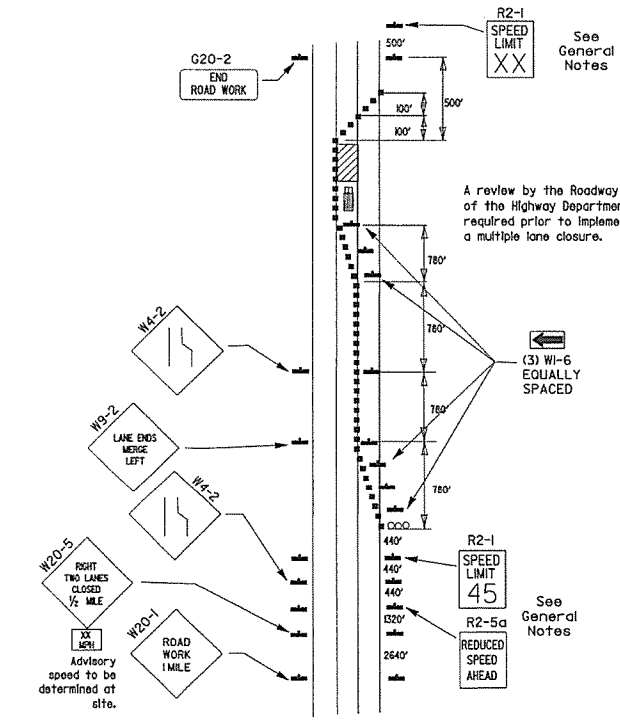
- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

GENERAL NOTES:

- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
- When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
- The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
- Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
- Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
- The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1 (1/2 MILE) signs are not required in advance of lane closures that begin inside the project limits.
- Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
- All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual for Assessing Safety Hardware (MASH).
- Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

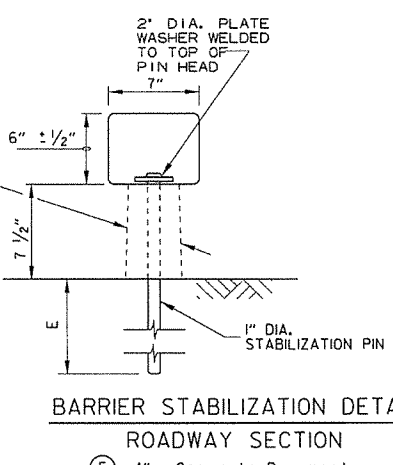
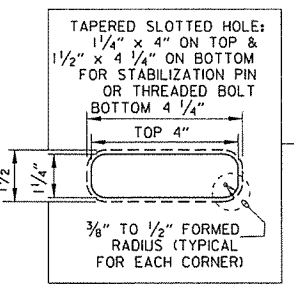
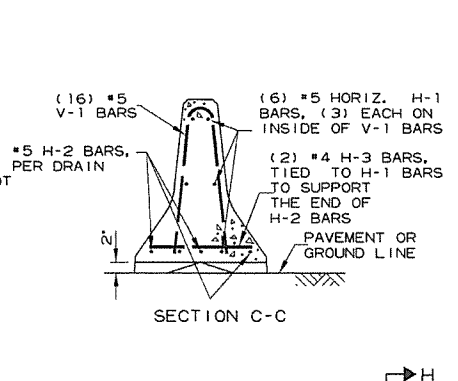
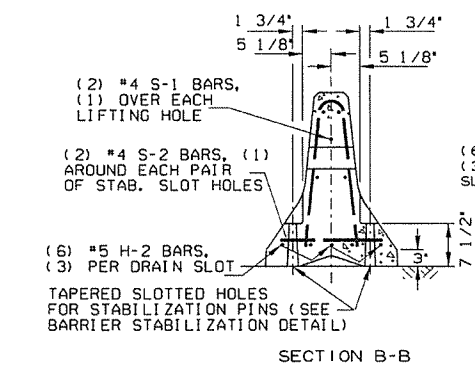
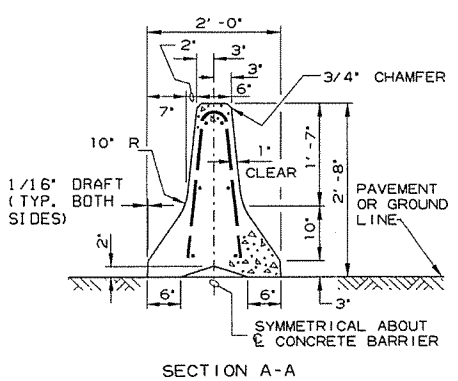
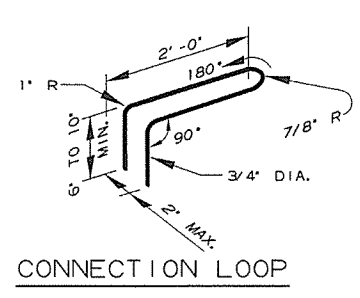
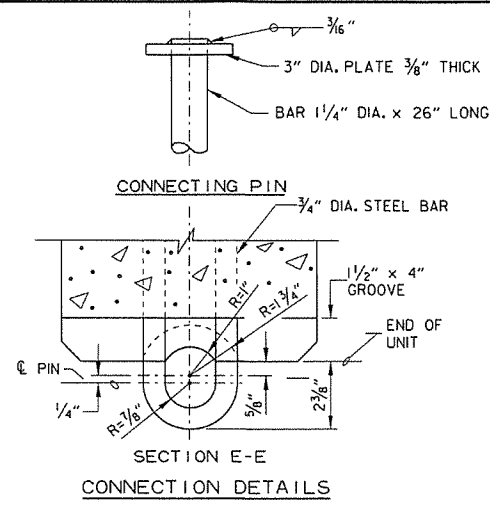


(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

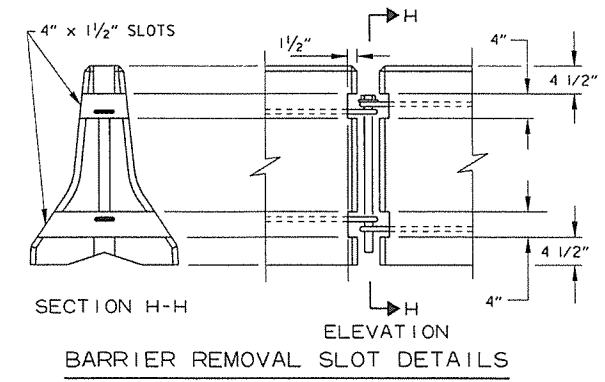


(D) Typical application - closing multiple lanes of a multilane highway.

