

SEPTEMBER 2019

LOCAL GOVERNMENT PROCEDURES FOR COMPLIANCE WITH THE NATIONAL BRIDGE INSPECTION STANDARDS



PREPARED AND DISTRIBUTED BY
THE ARKANSAS
DEPARTMENT OF TRANSPORTATION

INTRODUCTION

As a result of a catastrophic bridge collapse in December 1967 which took the lives of 46 people, the National Congress enacted legislation requiring that the Federal Highway Administration (FHWA) establish National Bridge Inspection Standards (NBIS) and a National Bridge Inventory (NBI). A copy of the current NBIS is shown in Appendix A.

Initially, inspections were to be performed only on state and federally owned bridges. The 1978 Surface Transportation Assistance Act expanded this requirement to include all bridge length structures on the public highway systems. (Appendix B defines a Bridge Length Structure and provides sketches of typical bridge length structures.) This includes all bridges under the jurisdiction of federal, state, and local (city/county) governments.

The principal objective of the program (NBIS) is to ensure public safety. Not only are structural deficiencies to be identified during the course of the inspection process, but if the bridge is not capable of safely supporting legal load vehicles, the owner is responsible for advising the traveling public of any weight restrictions (i.e, bridge posting) in a timely manner. If the bridge is determined to not be capable of safely supporting a minimum of a 3-ton vehicle, it is the owner's responsibility to close the bridge until it is adequately strengthened or replaced.

It is vital that weight restriction signs be properly maintained and promptly replaced if they are damaged or removed. Failure to post load limits on bridges a public agency has knowledge to be structurally inadequate of carrying legal loads constitutes negligence. The same is true for failure to close a structure incapable of supporting at least a 3-ton vehicle.

Arkansas Law, ACA 27-85-101, Conservation of Bridges, REQUIRES the "administrators of the various public highway, road, and street systems shall make every effort to conserve the safe function of the bridges under their jurisdiction pursuant to the findings and recommendations of the bridge safety inspections by the bridge inspection teams of the Arkansas Department of Transportation in accord with the national bridge inspection standards published in the Federal Register."

The NBIS also requires each state to prepare and maintain an inventory of all bridges subject to these standards. In order to keep the inventory current and accurate, newly completed structures or any modification of existing structures that would alter previous recorded data in the inventory system must be entered in the state's records **WITHIN 90 DAYS** for state owned bridges or **180 DAYS FOR ALL OTHER BRIDGES ON PUBLIC ROADS** within the state.

BACKGROUND/OVERVIEW

As a result of the Surface Transportation Assistance Act of 1978, the Arkansas Department of Transportation (ARDOT) became the coordinating agency between the Federal Highway Administration (FHWA) and the local governments for the NBIS program. Since 1979 the ARDOT has been responsible for the inventory, inspection, and load rating of bridge length structures on the state, city, and county public highway systems in order to be in compliance with the NBIS (a federal law).

Because of this responsibility, ARDOT has made notification of weight posting requirements to local government agencies for bridges under their jurisdiction using letters, forms, and, since 1985, an annual posting certification program. Local governments were made aware that failure to comply with their posting responsibilities could result in loss of Federal Aid Highway funding.

To assist the local agencies in their posting of weight restricted bridges, a voluntary program was developed where locals could secure load posting materials from ARDOT stock piles at a reduced cost if program procedures were followed. Initially, this assistance was only for the initial posting of the bridges, but it was expanded to enable the material requisition to be made as needed.

Some of the basic highlights of the program, administered by ARDOT, are as follows:

- A. ARDOT purchases and stockpiles bridge weight limit posting materials (posts, signs and decals) for use by local governments. Mounting hardware (nuts, bolts, etc.) will be the responsibility of the local governments. Appendix C illustrates the standard weight limit signs and posts.
- B. Local agencies are notified by letter, forms, an annual certification, etc., of bridge posting needs.
- C. Local agencies review the needs at the bridge locations and, if needed, can requisition the materials from ARDOT. See the procedures on the Flow Charts on pages 8 & 11.

- D. If the local agency installs and certifies the material installation within one month of the requisition date, they will be billed for only 20% of the material costs and FHWA has agreed to reimburse the ARDOT for the remaining 80% of the cost of materials. If this certification condition is not met, local governments will be billed for 100% of the cost of materials because State Highway funds cannot be expended off the State Highway System.

- E. New signing as well as replacement signing for situations such as sign vandalism or theft is eligible under this program.

