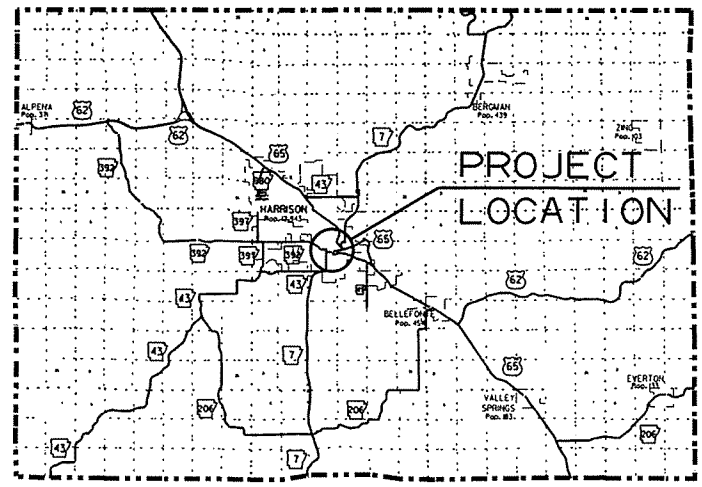


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. 090426	1	28
				② MAIN ST./BOWER AVE. SIGNAL REHAB. (HARRISON) (S)				



VICINITY MAP

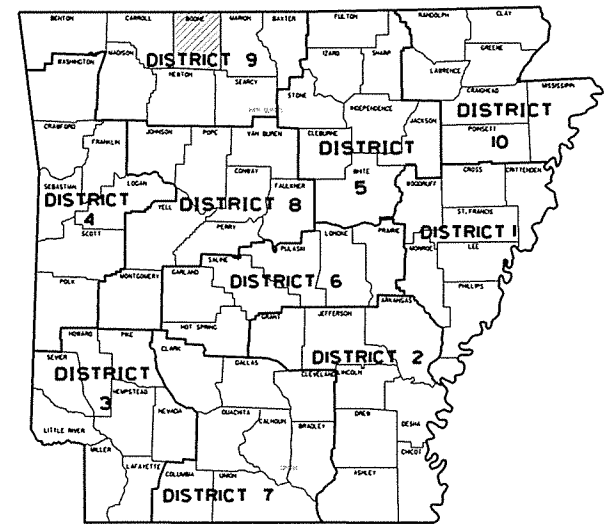
ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT
CONSTRUCTION PLANS FOR STATE HIGHWAY

MAIN ST./BOWER AVE. SIGNAL REHAB. (HARRISON) (S)

BOONE COUNTY

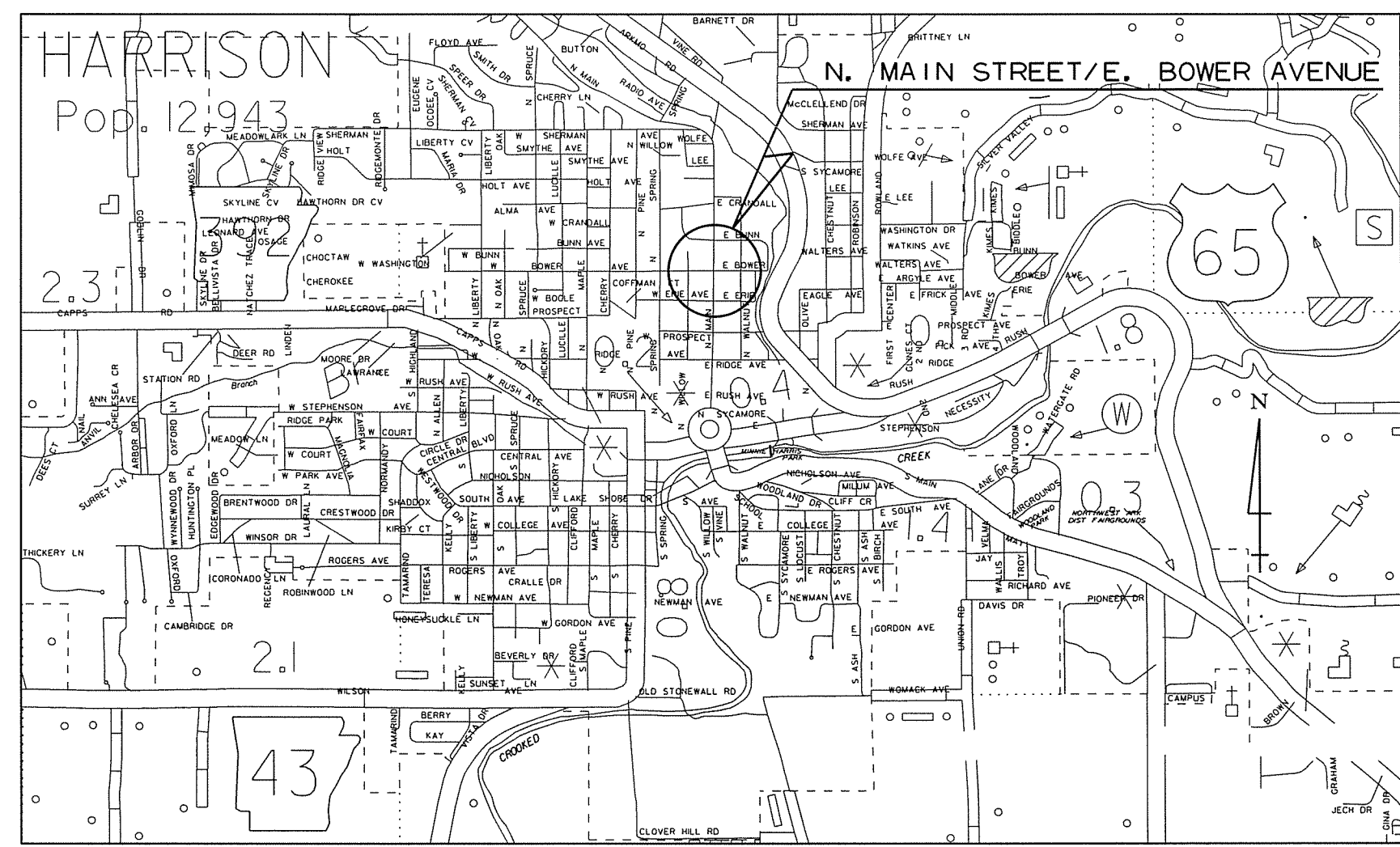
FED. AID PROJ. STPC-919(6)

JOB 090426



ARK. HWY. DIST. NO. 9

NOT TO SCALE



MID-POINT OF PROJECT
LAT. = N 36°13' 53"
LONG. = W 93°6' 25.9"

R 20 W

APPROVED



3-15-16
DEPUTY DIRECTOR
AND CHIEF ENGINEER

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4-14-16				6	ARK.			
						JOB NO.	090426	2 28

② INDEX OF SHEETS AND GOV. SPECIFICATIONS



INDEX OF SHEETS

SHEET NO.	TITLE	DRWG. NO.	DATE
1	TITLE SHEET		
2	INDEX OF SHEETS AND GOVERNING SPECIFICATIONS		
3	SYSTEM MAP		
4	GENERAL NOTES		
5	INTERSECTION IMPROVEMENT DETAILS		
6	MAINTENANCE OF TRAFFIC DETAILS		
7	PERMANENT PAVEMENT MARKING DETAIL		
8	SUMMARY OF QUANTITIES AND REVISIONS		
9	SURVEY CONTROL DETAIL		
10	TRAFFIC SIGNAL NOTES		
11	TRAFFIC SIGNAL QUANTITIES		
12 - 15	SIGNALIZATION PLAN SHEETS		
16	CURBING DETAILS	CG-1	11-29-07
17	DETAILS OF DRIVEWAYS & ISLANDS	DR-1	2-27-14
18	PAVEMENT MARKING DETAILS	PM-1	9-12-13
19	LOOP DETECTOR INSTALLATION	SD-4	9-12-13
20	CONTROLLER CABINET UTILITY DRAWER	SD-5	9-12-13
21	HEAVY DUTY PULL BOX	SD-6	9-02-15
22	SIGNAL HEAD PLACEMENT	SD-8	9-12-13
23	STEEL POLE WITH MAST ARM	SD-11	2-27-14
24	SERVICE POINT INSTALLATION WITH SUPPLEMENTAL GROUNDING ARRAY	SD-12	9-12-13
25	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-1	9-02-15
26	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-2	9-02-15
27	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-3	9-02-15
28	WHEELCHAIR RAMPS NEW CONSTRUCTION AND ALTERATIONS	WR-1	11-10-05

GOVERNING SPECIFICATIONS

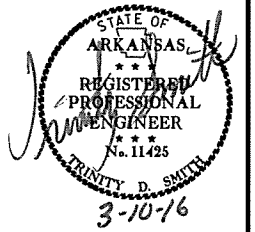
ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
108-1	LIQUIDATED DAMAGES
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 090426	BIDDING REQUIREMENTS AND CONDITIONS
JOB 090426	CABINET DRAWER ASSEMBLY
JOB 090426	CARGO PREFERENCE ACT REQUIREMENTS
JOB 090426	DECORATIVE TRAFFIC SIGNALS
JOB 090426	DOCUMENTATION OF PAYMENTS MADE TO DISADVANTAGED BUSINESS ENTERPRISES
JOB 090426	EDGE CARD VIDEO PROCESSOR
JOB 090426	ELECTRICAL CONDUCTORS FOR LUMINAIRES
JOB 090426	ELECTRICAL CONDUCTORS-IN-CONDUIT
JOB 090426	LED COUNTDOWN PEDESTRIAN SIGNAL HEAD
JOB 090426	LED LUMINAIRE ASSEMBLY (BUG U0 TYPE)
JOB 090426	LED TRAFFIC SIGNAL HEAD
JOB 090426	MANDATORY ELECTRONIC CONTRACT
JOB 090426	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 090426	REMOVAL OF TRAFFIC SIGNAL EQUIPMENT
JOB 090426	SERVICE POINT ASSEMBLY (TRAFFIC CONTROL DEVICES)
JOB 090426	STREET NAME SIGN (MAST ARM MOUNTED)
JOB 090426	SYSTEM LOCAL CONTROLLER
JOB 090426	UTILITY ADJUSTMENTS
JOB 090426	VIDEO DETECTOR (COLOR)

LOCATION: MAIN ST. /BOWER AVE. SIGNAL REHAB.
 CITY: HARRISON
 COUNTY: BOONE
 DISTRICT: 9 SCALE: N/A DRAWN BY: GWE

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090426		4	28

② GENERAL NOTES



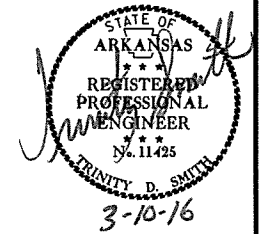
GENERAL NOTES

1. GRADE LINE DENOTES FINISHED GRADE WHERE SHOWN ON PLANS.
2. ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
3. ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U. S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
5. ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
6. ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
7. ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO 210-UNCLASSIFIED EXCAVATION.
8. UNLESS OTHERWISE INDICATED, ALL DIMENSIONS SHOWN ARE TO THE FACE OF CURB.
9. THIS PROJECT IS COVERED UNDER A NATIONWIDE 14 SECTION 404 PERMIT. REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS, EDITION OF 2014, FOR PERMIT REQUIREMENTS.

LOCATION: MAIN ST./BOWER AVE. SIGNAL REHAB.
 CITY: HARRISON
 COUNTY: BOONE
 DISTRICT: 9 SCALE: N/A DRAWN BY: GWE

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		5	28
				JOB NO.		090426		

2 INTERSECTION IMPROVEMENT DETAILS



N. MAIN ST. / E. BOWER AVE.
TOPSOIL FURNISHED AND PLACED

STATION	STATION	LOCATION	TOPSOIL FURNISHED AND PLACED CU. YD.
127+88.69	128+16.26	MAIN ST. ON RT.	
127+90.33	128+06.92	MAIN ST. ON LT.	
128+45.72	128+80.15	MAIN ST. ON RT.	2
TOTAL:			2

N. MAIN ST. / E. BOWER AVE.
REMOVAL AND DISPOSAL ITEMS

STATION	STATION	DESCRIPTION	CURB AND GUTTER LIN. FT.	WALKS SQ. YD.	SIGNS EACH	SIGN FOUNDATIONS EACH
127+88.69	128+16.26	MAIN ST. ON RT.	30	24		
127+90.33	128+06.92	MAIN ST. ON LT.	15	11		
128+45.72	128+80.15	MAIN ST. ON LT.	64	18	1	1
128+57.35		MAIN ST. ON LT.				
TOTALS:			109	53	1	1

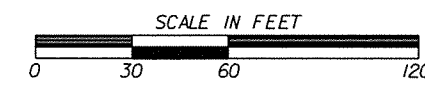
N. MAIN ST. / E. BOWER AVE.
CONCRETE ITEMS

STATION	STATION	LOCATION	SIDE	CONCRETE COMBINATION CURB AND GUTTER (TYPE A) (1' 6")	CONCRETE WALKS	WHEELCHAIR RAMPS (TYPE 3)
				LIN. FT.	SQ. YD.	SQ. YD.
127+88.69	128+16.26	MAIN ST.	ON RT.	29	44	11
127+90.33	128+06.92	MAIN ST.	ON LT.			12
128+45.72	128+80.15	MAIN ST.	ON RT.	80	12	11
TOTALS:				109	56	34

N. MAIN ST. / E. BOWER AVE.
EARTHWORK

STATION	STATION	LOCATION/DESCRIPTION	UNCLASSIFIED EXCAVATION CU. YD.	COMPACTED EMBANKMENT CU. YD.
127+88.69	128+16.26	MAIN ST. ON RT.	12	12
127+90.33	128+06.92	MAIN ST. ON LT.	4	4
128+45.72	128+80.15	MAIN ST. ON RT.	10	10
TOTALS:			26	26

NOTE: EARTHWORK QUANTITIES SHOWN SHALL BE PAID AS PLAN QUANTITY.



DATE: 03-10-16 FILE NAME: t090426.01.dgn

LOCATION: N. MAIN ST. / E. BOWER AVE.
CITY: HARRISON
COUNTY: BOONE
DISTRICT: 9 SCALE: 1" = 60' DRAWN BY: GWE

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		6	28

2 MAINTENANCE OF TRAFFIC DETAIL

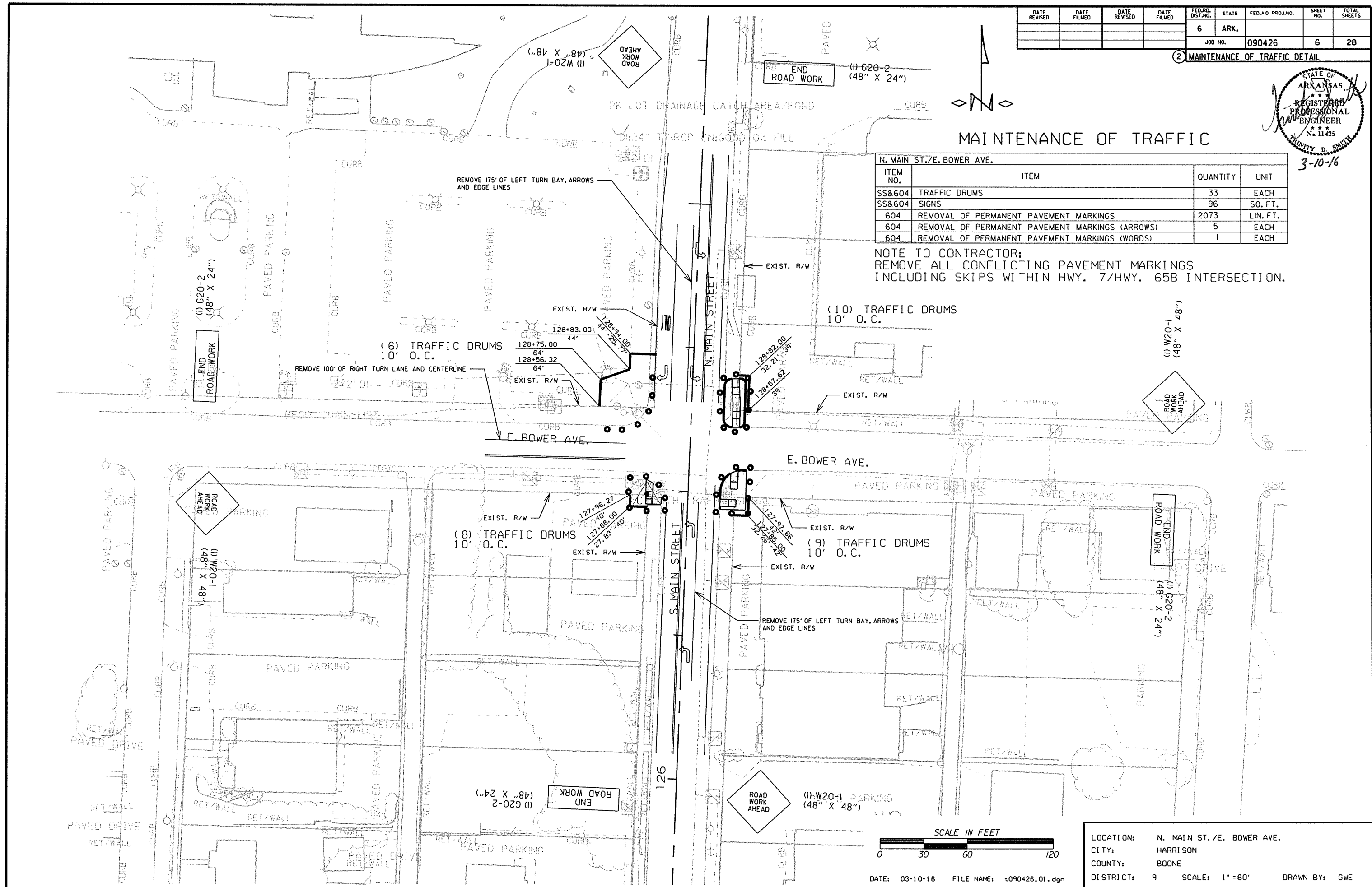


3-10-16

MAINTENANCE OF TRAFFIC

ITEM NO.	ITEM	QUANTITY	UNIT
SS&604	TRAFFIC DRUMS	33	EACH
SS&604	SIGNS	96	SO. FT.
604	REMOVAL OF PERMANENT PAVEMENT MARKINGS	2073	LIN. FT.
604	REMOVAL OF PERMANENT PAVEMENT MARKINGS (ARROWS)	5	EACH
604	REMOVAL OF PERMANENT PAVEMENT MARKINGS (WORDS)	1	EACH

NOTE TO CONTRACTOR:
REMOVE ALL CONFLICTING PAVEMENT MARKINGS INCLUDING SKIPS WITHIN HWY. 7/HWY. 65B INTERSECTION.

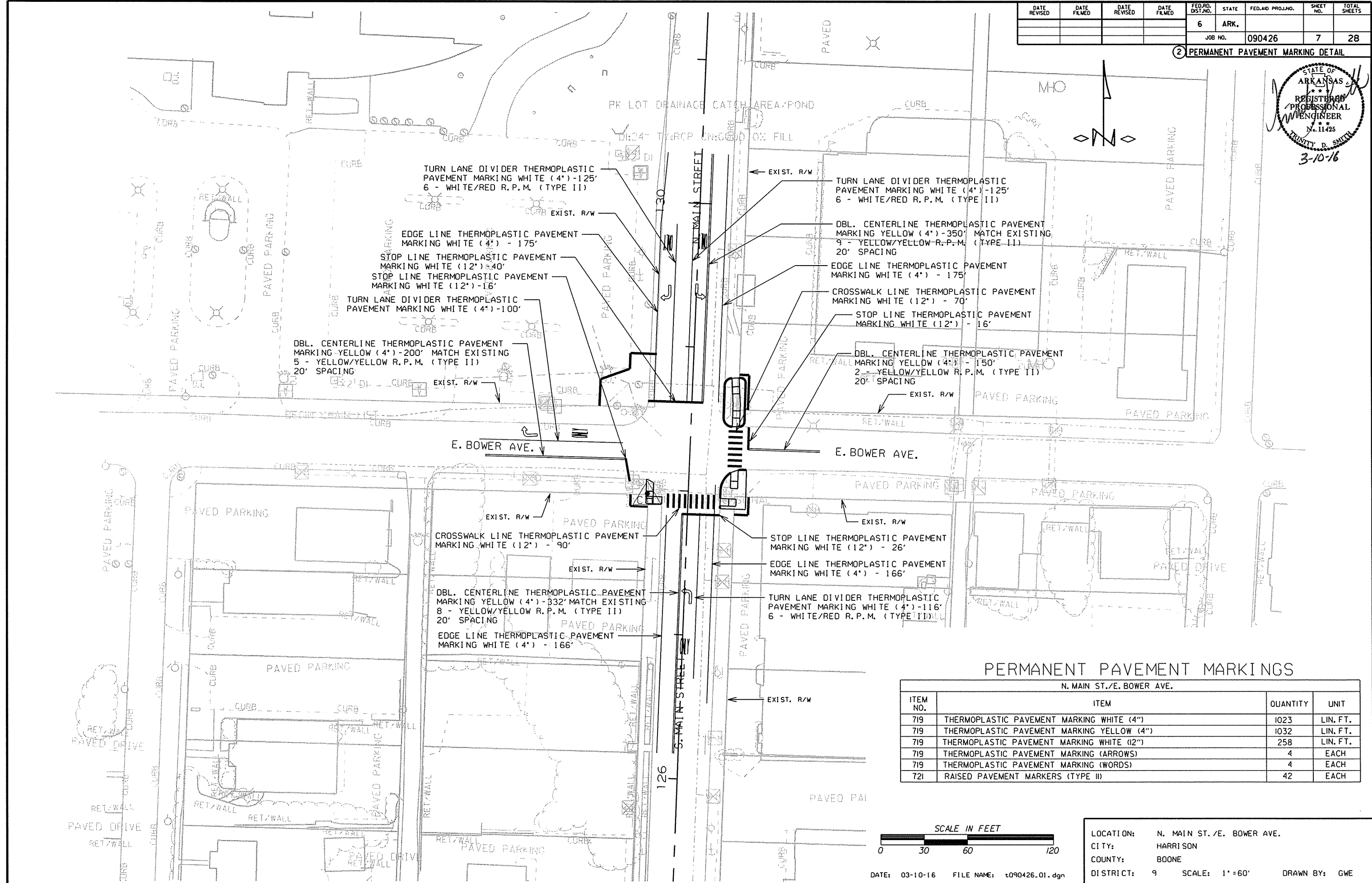
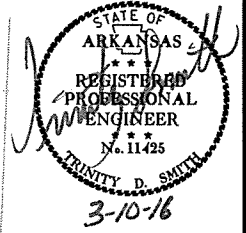


LOCATION:	N. MAIN ST./E. BOWER AVE.
CITY:	HARRISON
COUNTY:	BOONE
DISTRICT:	9
SCALE:	1" = 60'
DRAWN BY:	GWE

DATE: 03-10-16 FILE NAME: t090426.01.dgn

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		7	28

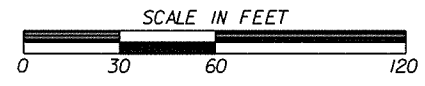
2 PERMANENT PAVEMENT MARKING DETAIL



PERMANENT PAVEMENT MARKINGS

N. MAIN ST./E. BOWER AVE.

ITEM NO.	ITEM	QUANTITY	UNIT
719	THERMOPLASTIC PAVEMENT MARKING WHITE (4")	1023	LIN. FT.
719	THERMOPLASTIC PAVEMENT MARKING YELLOW (4")	1032	LIN. FT.
719	THERMOPLASTIC PAVEMENT MARKING WHITE (12")	258	LIN. FT.
719	THERMOPLASTIC PAVEMENT MARKING (ARROWS)	4	EACH
719	THERMOPLASTIC PAVEMENT MARKING (WORDS)	4	EACH
721	RAISED PAVEMENT MARKERS (TYPE II)	42	EACH

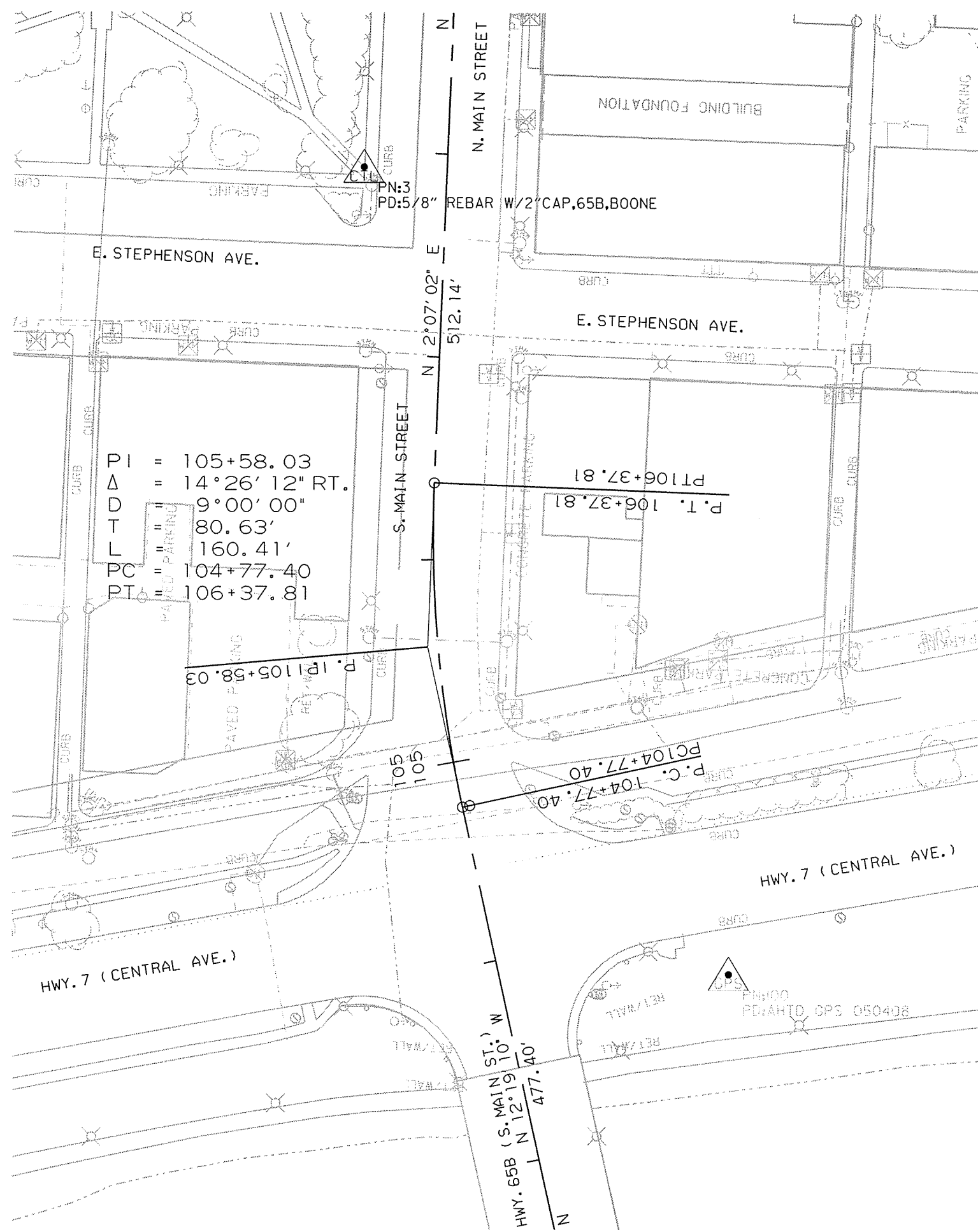
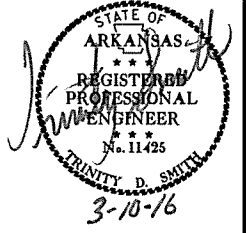


DATE: 03-10-16 FILE NAME: t090426.01.dgn

LOCATION: N. MAIN ST./E. BOWER AVE.
 CITY: HARRISON
 COUNTY: BOONE
 DISTRICT: 9 SCALE: 1" = 60' DRAWN BY: GWE

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO. 090426	9 28

② SURVEY CONTROL DETAIL



PI = 105+58.03
 $\Delta = 14^{\circ}26'12''$ RT.
 D = 9'00'00"
 T = 80.63'
 L = 160.41'
 PC = 104+77.40
 PT = 106+37.81

MAIN STREET

POINT NO.	TYPE	STATION	NORTHING	EASTING
8000	POB	100+00.00	691438.3810	985871.3453
8001	PC	104+77.40	691904.7875	985769.4860
8003	PT	106+37.81	692064.1374	985755.2612
8006	PC	111+49.95	692575.9299	985774.1816
8008	PT	112+11.94	692637.6687	985779.4818
8009	PC	112+90.27	692715.2892	985789.9715
8011	PT	113+50.65	692775.4265	985795.2105
8004	PI	115+73.06	692997.6623	985803.9865
8005	POE	132+68.19	694691.4050	985872.4015

SURVEY CONTROL COORDINATES

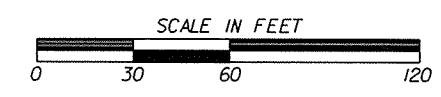
Project Name: s090339
 Date: 2/27/2012
 Coordinate System: ARKANSAS STATE PLANE - NORTH ZONE BASED ON GPS CONTROL, PROJECTED TO GROUND.
 Units: U.S. SURVEY FOOT

Point Name	Northing	Easting	Elev	Feature	Description
1	691717.3167	985210.7879	1047.701	CTL	5/8" REBAR W/2" CAP, 65B, BOONE
2	691943.2604	986677.3452	1049.896	CTL	5/8" REBAR W/2" CAP, 65B, BOONE
3	692219.7968	985720.0054	1044.480	CTL	5/8" REBAR W/2" CAP, 65B, BOONE
4	692746.1652	985826.6839	1056.940	CTL	5/8" REBAR W/2" CAP, 65B, BOONE
5	693551.9411	985804.4408	1070.579	CTL	5/8" REBAR W/2" CAP, 65B, BOONE
6	694221.9279	985824.8918	1101.397	CTL	5/8" REBAR W/2" CAP, 65B, BOONE
7	694647.1191	985907.0691	1112.388	CTL	5/8" REBAR W/2" CAP, 65B, BOONE
100	691822.5821	985900.3777	1052.242	GPS	AHTD GPS 050408
101	691024.2616	984971.3104	1055.016	GPS	AHTD GPS 050408A

*Note - Rebar and Cap - Standard - 5/8" Rebar with 2" Aluminum Cap stamped *(standard markings common to all caps), or as indicated (other markings indicated in the point description of the individual point).
 ALL DISTANCES ARE GROUND.
 USE CAF = 1.0 FOR STAKEOUT FOR THIS PROJECT.
 A PROJECT CAF OF 0.9999479879 HAS BEEN USED TO COMPUTE THE ABOVE GROUND COORDINATES. THIS CAF IS INTENDED FOR USE WITHIN THE PROJECT LIMITS.
 GRID DISTANCE = GROUND DISTANCE X CAF.
 GRID COORDINATES ARE STORED UNDER FILE NAME s090339gi.ct1
 HORIZONTAL DATUM: NAD 83 (1997)
 VERTICAL DATUM: NAVD 88 POSITIONAL ACCURACY THIRD ORDER, UNLESS SPECIFIED OTHERWISE AT A SPECIFIC POINT.