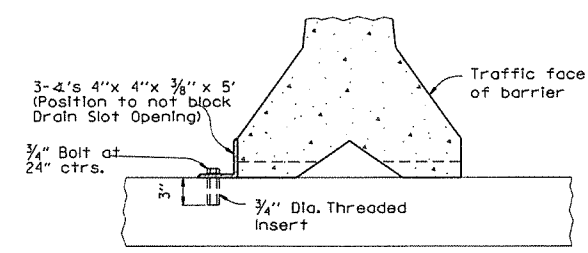
REINFORCING BAR TABLE PER BARRIER UNIT			
MARK	LOCATION	BAR SIZE	(NO. BARS)
H-1	HORIZONTAL IN BARRIER TIED INSIDE V-1 BARS	#5	(6)
H-2	CENTERED ABOVE DRAIN SLOTS LONG. & TRANSVERSELY	#5	(6)
H-3	TIED ABOVE H-1 BARS TO SUPPORT H-2, TIED TO V-1	#4	(2)
S-1	OVER LIFT HOLES	#4	(2)
S-2	HORIZ. AROUND SLOTS BETWEEN V-1'S & DRAIN SLOTS	#4	(2)
V-1	VERTICAL IN BARRIER (3) EACH END & (2) AT EACH DRAIN SLOTS	#5	(16)



BARRIER STABILIZATION DETAIL
ROADWAY SECTION
E 4" - Concrete Pavement
8" - Asphalt Pavement
12" - Shoulder Areas

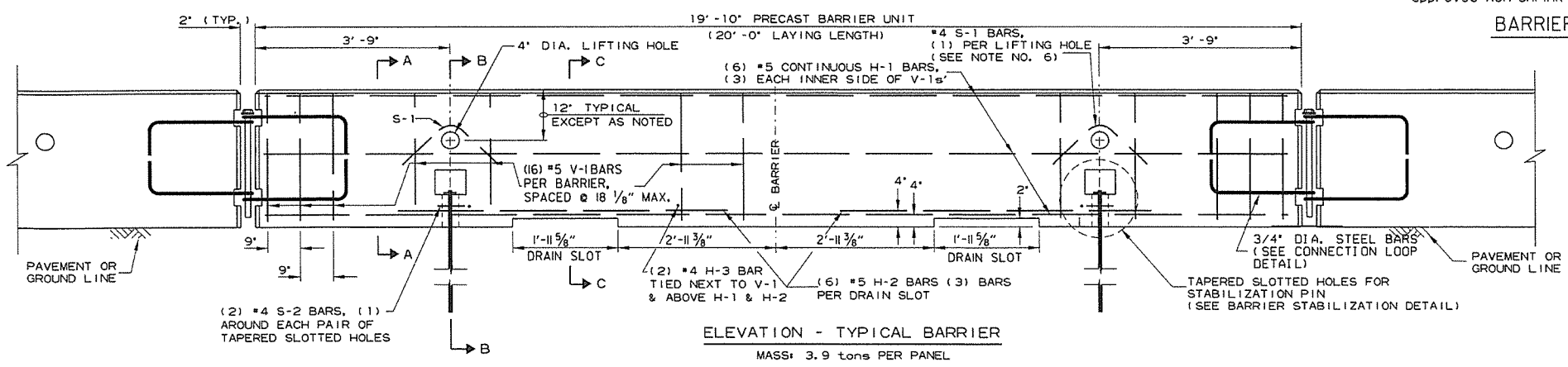


BARRIER REMOVAL SLOT DETAILS



NOTE: 3/4" Threaded Inserts shall be cast in place for all new bridge decks and drilled and grouted for existing bridge decks. Inserts shall have a minimum ultimate load capacity of 8000 lbs. in tension. After removal of barrier, bolts, and angles, the Inserts shall be filled with approved non-shrink epoxy.

BARRIER STABILIZATION DETAIL
BRIDGE DECKS



ELEVATION - TYPICAL BARRIER
MASS: 3.9 tons PER PANEL

- General Notes**
- The contractor shall furnish the Precast Concrete Barrier Units and shall be responsible for the manufacture, shipment, storage, placement and removal. At the completion of the project, the precast units will remain the property of the contractor.
 - Materials shall meet the following minimum requirements:
Concrete: 2500 psi compressive strength at 28 days.
Reinforcing Steel: AASHTO M 31 or M 53, Grade 60
Structural Steel: AASHTO-M270 Grade 36 shall be used for the Connection Pin, Connection Loops, and Stabilization Pins. A One Piece Pin with a 3" rounded top may be used in place of the detailed Connection Pin.
Delineators: Delineators shall be mounted at 10' spacing on top of precast barrier.

In applications where barrier walls within 6 feet of a traffic lane, additional delineators shall be placed on the barrier at 10' spacing approximately one (1) foot from the top of the barrier. Delineators shall be on the AHTD Qualified Products List for Construction Concrete Barrier Markers. Delineator color shall be in accordance with the Manual on Uniform Traffic Control Devices.
Payment for delineators shall be considered included in the price bid per Lin. Ft. for "Furnishing and Installing Precast Concrete Barrier". The contractor shall certify to the Engineer that the material and the design used in the precast barrier units meets the requirements as shown on this standard drawing.
 - Other Precast Concrete Barriers that have been crash tested and approved by the Federal Highway Administration to meet the requirements of NCHRP-350 test level 3 or Manual For Assessing Safety Hardware (MASH) will be accepted in lieu of the barrier shown. Drain slots shall be provided as needed or as directed by the Engineer. The Contractor shall furnish a certification of NCHRP Report 350 or Manual For Assessing Safety Hardware (MASH) compliance for any other types of precast barrier to be used. The certification shall state that the precast concrete barrier meets the requirements of NCHRP Report 350 or Manual For Assessing Safety Hardware (MASH) and include a copy of the Federal Highway Administration's (FHWA) approval letter with all attachments. Precast concrete barrier units shall be fabricated and installed in accordance with crash testing and documentation provided in the FHWA approval letter. Mixing of shapes will not be allowed in a continuous line of units.
 - Dowel holes in pavement or bridge slabs that are to remain in place shall be filled. Holes in concrete pavement and bridge slabs shall be filled with an approved non-shrink epoxy grout. Holes in asphalt pavement shall be filled with an approved asphalt joint filler. Payment for drilling and filling holes to be included in the price for various barrier items.
 - Attach Units To Roadway Surface with Stabilization Pins and to Deck Slabs using bolts when required.
 - A 4" White PVC Sleeve may be used to form the Lifting Hole and if used the Sleeve is to be left in place.