As shown in Appendix D, Arkansas is divided into ten highway districts. The location of each District Headquarters is shown on page D-2. Communication between the ARDOT and the local government will be through the District Construction Engineer designated in Appendix E and the local government administrator or the contact person designated by the local government on Form VII as shown in Appendix F.

The information in the following sections explains the responsibilities and procedures for NBIS compliance.

RESPONSIBILITIES OF LOCAL GOVERNMENT (CITY/COUNTY)

- 1) ARDOT makes recommendations regarding bridge maintenance needs, load postings & closings, but each local government with bridge length structures is ultimately responsible for the safety of bridges in their jurisdiction.
- 2) **Notification of Bridge Information**
 - a) ARDOT will notify local governments regarding maintenance needs, load posting, and closures using correspondence such as a Maintenance Needs report, the annual “Bridge Inventory/Posting Certification” report, or other communication. It is the owner’s responsibility to promptly address any noted deficiency or need.
 - b) Each month, a reminder will be sent to the email address on file for any bridge with a Maintenance Need that has not been shown as Completed for any issue with a “critical finding” priority, or a load posting deficiency with an “A” priority.
- 3) Local governments with bridge length structures must participate in the annual certification project. Each will receive by October 1 of each year a "Bridge Inventory / Posting Certification" report from the ARDOT. This report will indicate the current status of the complete active bridge inventory under the owner's jurisdiction. Unless noted otherwise, it is required that each sheet of this report be reviewed and initialed, the last sheet dated and signed by the local government official and the entire report returned by December 31 of each year to the ARDOT District Construction Engineer for transmittal to the ARDOT Bridge Division. ***This signed report will serve as certification that all bridges have been posted or closed as required.*** See Appendix L for an example. A copy of this report should be retained by the local government for their records.
- 4) **FAILURE TO SUBMIT A PROPERLY ENDORSED CERTIFICATION BY DECEMBER 31 OF EACH YEAR WILL RESULT IN FEDERAL AID FUNDS FOR FUTURE CONSTRUCTION PROJECTS UNDER YOUR JURISDICTION BEING WITHHELD, UNLESS NOTED OTHERWISE.**
- 5) **Bridge Closure for Non-State Owned Bridges**
 - A. If inspection reveals a dangerous situation that the inspector is sure requires immediate closing, he should:
 - a. As able, restrict access to the bridge and inform the traveling public of the danger.
 - b. Immediately notify the owner that the bridge should be closed and notify the District Construction Engineer (DCE).
 - c. The DCE will:
 - i. Immediately investigate the need to close the bridge.
 - ii. Promptly notify the owner of his findings and whether the bridge should remain closed.
 - iii. Promptly provide the inspection report and other appropriate information to the Bridge Division.
 - d. Bridge Division will promptly send by certified mail an official notification letter to the owner and local law enforcement.
 - B. If inspection reveals a dangerous situation, but the inspector is not sure immediate closing is required, he should:
 - a. Immediately notify the District Construction Engineer (DCE).
 - b. The DCE will:
 - i. Immediately investigate the need to close the bridge.
 - ii. If bridge closing is required, immediately as able, restrict access to the bridge and notify the owner of the need to close the bridge.
 - iii. Promptly provide the inspection report and other appropriate information to the Bridge Division.

- c. Bridge Division will promptly send by certified mail an official notification letter to the owner.
 - C. Since federal regulations state that a bridge incapable of supporting a 3-ton load limit requires closure, if Bridge Division determines by load analysis that a bridge should be closed, the following steps should be taken:
 - a. Bridge Division will:
 - i. Send by certified mail an official notification letter to the owner.
 - ii. Inform the DCE by telephone.
 - iii. The DCE will make prompt, verbal notification to the owner.
 - D. Upon notification by ARDOT, the owner should take **immediate** action to physically close the structure with acceptable barriers until it has been rehabilitated or replaced.
 - E. If the local owner has not closed the bridge within two business days of being notified of the need for closure, an Administration Official (Assistant Chief or higher) of the ARDOT will directly contact the local owner to persuade the owner to immediately comply with the bridge closure.
- 6) It is recommended that local governments notify school districts in their jurisdiction regarding the location of bridges with load postings less than 15 tons for planning safe travel of school buses.
- 7) Using the standard Form VII shown in Appendix F, the local government is responsible for designating, in writing, a contact person for bridge inspection related matters.
- 8) A responsible representative for the local government will make timely notification to the ARDOT District Construction Engineer of any changes in bridge status affecting the NBIS data by utilizing the Standard Form VIII shown in Appendix I, or other correspondence. (See Appendix J for typical examples of utilization and Appendix I for examples of completed Form VIII's).
- 9) In the event of an ownership change (i.e., city annexation) the new owner involved should note the limits of change on a map. This map will be dated, signed by both the new and old owners and forwarded to the ARDOT District Construction Engineer.
- 10) Load Posting Materials Assistance
 - a) Participation in the cost sharing program for posting materials is voluntary, but to be eligible, the Resolution (City) or Court Order (County) must be completed and returned to the ARDOT District Construction Engineer shown in Appendix E. If the local government already has an agreement on file, it is not necessary to redo it. See Appendix G for the Resolution and Appendix H for the Court Order.
 - b) The local government is responsible for providing the proper information when requesting posting materials (see Form IX, Appendix K) and for also certifying that the installation has been completed. Identification and current load posting information for completing Form IX may be obtained from the most current "Bridge Inventory/Posting Certification" report or other supplemental ARDOT notification. See "Instructions for Completing" at the bottom of Form IX.
 - c) The decals to correct posting values can be sent by mail for a bridge that has signs and posts in place at both bridge ends. A Form IX with the requisition data entered will be sent with the decals. After they are placed on the signs, the installation date should be added and the signed form sent to the DCE to receive the reduced material cost.
- 11) Procedures for Scour Critical/Susceptible Bridges:

An ARDOT representative has previously met with the administrator of each local government having scour critical/susceptible bridges, to explain the responsibility of assessing a bridge for a high water event that could affect the stability of the bridge. The administrator was given a copy of the affected bridges' Scour Plan of Action (SPOA) which details the procedures to follow for the bridge.

The following steps summarize the general procedures to follow, as defined in a SPOA:

 - a. The government should be aware of weather conditions that could potentially create water flow under the bridge that could reach the "triggering event" stage.

- b. If the stage is reached, government personnel take the recommended action steps in the SPOA. If hazardous conditions are found, access to the bridge is blocked and ARDOT notified.
 - c. The government records the actions taken at the bridge using a copy of the “SPOA Event Report”, available on the ARDOT website (http://www.arkansashighways.com/bridge_division/bridge_rating_inventory.aspx), and submits it to the District Construction Engineer for processing and archiving in the inspection record.
 - d. These steps should be followed whenever the “triggering event” stage for a bridge is reached.
- 12) Billing statements will be sent by ARDOT Fiscal Services to local governments with bridge length structures for two separate situations:
- a. Annually, an invoice will be sent for the cost of a locals’ portion of bridges inspections performed during the year. The amount depends on the number of inspections made and the type of bridges inspected.
 - b. An invoice will be sent for the cost of a locals’ portion of any bridge posting materials requisitioned from ARDOT. No bill will be sent unless materials are requisitioned.

The local government is responsible for forwarding to the ARDOT Fiscal Services Division in Little Rock a check made payable to the Arkansas Department of Transportation. Please note on the reimbursement check, for “Bridge Inspections” or "Bridge Posting Materials", as applicable.

13) Abandonment of Bridges by Local Governments

According to 23 U.S.C. 101(a) (21), The term public road means “any road or street under the jurisdiction of and maintained by a public authority and open to public travel.” As such, the public authority has a responsibility to the public to maintain any bridge that can be traversed by the public. To remove a bridge from the local inventory the public authority must provide certification to the District Construction Engineer of one of the following:

- a. The ownership and responsibility has been transferred to another public authority.
- b. The owner must maintain restricted access to the structure so that the public may not traverse the structure.

14) Providing Details for Bridges Constructed by the Local Government

For bridges constructed by the local governments’ staff or on a project contracted solely by the local government, bridge structure details are very beneficial in determining a bridges’ safe load capacity or scour susceptibility. Thus, when these details are available, they should be provided to ARDOT personnel to use in the bridge evaluation. These details may include:

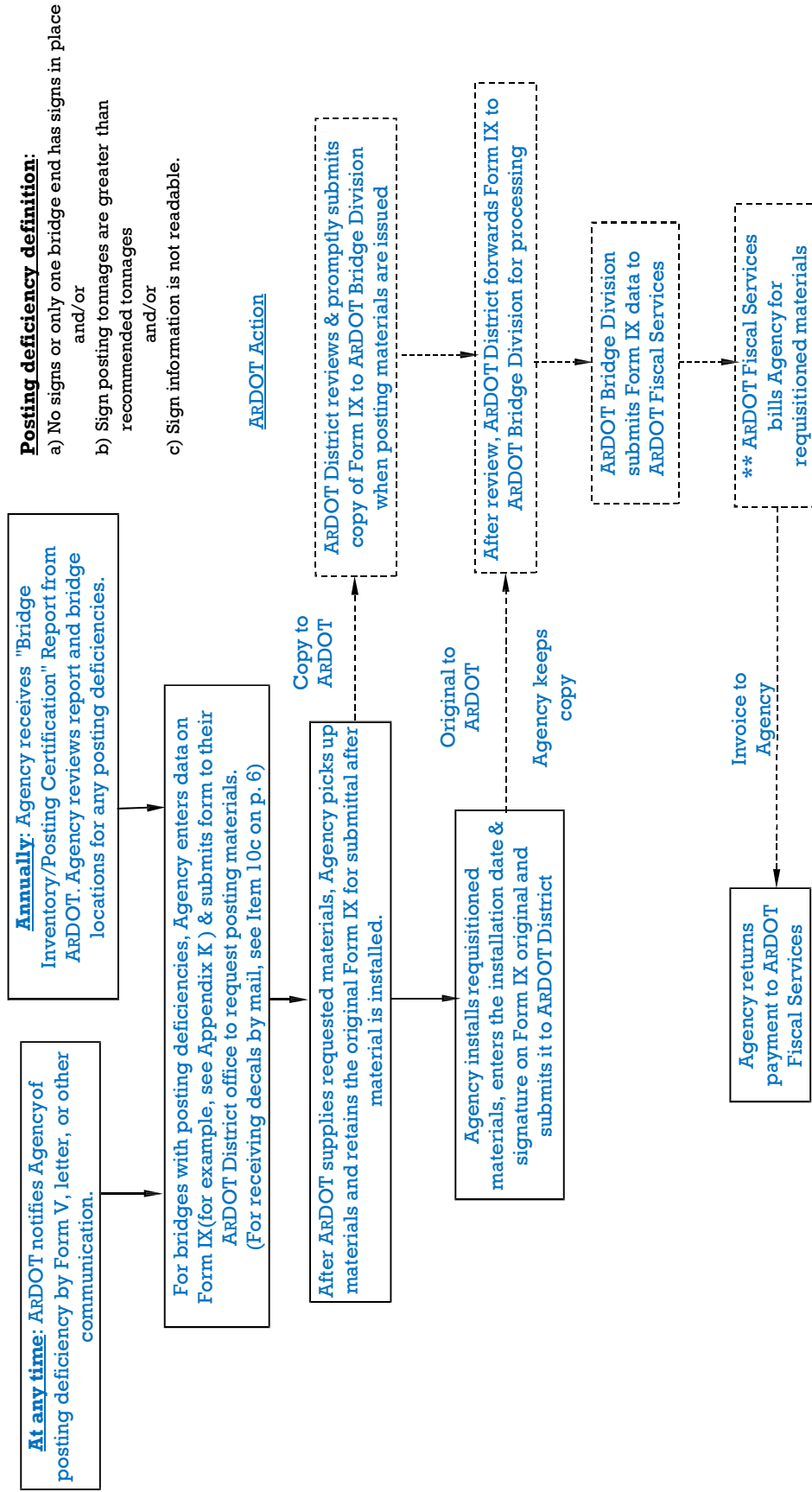
- a) Contract drawings for bridge members used in a bridges’ construction.
- b) Final length of piling driven in a bridge pier/bent. (For scour assessment)
- c) Depth or elevation of the bottom of a pier/bent footing. (For scour assessment)
- d) Details of a steel girder/ beam that is new or salvaged from another bridge or other situation. This could include the girder size, steel type and grade, if shear studs are used to make the concrete deck composite with the girder, etc...
- e) Concrete compressive strength and reinforcing steel size and spacing used in a girder, footing or slab.

15) Evidence of Posting Deficiency Correction

When a bridge posting deficiency (Signing not in place; Incorrect posting tonnages, etc...) is corrected, a dated photo of the posting sign on each end should be included with the posting correction notification to the ARDOT district to show that the correction has been completed. It is preferred that the photos be transmitted via an email sent to the ARDOT personnel.

FLOW CHART I - POSTING MATERIALS REQUISITION by LOCAL AGENCIES (CITY/COUNTY)

(Voluntary program; Requires preapproved agreement to be eligible)



**** NOTE:** THE LOCAL AGENCY WILL BE BILLED FOR 20% OF THE MATERIAL COST IF ALL REQUISITION AND INSTALLATION INFORMATION ON FORM IX IS COMPLETED AND THE ORIGINAL IS RETURNED TO THE ARDOT DISTRICT WITHIN ONE MONTH OF THE REQUISITION DATE. IF THE ORIGINAL FORM IS NOT RETURNED, OR IS RETURNED BEYOND THIS ONE MONTH LIMIT, THE LOCAL AGENCY WILL BE BILLED FOR 100% OF THE MATERIAL COSTS.