REFERENCE POINTS (1500 SERIES) ARE TO BE USED TO ESTABLISH CONTROL IF THE PRIMARY CONTROL POINTS LISTED ABOVE HAVE BEEN DESTROYED. REFERENCE POINTS ARE NOT TO BE USED FOR VERTICAL CONTROL.

BASIS OF BEARING:
 ARKANSAS STATE PLANE GRID BEARINGS - 0301-NORTH ZONE
 DETERMINED FROM GPS CONTROL POINTS: 050408, 050408A
 CONVERGENCE ANGLE: 00-38-40 LEFT AT LT: 36-14-0.9 LG: 093-06-26.3
 GRID AZIMUTH = ASTRONOMICAL AZIMUTH - CONVERGENCE ANGLE.



DATE: 03-10-16 FILE NAME: t090426.01.dgn

LOCATION: HWY. 7 (CENTRAL AVE.) / HWY. 65B & S. MAIN ST.
 CITY: HARRISON
 COUNTY: BOONE
 DISTRICT: 9 SCALE: 1" = 60' DRAWN BY: GWE

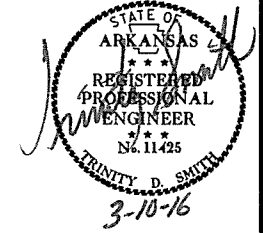
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						090426	10	28

TRAFFIC SIGNAL NOTES:

1. PERFORM ELECTRICAL WORK IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE NFPA 70 (2014) NATIONAL ELECTRICAL CODE, NFPA 101(2012) LIFE SAFETY CODE, STATE ELECTRICAL CODE AND LOCAL ELECTRICAL CODE.
2. EXTEND GREEN EQUIPMENT GROUNDING CONDUCTOR (EGC) FROM GROUND BAR AT MAIN BREAKER TO CONTROL PANEL AND TO FIRST POLE. SOLIDLY BOND EGC TO GROUND LUG OF CONTROL CABINET AND TO POLE GROUND. ENSURE THAT ONLY ONE NEUTRAL-TO-GROUND BOND EXISTS IN THE SYSTEM AND THAT IT IS AT THE MAIN BREAKER.
3. ELECTRICAL SERVICE SHALL BE PROVIDED BY THE CITY TO A SERVICE POLE WITH EXTERNAL RAIN TIGHT BREAKER (MAIN BREAKER), GALVANIZED STEEL SERVICE RISER, METER LOOP (IF REQUIRED), AND WEATHERHEAD AT A MUTUALLY ACCEPTABLE POINT WITHIN THE RIGHT-OF-WAY. IF THE SERVICE POINT IS OVER 10 FEET FROM THE CONTROLLER, THE CONTRACTOR SHALL PROVIDE AND INSTALL A SEPARATE TWO CIRCUIT EXTERNAL BREAKER (SECONDARY BREAKER) ON OR NEAR THE TRAFFIC SIGNAL CONTROLLER CABINET AND SHALL INSTALL CONDUIT, ELECTRICAL SERVICE WIRE (2c/*6 USE RATED, WITH GROUND TYPICAL), AND PERFORM WIRING TO TAP INTO THE CITY'S MAIN BREAKER AS PART OF THIS CONTRACT. CONDUIT IS PAID FOR AS A SEPARATE ITEM OF THIS CONTRACT. TWO CIRCUIT BREAKERS, CONSIDERED SUBSIDIARY TO THE CONTROL EQUIPMENT, ARE NEEDED WHERE STREET LIGHTING IS INCLUDED. AS PART OF THE SIGNAL INSTALLATION, STREET LIGHTING CIRCUIT (2c/*12 AWG UF RATED, TYPICAL) SHALL BE KEPT FROM THE CIRCUIT SERVING THE TRAFFIC SIGNAL CONTROL EQUIPMENT FROM THE POINT OF TIE-IN AT THE SECONDARY BREAKER PROVIDED BY THE CONTRACTOR.
4. CONTRACTOR SHALL CONNECT A SEPARATE NEUTRAL FOR EACH LOAD SWITCH REPRESENTED ON EACH SIGNAL POLE.
5. TRAFFIC CONTROLLER CABINET AND LAYOUT SHALL BE SUCH THAT IT IS NOT NECESSARY TO SHUT DOWN POWER OR REMOVE LOAD SWITCHES IN ORDER TO EASILY TEST OR MODIFY DETECTOR INPUTS TO THE CONTROLLER.
6. CONTROLLER CABINET SHALL BE WIRED SUCH THAT DURING FLASH OPERATIONS POWER TO THE LOAD SWITCHES CANNOT BACKFEED TO LOAD SWITCH POWER BUSS.
7. ALL PARTS OF THIS INSTALLATION SHALL BE IN ACCORDANCE WITH THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARDS AND DETAILS AND WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITIONS.
8. CONDUIT INSTALLED UNDER ROADWAY SURFACES SHALL BE INSTALLED BY PUSHING OR BORING METHODS. IF THE ENGINEER DETERMINES THIS IS NOT FEASIBLE, THEN A TRENCHING METHOD AS SHOWN IN THE DETAILS MAY BE USED.
9. TRAFFIC SIGNAL POLES SHALL BE GALVANIZED. BACKPLATES SHALL BE SUPPLIED FOR ALL SIGNAL HEADS.
10. PAVEMENT MARKING SHOWN FOR REFERENCE ONLY. SEE PAVEMENT MARKING PLAN SHEETS.
11. FOUNDATION FOR ALL POLES SHALL BE EXTENDED IF NECESSARY TO ACCOMMODATE THE REQUIREMENTS FOR SIGNAL HEAD CLEARANCE ABOVE ROADWAY ONLY AT LOCATIONS WHERE THE GROUND ELEVATION AT THE POLE IS BELOW THE ELEVATION OF THE ROADWAY (SEE NOTES ON SPECIAL DETAILS). PAYMENT WILL BE INCLUDED IN SECTION 714, AHTD STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
12. ALL BOXES SHALL BE (TYPE 2 HD) UNLESS OTHERWISE INDICATED. ALL CONDUIT SHALL BE 3" DIAMETER UNLESS SPECIFIED ON PLANS.
13. CONTRACTOR SHALL NOTIFY ALL EXISTING UTILITY OWNERS BEFORE BEGINNING WORK ON THIS PROJECT.
14. LUMINAIRE ASSEMBLIES SHALL BE OF THE FULL CUTOFF TYPE.
15. HARDWARE INPUTS MAY BE DETERMINED BY SUPPLIER. EACH DETECTOR OUTPUT SHALL INPUT THE CONTROLLER THROUGH A SEPARATE INPUT UNLESS OTHERWISE NOTED AND BE PROGRAMMED TO ACTUATE THE ASSOCIATED PHASE. COMBINATION (COMB.) DETECTORS SHALL ALSO BE PROGRAMMED TO PROVIDE VEHICLE COUNT/OCCUPANCY DATA.
16. THE LOCAL RADIO WITH ANTENNA SHALL BE COMPATIBLE WITH THE EXISTING CLOSED LOOP COORDINATION SYSTEM IN THE CITY.
17. TO DETERMINE UTILITY CLEARANCES ABOVE THE TRAFFIC SIGNAL POLE, REFER TO THE POLE SCHEDULE FOR VERTICAL SHAFT HEIGHT. WHERE THE POLE SCHEDULE INDICATES THAT A LUMINAIRE ARM WILL BE USED, 38 FEET SHOULD BE USED TO DETERMINE UTILITY CLEARANCE ABOVE THE LUMINAIRE ARM. WHERE THE POLE SCHEDULE INDICATES A TRAFFIC SIGNAL POLE WITHOUT A LUMINAIRE ARM, A HEIGHT OF 21' SHOULD BE USED TO DETERMINE UTILITY CLEARANCE ABOVE THE TRAFFIC SIGNAL MAST ARM. AN ADDITIONAL 6 FEET SHOULD BE USED DIRECTLY ABOVE "VIDEO DETECTOR" AT LOCATIONS SHOWN ON THE SIGNAL PLANS.
18. THE DESIRABLE MINIMUM DISTANCE FROM THE FACE OF ROADWAY CURB OR SHOULDER EDGE TO THE FACE OF NON-BREAKAWAY POLE OR OBSTRUCTION IS 6 FEET. REFER TO TRAFFIC SIGNAL PLANS FOR SPECIFIC LOCATION OF POLES, CONTROLLER AND ANY OTHER NON-BREAKAWAY OBSTRUCTIONS. REFER TO "DESIGN PARAMETERS, MINIMUM CLEAR ZONE DISTANCE" FOR MINIMUM DISTANCE FROM THE EDGE OF TRAVELED WAY TO THE FACE OF A NON-BREAKAWAY POLE OR OBSTRUCTION. TRAFFIC SIGNAL POLES OR ANY OTHER NON-BREAKAWAY OBSTRUCTION SHALL NOT BE INSTALLED WITHIN THE CLEAR ZONE.
19. AS DETERMINED BY THE ENGINEER, FOUNDATION EMBEDMENT MAY BE DECREASED BY A MAXIMUM OF TWO FEET IF COMPETENT ROCK IS ENCOUNTERED PRIOR TO ACHIEVING PLAN EMBEDMENT AND AT LEAST HALF OF THE REMAINING PLAN EMBEDMENT LENGTH IS KEYED INTO COMPETENT ROCK.
20. CONNECTION OF TRAFFIC SIGNAL DISPLAY TO FIELD WIRING SHALL UTILIZE AN APPROVED TERMINAL STRIP BEHIND HAND-HOLE COVER AT BASE OF POLE. TERMINAL STRIP SHALL PROVIDE PROTECTION TO PREVENT EXPOSURE TO THE PUBLIC IN THE EVENT THAT POLE COVER IS MISSING. PAYMENT FOR TERMINAL STRIPS SHALL BE INCLUDED IN ITEM 714-TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION.
21. CONTROLLER CABINET LAYOUT AND ORIENTATION SHALL CONFORM TO ISMA STANDARDS.
22. ONE VIDEO PROGRAMMING MODULE SHALL BE PROVIDED FOR AIMING AND SETUP OF DETECTORS IF THE VIDEO SYSTEM CANNOT BE ADJUSTED THROUGH HARDWARE AND SOFTWARE PROVIDED BY ITEMS WITHIN THE JOB.
23. TRAFFIC SIGNAL CONTRACTOR MUST NOTIFY RESIDENT ENGINEER OR ASSIGNED DEPARTMENT PROJECT INSPECTOR EACH DAY PRIOR TO SIGNAL RELATED WORK. NO WORK ON TRAFFIC SIGNALS WILL BE ALLOWED OR APPROVED WITHOUT THIS PRIOR NOTIFICATION.
24. ALL STEEL POLES SHALL BE DESIGNED TO MEET THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION (2001) WITH 2003 AND 2006 INTERIMS.

25. DOOR PANEL TEST PUSH BUTTONS SHALL ACTUATE INDICATED PHASES. DETECTOR ASSIGNMENTS AND/OR SIDE PANEL JUMPLERS MAY REQUIRE MODIFICATION.
26. ALL SYSTEM DETECTOR RACKS AND ASSOCIATED EQUIPMENT SHALL BE PROTECTED BY THE MAIN CONTROLLER CABINET POWER SURGE PROTECTION.

② TRAFFIC SIGNAL NOTES



LOCATION:	MAIN ST./BOWER AVE. SIGNAL REHAB.
CITY:	HARRISON
COUNTY:	BOONE
DISTRICT:	9
SCALE:	N/A
DRAWN BY:	GWE

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090426		11	28

2 TRAFFIC SIGNAL QUANTITIES

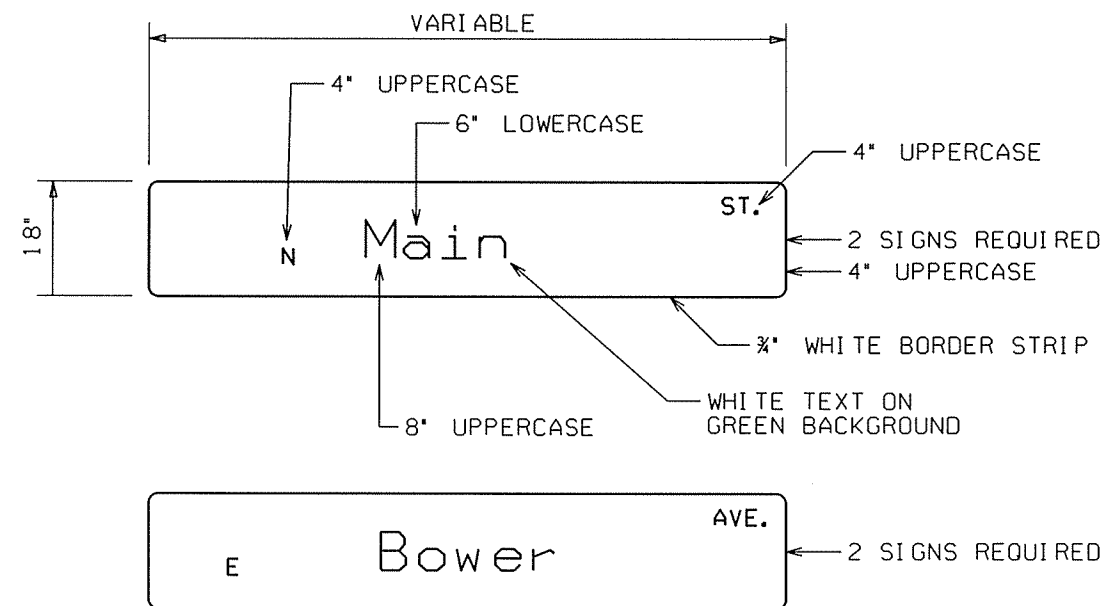


TRAFFIC SIGNAL QUANTITIES

ITEM NO.	ITEM	QUANTITY	UNIT
SP&701	SYSTEM LOCAL CONTROLLER TS 2-TYPE 2 (8 PHASES)	1	EACH
SP&706	TRAFFIC SIGNAL HEAD, LED, (3 SECTION, 1WAY)	8	EACH
SP&706	TRAFFIC SIGNAL HEAD, LED, (4 SECTION, 1WAY)	2	EACH
SP&707	COUNTDOWN PEDESTRIAN SIGNAL HEAD, LED	4	EACH
708	TRAFFIC SIGNAL CABLE (5C/14 A.W.G.)	1399	LIN. FT.
708	TRAFFIC SIGNAL CABLE (7C/14 A.W.G.)	210	LIN. FT.
708	TRAFFIC SIGNAL CABLE (12C/14 A.W.G.)	155	LIN. FT.
708	TRAFFIC SIGNAL CABLE (20C/14 A.W.G.)	218	LIN. FT.
709	GALVANIZED STEEL CONDUIT (1.25")	20	LIN. FT.
710	NON-METALLIC CONDUIT (1.25")	20	LIN. FT.
710	NON-METALLIC CONDUIT (2")	20	LIN. FT.
710	NON-METALLIC CONDUIT (3")	249	LIN. FT.
711	CONCRETE PULL BOX (TYPE 1)	1	EACH
711	CONCRETE PULL BOX (TYPE 2 HD)	3	EACH
711	CONCRETE PULL BOX (TYPE 2)	2	EACH
SP&714	TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (30')	1	EACH
SP&714	TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (34')	1	EACH
SP&714	TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (36'-26')	1	EACH
SP&715	TRAFFIC SIGNAL PEDESTAL POLE WITH FOUNDATION	2	EACH
733	VIDEO CABLE	923	LIN. FT.
• SP&733	VIDEO DETECTOR (CLR)	6	EACH
733	VIDEO MONITOR (CLR)	1	EACH
• SP&733	VIDEO PROCESSOR, EDGE CARD (2 CAMERA)	3	EACH
SP&733	VEHICLE DETECTOR RACK (16 CHANNEL)	1	EACH
SP	ANTENNA CABLE (TYPE 6)	70	LIN. FT.
SP	ELECTRICAL CONDUCTORS FOR LUMINAIRES	86	LIN. FT.
SP	ELECTRICAL CONDUCTORS-IN-CONDUIT (1C/8 A.W.G., E.G.C.)	378	LIN. FT.
SP	ELECTRICAL CONDUCTORS-IN-CONDUIT (1C/12 A.W.G., E.G.C.)	50	LIN. FT.
SP	ELECTRICAL CONDUCTORS-IN-CONDUIT (2C/6 A.W.G.)	20	LIN. FT.
SP	LOCAL RADIO WITH ANTENNA	1	EACH
SP	LED LUMINAIRE ASSEMBLY	1	EACH
SP	REMOVAL OF TRAFFIC SIGNAL EQUIPMENT	1.00	LUMP SUM
SP	SERVICE POINT ASSEMBLY (2 CIRCUITS)	1	EACH
SP	18" STREET NAME SIGN	4	EACH

• QUANTITIES INCLUDE ONE SPARE VIDEO DETECTOR AND ONE SPARE VIDEO PROCESSOR.

OVERHEAD STREET NAME MARKER STANDARD MAST ARM MOUNTED



NOTES:

1. REFLECTIVE SHEETING SHALL COMPLY WITH ASTM 4956 TYPE 8 OR 9. REFLECTIVE SHEETING, SHEETING AND LEGEND SHALL BE APPLIED IN SUCH A MANNER TO PROVIDE WRINKLE AND BUBBLE FREE SURFACES. APPLICATION OF SHEETING IS CAUSE FOR REJECTION OF MATERIALS DUE TO WORKMANSHIP.

2. ALUMINUM SIGN BLANK SHALL BE ALLOY 6061-T6 OR 5052-H38. THE ALUMINUM SIGN SHALL ALSO BE ANODIZED. THE ALUMINUM SHEETING SHALL BE 0.100 INCH NOMINAL THICKNESS AND OF THE SIZE SHOWN WITH 1.5" CORNER RADIUS. PRIOR TO FABRICATION OF THE SIGNS, THE LAYOUT SHALL FIRST BE APPROVED BY AN AGENT OF THE CITY.

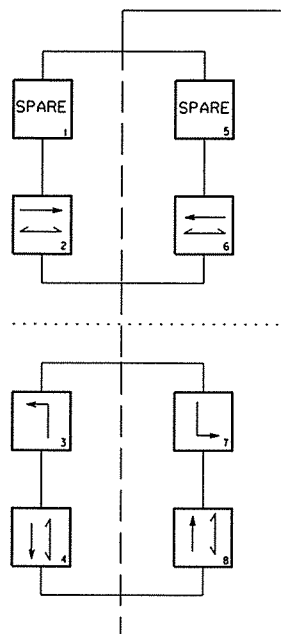
3. WHEN CROSSROAD HAS TWO NAMES, THE SIGN FOR THE CROSSROAD TO THE LEFT MAY BE INSTALLED ON THE BACKSIDE OF THE MAST ARM OF THE NEAR SIDE LEFT POLE. SEE STD. DETAIL SHEET FOR MORE INFORMATION FOR MOUNTING ON MAST ARM ASSEMBLY.

4. THE C 2000 STANDARD ALPHABET SHALL BE USED FOR ALL LETTERS.

LOCATION: N. MAIN ST. / E. BOWER AVE.
CITY: HARRISON
COUNTY: BOONE
DISTRICT: 9 SCALE: N/A DRAWN BY: GWE

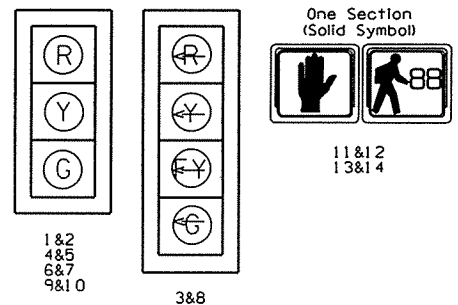
DATE: 03-10-16 FILE NAME: t090426.01.dgn

PHASING DIAGRAM



SIGNAL FACES

12" LENSES



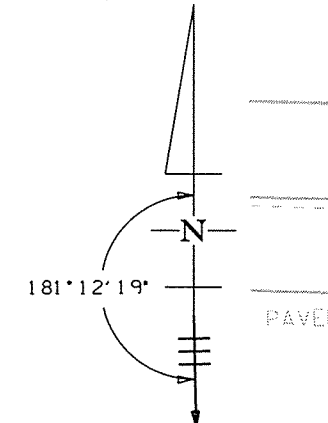
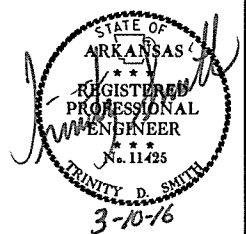
- NOTES:
1. ALL SIGNAL HEADS SHALL HAVE BACKPLATES.
 2. REFER TO SPECIAL PROVISIONS FOR DETAILS ON NEW REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
 3. ALL PEDESTRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A.D.A.S. STANDARDS.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090426		12	28

2 SIGNALIZATION PLAN SHEET

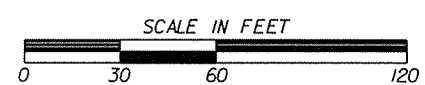
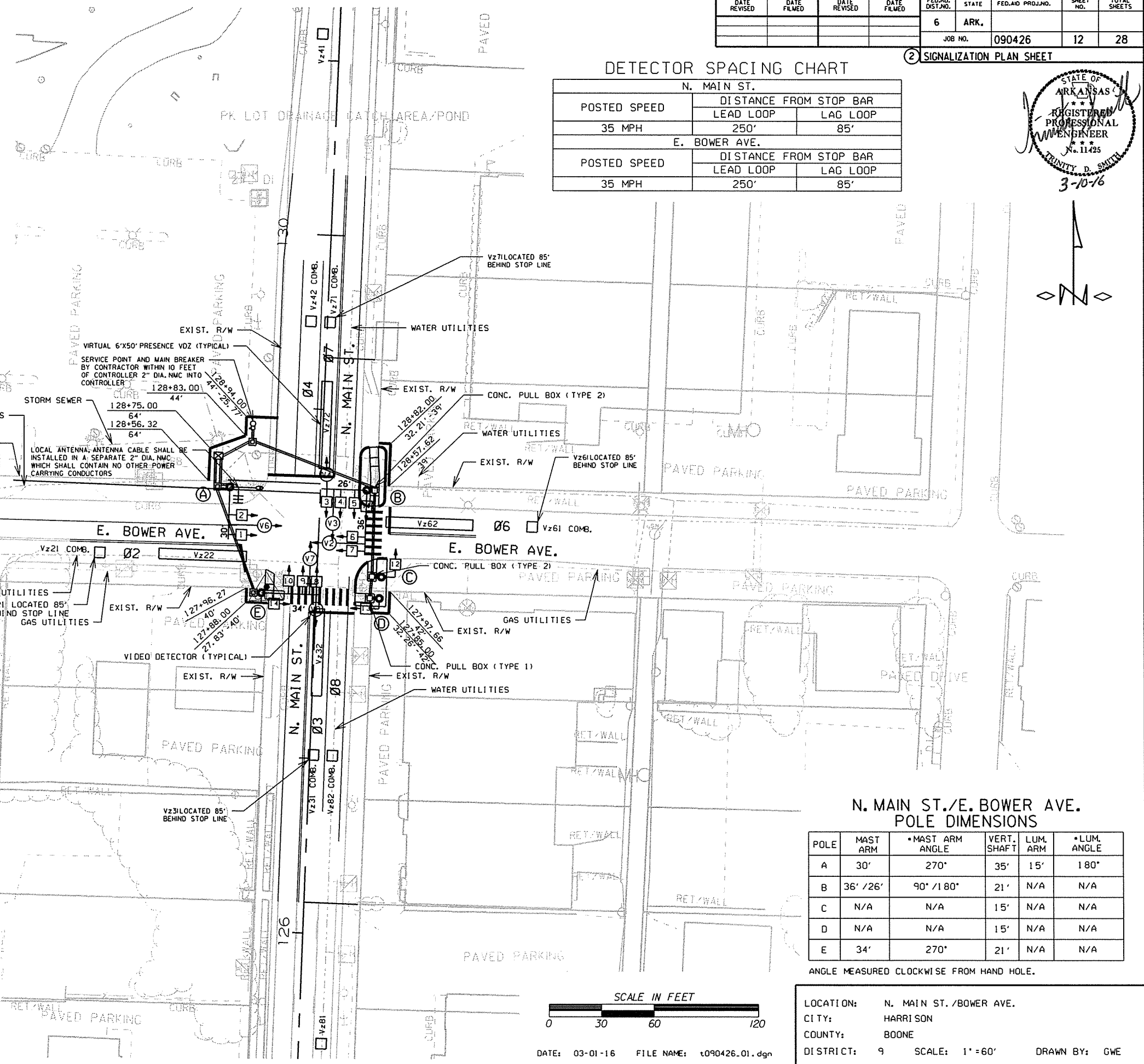
DETECTOR SPACING CHART

N. MAIN ST.		
POSTED SPEED	DISTANCE FROM STOP BAR	
35 MPH	LEAD LOOP	LAG LOOP
	250'	85'
E. BOWER AVE.		
POSTED SPEED	DISTANCE FROM STOP BAR	
35 MPH	LEAD LOOP	LAG LOOP
	250'	85'



ANTENNA ORIENTATION
 N. MAIN ST./E. BOWER AVE.
 ORIENTED TO MASTER ANTENNA
 LOCATED AT THE INTERSECTION OF
 N. MAIN ST./E. STEPHENSON AVE.
 IN THE CITY OF HARRISON

NOTE TO CONTRACTOR:
 TRAFFIC SIGNAL OPERATIONS SHALL BE MAINTAINED THROUGHOUT ALL CONSTRUCTION PHASES. IT IS RECOMMENDED THAT THE EXISTING TRAFFIC SIGNAL BE LEFT IN OPERATION UNTIL THE PERMANENT TRAFFIC SIGNAL AND SERVICE POINT ARE COMPLETED AND CONVERSION TO FULL OPERATION CAN BE MADE.