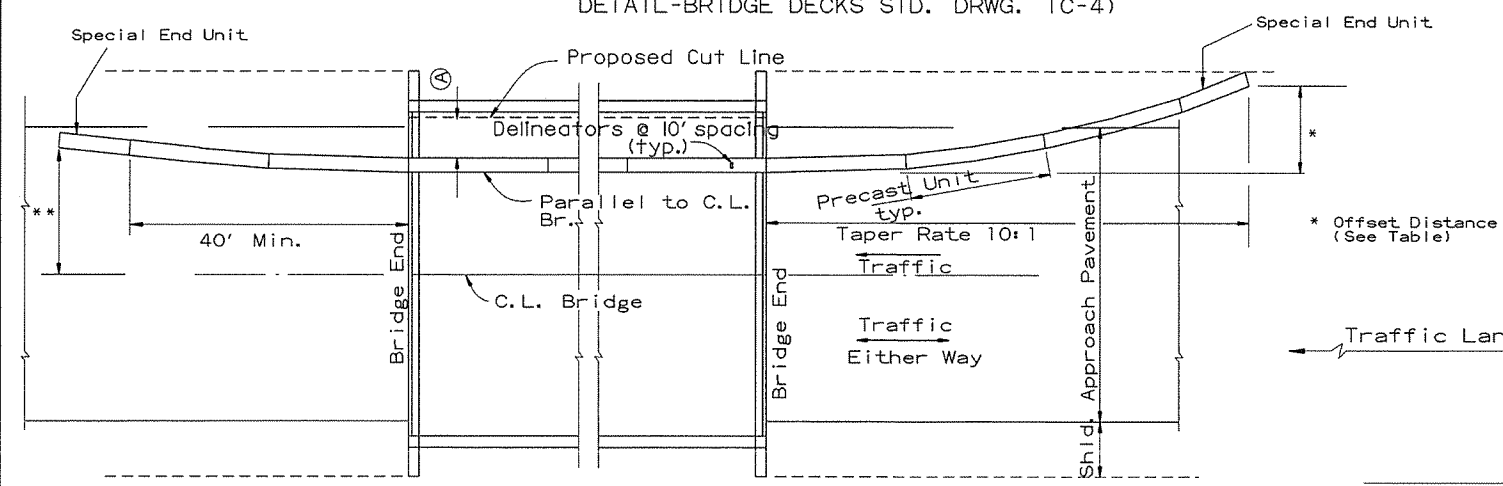
DATE	REVISION	FILMED
2-27-14	REVISED BARRIER STABILIZATION DETAIL	
10-15-09	ADDED REFERENCE TO MASH	
8-5-09	REV. NOTE 3 CONCERNING DRAIN SLOTS	
11-29-07	REVISED NOTE 3	
5-25-06	DELETED GENERAL NOTE 7	
11-18-04	REVISED BARRIER STABILIZATION DETAIL BRIDGE DECKS	
4-10-03	REVISED GENERAL NOTE 2	
8-22-02	ISSUED NEW DRAWING	

ARKANSAS STATE HIGHWAY COMMISSION

STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION -
TEMPORARY PRECAST BARRIER

STANDARD DRAWING TC-4

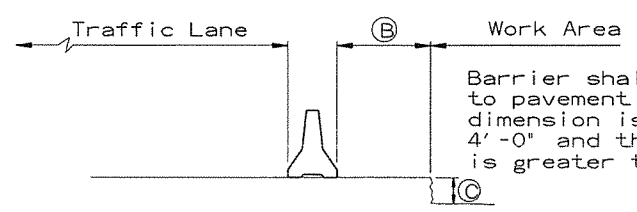
(A) 4 feet or greater preferred. If less than 4 feet, Precast Units shall be connected to slab (SEE BARRIER STABILIZATION DETAIL-BRIDGE DECKS STD. DRWG. TC-4)



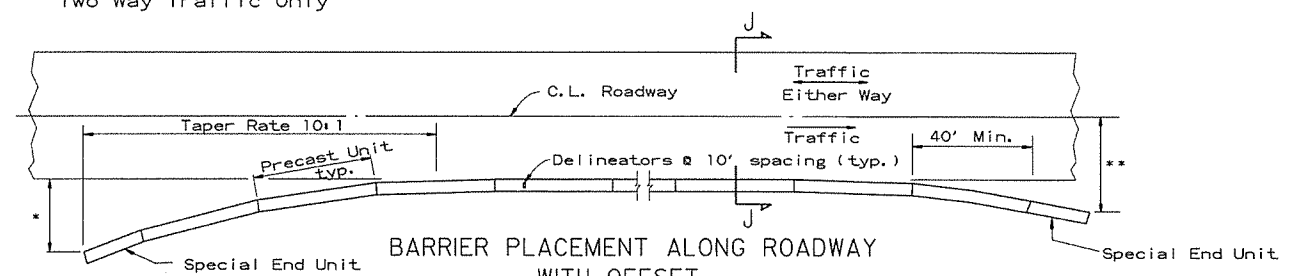
BARRIER PLACEMENT ALONG BRIDGE WITH OFFSET

No Scale

** Offset Distance for Two Way Traffic Only



SECTION J-J
No Scale



BARRIER PLACEMENT ALONG ROADWAY WITH OFFSET

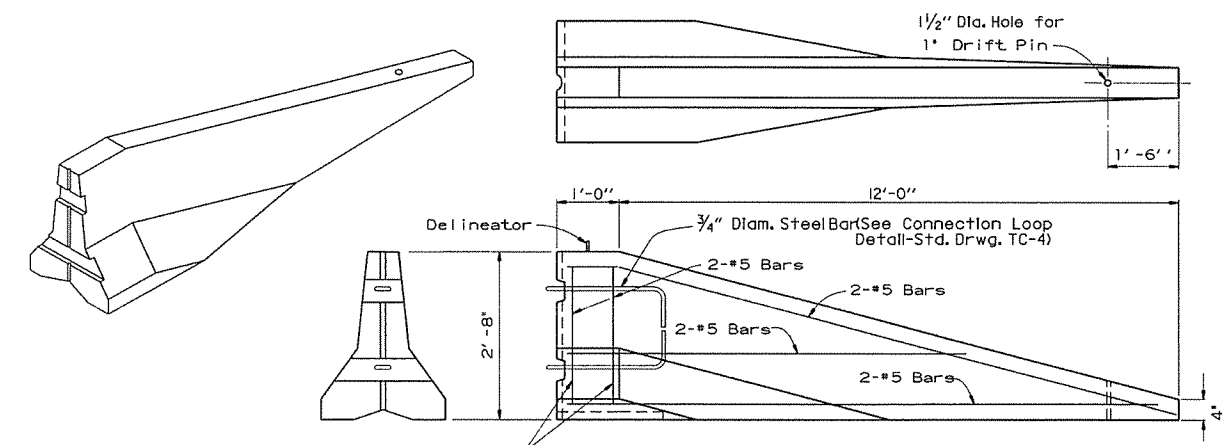
No Scale

** Offset Distance for Two Way Traffic Only

* Offset Distance (See Table)