DATE: 03-01-16 FILE NAME: t090426_01.dgn

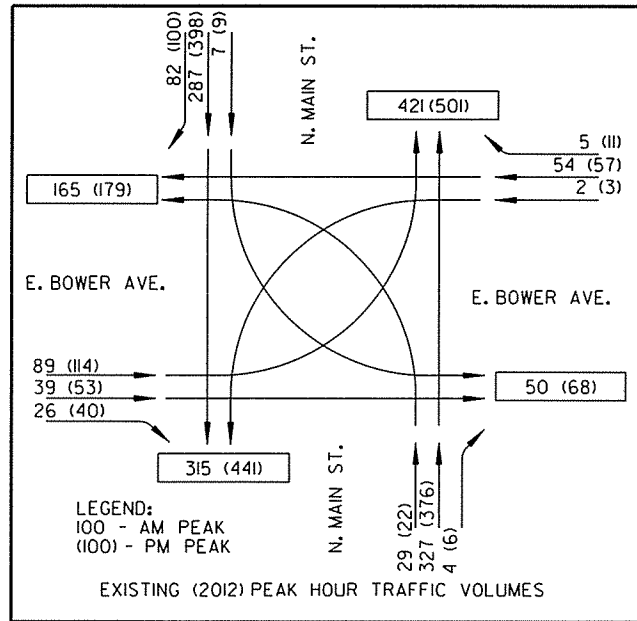
N. MAIN ST./E. BOWER AVE. POLE DIMENSIONS

POLE	MAST ARM	MAST ARM ANGLE	VERT. SHAFT	LUM. ARM	LUM. ANGLE
A	30'	270°	35'	15'	180°
B	36' / 26'	90° / 180°	21'	N/A	N/A
C	N/A	N/A	15'	N/A	N/A
D	N/A	N/A	15'	N/A	N/A
E	34'	270°	21'	N/A	N/A

ANGLE MEASURED CLOCKWISE FROM HAND HOLE.

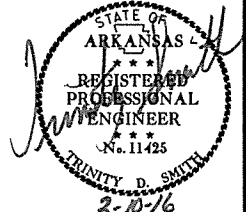
LOCATION: N. MAIN ST./BOWER AVE.
 CITY: HARRISON
 COUNTY: BOONE
 DISTRICT: 9 SCALE: 1" = 60' DRAWN BY: GWE

TRAFFIC FLOW DIAGRAM



DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. PROJ. NO.	STATE	FED. PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	090426		13	28

SIGNALIZATION PLAN SHEET

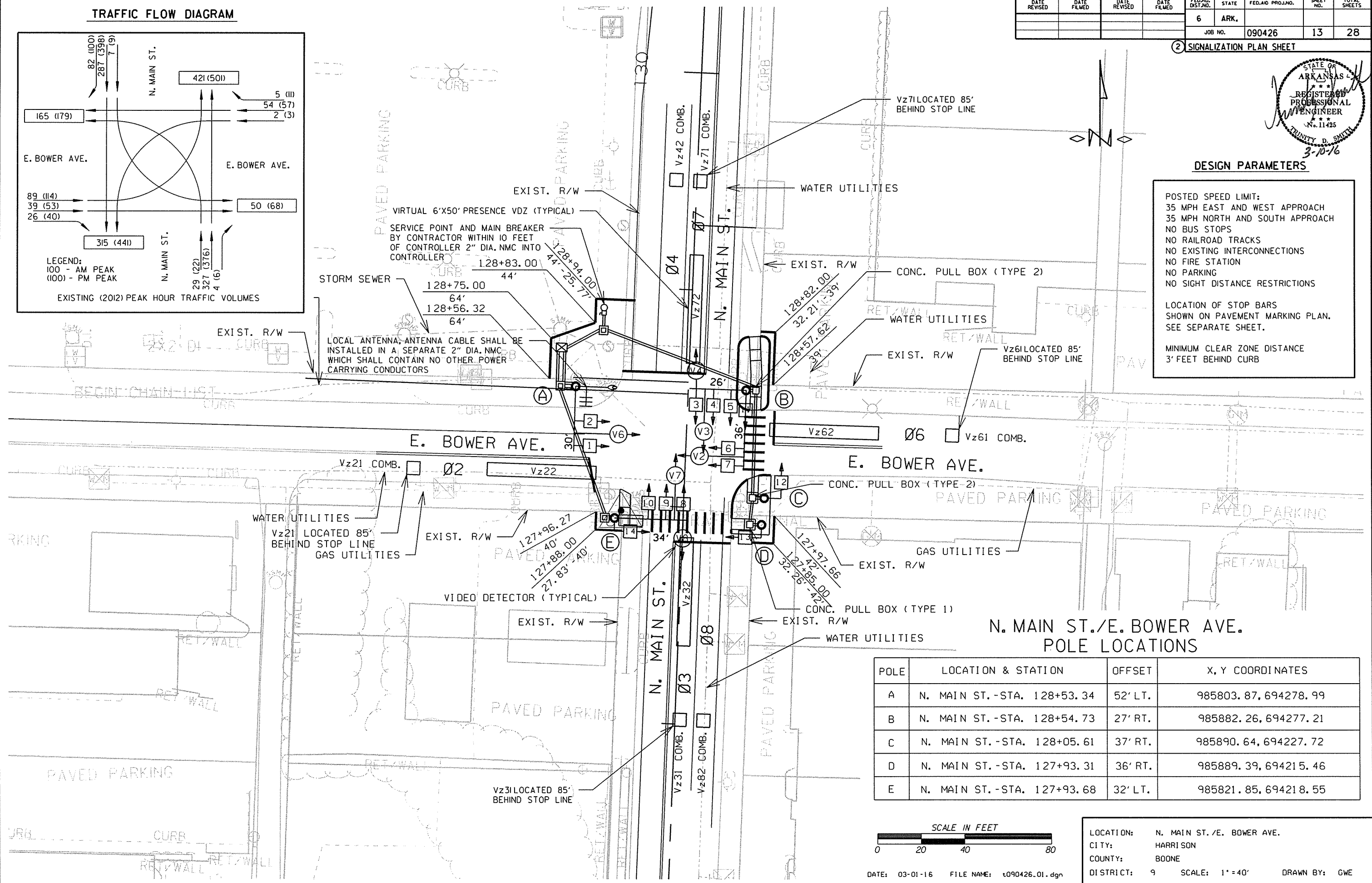


DESIGN PARAMETERS

POSTED SPEED LIMIT:
 35 MPH EAST AND WEST APPROACH
 35 MPH NORTH AND SOUTH APPROACH
 NO BUS STOPS
 NO RAILROAD TRACKS
 NO EXISTING INTERCONNECTIONS
 NO FIRE STATION
 NO PARKING
 NO SIGHT DISTANCE RESTRICTIONS

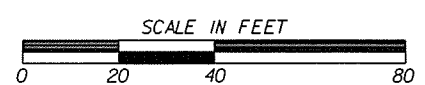
LOCATION OF STOP BARS
 SHOWN ON PAVEMENT MARKING PLAN.
 SEE SEPARATE SHEET.

MINIMUM CLEAR ZONE DISTANCE
 3' FEET BEHIND CURB



N. MAIN ST./E. BOWER AVE. POLE LOCATIONS

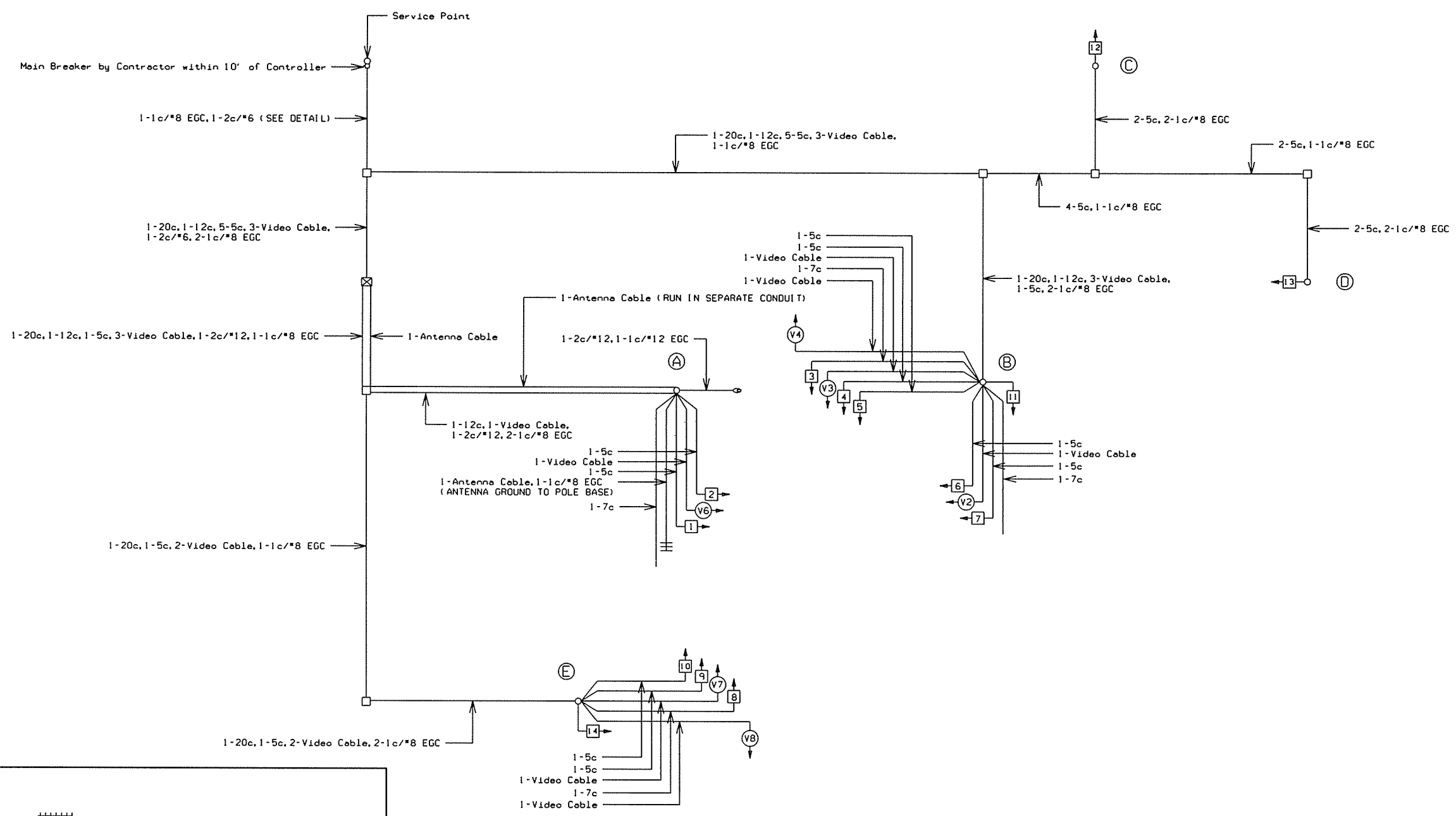
POLE	LOCATION & STATION	OFFSET	X, Y COORDINATES
A	N. MAIN ST. - STA. 128+53.34	52' LT.	985803.87, 694278.99
B	N. MAIN ST. - STA. 128+54.73	27' RT.	985882.26, 694277.21
C	N. MAIN ST. - STA. 128+05.61	37' RT.	985890.64, 694227.72
D	N. MAIN ST. - STA. 127+93.31	36' RT.	985889.39, 694215.46
E	N. MAIN ST. - STA. 127+93.68	32' LT.	985821.85, 694218.55



LOCATION: N. MAIN ST./E. BOWER AVE.
 CITY: HARRISON
 COUNTY: BOONE
 DISTRICT: 9 SCALE: 1" = 40' DRAWN BY: GWE

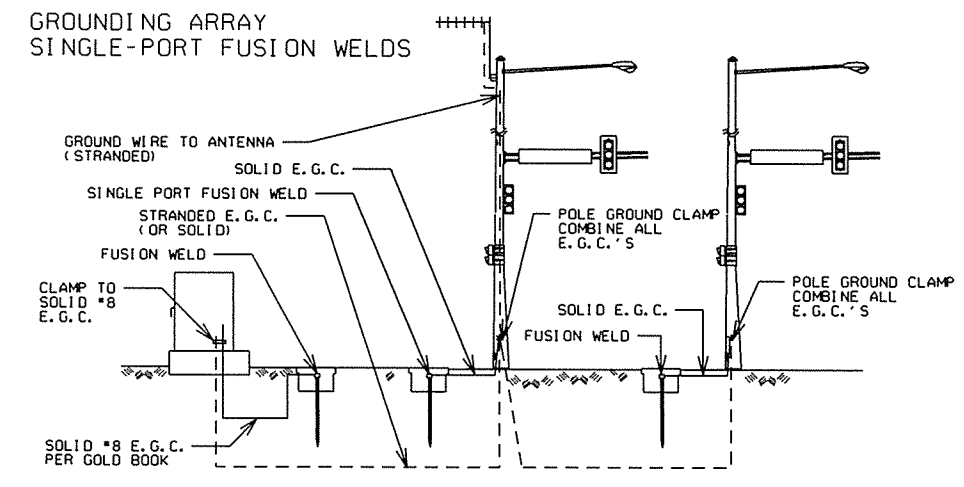
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 090426							14	28

2 SIGNALIZATION PLAN SHEET



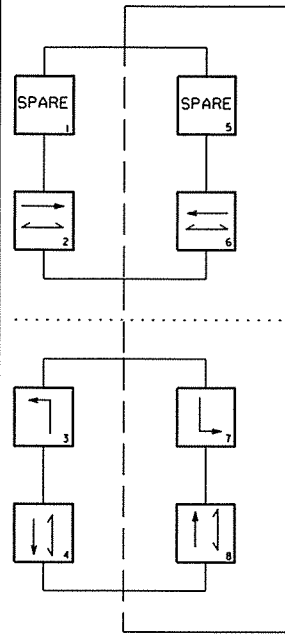
WIRING DIAGRAM

- NOTES TO CONTRACTOR:
- ONE SEPARATE 1-5c IS RUN TO EACH POLE FOR THE PEDESTRIAN PUSH BUTTON.
 - ALL DETECTOR RACK CHANNELS, INCLUDING UNUSED, SHALL BE BROUGHT TO TERMINAL STRIP IN DETECTOR AREA OF CABINET.
 - THE LOCAL GOVERNMENT SHALL BE RESPONSIBLE FOR PROVIDING POWER TO THE SERVICE POINT.



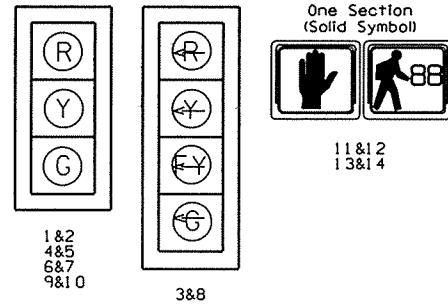
LOCATION: N. MAIN ST./E. BOWER AVE.
 CITY: HARRISON
 COUNTY: BOONE
 DISTRICT: 9 SCALE: N/A DRAWN BY: GWE

PHASING DIAGRAM



SIGNAL FACES

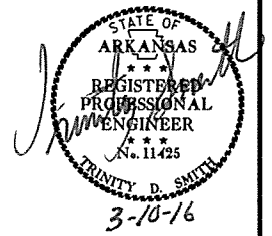
12" LENSES



- NOTES:
1. ALL SIGNAL HEADS SHALL HAVE BACKPLATES.
 2. REFER TO SPECIAL PROVISIONS FOR DETAILS ON NEW REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
 3. ALL PEDESTRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A. D. A. S. STANDARDS.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO.	090426
							SHEET NO.	15
							TOTAL SHEETS	28

2 SIGNALIZATION PLAN SHEET



DETECTOR SYSTEM DESCRIPTION: JOB 090426

DET. ID#	LOCATION DIRECTION	TYPE	DET. #	HARDWARE INPUTS BY SUPPLIER			PROGRAM ASSIGNMENTS		COMMENTS	TUBE LENGTHS
				CAB. TRM #	AMP CHN. #	CON. INP. #	LOCAL SYSTEM DET. #	MASTER SYSTEM DETECTOR NUMBERS		
Vz31	NB LEFT TURN FAR	COMB.			5	V11	3	3	CAMERA V3	23'
Vz32	NB LEFT TURN	LOCAL			6	V3	3		CAMERA V3	23'
Vz41	SB ADVANCE	LOCAL			9	V4	4		CAMERA V4	74'
Vz42	SB NEAR	COMB.			10	V12	4	4	CAMERA V7	23'
Vz21	EB LEFT TURN FAR	COMB.			1	V10	2	2	CAMERA V2	23'
Vz22	EB LEFT TURN	LOCAL			2	V2	2		CAMERA V2	23'
Vz61	WB ADVANCE	LOCAL			3	V6	6		CAMERA V6	23'
Vz62	WB NEAR	COMB.			4	V14	6	6	CAMERA V6	23'
Vz71	SB LEFT TURN FAR	COMB.			11	V15	7	7	CAMERA V7	23'
Vz72	SB LEFT TURN	LOCAL			12	V7	7		CAMERA V7	23'
Vz81	NB ADVANCE	LOCAL			7	V8	8		CAMERA V8	74'
Vz82	NB NEAR	COMB.			8	V16	8	8	CAMERA V3	23'
PB6A&B	E. BOWER AVE. E. LEG	PED.				P2	2			
PB8A&B	N. MAIN ST. S. LEG	PED.				P8	8			
SPARE 13, 14, 15 & 16										

CONTROLLER INPUT ABBREVIATIONS:
V = VEHICLE INPUT
D = SYSTEM OR AUXILIARY INPUT
P = PEDESTRIAN INPUT

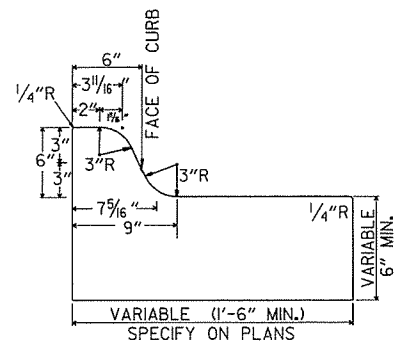
NOTE:
AMP CHN= REFERS TO THE DETECTOR RACK OUTPUT POSITION. THIS IS WIRED TO CONTROLLER INPUT DETECTOR NUMBER WHICH IS PROGRAMMED TO ACTUATE THE DESIGNATED PHASE. EXAMPLE: V9 = SYSTEM DETECTOR 1, V10 = SYSTEM DETECTOR 2

INTERVAL CHART

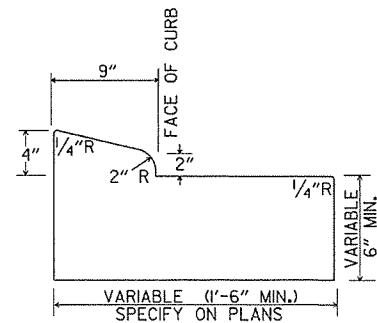
SIGNAL FACES	N. MAIN ST./E. RUSH AVE.								FLASH SEQ.	
	2+6	CLR.	3+7	CLR.	3+8	CLR.	4+7	CLR.		4+8
1&2	G	**	R	R	R	R	R	R	R	R
3	←R	←R	←G	•	←FY	***	←R	←R	←FY	***
4&5	R	R	R	R	G	**	R	R	G	**
6&7	G	**	R	R	R	R	R	R	R	R
8	←R	←R	←G	•	←FY	***	←G	•	←FY	***
9&10	R	R	R	R	R	R	G	**	G	**
11&12	DW	DW	DW	DW	DW	DW	DW	DW	W	FDW
13&14	W	FDW	DW	DW	DW	DW	DW	DW	DW	BLK

- DENOTES GREEN OR YELLOW ARROW DEPENDING ON NEXT PHASE
- ** DENOTES GREEN OR YELLOW BALL DEPENDING ON NEXT PHASE
- *** DENOTES FLASHING YELLOW ARROW OR YELLOW ARROW DEPENDING ON NEXT PHASE

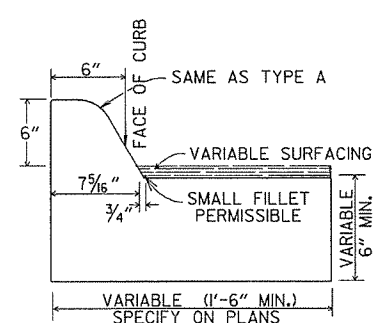
LOCATION: N. MAIN ST./E. BOWER AVE.
CITY: HARRISON
COUNTY: BOONE
DISTRICT: 9 SCALE: N/A DRAWN BY: GWE



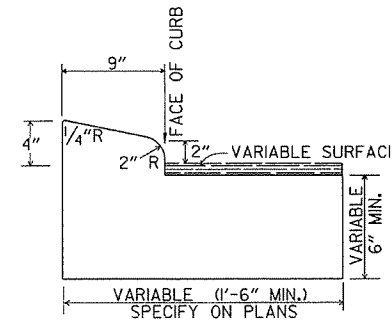
TYPE A



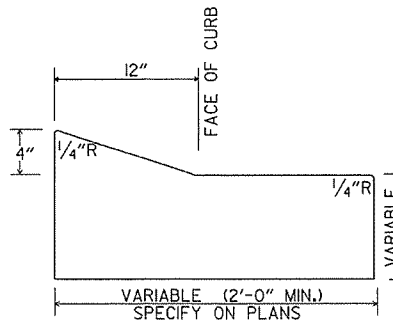
TYPE B-1



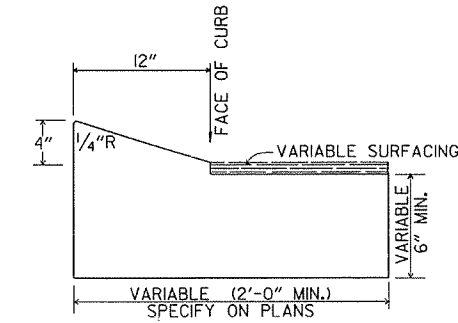
TYPE C



TYPE B-2

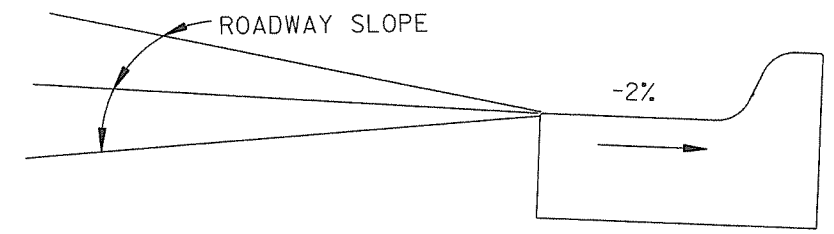


TYPE E-1

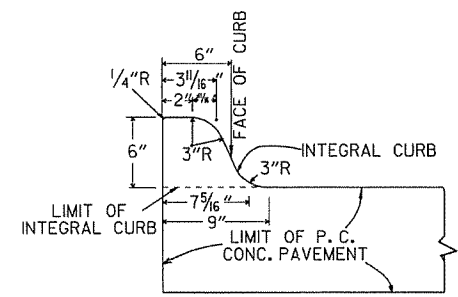


TYPE E-2

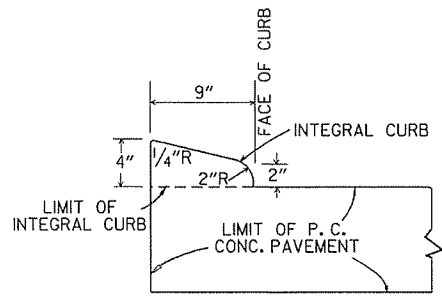
CONCRETE COMBINATION CURB AND GUTTER



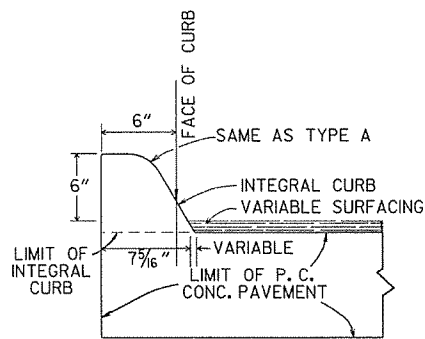
DETAIL OF GUTTER SLOPE
GUTTER SHALL BE CONSTRUCTED ON 2% SLOPE AWAY FROM ROADWAY, REGARDLESS OF ROADWAY SLOPE.



TYPE A

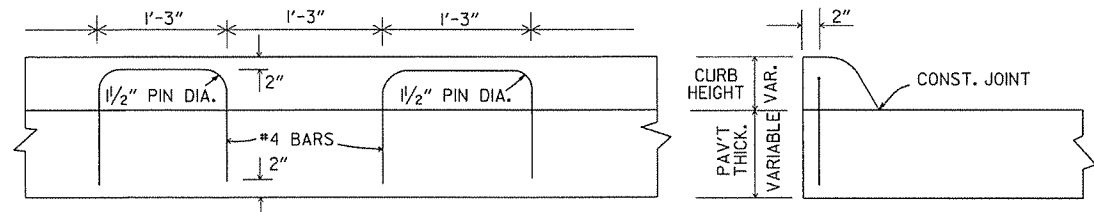


TYPE B



TYPE C

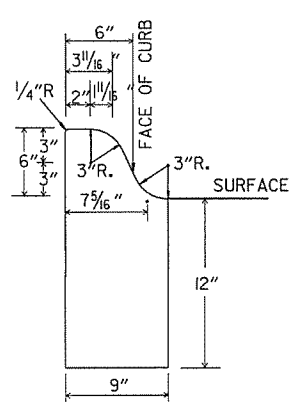
INTEGRAL CURB



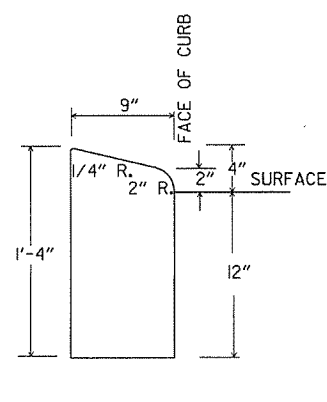
LONGITUDINAL SECTION

ELEVATION

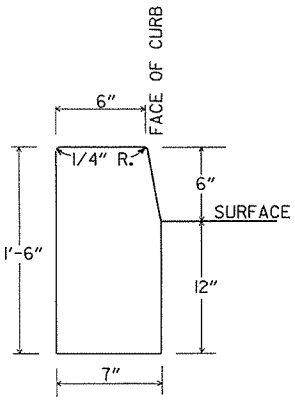
ALTERNATE CONSTRUCTION METHOD FOR INTEGRAL CURB



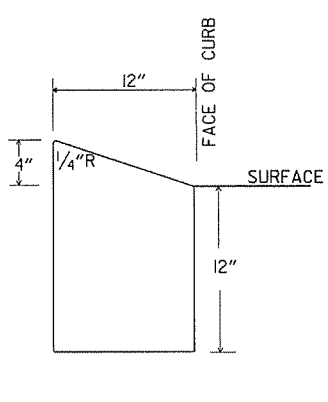
TYPE A



TYPE B

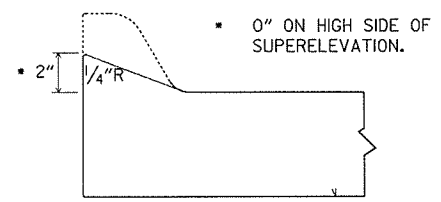


TYPE D



TYPE E

CONCRETE CURB



NOTE: USE MODIFIED CURB AS SPECIFIED ON STD. GR-1. COMPENSATION FOR MODIFIED CURB WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE TYPE OF CURB OR CURB AND GUTTER SPECIFIED.

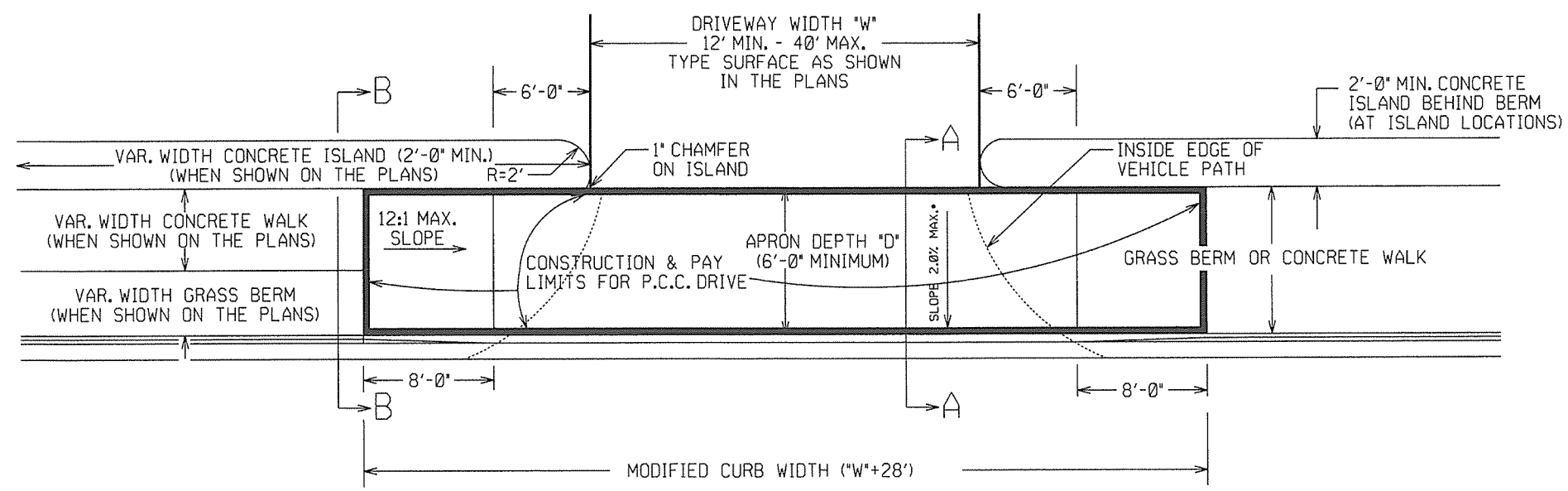
DETAILS OF MODIFIED CURB

DATE	REVISION	DATE FILMED
11-29-07	REVISED GUTTER SLOPE & MODIFIED CURB DETAILS	
11-10-05	ADDED DETAILS OF TYPE E CURBS	
11-16-01	REVISED CONCRETE CURB TYPE B	
11-18-98	REVISED MODIFIED CURB	
6-2-94	ADDED NOTE TO SPECIAL MODIFIED CURB	
8-5-93	CORRECTED GUTTER SLOPE	8-5-93
10-1-92	ADDED DETAILS OF GUTTER SLOPE	10-1-92
5-24-90	ADDED DETAILS OF MODIFIED CURB	5-24-90
11-30-89	VARIABLE DEPTH TYPE A & B 1	11-30-89
7-15-88	REVISED MODIFIED CURB	630-7-15-88
11-1-73	REVISED MODIFIED CURB	500-11-1-73
10-2-72	REVISED AND REDRAWN	512-10-2-72

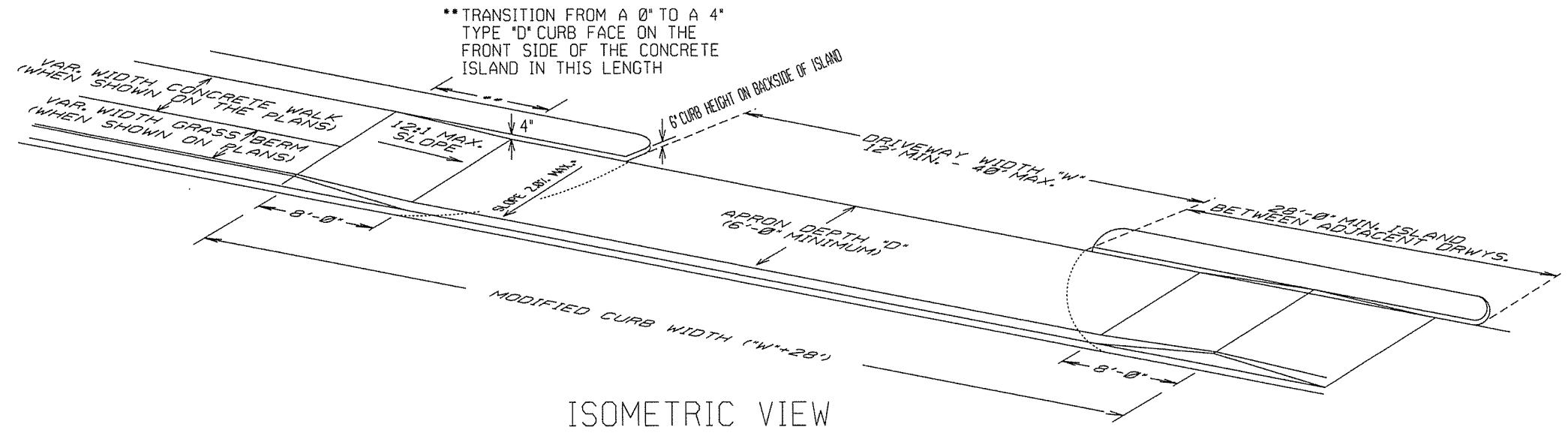
ARKANSAS STATE HIGHWAY COMMISSION

CURBING DETAILS

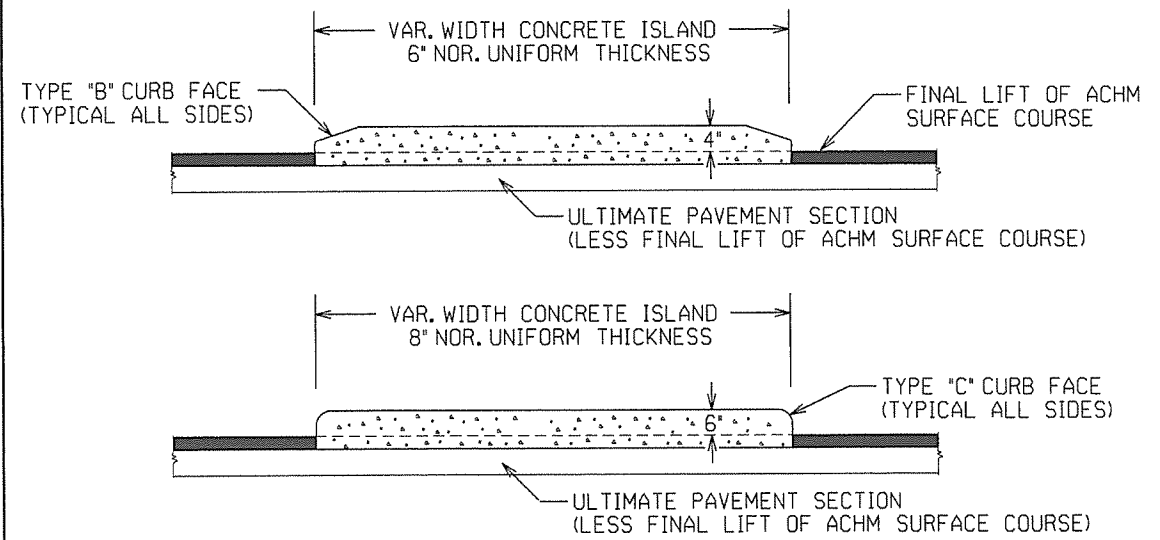
STANDARD DRAWING CG-1



PLAN VIEW

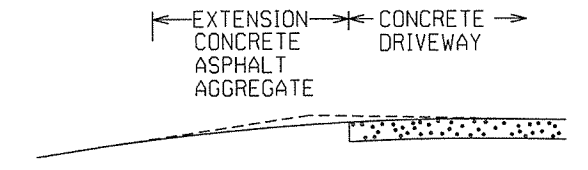


ISOMETRIC VIEW



CURBED ISLANDS FOR CHANNELIZATION

REFER TO PLANS FOR TYPE OF CURB FACE TO BE USED. NO DIRECT PAYMENT WILL BE MADE FOR THE CURB FACES SHOWN ON THE ISLAND DETAILS. PAYMENT FOR THE CURB FACE WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEM "CONCRETE ISLAND".

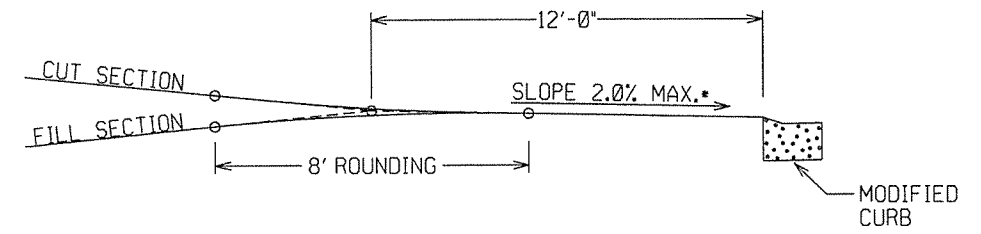


EXTENSION TYPICAL SECTIONS

- 1: CONCRETE - 6" P.C. CONCRETE DRIVEWAY
- 2: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
4" ACHM BINDER COURSE (1") OR
4" ACHM BASE COURSE (1-1/2")
- 3: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
7" AGGREGATE BASE COURSE
- 4: AGGREGATE - 6" AGGREGATE BASE COURSE

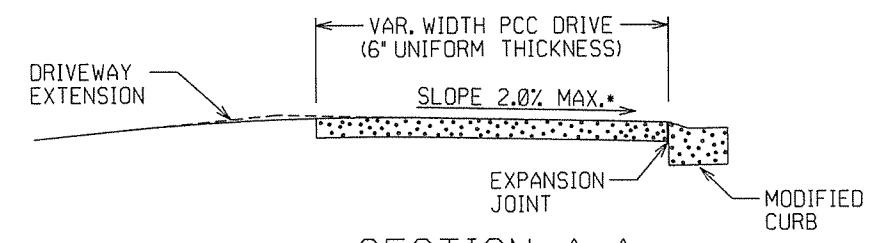
THE TYPE OF EXTENSION SHALL BE AS SHOWN IN THE PLANS. THE CONTRACTOR MAY, WITH THE APPROVAL OF THE ENGINEER, SUBSTITUTE A LOWER NUMBERED TYPE OF EXTENSION IN LIEU OF THE TYPE SPECIFIED IN THE PLANS, BUT AT NO ADDITIONAL COST TO THE DEPARTMENT.

DRIVEWAY EXTENSION DETAILS

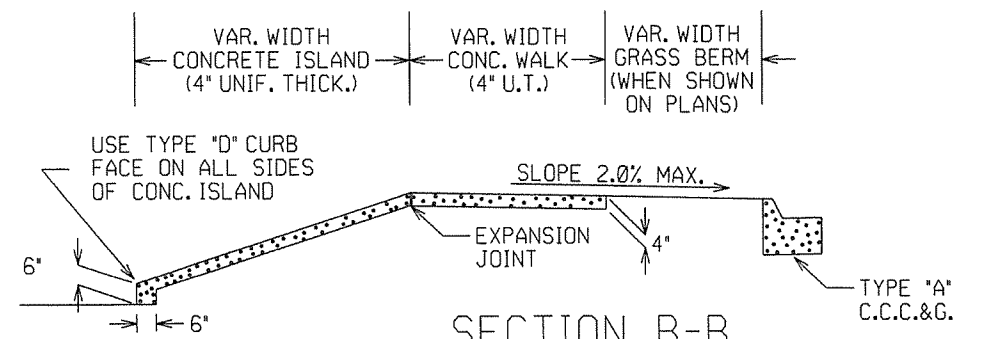


DRIVEWAY VERTICAL ALIGNMENT DETAILS

NOTE: DRIVEWAYS MAY NOT BE SLOPED AWAY FROM THE ROADWAY UNLESS APPROVED BY THE ENGINEER.

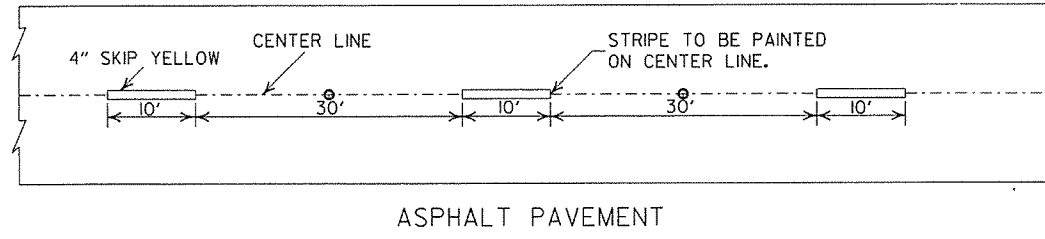
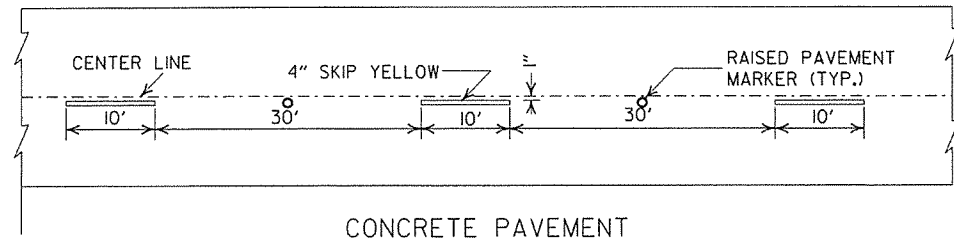


SECTION A-A



SECTION B-B
CURBED ISLAND BEHIND WALK

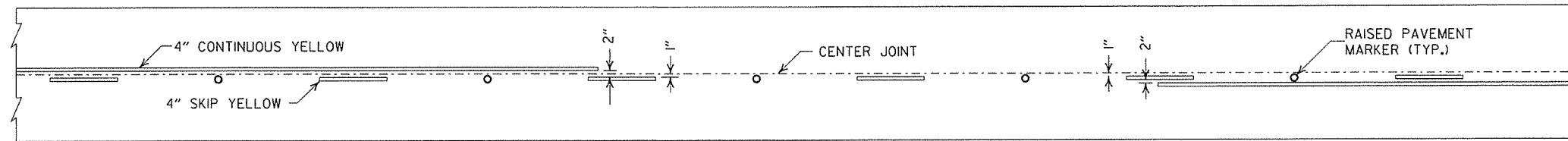
DATE	REV/DATE FILMED	DESCRIPTION
2-27-14		REVISED PLAN & ISOMETRIC VIEW
11-29-07		ADDED CHANNELIZATION ISLAND WITH TYPE C CURB FACE & REVISED DRIVEWAY SLOPE NOTE & VERTICAL ALIGNMENT DETAIL
11-10-05		REV. APRON SLOPE & DEPTH OF AGG. BASE.
8-22-02		ADDED ISLAND DETAILS & NOTES
3-30-00		REV. MOD. CURB WIDTH & TRANS. NOTE
11-19-98		REVISED NOTES
11-18-98		REDRAWN AND REISSUED



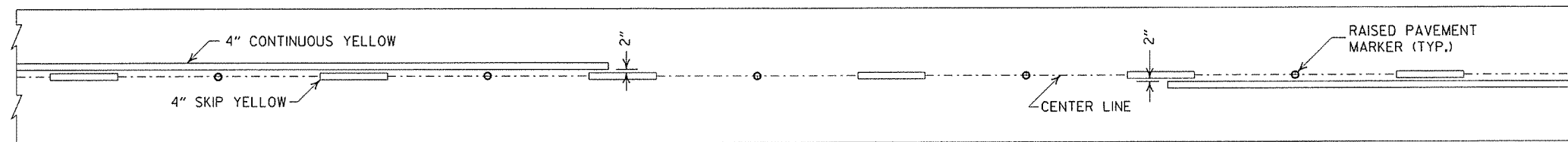
CONCRETE PAVEMENT

ASPHALT PAVEMENT

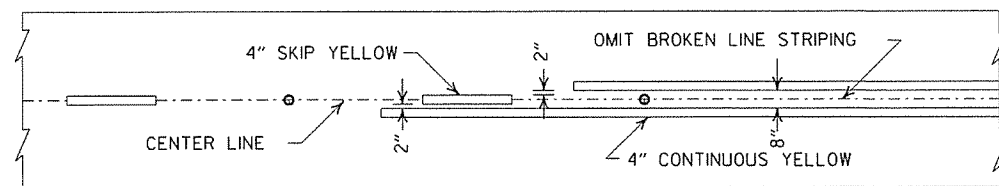
BROKEN LINE STRIPING



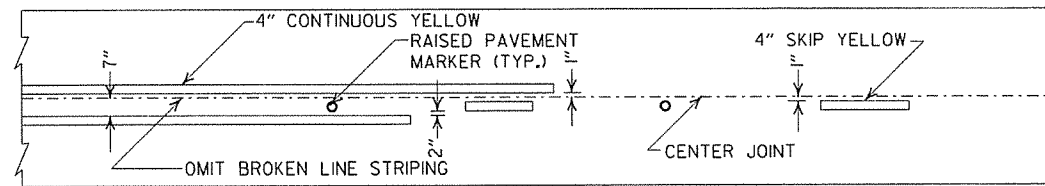
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

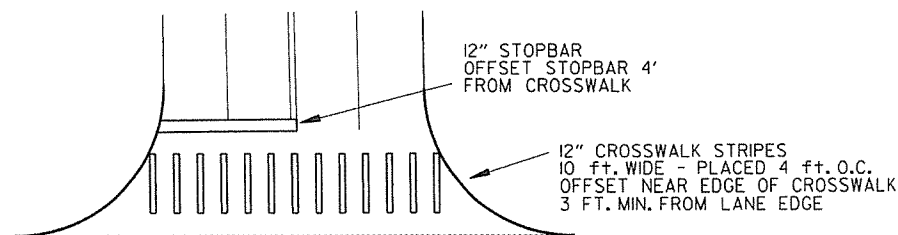


ASPHALT PAVEMENT



CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

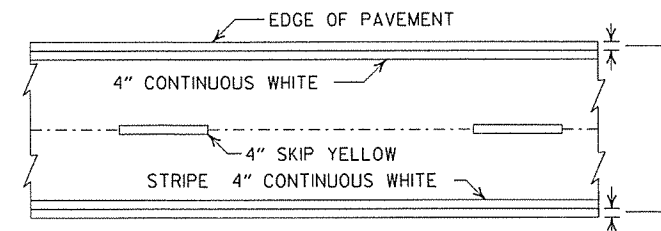


CROSSWALK AND STOPBAR DETAILS

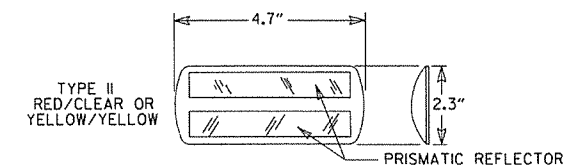
NOTES:

1. ALL LINES SHALL HAVE A WIDTH OF 4 INCHES.
2. THE THICKNESS AND RATE OF PAINT APPLICATION SHALL BE AS SPECIFIED IN SECTION 718 OF THE STANDARD SPECIFICATIONS.
3. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. RAISED PAVEMENT MARKERS SHALL BE CENTERED BETWEEN SKIP LINES ON 40 FEET SPACING UNLESS OTHERWISE SHOWN ON THE PLANS.

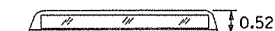
2" FOR ASPHALT OR CONCRETE PAVEMENT
6" FOR BITUMINOUS SURFACE TREATMENT



PAVEMENT EDGE LINE MARKING



NOTE:
THE RED LENS OF THE
TYPE II R.P.M. SHALL
FACE THE INCORRECT
TRAFFIC MOVEMENT.



DETAIL OF
STANDARD
RAISED PAVEMENT MARKERS

GENERAL NOTES:

THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND RAISED PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:

DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER, REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS

STANDARD DRAWING PM-1

LOOP DETECTOR INSTALLATION AND TESTING

NOTES:

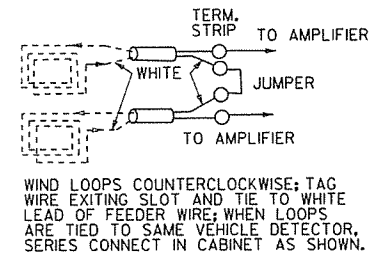
- LOOPS WITH A PERIMETER GREATER THAN 40' SHALL HAVE TWO TURNS. LOOPS WITH A PERIMETER LESS THAN OR EQUAL TO 40' SHALL HAVE THREE TURNS, UNLESS OTHERWISE NOTED ON THE PLANS. QUADRUPOLE LOOPS SHALL BE TWO TURNS (2-4-2 CONFIGURATION) UNLESS OTHERWISE NOTED.
- LOOP AND FEEDER WIRE SHALL BE CONTINUOUS WITHOUT SPLICES EXCEPT AT THE LOOP/FEEDER WIRE SPlice AS SHOWN. SPlice SHALL BE ROSIN SOLDERED AND WATERPROOFED WITH AN ACCEPTED SPlice KIT. DRAIN WIRE SHALL BE GROUNDED IN CABINET AND INSULATED AT LOOP TO FEEDER SPlice.
- THE LOOP TO FEEDER SPlice, FEEDER JACKET AND JACKET OF LOOP WIRE IN DUCT SHALL BE COMPLETELY SEALED AND WATERPROOFED.
- CONTRACTOR MAY MAKE CONNECTIONS TO SIGNAL CABLE AND LOOP TO FEEDER CONNECTION AT TERMINAL STRIPS MOUNTED TO POLE INSIDE HAND HOLD COVER AS SHOWN IN DETAIL. TERMINALS MUST BE EASILY ACCESSIBLE, BUT PROTECTED AGAINST ACCIDENTAL CONTACT. CONNECTION OF POWER CARRYING CIRCUITS MUST BE SEPARATED FROM LOOP OR LOGIC CIRCUITS. ALL CONNECTIONS TO TERMINAL STRIPS SHALL UTILIZE SPADE LUGS OR AS APPROVED BY THE ENGINEER.
- EACH LOOP SHALL HAVE A SEPARATE "FEEDER WIRE" UNLESS OTHERWISE NOTED. ALL FEEDER WIRES SHALL BE LABELED AS TO LOOP NUMBER AS DESIGNATED ON THE PLANS.
- ALL LOOP WIRE ENTERING PULL BOXES SHALL BE ENCLOSED IN CONDUIT. EACH LOOP WIRE SHALL ENTER PULL BOX OR POLE BASE THROUGH A SEPARATE PIECE OF ONE INCH (1") CONDUIT.
- LOOP WIRE FROM LOOP TO CONDUIT IS NOT TWISTED. LOOP WIRE IN THE CONDUIT MUST BE TWISTED TWO TO FIVE TURNS PER FOOT.
- WARRANTY PERIOD FOR LOOPS SHALL NOT COMMENCE UNTIL TESTED BY THE CONTRACTOR AND ACCEPTED BY THE ENGINEER. CONTRACTOR SHALL PERFORM TEST AND PROVIDE A RECORD TO THE ENGINEER AS LISTED IN THE DETECTOR LOOP TESTING PROCEDURE.
- UNLESS OTHERWISE APPROVED BY THE ENGINEER, BACKER ROD SHALL BE INSTALLED IN SHORT SECTIONS SPACED NOT MORE THAN 18" APART AND WEDGED INTO SLOT TO HOLD CABLE IN PLACE. CABLE SHALL BE TOTALLY ENCAPSULATED IN SEALER.
- "HOT POUR" SEALER SHALL NOT BE ALLOWED WITH 705-LOOP WIRING IN DUCT.
- WHERE UNDERGROUND SPLICES OF SIGNAL CABLE ARE REQUIRED, CONNECTIONS SHALL BE SOLDERED AND COMPLETELY WATERPROOFED TO THE SATISFACTION OF THE ENGINEER. WATERPROOFING SHALL EXTEND A MINIMUM OF TWO INCHES PAST THE SIGNAL CABLE JACKET AND SHALL COMPLETELY COVER ALL INDIVIDUAL CONDUCTORS OF THE SIGNAL CABLE. WATERPROOFING DOES NOT APPLY TO CONNECTIONS MADE IN POLE BASES.
- CONTRACTOR SHALL CONNECT A SEPARATE NEUTRAL FOR EACH LOAD SWITCH REPRESENTED ON EACH SIGNAL POLE. ONLY ONE NEUTRAL IS REQUIRED FOR PEDESTRIAN SIGNALS. A SEPARATE 5C (TYPICAL) IS PROVIDED FOR PEDESTRIAN PUSH BUTTONS.
- TRAFFIC CONTROLLER CABINET AND LAYOUT SHALL BE SUCH THAT IT IS NOT NECESSARY TO SHUT DOWN POWER OR REMOVE LOAD SWITCHES IN ORDER TO EASILY TEST OR MODIFY DETECTOR INPUTS TO CONTROLLER. CONTROLLER CABINET SHALL BE WIRED SUCH POWER TO LOAD SWITCHES CANNOT BACKFEED TO LOAD SWITCH POWER BUSS DURING FLASH OPERATION.

TYPICAL PROCEDURE FOR DETECTOR LOOP TESTING

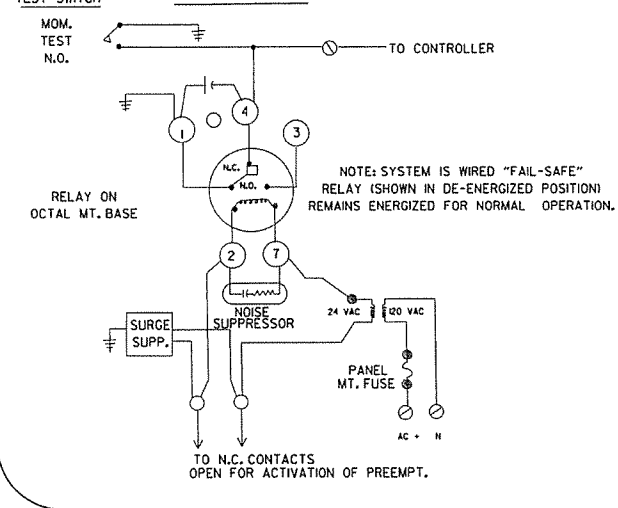
- DISCONNECT AND TEST CONTINUITY (< 10 OHMS) IF CONTINUITY IS BAD, GO TO TEST 3
- TEST INSULATION (@ 500 VOLT TEST > 10 MEG-OHM) IF TESTS 1 & 2 ARE GOOD, NO FURTHER TESTING IS NECESSARY. RECORDED RESULTS CONSIST OF TESTS 1 & 2 FROM CONTROL CABINET WITH FEEDER WIRE CONNECTED TO LOOP.
- OPEN SPlice (DO NOT BREAK CONNECTION) REPEAT TEST 1 & 2 IF TEST 3 IS BAD, GO TO TEST 4
- BREAK SPlice, INSTALL JUMPER IN CABINET, REPEAT TESTS 1 & 2 SEPARATELY FOR FEEDER AND FOR LOOP

FAILURES TYPICALLY RESULT FROM BROKEN WIRE IN PAVEMENT, FAULTY INSULATION OF LOOP OR FEEDER WIRE, OR POORLY INSULATED SPlice CONNECTION.