Speed (MPH)	Offset Distance (FT.)
≤ 45	12
> 45	18

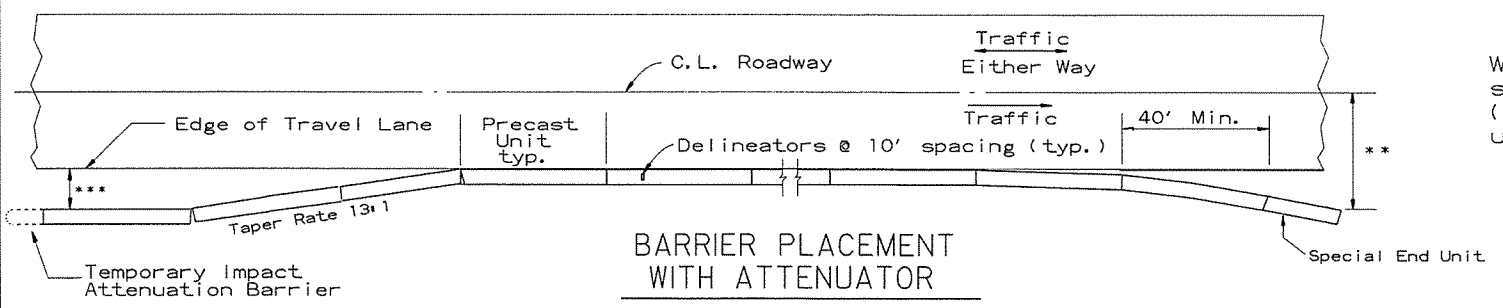
If offset distance is not attainable, then see 'Barrier Placement With Attenuator' Detail shown below.



SPECIAL END UNIT
No Scale

General Notes

When shown on the Plans, the ends of the Temporary Precast Concrete Barrier shall be protected with an NCHRP-350 or Manual For Assessing Safety Hardware (MASH) approved Crash Cushion. Payment for Crash Cushions shall be made under the item of 'Temporary Impact Attenuation Barrier.'