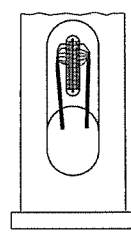
SERIES CONNECTED LOOPS



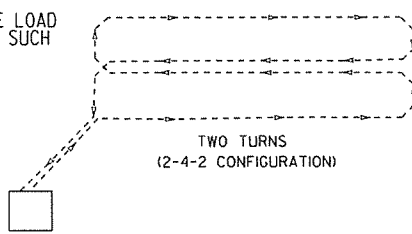
TRAFFIC SIGNAL PRE-EMPTION INTERFACE WIRING DIAGRAM



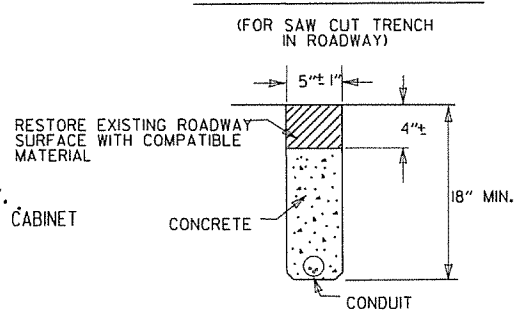
HANDHOLE TERMINAL



QUADRUPOLE LOOP

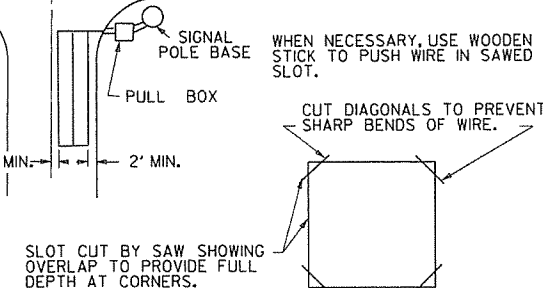
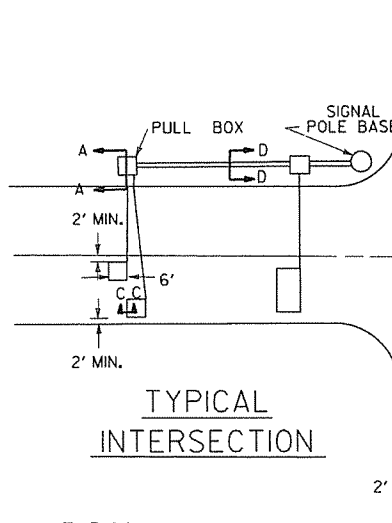


TRENCHING DETAIL

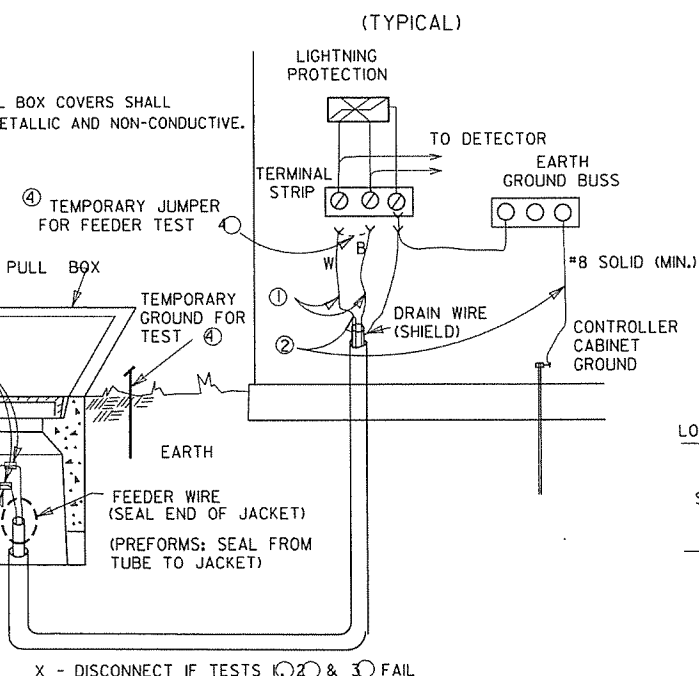
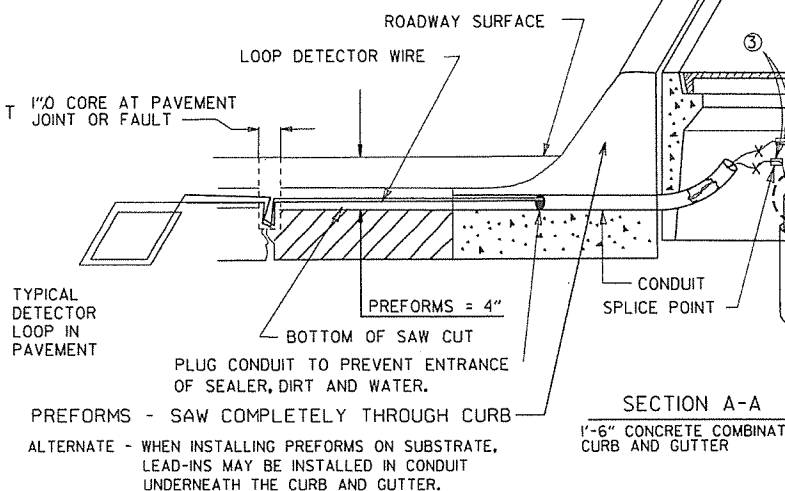
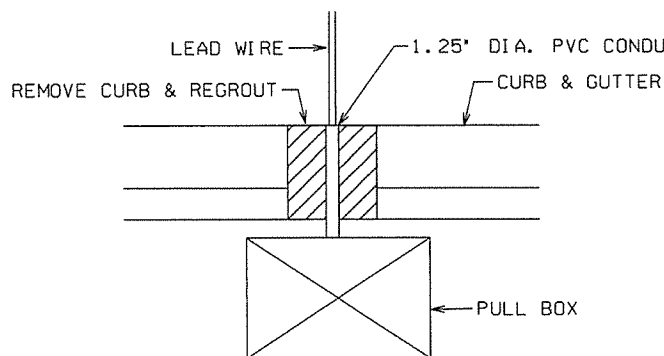
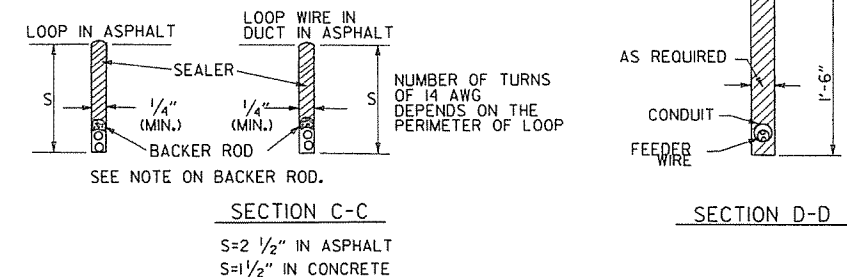


NOTE: CONDUIT SHALL BE INSTALLED IN CURB AS SHOWN OR AS DIRECTED BY THE ENGINEER. END OF CONDUIT SHALL BE WATER-TIGHT.

TYPICAL INTERSECTION



TYPICAL SECTIONS FOR PULSE AND PRESENCE LOOP DETECTORS

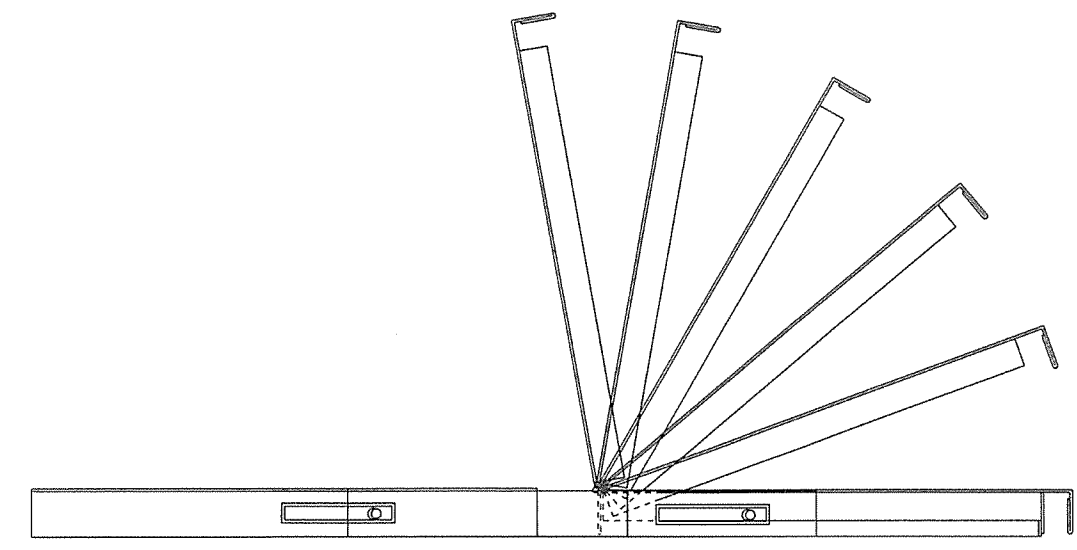
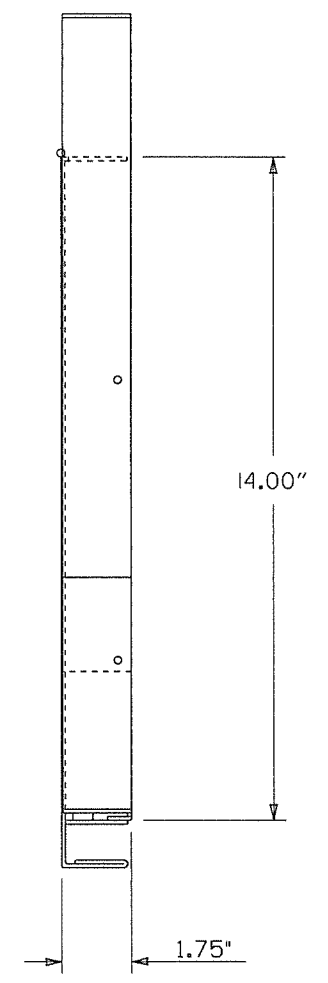
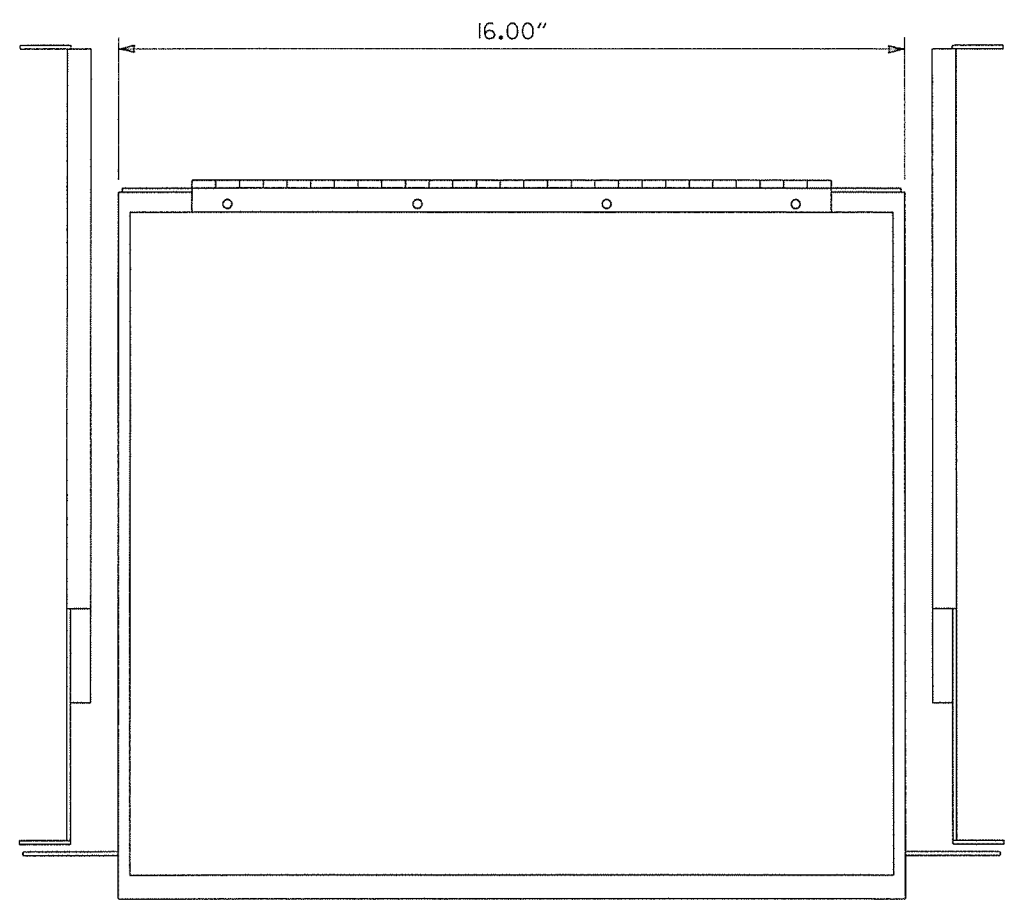


SPECIAL NOTE
IF FEEDER WIRE JACKET IS LEFT UNSEALED AND WATER IS ALLOWED TO ENTER JACKET, CONTRACTOR WILL BE REQUIRED TO REPLACE FEEDER AT NO COST TO THE DEPARTMENT.

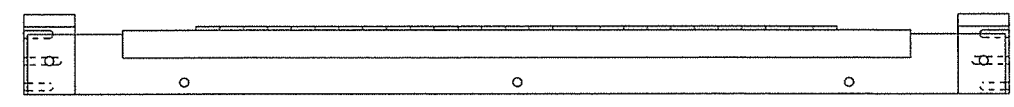
DATE	REVISION	DATE FILM
9-12-13	ISSUED AS STANDARD DRAWING	
5-17-01	REVISED	
4-11-01	REVISED	
2-4-00	REVISED PRE-EMPTION TEST SWITCH	
11-18-98	REVISED NOTES	
11-21-95	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION
LOOP DETECTOR INSTALLATION
STANDARD DRAWING SD-4

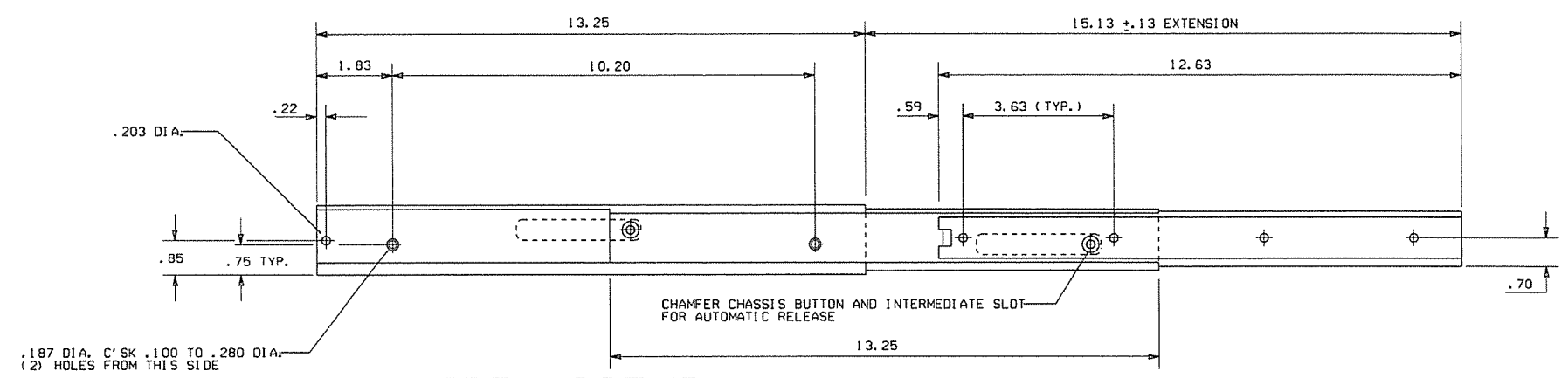
DRAWER PLAN VIEW



- NOTES:
 1. RIGHT HAND SLIDE SHOWN, LEFT SLIDE OPPOSITE.
 2. GENERAL DEVICES (CC3002-99-0102) OR EQUAL AND CONTAINS (1) RIGHT HAND SLIDE ASSEMBLY, (1) LEFT HAND SLIDE ASSEMBLY.
 3. ALL HARDWARE NECESSARY TO FASTEN SLIDE ASSEMBLY TO UNDERSIDE OF CONTROLLER SHELF SHALL BE INCLUDED.



FRONT VIEW

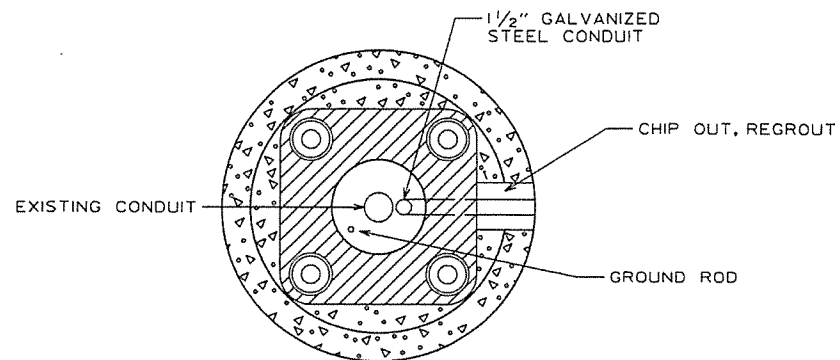


.187 DIA. C'SK .100 TO .280 DIA.
 (2) HOLES FROM THIS SIDE

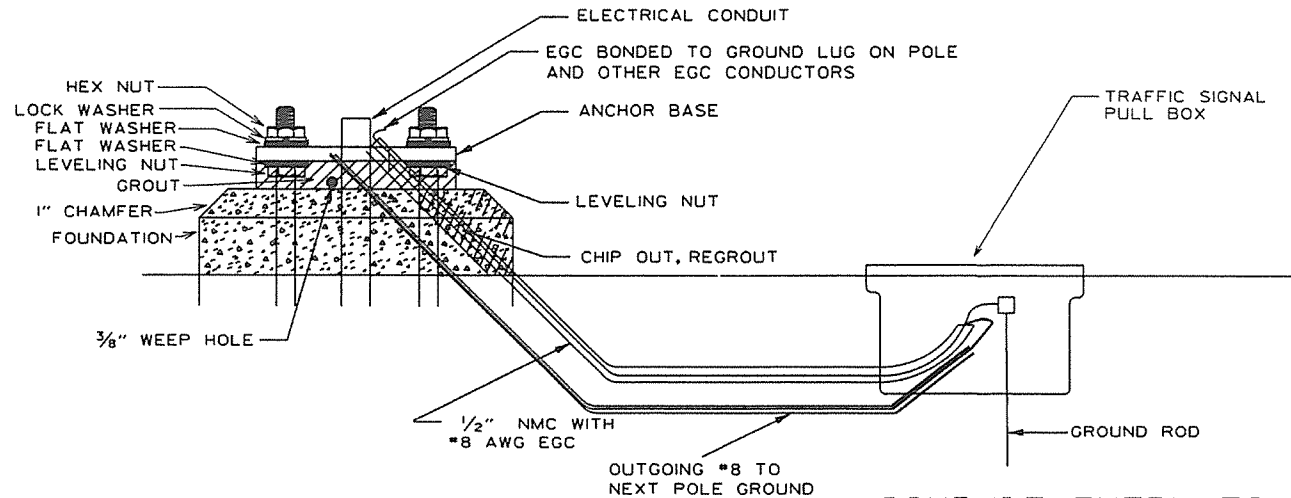
RIGHT SIDE ASSEMBLY

			ARKANSAS STATE HIGHWAY COMMISSION
			CONTROLLER CABINET UTILITY DRAWER
9-12-13	ISSUED AS STANDARD DRAWING		STANDARD DRAWING SD-5
6-15-05	ISSUED		
DATE	REVISION	DATE FILM	

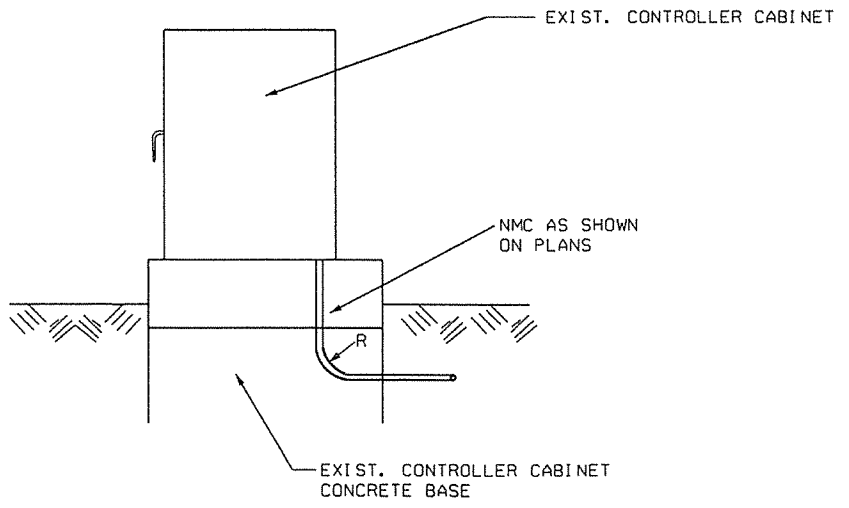
CONDUIT ENTRY TO EXISTING POLE BASE



ANCHOR BASE

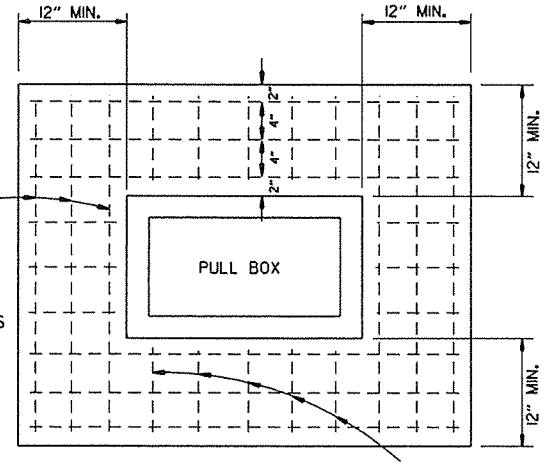


CONDUIT ENTRY TO EXISTING CONTROLLER CABINET

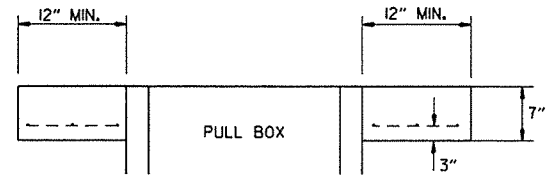


NOTE: ENTRY TO CABINET SHALL BE THROUGH A CUT IN THE BASE SUFFICIENT TO PROVIDE ADEQUATE CONDUIT RADIUS FOR ITEM.

3- #6 REINF. BARS EACH SIDE
NOTE: ALL REINFORCING BARS TO BE GRADE 60

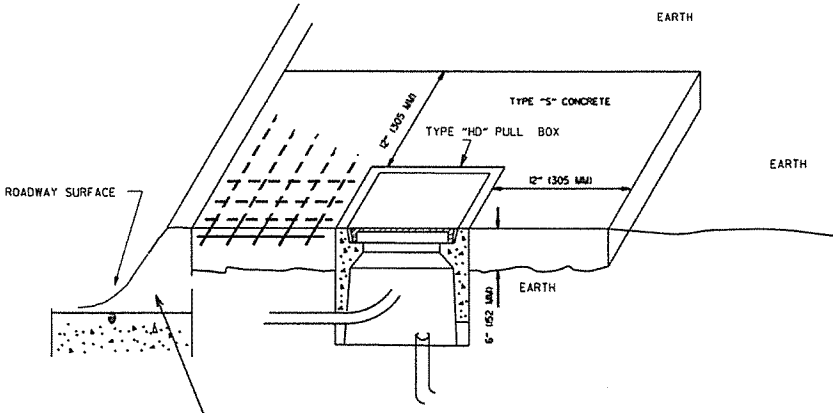


TOP



ELEVATION

TYPE "HD" CONCRETE PULL BOX DETAIL



2" CLEAR FROM TOP (TOLERANCE +/- 0.5")

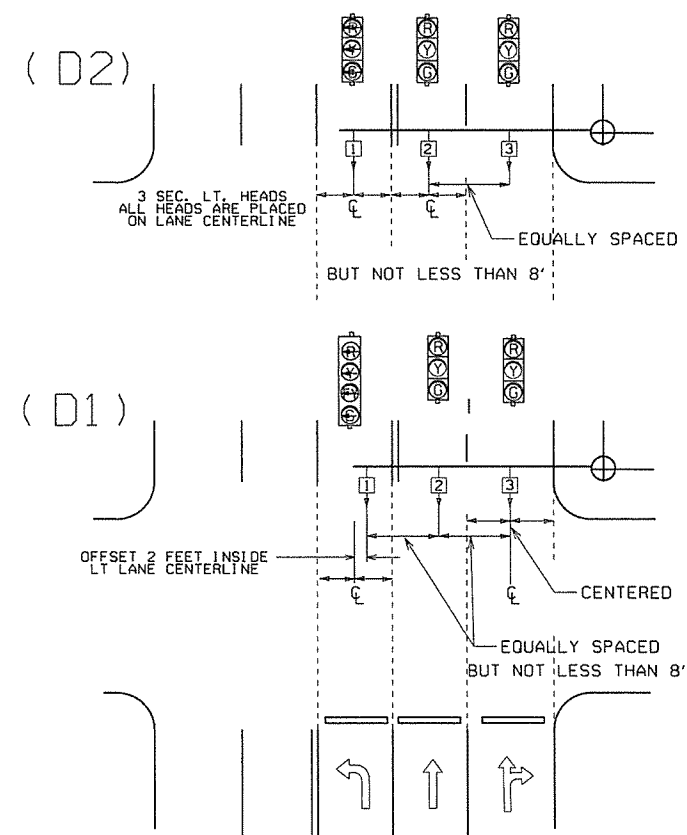
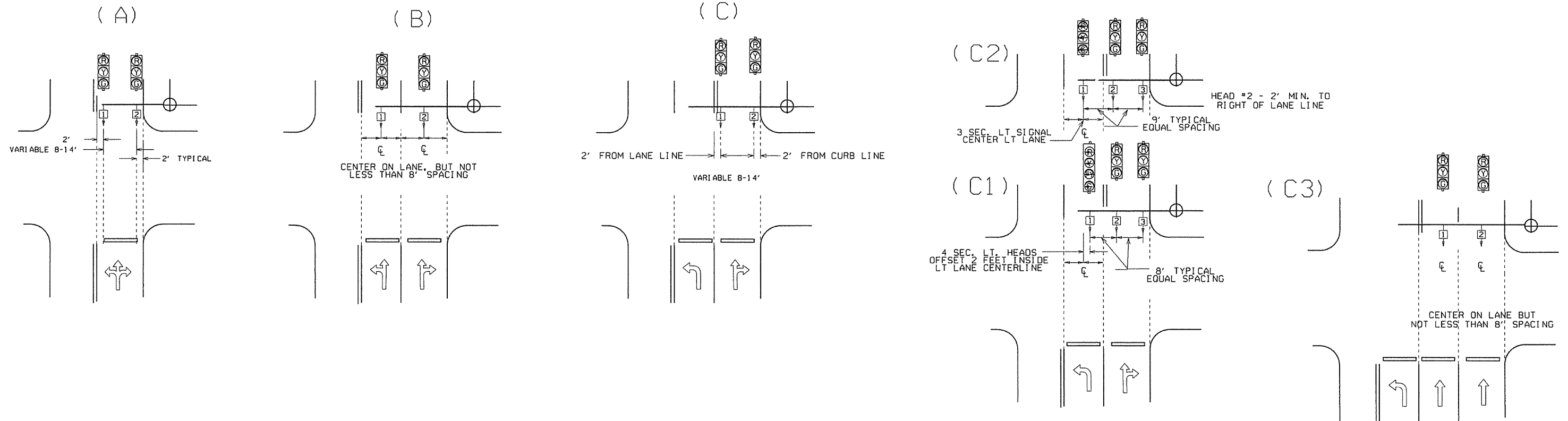
NOTE: ALL TYPE 1 AND TYPE 2 HD PULL BOXES ARE INSTALLED WITH AN APRON OF CONCRETE 12" (305 MM) WIDE AND 7" (178 MM) IN DEPTH. ALL PAYMENT SHALL BE INCLUDED IN THE PRICE OF THE TYPE HD PULL BOX. PULL BOX SHALL BE INSTALLED FLUSH TO SURROUNDING GRADE UNLESS OTHERWISE INSTRUCTED BY THE ENGINEER. THE CONCRETE SHALL BE CLASS "S." THREE #6 REINFORCING BARS IN THE APRON ON ALL SIDES OF THE PULL BOX IS REQUIRED IN CONCRETE.

9-2-15	REVISED PULL BOX DEPTH	
9-12-13	ISSUED AS STANDARD DRAWING	
5-21-09	REVISED GROUNDING	
7-31-08	ADDED & REVISED CONDUIT ENTRY	
6-23-04	REVISED CLEARANCE AT CURB ENTRY	
1-4-02	ADDED REINFORCING TO BOX APRON	
7-2-01	REVISED	
12-27-99	REVISED NOTES	
11-18-98	ISSUED	
DATE	REVISION	DATE FILM

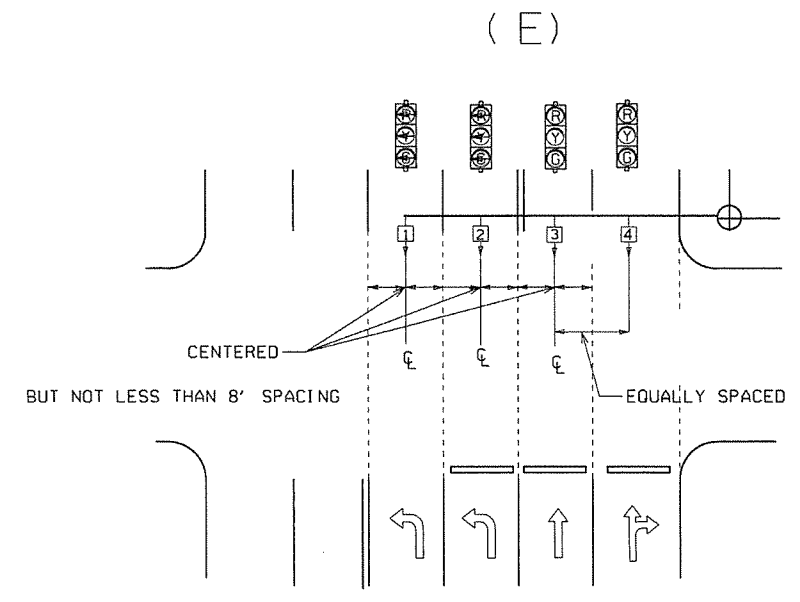
ARKANSAS STATE HIGHWAY COMMISSION

HEAVY DUTY PULL BOX

STANDARD DRAWING SD-6



NOTE: WHERE LEFT TURN HEAD (HEAD 1 ON D1 AND D2) IS NOT CALLED FOR ON PLANS, MAST ARM LENGTH MAY STILL BE ALLOWED FOR FUTURE INSTALLATION. HEADS FOR THROUGH MOVEMENTS SHALL STILL BE ALIGNED WITH THROUGH LANES AS SHOWN ON DETAILS.



GENERAL NOTES:

1. FOUR SECTION "PROTECTED/PERMISSIVE" LEFT TURN HEADS SHOULD BE PLACED A MINIMUM OF TWO (2') FEET TO THE RIGHT OF THE CENTERLINE OF THE APPROACHING LEFT TURN LANE.
2. THREE SECTION "PROTECTED" LEFT TURN HEADS SHOULD BE PLACED ON THE CENTERLINE OF THE APPROACHING LEFT TURN LANE.
3. WHEN IT IS NECESSARY TO PLACE POLES OTHER THAN AS SHOWN ON PLAN SHEET(S) RESULTING IN MAST ARM EXTENDING MORE THAN TWO FEET PAST (TO THE LEFT OF) THE CENTERLINE OF THE APPROACHING LEFT TURN LANE, MAST ARM SHALL BE CUT TO APPROPRIATE LENGTH AS DETERMINED BY THE ENGINEER, AND A NEW END CAP PROVIDED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THIS PRIOR TO INSTALLING THE MAST ARM IF ADDITIONAL COMPENSATION IS REQUIRED.
4. SIGNAL HEAD SPACING SHALL, IN NO CASE, BE LESS THAN EIGHT (8') FEET BETWEEN HEADS ON CENTER, MEASURED HORIZONTALLY PERPENDICULAR TO THE APPROACH.
5. ALL SIGNAL HEADS SHOWN ON THIS DETAIL SHEET SHALL BE LOCATED ACCORDING TO THE DIMENSIONS SHOWN IN RELATION TO THE APPROACH SIDE OF THE INTERSECTION.
6. MAXIMUM MOUNTING HEIGHT OF SIGNAL FACES LOCATED BETWEEN 40 FEET AND 53 FEET FROM STOP BAR SHALL BE IN ACCORDANCE WITH FIGURE 4D-1 OF 2009 MUTCD.

℄ = CENTER OF LANE FROM APPROACH SIDE

			ARKANSAS STATE HIGHWAY COMMISSION
9-12-13	ISSUED AS STANDARD DRAWING		SIGNAL HEAD PLACEMENT
3-11-10	2009 MUTCD		
12-9-99	ISSUED		
DATE	REVISION	DATE FILM	STANDARD DRAWING SD-8

NOTES, PED AND TRAFFIC SIGNAL HEAD SIGNS:
EACH ITEM 'TRAFFIC SIGNAL HEAD (4 SEC., 1-WAY)' SHALL INCLUDE A SPECIAL SIGN AS SHOWN, ATTACHED TO THE MAST ARM OR SPAN ASSEMBLY 12" TO THE RIGHT OF THE SIGNAL HEAD UNLESS REMOVED WITHIN THE SIGNAL PLAN NOTES.

EACH ITEM 'TRAFFIC SIGNAL HEAD (3 SEC., 1-WAY)' TO BE USED AS A LEFT TURN INDICATION ONLY SHALL INCLUDE A SIGN (R10-10) AS SHOWN, ATTACHED TO THE MAST ARM OR SPAN ASSEMBLY 12" TO THE RIGHT OF THE SIGNAL HEAD.

EACH PEDESTRIAN PUSHBUTTON SHALL HAVE ONE R10-3E SIGN ATTACHED TO THE POLE ABOVE THE BUTTON. ALL SIGN FACES SHALL BE CONSTRUCTED OF HIGH INTENSITY SHEETING (TYPE III) WITH SILKSCREEN LEGEND AND BORDER.

ALL SIGN BLANKS SHALL BE CONSTRUCTED OF ALUMINUM ALLOY (ASTM DESIGNATION B-209, ALLOY 5052-H38) WITH THICKNESS OF 0.100 INCH.

GENERAL NOTES:
1. MAST ARM POLES SHALL BE MOUNTED A MINIMUM OF 4 FT. BEHIND CURB OR SHOULDER.

2. OCTAGONAL POLES AND ARMS MEETING THE REQUIREMENTS OF THE PLANS AND SPECIFICATIONS CAN BE INSTALLED IN LIEU OF ROUND. ALL POLES AND ARMS IN A JOB MUST BE THE SAME SHAPE.

3. MINIMUM STRUCTURAL REQUIREMENTS:
DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION (2001) WITH 2003 AND 2006 INTERIMS.

USE FATIGUE CATEGORY I FOR ALL STRUCTURES ON ROUTES WHERE THE SPEED LIMIT IS 65 MPH AND GREATER AT THE STRUCTURE LOCATION AND ON ROUTES WHERE SPEED LIMIT IS GREATER THAN 45 MPH WITH AN ARM 60' OR LONGER.

USE FATIGUE CATEGORY II FOR STRUCTURES ON ROUTES WITH A SPEED LIMIT LESS THAN 65 MPH AND GREATER THAN 45 MPH WITH ARMS LESS THAN 60' AND ROUTES WITH SPEED LIMITS OF 45 MPH AND LESS WITH AN ARM 60' OR LONGER.

USE FATIGUE CATEGORY III FOR ALL STRUCTURES WHERE SPEED LIMIT IS 45 MPH AND LESS AND ARMS LESS THAN 60'.

CONSTRUCTION SPECIFICATIONS: ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION) WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

BASE WIND SPEED: 90 MPH.

STEEL MEMBERS CONSIDERED MAIN LOAD CARRYING MEMBERS WITH A THICKNESS GREATER THAN 1/2" SHALL MEET THE LONGITUDINAL CHARPY V-NOTCH TEST SPECIFIED IN SUBSECTION 807.05 OF THE STANDARD SPECIFICATIONS.

DEAD LOAD: AS A MINIMUM, DESIGN SHALL BE BASED ON THE FIXED ATTACHMENTS SHOWN BELOW OR AS MODIFIED IN THE PLANS.

ALL SIGNAL HEADS TO BE ONE WAY, 12 INCH, AND HAVE 5 IN. BACK PLATES.

HEADS AT END OF ARM - ONE 4 SEC., 85 LB., 16.0 SQ. FT. ONE SIGN MOUNTED 3 FT. FROM SIGNAL * 2' X 0' X 2' * 6"; 20 LB. REMAINING HEADS SPACED A 8 FT. * 3 SEC., 56 LB., TWO 5 SEC.); 14.4 SQ. FT. DESIGN TO ACCOMMODATE (INCLUDING 2 HEADS FOR ARMS 10 TO 16 FT., 2 HEADS FOR ARMS 10 TO 16 FT.; INCLUDING LB., 3 HEADS FOR 18 TO 24 FT. ARMS, 4 HEADS FOR OVER 26 FT. ARMS.

STREET NAME SIGN -- 72" X 18", 36 LB., MOUNTED SUCH THAT OUTSIDE EDGE IS NOT GREATER THAN 12 FT. FROM POLE. DEPENDING UPON POSITION OF SIGNAL HEAD ADJACENT TO POLE, SIGN MAY OVERLAP POLE SHAFT ROADWAY LUMINAIRES (WHERE REQUIRED ON PLAN SHEET) * VARIABLE ARM LENGTH (MAX.), 3.3 SQ. FT., 75 LB. PED SIGNALS -- TWO 2 SEC. 12 INCH MOUNTED 8 FT. FROM BASE OF POLE. POST MOUNTED 3 SEC. SIGNAL HEAD AT 10 FT. ON SIDE OF POLE.

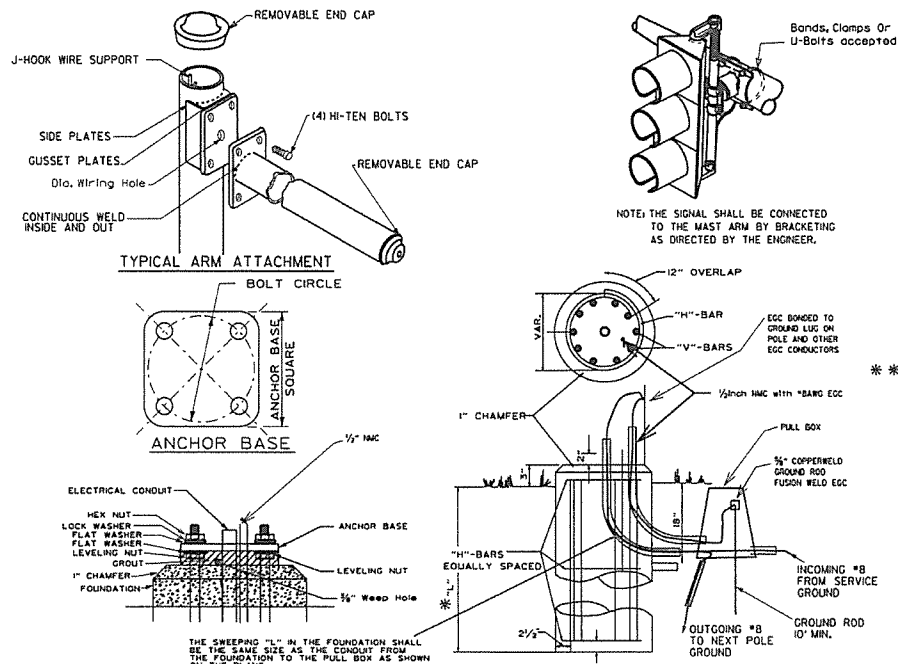
4. POLE/MAST ARM CAP -- POLE AND MAST ARMS CAPS SHALL BE PROVIDED, FABRICATED OF EITHER STEEL OR CAST ALUMINUM.

5. HAND HOLE -- HAND HOLES SHALL BE 4 X 6 INCHES FOR STANDARD, AND 3 X 5 INCHES FOR PED POLES. MINIMUM PLACED APPROXIMATELY 12 INCHES FROM BASE, AND SHALL BE FIXED WITH A BOLT DOWN COVER. A VACUUM FORMED ABS COVER IS AN ACCEPTABLE ALTERNATE TO STEEL. POLES GREATER THAN 21 FT. IN HEIGHT (FOR ROADWAY LUMINAIRE ATTACHMENT) SHALL INCLUDE A HAND HOLD WITHIN 12 INCHES OF MAST ARM(S) ATTACHMENT(S).

6. POLE/MAST ARM TAPER AND SLOPE - AVERAGE TAPER OF SIGNAL ARMS AND POLE SHALL BE 0.125 TO 0.15 INCHES PER FT.

MAST ARM CENTERLINE ANGLE AT ATTACHMENT POINT WITH POLE SHALL MAINTAIN NOT LESS THAN 0.5 DEGREES OR MORE THAN 4 DEGREES POSITIVE SLOPE WITH A LINE PERPENDICULAR TO THE POLE CENTERLINE. THE ARM SHALL MAINTAIN A POSITIVE AFTER IT IS PLACED UNDER LOAD.

7. NUT COVERS - EACH POLE SHALL INCLUDE A BOLT DOWN NUT COVER FOR EACH ANCHOR BOLT.

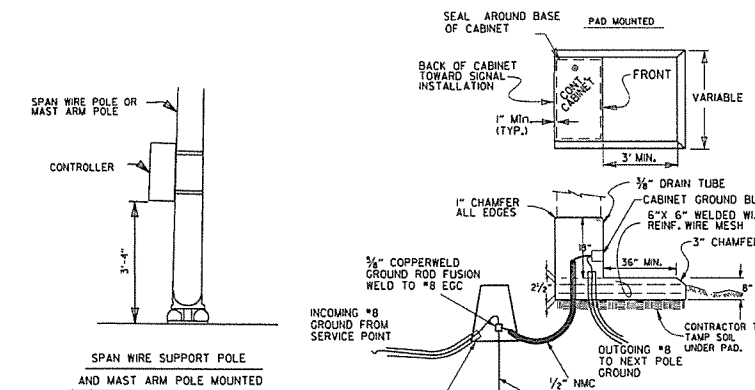


THE GROUND ROD SHALL BE FUSION WELDED TO A 1C/*8 A.W.G. SOLID COPPER GROUND WIRE. ATTACHMENT TO THE PRIMARY GROUND MAY BE BY AN APPROVED CLAMP. THE ROD IS TO BE LOCATED IN THE CONCRETE PULL BOX.

TYPICAL FOUNDATION DETAILS

POLE FOUNDATION MINIMUM DIMENSIONS AND STEEL REINFORCING. ALL REINFORCING STEEL SHALL BE GRADE 40 MIN.

ARM LENGTH	FON. DIAMETER	DEPTH 'L' *	STEEL		
			VERT.	HORZ.	O/C.
PED	30'	7'-0"	12-*7 (6'-6")	10-*4	8.44'
2' to 12'	30'	10'-6"	12-*7 (10'-0")	15-*4	8.42'
over 12' to 20'	30'	11'-6"	12-*7 (11'-0")	16-*4	8.66'
over 20' to 35'	36'	12'-6"	13-*8 (12'-0")	17-*4	8.88'
over 35' to 50'	36'	13'-6"	13-*8 (13'-0")	19-*4	8.56'
over 50' to 72'	42'	14'-6"	18-*8 (14'-0")	20-*4	8.74'
Twins to 20'	30'	16'-0"	12-*6 (15'-6")	22-*4	8.76'
Twins over 20' to 44'	36'	16'-0"	13-*8 (15'-6")	22-*4	8.76'
Twins over 44' to 50'	42'	16'-0"	18-*8 (15'-6")	22-*4	8.76'
Twins over 50' to 72'	42'	16'-6"	18-*8 (16'-0")	23-*4	8.64'



CONTROLLER CABINET MOUNTING DETAILS

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, CABINET ORIENTATION SHALL BE SUCH THAT THE BACK OF THE CABINET IS PARALLEL TO THE STREET AND POSITIONED TO ALLOW VISIBILITY OF THE SIGNAL DISPLAY WHILE OBSERVING THE CONTROLLER FRONT PANEL.

8. GROUND ROD - A 10' X 5/8" GROUND ROD SHALL BE INSTALLED IN THE PULL BOX FOR EACH POLE AND THE CONTROLLER. PAYMENT FOR THE GROUND ROD AND 1/2" NMC SHALL BE INCLUDED IN ITEM 714 FOR SIGNAL POLES AND ITEM 701 FOR THE CONTROLLER. THE PULL BOX AND CONDUCTOR BOX SHALL BE PAID FOR SEPARATELY.

9. POLE BASE/FOUNDATION - ANCHOR BOLTS SHALL INCLUDE AS A MINIMUM, ONE LEVELING NUT, TWO FLAT WASHERS, ONE LOCK WASHER, AND ONE HEX. NUT. PERIMETER OF ANCHOR BASE SHALL BE GROUDED WITH A 1/4" WEEP HOLE. ALL CONCRETE SHALL BE CLASS 'S' OR GREATER.

10. CONCRETE - ALL CONCRETE FOR CONTROLLER CABINET AND POLE FOUNDATIONS SHALL BE CLASS 'S' OR GREATER.

11. PEDESTRIAN PHASES - PEDESTRIAN MOVEMENTS SHALL BE PUSH BUTTON ACTUATED AND CONCURRENTLY TIMED. UNLESS OTHERWISE INDICATED ON THE PLAN SHEET(S), FURNISHING AND INSTALLING PED PUSH SWITCH SHALL BE CONSIDERED SUBSIDIARY TO THE ITEM PEDESTRIAN SIGNAL HEAD.

SIGNAL OPERATION NOTES:

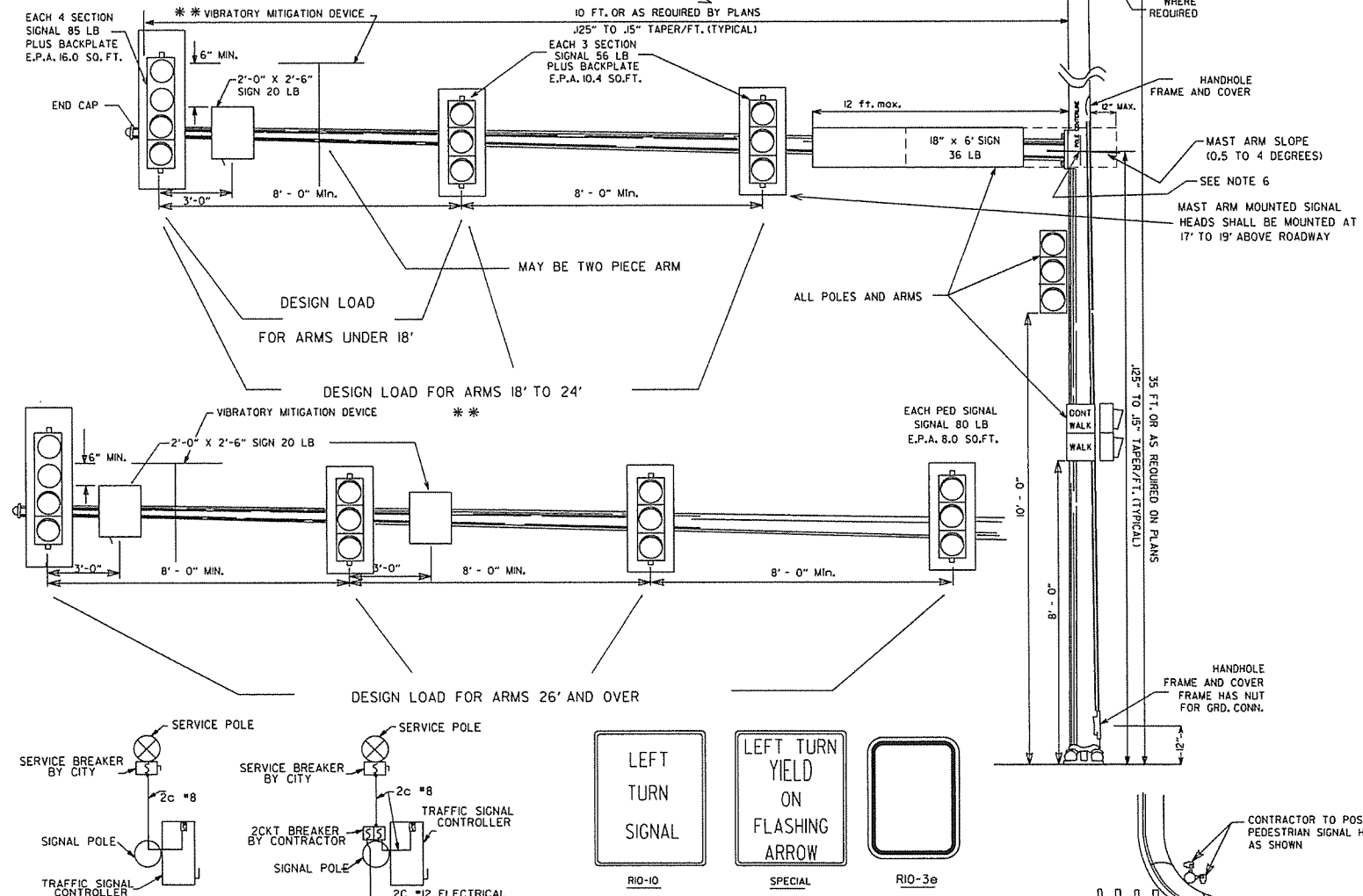
FLASHING OPERATION - PRIOR TO NORMAL OPERATION, SIGNAL SHALL BE FLASHED FOR A PERIOD OF 3 TO 5 WORK DAYS OR AS DIRECTED BY THE ENGINEER. SIGNAL SHALL BE PLACED IN OPERATION ONLY ON A REGULAR WORK DAY, EXCEPT FRIDAY.

THE CONTRACTOR MAY BE REQUIRED TO ALTER THE FLASHING DISPLAY DURING THE TEMPORARY FLASH PERIOD. AT THE TIME INTERSECTION IS PLACED IN PERMANENT OPERATION, THE FLASH SEQUENCE SHALL BE RETURNED TO THAT INDICATED ON THE PLAN SHEETS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THESE ALTERATIONS IN FLASH SEQUENCE.

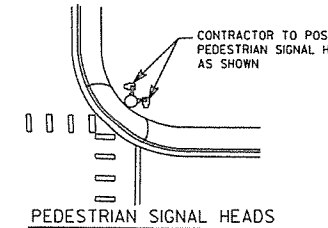
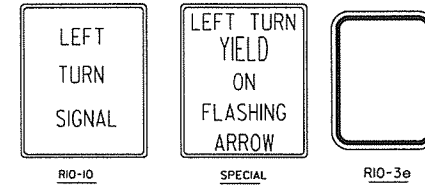
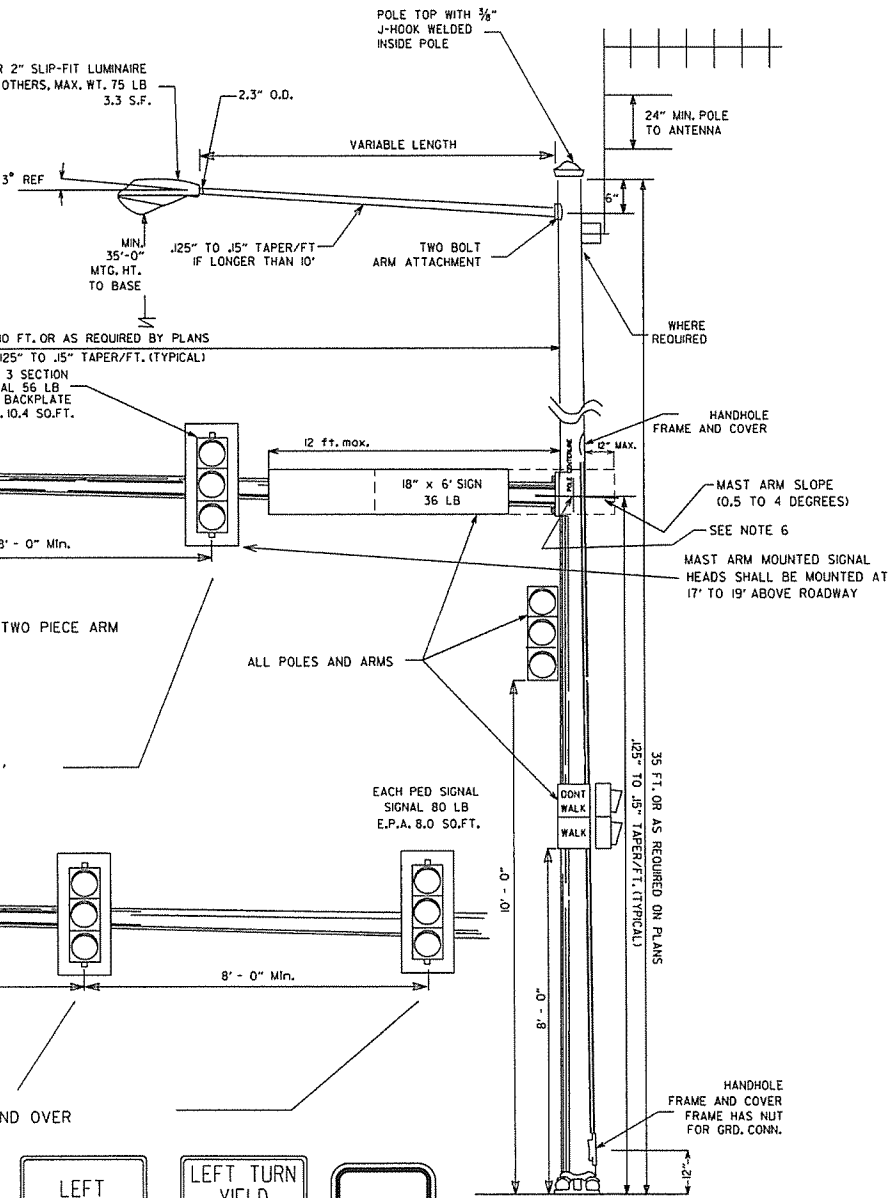
* WHEN THE GROUND ELEVATION AT THE POLE IS LOWER THAN THE ROADWAY ELEVATION, THE LENGTH OF FOUNDATION ABOVE THE GROUND MAY BE INCREASED TO PROVIDE THE REQUIRED SIGNAL HEAD CLEARANCE ABOVE THE ROADWAY. WHEN THE REQUIRED LENGTH OF FOUNDATION ABOVE THE GROUND IS 18" OR LESS, NO INCREASE IN DEPTH "L" WILL BE REQUIRED. WHEN THE REQUIRED LENGTH OF FOUNDATION ABOVE THE GROUND IS 5'-6" OR LESS, INCREASE DEPTH "L" BY 1'-0". FOR LENGTHS GREATER THAN 5'-6", DEPTH "L" SHALL BE ADJUSTED AS DIRECTED BY THE ENGINEER. LONGITUDINAL REINFORCING, AS SHOWN IN THE TABLE, SHALL BE PROVIDED FOR THE LENGTH OF THE EXTENDED SHAFT AND #4 TIES SHALL BE PROVIDED AT A SPACING NOT TO EXCEED 9" ON CENTERS. PAYMENT WILL BE IN ACCORDANCE WITH SECTION 714 OF THE STANDARD SPECIFICATIONS.