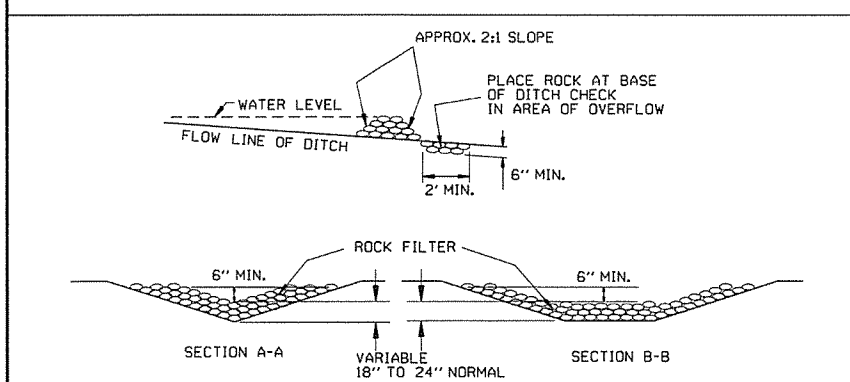
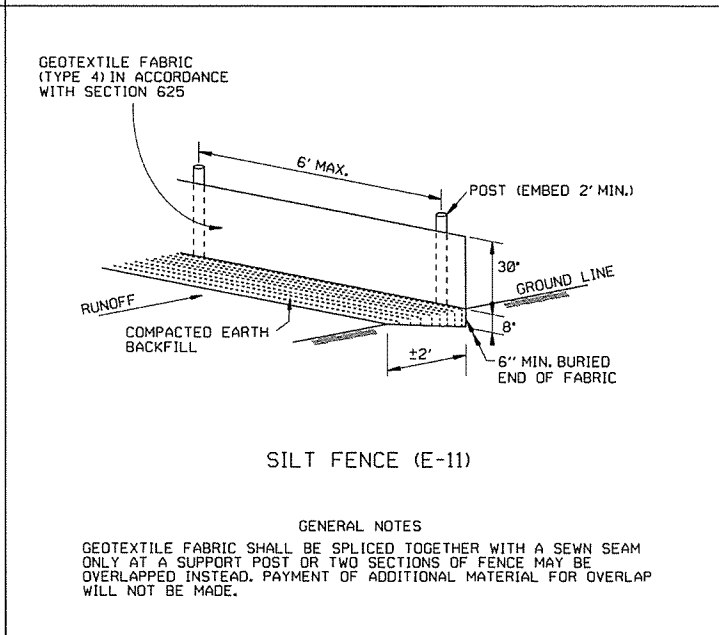
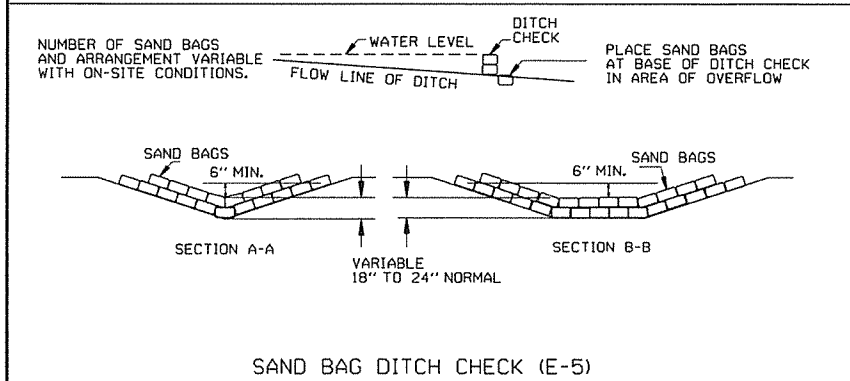
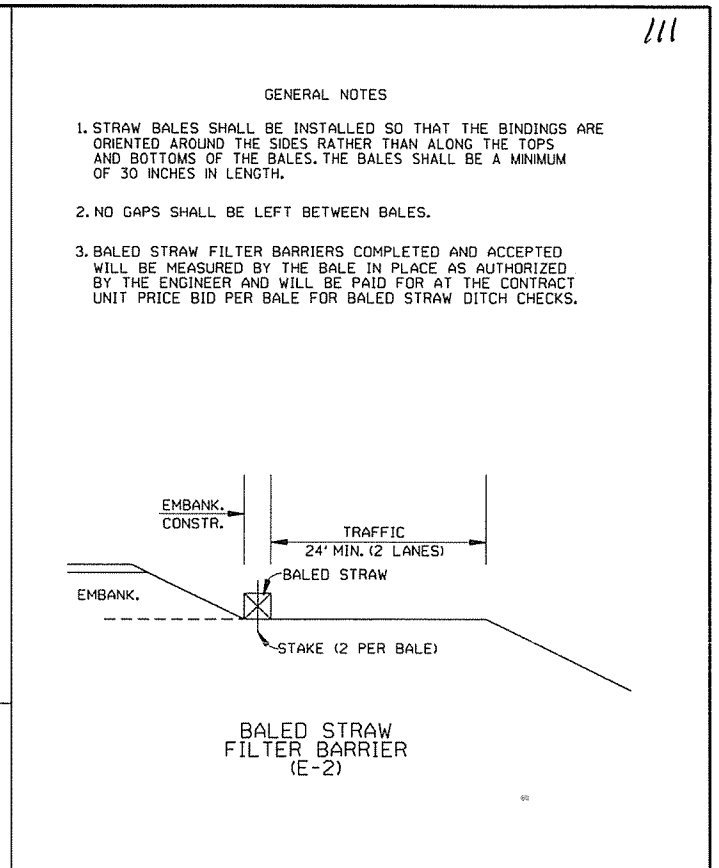
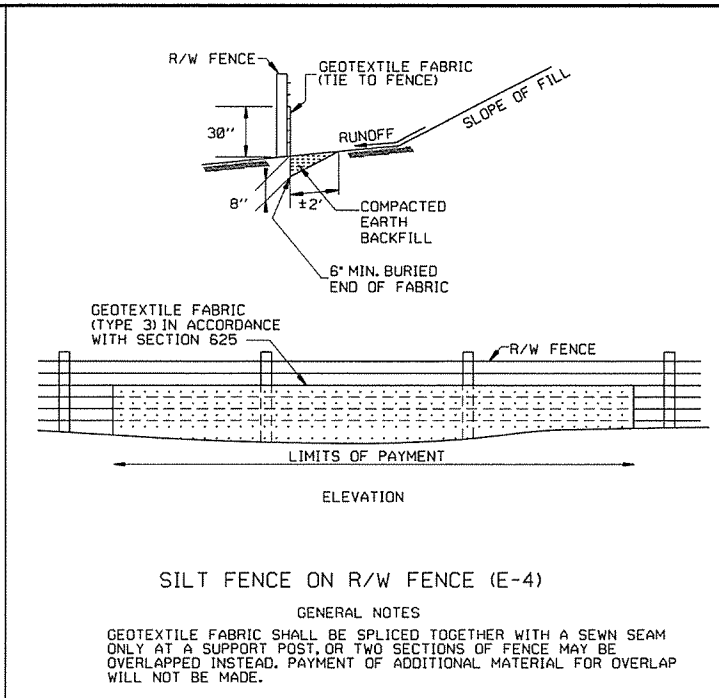
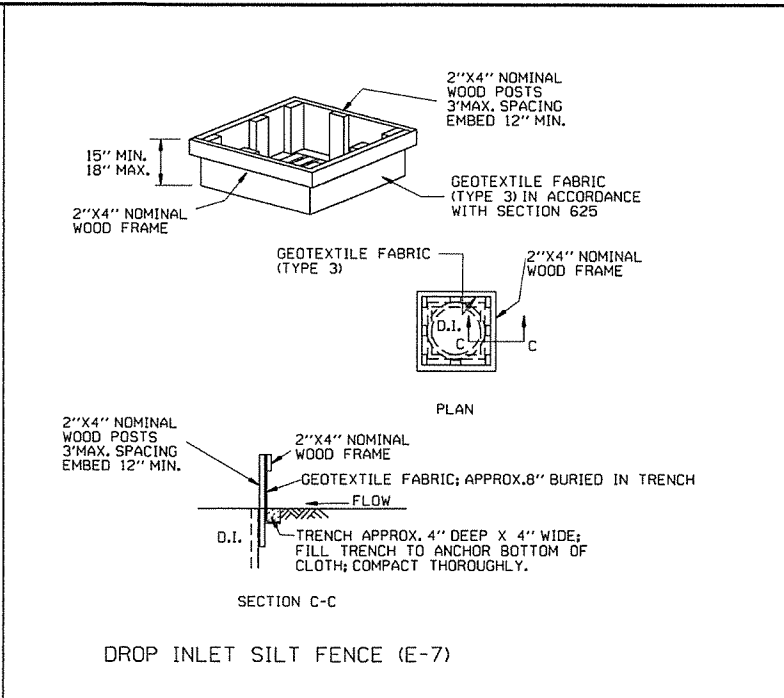
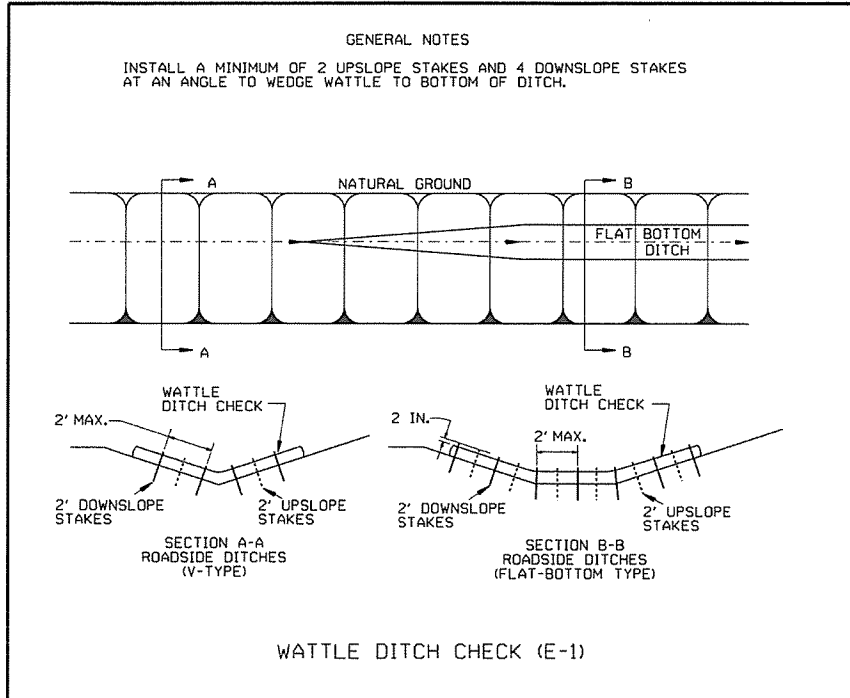
BARRIER PLACEMENT WITH ATTENUATOR