* * IN LIEU OF DESIGNING THE STRUCTURE TO RESIST PERIODIC GALLOPING, A VIBRATORY MITIGATION DEVICE MAY BE PROVIDED BY THE POLE MANUFACTURER. THE VIBRATORY MITIGATION DEVICE SHALL BE AN ANTI-GALLOPING PANEL CONSISTING OF A 60"X16"X0.125" SIGN BLANK MOUNTED NEAR THE END OF THE MAST ARM NOT TO EXCEED ONE QUARTER OF THE LENGTH OF THE MAST ARM FROM THE END OF THE MAST ARM WITH THE LONG AXIS OF THE PANEL COLLINEAR WITH THE LONG AXIS OF THE MAST ARM. THE PANEL SHOULD BE MOUNTED AT SUCH A HEIGHT AS TO PROVIDE AT LEAST 6" CLEAR FROM THE TOP OF ANY SIGNAL ASSEMBLY OR SIGN PANEL LOCATED ON THE MAST ARM WITHIN THE LENGTH OF THE ANTI-GALLOPING PANEL.

TRUCK-INDUCED GUST LOADS SHALL BE EXCLUDED FOR FATIGUE DESIGN FOR ALL STRUCTURES EXCEPT MAST ARMS MOUNTED OVER FACILITIES WITH POSTED SPEEDS OF 65 MPH OR GREATER AT THE LOCATION OF THE STRUCTURE.



SPECIAL NOTE: 90 MPH WIND ZONE DESIGN, SEE NOTE 3. MINIMUM STRUCTURAL REQUIREMENTS.

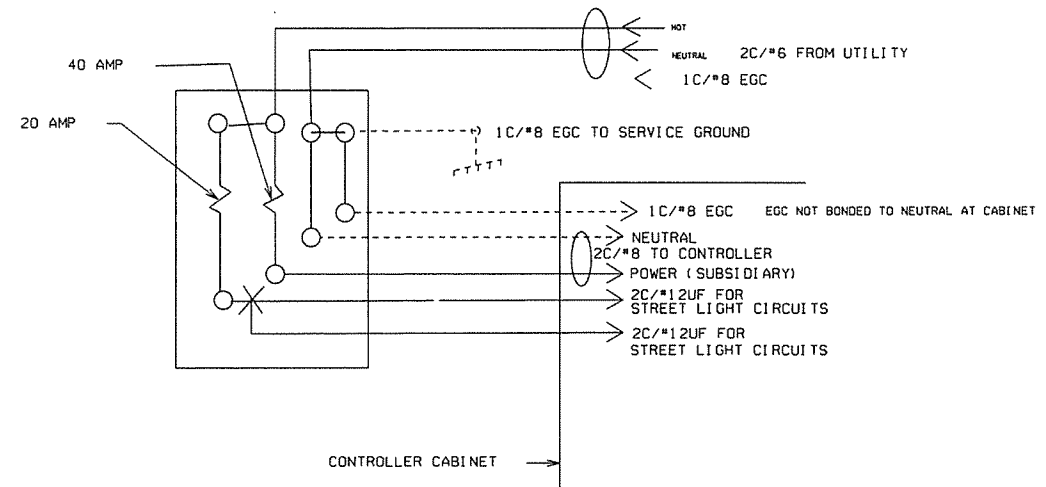
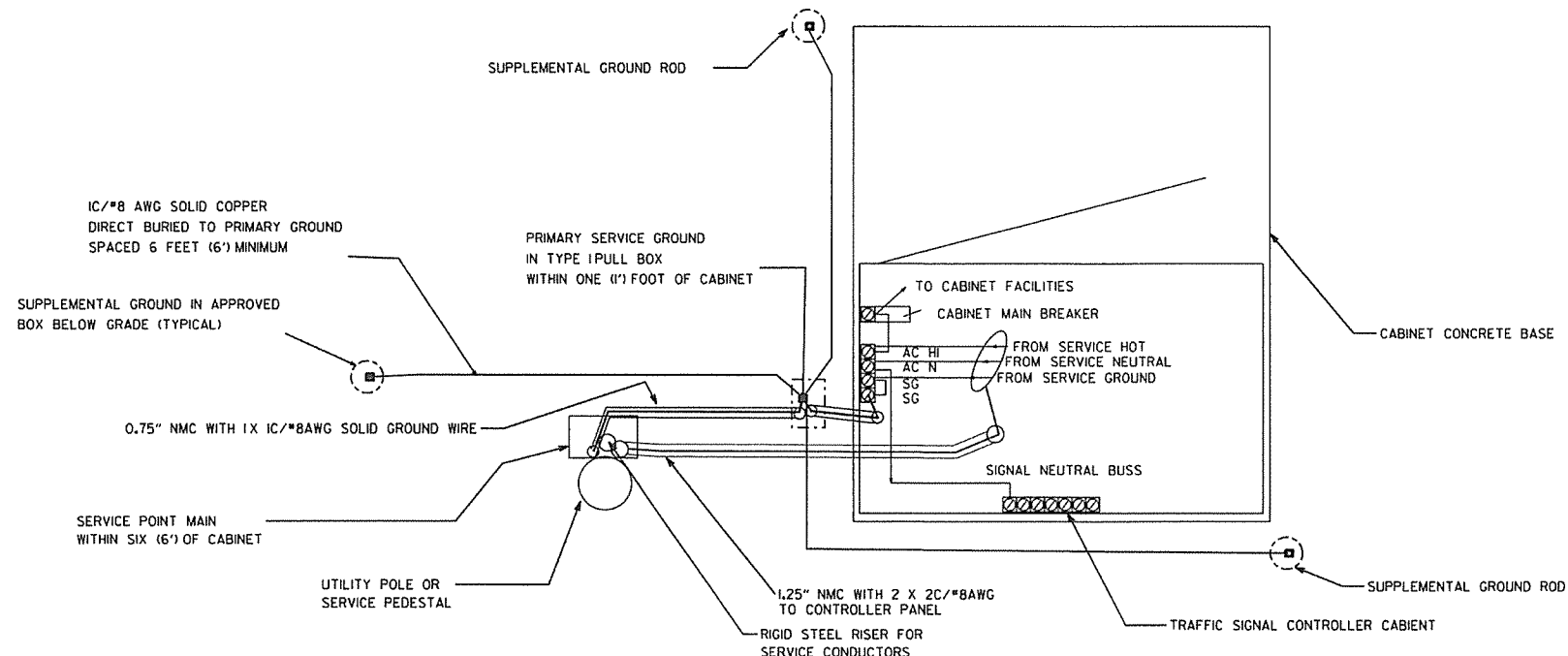


DATE	REVISION	DATE FILM
2-27-14	REVISED NOTES.	
9-12-13	ISSUED AS STANDARD DRAWING	
7-21-11	REVISED WIND SIGNAL HEADS	
5-21-09	REVISED GROUNDING	
12-31-08	REVISED GROUNDING	
4-25-08	ADDED VIBRATORY MITIGATION DEVICE & NOTES	
4-8-08	REVISED AASHTO NOTES	
4-7-08	REVISED TO 2001 AASHTO STANDARDS	
10-12-04	REVISED CABINET ORIENTATION	
6-23-04	REVISED	
5-4-04	REV. NOTE 3/AASHTO REQUIREMENTS	
6-1-01	REV. NOTES & POLE MAST ARM SLOPE	
4-8-01	REVISED POLE TAPERS	
4-25-00	REV. NOTES & SIGNAL HEAD PLACEMENT	
11-22-99	REVISED FOUNDATION DETAILS	
11-17-98	REVISED DETAILS AND NOTES	
11-21-98	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

STEEL POLE WITH MAST ARM

STANDARD DRAWING SD-II



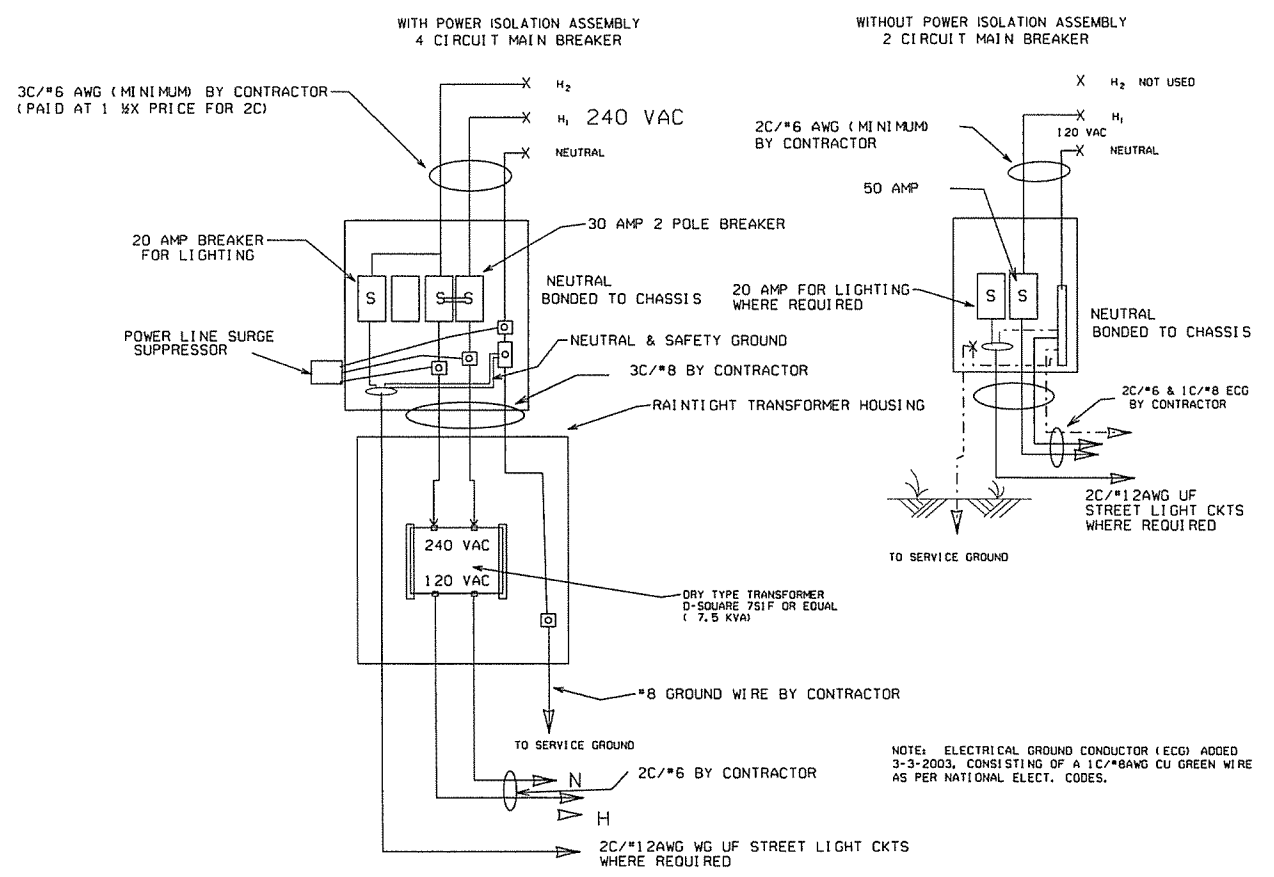
MAIN BREAKER WIRING (TYPICAL)

SERVICE GROUND IS TYPICALLY TIED TO NEUTRAL AT THE MAIN BREAKER. AS SUCH, CONTROLLER GROUND IS NOT TIED TO NEUTRAL AT SECONDARY BREAKER OR IN CONTROLLER CABINET.


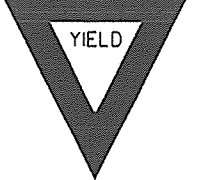
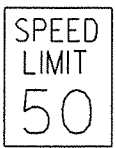






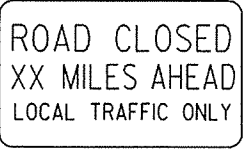
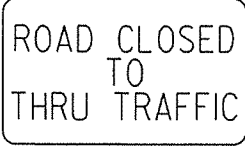
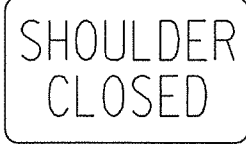
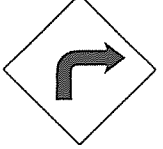
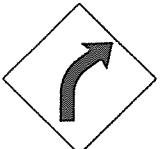
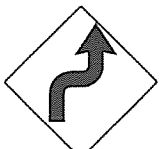

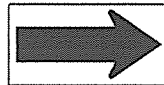
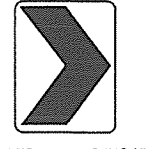
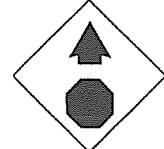
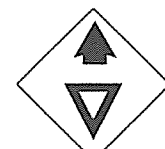
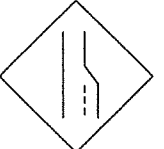

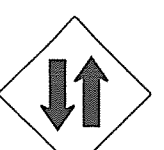

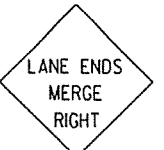
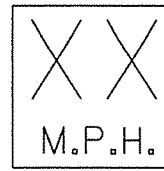





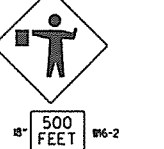


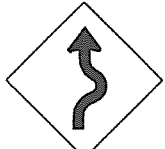



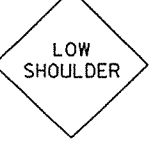
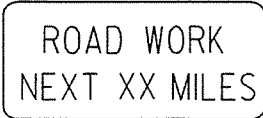
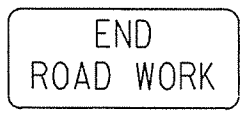
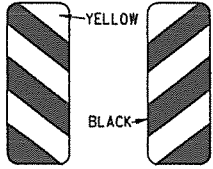

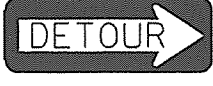
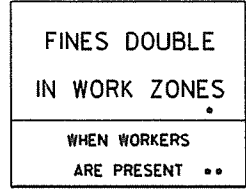
1. LOCATION OF SERVICE: TO MEET THE REQUIREMENTS FOR SAFETY AND MAXIMIZE LIGHTNING PROTECTION, THE "SERVICE POINT MAIN" FROM THE UTILITY PRIMARY SERVICE POINT MUST BE WITHIN SIX (6') FEET OF THE TRAFFIC SIGNAL CONTROLLER CABINET. ELECTRICAL SERVICE SHALL BE PROVIDED BY THE CITY/COUNTY TO A SERVICE POLE OR PEDISTAL WITH EXTERNAL RAIN TIGHT BREAKER (MAIN BREAKER) AT A MUTUALLY ACCEPTABLE POINT WITHIN THE RIGHT-OF-WAY. SERVICE POINT INCLUDES GALVANIZED STEEL CONDUIT TO A POINT 18" BELOW GROUND LINE, TWO CIRCUIT MAIN BREAKER, POWER ISOLATION ASSEMBLY WHERE REQUIRED, METER LOOP IF REQUIRED BY LOCAL UTILITY, ELECTRICAL CONDUCTORS AND WEATHERHEAD. WHERE STREET LIGHTING IS INCLUDED AS PART OF SIGNAL INSTALLATION, STREET LIGHTING CIRCUIT (2C/#12 AWG UF RATED, TYPICAL) SHALL BE KEPT SEPARATE FROM THE CIRCUIT SERVING TRAFFIC SIGNAL. SERVICE WIRE AND WIRING FROM THE CONTROLLER TO MAIN BREAKER IS PROVIDED BY THE CONTRACTOR AS A PART OF THIS CONTRACT. WIRE AND WIRING FROM MAIN BREAKER, AND CONNECTION TO THE UTILITY IS THE RESPONSIBILITY OF THE CITY/COUNTY.

2. METER LOOP: ALL COMPONENTS OF THE SERVICE POINT WITH THE EXCEPTION OF THE WIRE AND WIRING ABOVE THE MAIN BREAKER IS FURNISHED AND INSTALLED BY THE CONTRACTOR. WIRING FROM MAIN BREAKER INCLUDING CONNECTION TO THE UTILITY, IS THE RESPONSIBILITY OF THE CITY/COUNTY. IF METER LOOP IS REQUIRED, METER BASE AND HARDWARE IS PROVIDED BY THE CITY/COUNTY AND INSTALLED BY THE CONTRACTOR.

3. SUPPLEMENTAL GROUND RODS: SUPPLEMENTAL GROUND RODS ARE FUSION WELDED TO 1 C/#8AWG. SOLID COPPER GROUND WIRE. ATTACHMENT TO PRIMARY GROUND MAY BE BY AN APPROVED CLAMP. RODS ARE LOCATED IN A BOX APPROVED BY THE ENGINEER MEETING THE SAME LOADING REQUIREMENTS AS SECTION 711 CONCRETE PULL BOX OF THE STANDARD SPECIFICATION, WITH THE EXCEPTION TO DIMENSIONS. BOX MAY BE EITHER ROUND OR SQUARE APPROXIMATELY SIX (6") INCHES MINIMUM INSIDE DIMENSIONS AND SIX (6") INCHES DEPTH. (STRONGWELL PC0608BA06 WITH PC0608CA00 LID OR EQUAL)



			ARKANSAS STATE HIGHWAY COMMISSION
			SERVICE POINT INSTALLATION WITH SUPPLEMENTAL GROUNDING ARRAY
9-12-13	ISSUED AS STANDARD DRAWING		
1-17-08	ISSUED		
DATE	REVISION	DATE	FILM
			STANDARD DRAWING SD-12

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>500 FEET 24" R16-2</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60" • USE 6" C LETTERS •• USE 4" D LETTERS</p>

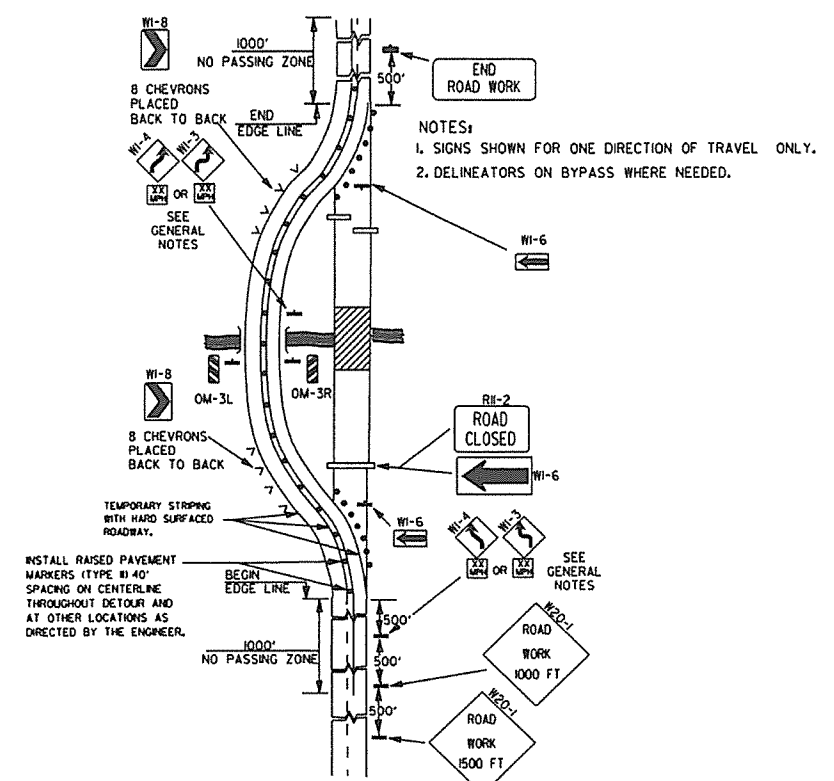
ADVANCE DISTANCES (XXXX)

500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
 - TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
 - EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
 - SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
 - SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
 - POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
 - ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
 - FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
 - MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
 - R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.
- * NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

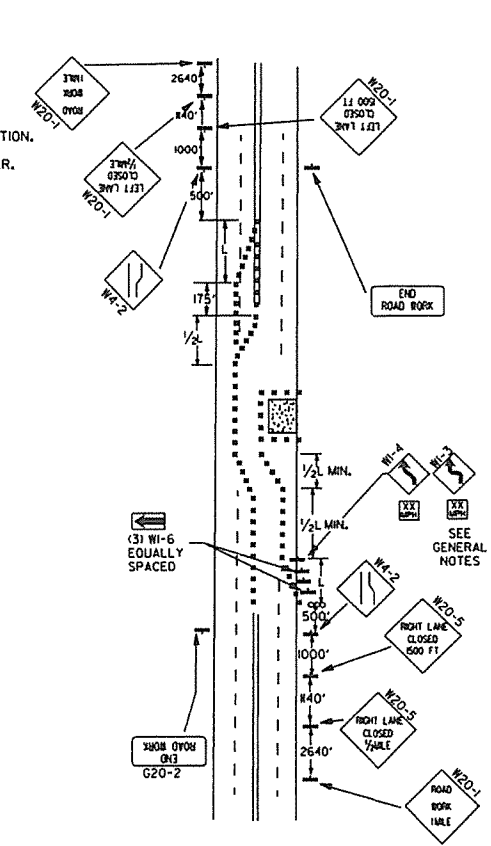
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-4	REVISED W24-1	
1-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
8-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
1-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
1-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-94	DRAWN AND PLACED IN USE	
DATE	REVISION	FILED



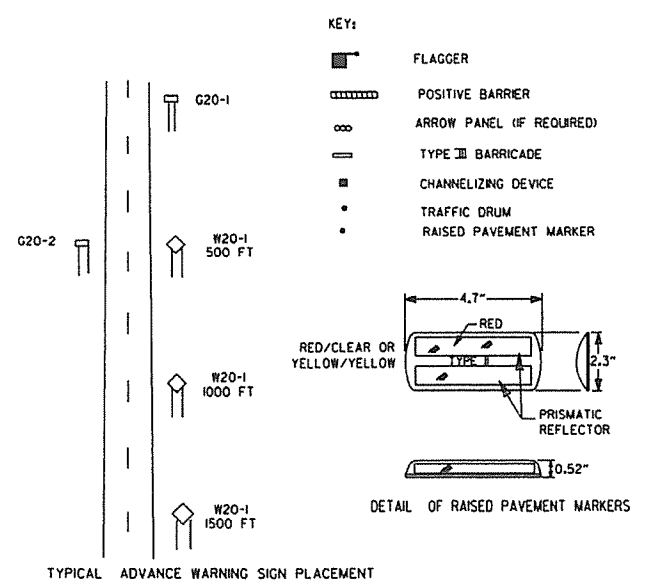
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.

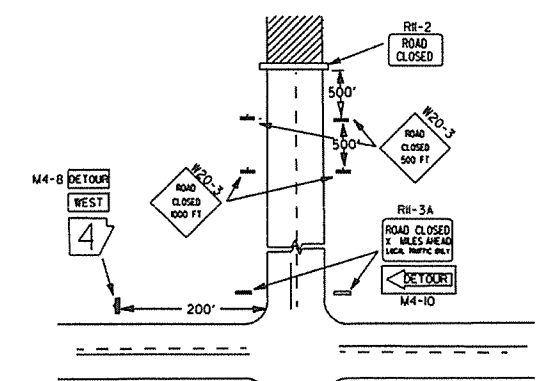


(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



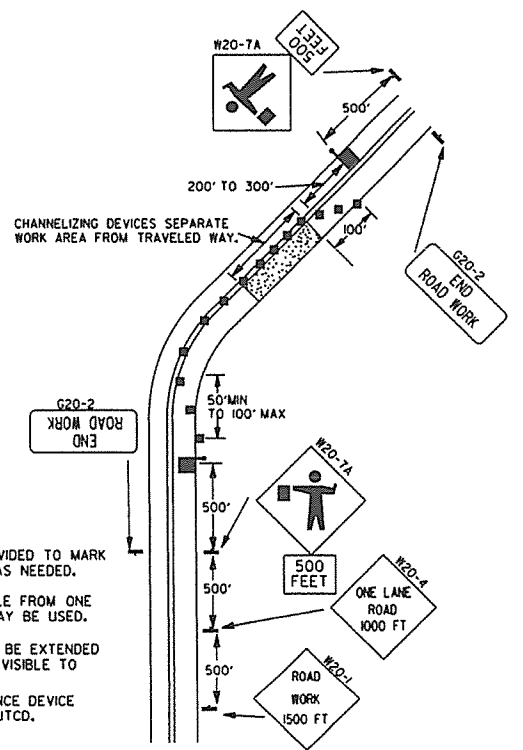
TAPER FORMULAE:
 $L = SXW$ FOR SPEEDS OF 45MPH OR MORE.
 $L = \frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
 WHERE:
 L = MINIMUM LENGTH OF TAPER.
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
 W = WIDTH OF OFFSET.

- GENERAL NOTES:
 1. ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-(K55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS, AT THE END OF THE WORK AREA A R2-(KXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-(K65) SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS, AT THE END OF THE WORK AREA A R2-(KXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER, WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.
 8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

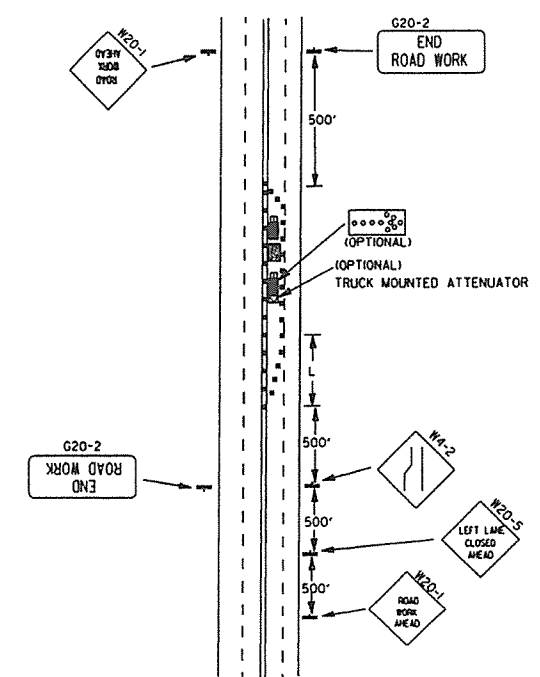


- NOTES:
 1. REGULATORY TRAFFIC CONTROL DEVICES TO BE MODIFIED AS NEEDED FOR THE DURATION OF THE DETOUR.
 2. STREET NAMES MAY BE USED WHEN DESIRABLE FOR DIRECTING DETOURED TRAFFIC.