No Scale

** Offset Distance for Two Way Traffic Only

*** Min. 3'-0" From Edge of Travel Lane to Nearest Edge of Attenuator

			ARKANSAS STATE HIGHWAY COMMISSION
			STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER
			STANDARD DRAWING TC-5
10-15-09	ADDED REFERENCE TO MASH		
5-25-06	REVISED BARRIER PLACEMENT		
8-22-02	ISSUED NEW DRAWING		
DATE	REVISION	FILMED	

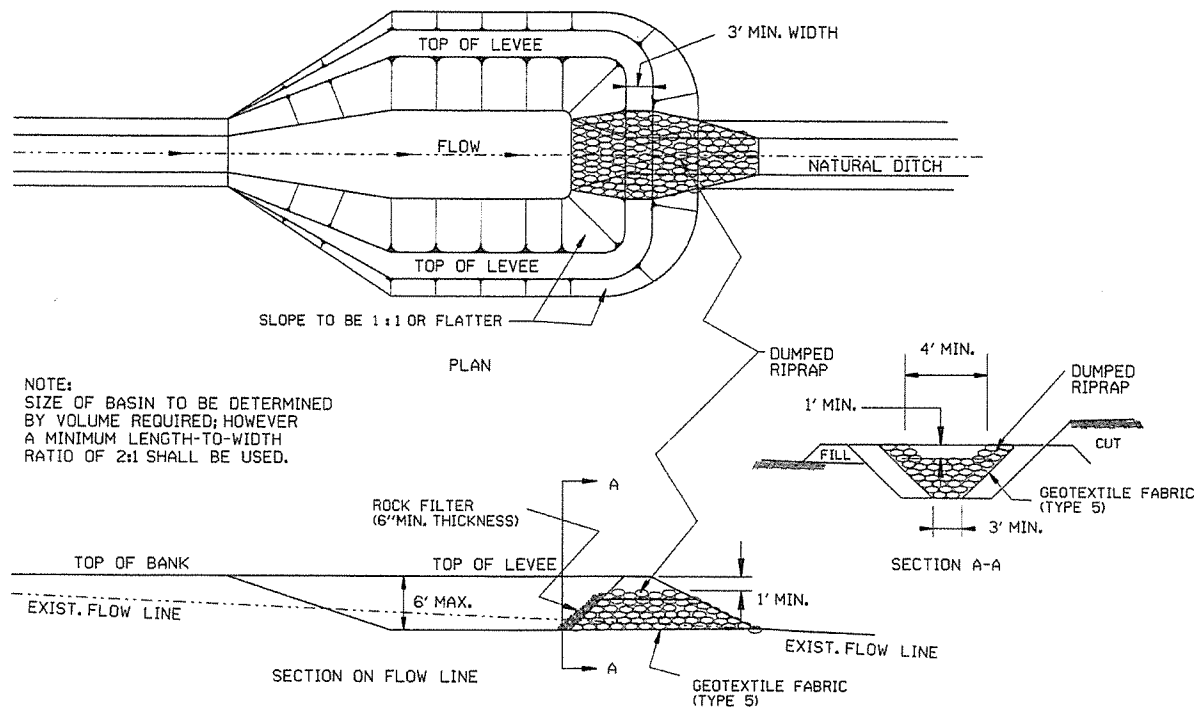


DATE	REVISION	DATE
12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
11-18-98	ADDED NOTES	
7-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
7-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
7-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC	
6-2-94	REVISED E-1, 4, 7 & 11; DELETED E-2 & 3	6-2-94
4-1-93	REDRAWN	
10-1-92	REDRAWN	
8-2-76	ISSUED R.D.M.	298-7-28-76
		FILMED

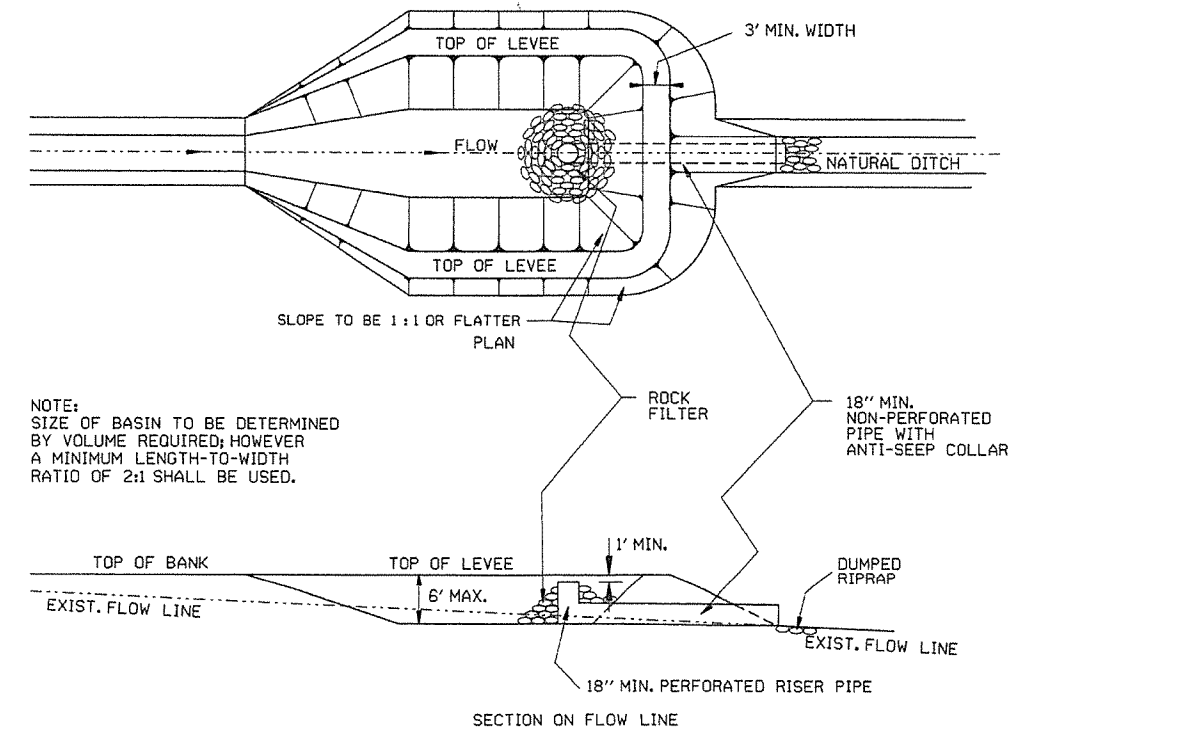
ARKANSAS STATE HIGHWAY COMMISSION

TEMPORARY EROSION CONTROL DEVICES

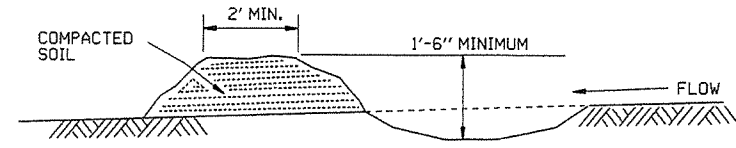
STANDARD DRAWING TEC-1



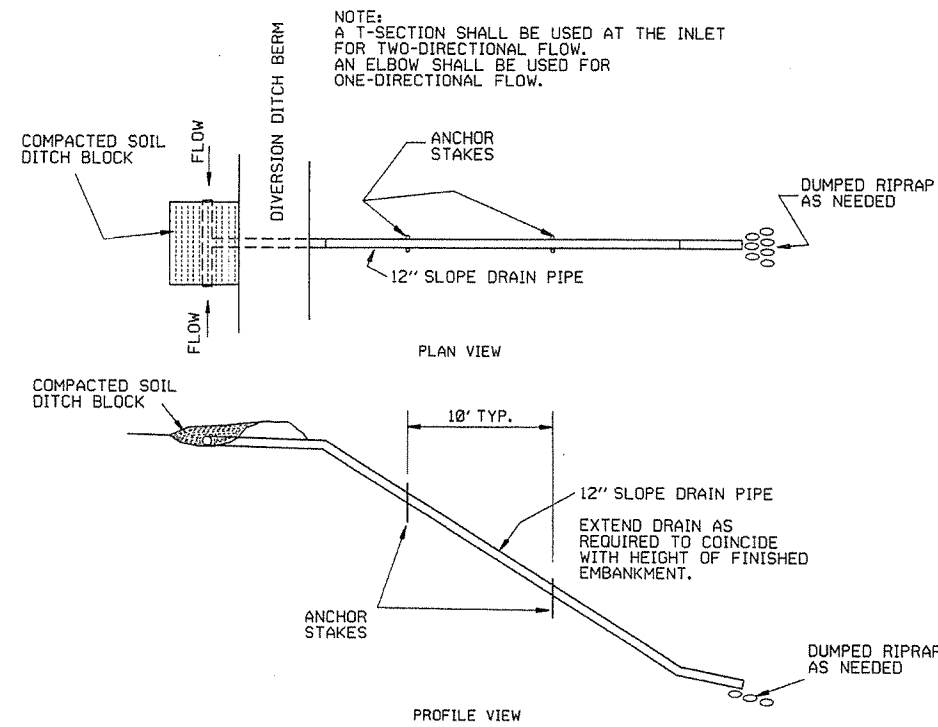
SEDIMENT BASIN WITH RIPRAP OUTLET (E-9)



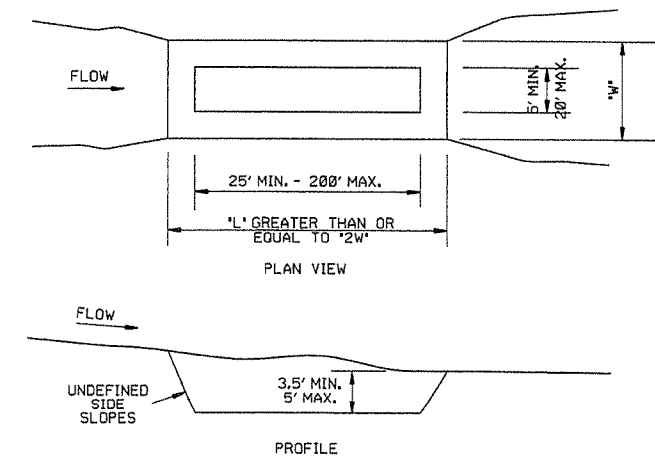
SEDIMENT BASIN WITH PIPE OUTLET (E-10)



DIVERSION DITCH (E-8)



SLOPE DRAIN (E-12)



SEDIMENT BASIN (E-14)

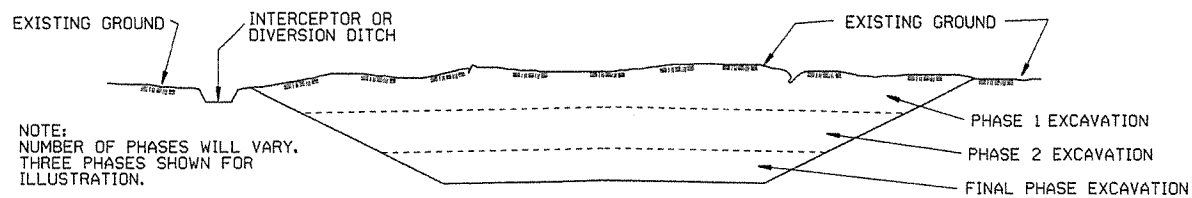
		ARKANSAS STATE HIGHWAY COMMISSION	
		TEMPORARY EROSION CONTROL DEVICES	
6-2-94	Revised E-8 & E-12; Added E-14 & Deleted E-13		
4-1-93	ISSUED		
DATE	REVISION		FILMED

CLEARING AND GRUBBING

CONSTRUCTION SEQUENCE

1. PLACE PERIMETER CONTROLS (I.E. SILT FENCES , DIVERSION DITCHES, SEDIMENT BASINS, ETC.)
2. PERFORM CLEARING AND GRUBBING OPERATION.

EXCAVATION



NOTE:
NUMBER OF PHASES WILL VARY.
THREE PHASES SHOWN FOR
ILLUSTRATION.

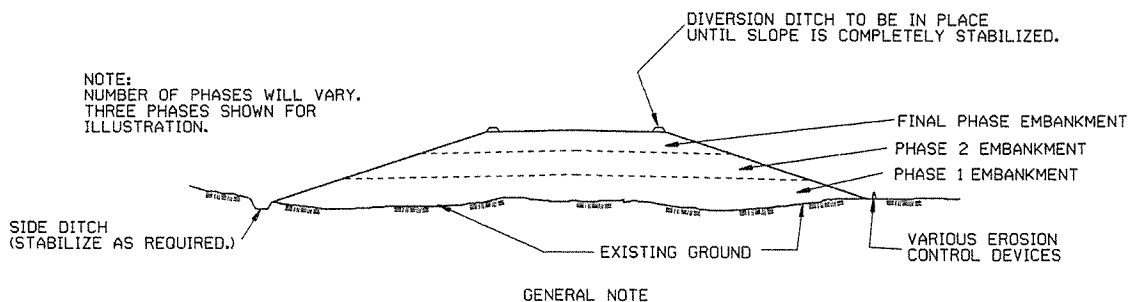
GENERAL NOTE

ALL CUT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE EXCAVATED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

1. EXCAVATE AND STABILIZE INTERCEPTOR AND/OR DIVERSION DITCHES.
2. PERFORM PHASE 1 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
3. PERFORM PHASE 2 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
4. PERFORM FINAL PHASE OF EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING. STABILIZE DITCHES. CONSTRUCT DITCH CHECKS, DIVERSION DITCHES, SEDIMENT BASINS, OR OTHER EROSION CONTROL DEVICES AS REQUIRED.