(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



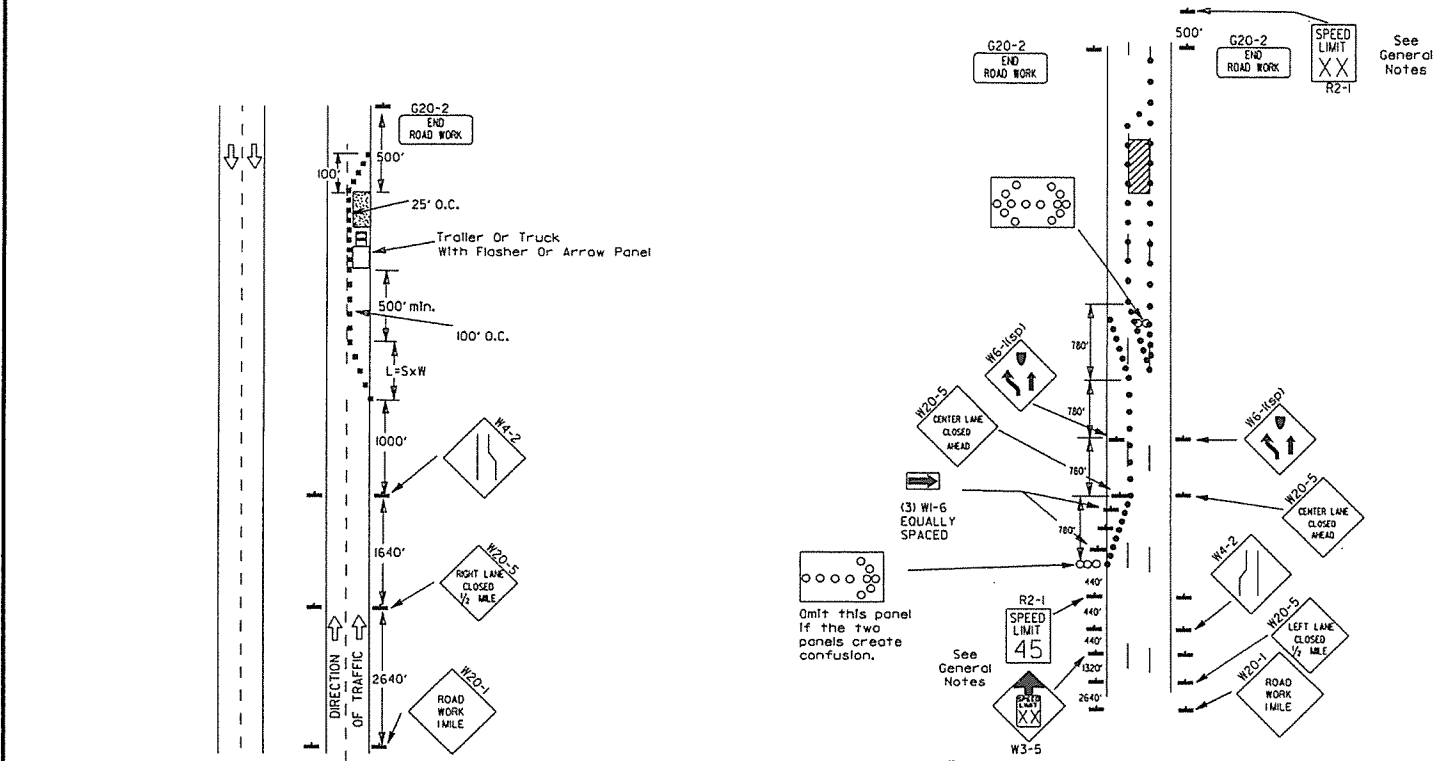
(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



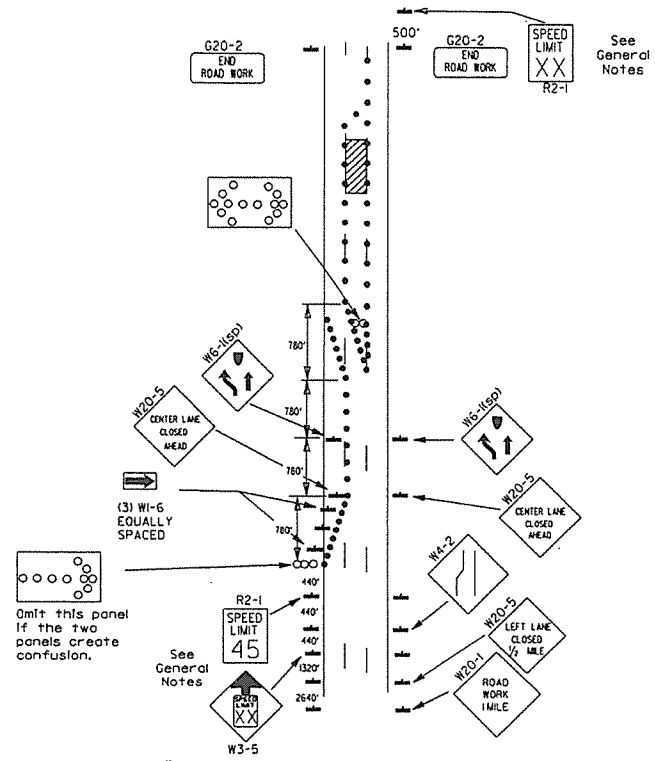
(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

DATE	REVISION	FILMED
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-1-10	ADDED (AFAD)	
1-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (G) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

Channelizing devices



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.



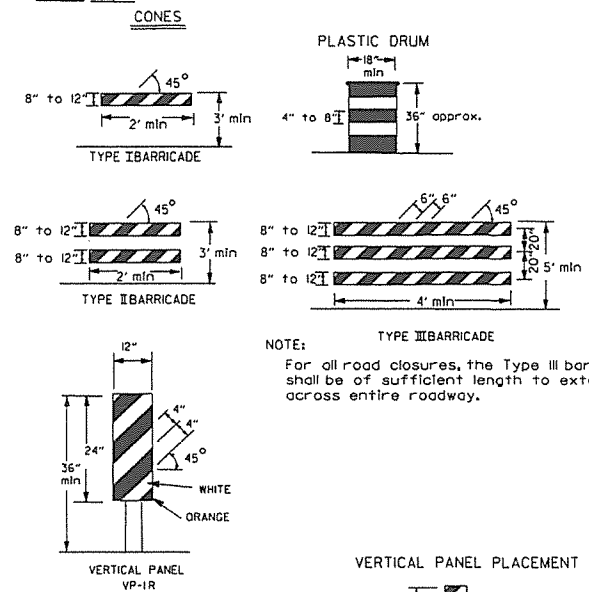
(B) Typical application - 3-lane oneway roadway where center lane is closed.

- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

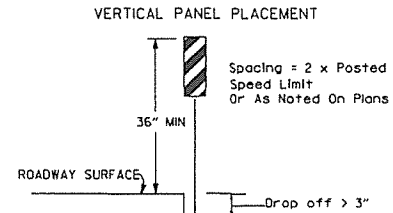
- GENERAL NOTES:
1. A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
 2. When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the W3-5 shall be installed at that location. Additional R2-1(45) speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
 3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-1(55) speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
 4. The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
 5. Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
 6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
 7. The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1 (1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
 8. Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
 9. All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
 10. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

When cones are used on freeways and multi-lane highways, they shall be 28" min. During hours of darkness, 28" cones shall be used on all roadways, and shall be reflectorized in accordance with the M.U.T.C.D.



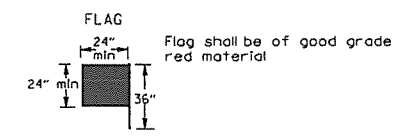
NOTE: For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



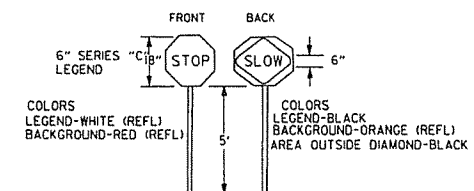
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	WB-11
1" to 3"	Edge of shoulder	WB-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-land vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

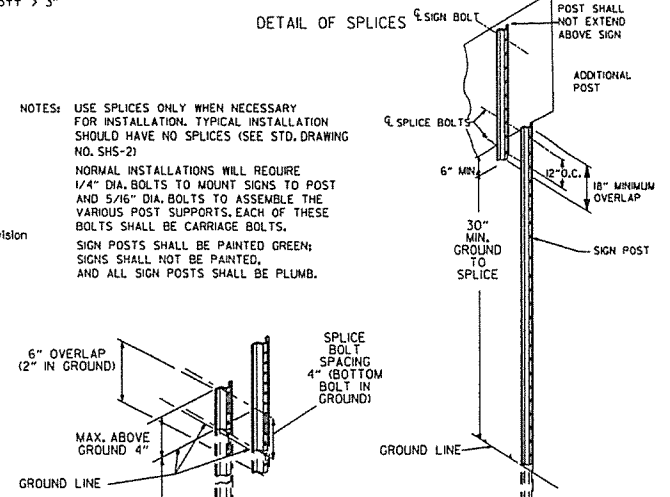
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



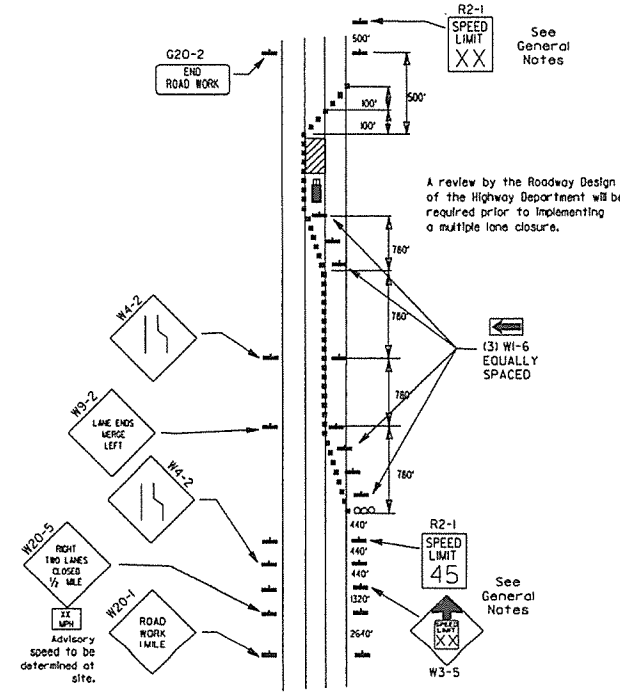
STOP SLOW PADDLE



DETAIL OF SPLICES



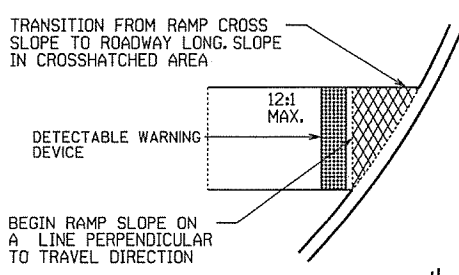
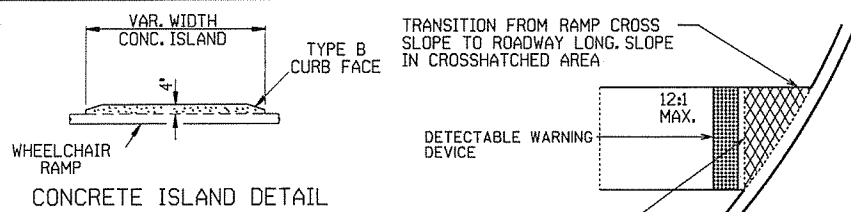
NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2). NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



(D) Typical application - closing multiple lanes of a multi-lane highway.

DATE	REVISION	FILED
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5	
10-15-09	ADDED REFERENCE TO MASH	
8-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

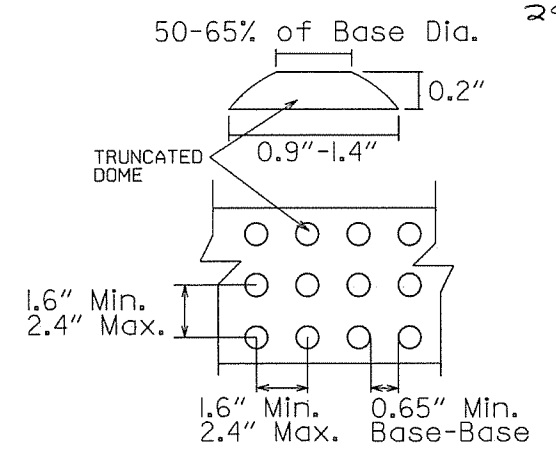
ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-3



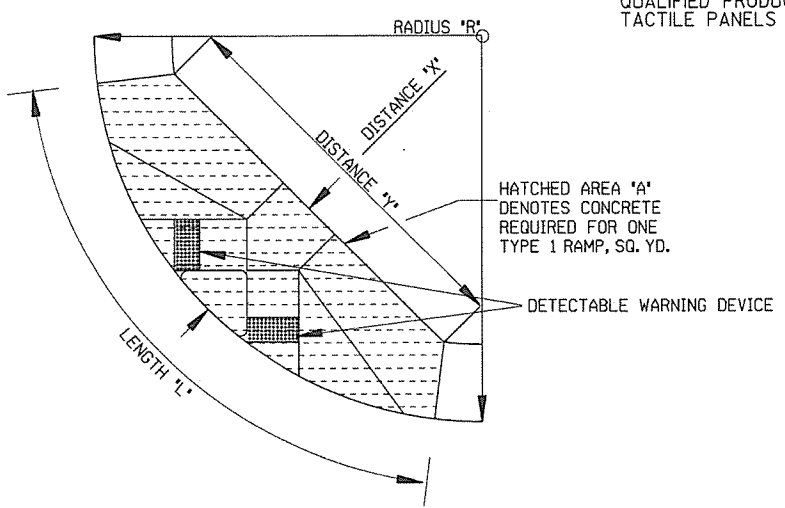
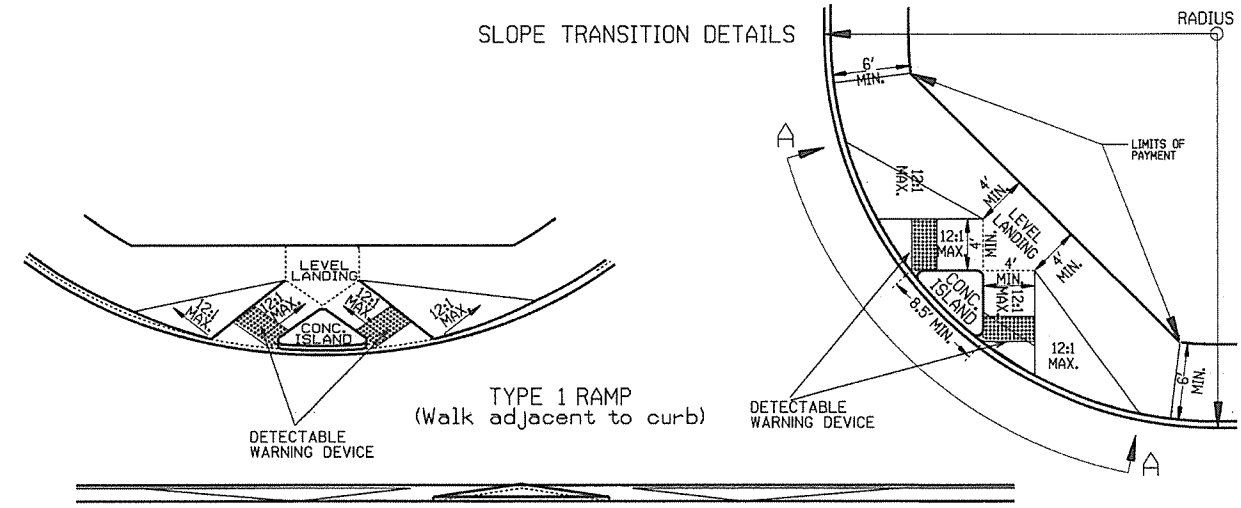
TYPE 1 RAMP DIMENSIONS AND QUANTITIES

RADIUS ft	DISTANCE ft	DISTANCE ft	LENGTH ft	RAMP AREA sq. yd.
15	11.67	18.82	32.18	26.21
20	11.52	22.28	35.46	30.07
25	11.43	26.60	38.77	33.80
30	11.37	30.26	40.93	36.90
35	11.33	33.51	43.11	39.77
40	11.30	36.45	45.26	42.45
45	11.27	39.16	47.34	44.97
50	11.25	41.69	49.36	47.35
55	11.24	44.07	51.31	49.63
60	11.22	46.33	53.21	51.80

GENERAL NOTES FOR DETECTABLE WARNING DEVICES
 THE DETECTABLE WARNING DEVICE SHALL BE LOCATED SO THAT THE NEAREST EDGE OF THE DEVICE IS 6 TO 8 INCHES FROM THE FACE OF THE CURB.
 TRUNCATED DOMES IN THE DETECTABLE WARNING SURFACE SHALL MEET THE REQUIREMENTS OF THE GEOMETRIC CONFIGURATION SHOWN.
 DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.
 DETECTABLE WARNING DEVICE SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE.
 DETECTABLE WARNING DEVICE SHALL BE ON THE AHTD QUALIFIED PRODUCTS LIST FOR CAST-IN-PLACE TACTILE PANELS (ADA DETECTABLE WARNING).

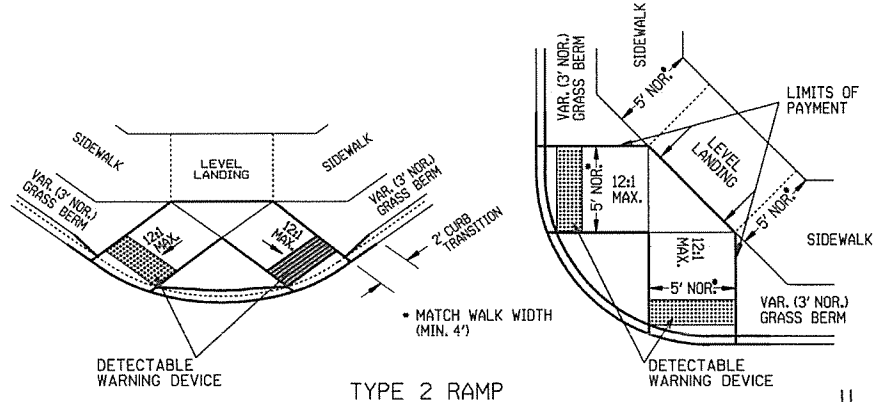


DETECTABLE WARNING DEVICE DETAIL

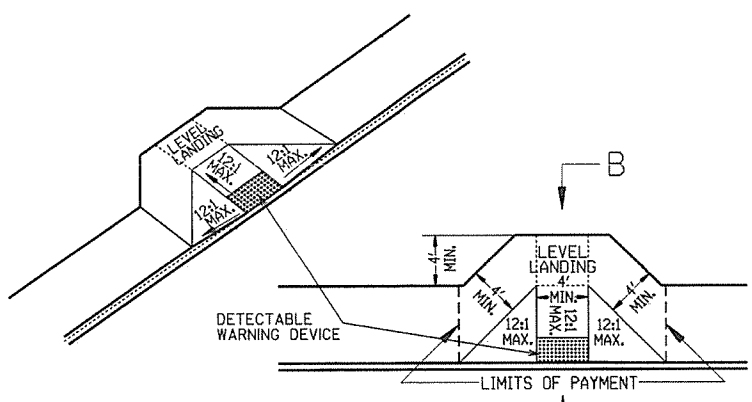


SECTION A-A

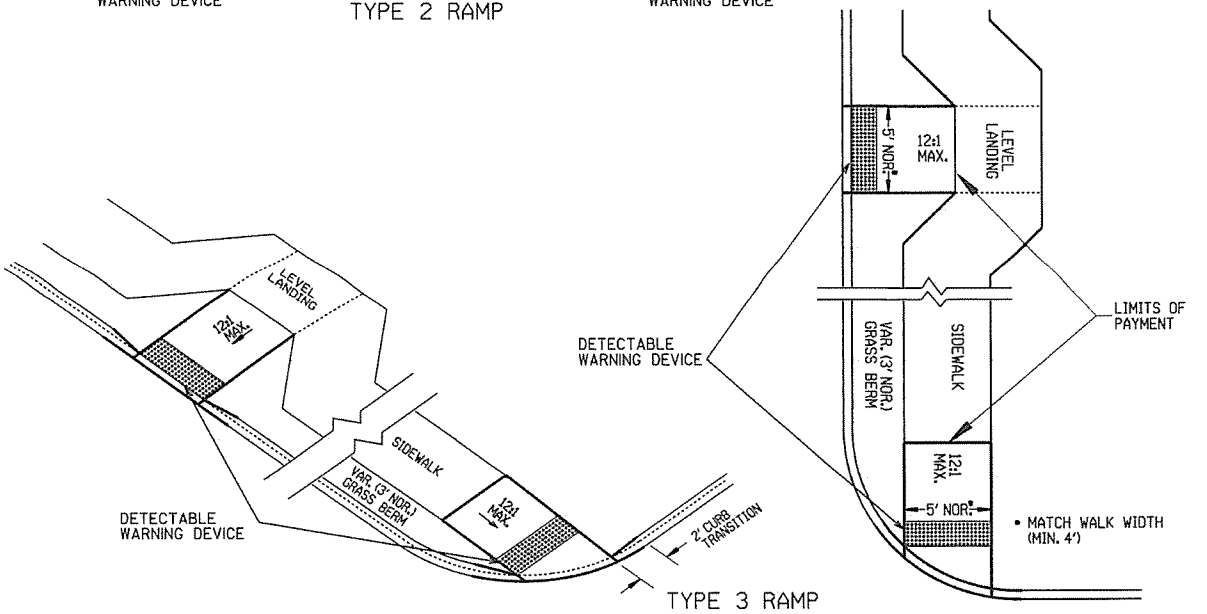
NOTE:
 THE CROSS SLOPE OF THE RAMPS, LEVEL LANDINGS, AND SIDEWALKS SHALL NOT EXCEED 2.0% UNLESS REQUIRED TO MATCH STREET LONGITUDINAL GRADE.



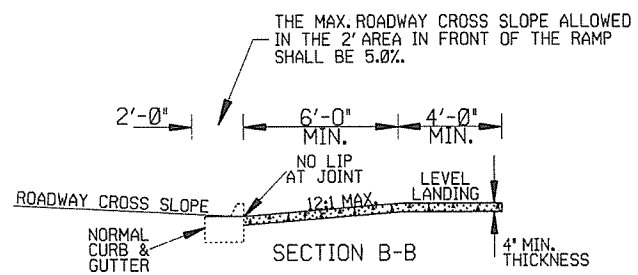
TYPE 2 RAMP



TYPE 4 RAMP (Walk adjacent to curb)



TYPE 3 RAMP



THE MAX. ROADWAY CROSS SLOPE ALLOWED IN THE 2' AREA IN FRONT OF THE RAMP SHALL BE 5.0%.

GENERAL NOTES:
 IN NEW CONSTRUCTION, UNLESS OTHERWISE INDICATED ON THE PLANS, WHEELCHAIR RAMPS ARE TO BE PROVIDED AT ALL CORNERS OF CURBED STREET INTERSECTIONS AND MID-BLOCK CROSSWALK LOCATIONS.
 IN ALTERATIONS WHEELCHAIR RAMPS ARE TO BE PROVIDED AT CURBED STREET INTERSECTIONS WITH PEDESTRIAN TRAFFIC AND MID-BLOCK CROSSWALK LOCATIONS.
 THE LENGTH OF THE RAMP SHALL BE SUCH THAT THE SLOPE DOES NOT EXCEED 12:1. THE SURFACE TEXTURE OF THE RAMP SHALL CONFORM TO A CLASS 6 FINISH ACCORDING TO SECTION 802.19.
 THE NORMAL GUTTER GRADE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP.
 ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION.
 THE MINIMUM THICKNESS OF THE RAMP, WALK, & LANDING SHALL BE 4". THE MINIMUM WIDTH OF THE RAMPS SHALL BE THE WALK WIDTH OR 36", WHICHEVER IS GREATER.
 RAMPS SHALL BE MODIFIED AS NECESSARY TO INSURE THAT THEY ARE PARALLEL TO A LINE DRAWN FROM THE CENTER OF ONE RAMP TO THE CENTER OF THE RAMP ON THE OPPOSITE SIDE OF THE INTERSECTION.
 THE DIMENSIONS AND QUANTITIES SHOWN ON THIS DRAWING ARE FOR A 90° INTERSECTION ONLY. DIMENSIONS AND QUANTITIES FOR SKEWED INTERSECTIONS WILL VARY, AND ARE TO BE DETERMINED BY THE ENGINEER.

RAMP SELECTION CRITERIA

CHOICE	TYPE	DESCRIPTION
FIRST CHOICE	TYPE 1	CORNER LOCATIONS WITH THE WALK ADJACENT TO THE CURB (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 2	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE INSUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 3	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE SUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 4	TANGENT LOCATIONS (BOTH NEW CONSTRUCTION AND ALTERATIONS).
SECOND CHOICE	TYPE 5	TANGENT LOCATIONS (ALTERATIONS ONLY).
THIRD CHOICE	TYPE 6	CORNER LOCATIONS (ALTERATIONS ONLY). THIS RAMP MAY BE USED ONLY IF THE TYPE 5 RAMPS CANNOT BE PLACED AT THE ENDS OF THE RADIUS.
FOURTH CHOICE		IF SITE CONSTRAINTS PREVENT THE CONSTRUCTION OF ANY OF THE TYPES LISTED, THEN AND ONLY THEN CAN THE 12:1 MAX. SLOPE ON THE RAMP BE EXCEEDED TO PROVIDE ACCESS TO THE STREET LEVEL (ALTERATIONS ONLY). THE SLOPE CAN BE STEEPENED TO A 10:1 MAX. FOR A MAX. LENGTH OF 5' OR A 8:1 MAX. FOR A MAX. LENGTH OF 2'. SLOPES STEEPER THAN 8:1 ARE NOT ALLOWED UNDER ANY CIRCUMSTANCES.

NOTE: IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED SHALL BE BASED ON THE AMOUNT OF RIGHT-OF-WAY AVAILABLE, AND ON THE PRESENCE OF OTHER SITE CONSTRAINTS (UTILITIES, BUILDINGS, ETC.). THE TABLE ABOVE LISTS THE ORDER IN WHICH THE RAMPS ARE TO BE CONSIDERED. AN ALTERATION IS DEFINED AS A PROJECT THAT CHANGES OR AFFECTS THE USE OF A PEDESTRIAN PATHWAY (OVERLAYS, SIGNALIZATION PROJECTS, ETC.) BUT DOES NOT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY. ALL PROJECTS THAT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY WILL USUALLY BE CONSIDERED NEW CONSTRUCTION FOR THE PURPOSES OF THE CHART ABOVE.

DATE	REVISION	DATE FILM
11-10-05	REVISED TO NEW SIDEWALK POLICY	
10-9-03	REVISED GEN. NOTES & ADDED NOTE	
4-10-03	REV. DETECTABLE WARNING DEVICES	
8-22-02	ADD DETECTABLE WARNING DEVICES	
3-30-00	ADD SLOPE TRANS. & REV. ISL. DIMS.	
11-18-98	REVISED NOTES	
8-12-98	REVISED TEXTURE	
7-02-98	REDRAWN & REISSUED	
10-18-96	CORRECTED DIMENSIONS	10-18-96
5-24-90	FROM 8:1 TO 12:1 MAX. SLOPES	5-24-90
7-15-88	ADJUSTED MAX. SLOPE	652-7-15-88
7-14-88	INCL. "CONC. ISLAND" IN PAY ITEM	
6-02-76	ISSUED-P.H.D.	299-7-28-76
	DATE	REVISION

ARKANSAS STATE HIGHWAY COMMISSION

WHEELCHAIR RAMPS
 NEW CONSTRUCTION
 AND ALTERATIONS

STANDARD DRAWING WR-1