EMBANKMENT



NOTE:
NUMBER OF PHASES WILL VARY.
THREE PHASES SHOWN FOR
ILLUSTRATION.

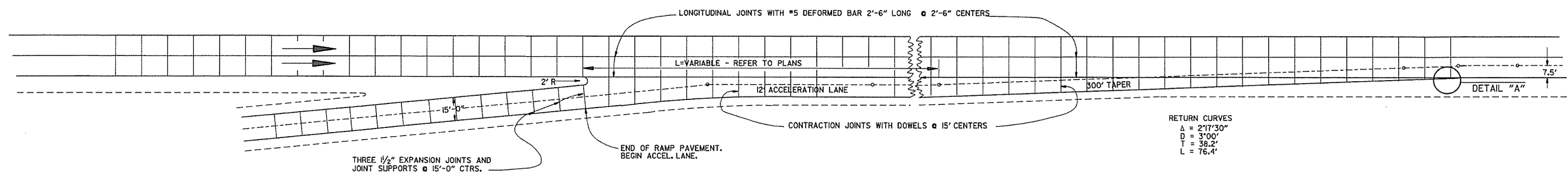
GENERAL NOTE

ALL EMBANKMENT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE CONSTRUCTED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

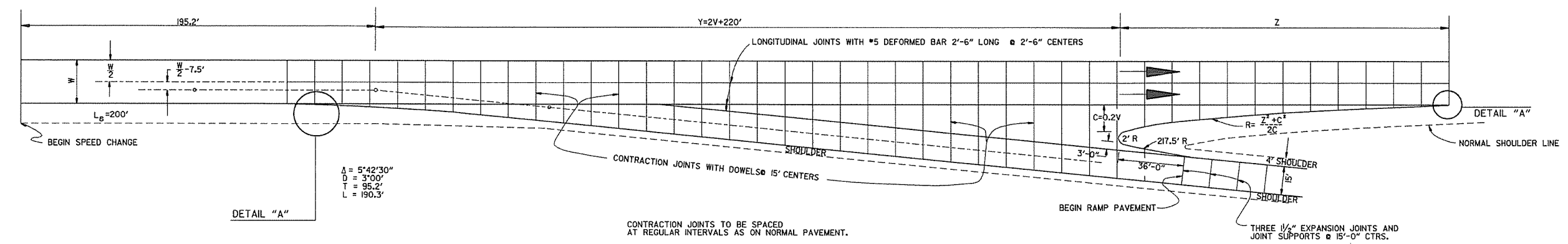
1. CONSTRUCT DIVERSION DITCHES, DITCH CHECKS, SEDIMENT BASINS, SILT FENCES, OR OTHER EROSION CONTROL DEVICES AS SPECIFIED.
2. PLACE PHASE 1 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
3. PLACE PHASE 2 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
4. PLACE FINAL PHASE OF EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PLACE DIVERSION DITCHES AND SLOPE DRAINS AND MAINTAIN UNTIL ENTIRE SLOPE IS STABILIZED.

ARKANSAS STATE HIGHWAY COMMISSION		
TEMPORARY EROSION CONTROL DEVICES		
STANDARD DRAWING TEC-3		
11-03-94	CORRECTED SPELLING	
6-2-94	Drawn & Issued	6-2-94
DATE	REVISION	FILMED



ENTRANCE RAMP

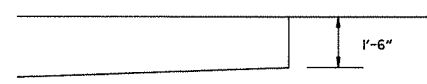
NOTE: JOINT SPACING ON THE MAIN LANES SHALL BE ADJUSTED AS NECESSARY TO CONFORM TO THESE JOINT LAYOUTS. THE MAIN LANE JOINT SPACING MAY BE REDUCED TO A 12' MINIMUM.



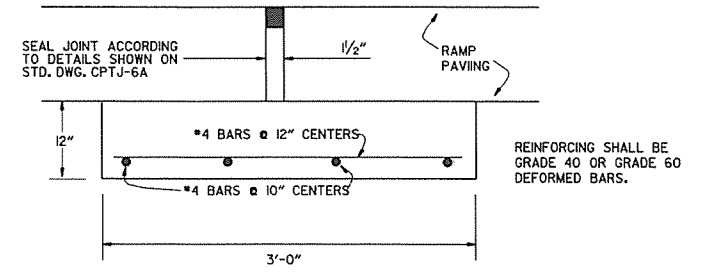
EXIT RAMP

EXIT RAMP

DESIGN SPEED V	Y	NOSE OFFSET C	LENGTH NOSE TAPER Z	RETURN RADIUS R	ADDITIONAL SURFACING SQ. YDS.
40	300.0	8.0	96.0	580.0	602.43
50	320.0	10.0	120.0	725.0	687.29
60	340.0	12.0	168.0	1182.0	790.55
70	360.0	14.0	210.0	1582.0	902.27



DETAIL "A"



DETAIL OF EXPANSION JOINT & JOINT SUPPORT

NOTE: THE EXPANSION JOINTS SHALL BE MEASURED AND PAID FOR AS P.C.C. PAVEMENT (RAMP THICKNESS). WHEN RAMP PAVING IS ASPHALT, EXPANSION JOINT IS NOT REQUIRED. THE JOINT SUPPORT MAY BE CONSTRUCTED WITH CLASS "A", "S", OR PAVING CONCRETE. PAYMENT FOR THE JOINT SUPPORT SHALL BE FOR THE CONTRACT UNIT PRICE BID FOR THE CLASS OF CONCRETE USED. ALL OTHER WORK AND MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE JOINT SUPPORT SHALL BE INCLUDED IN THE PRICE BID FOR THE ABOVE ITEMS.

DATE	REVISION	DATE FILED
8-22-02	DELETED NOTE	
11-16-01	CORRECTED SPELLING ON ENTRANCE RAMP NOTE	
5-13-99	ADDED, EDITED AND DELETED NOTES	
11-03-94	ADDED NOTE RE: REINF. BARS	
10-1-92	ADDED DETAIL "A" & OTHER MINOR CHANGES	10-1-92
1-25-90	REVISED EXPANSION JOINT	1-25-90
7-15-88	CONFORM'D TO 1988 SPECIFICATIONS	65C-7-15-88
3-2-81	ISSUED	511-10-2-72

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF STANDARD TURNOUT

FOR

ENTRANCE & EXIT RAMPS (NON-REINFORCED)

STANDARD DRAWING TR-1A