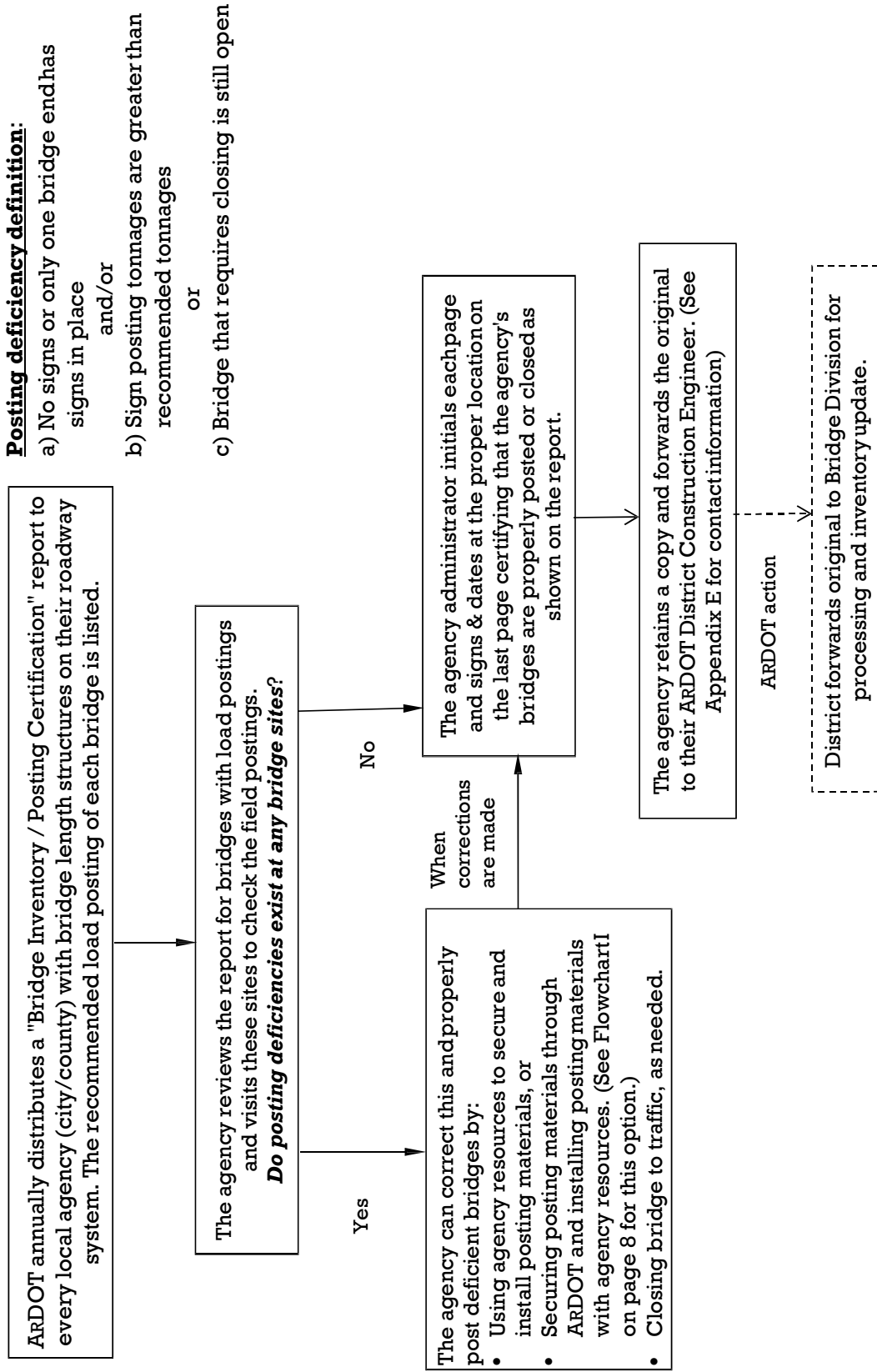
**RESPONSIBILITIES OF
THE ARKANSAS DEPARTMENT OF TRANSPORTATION**

- 1) The District Construction Engineer will be the designated contact person for the ARDOT Bridge Inspection matters (see Appendix E for statewide list).
- 2) ARDOT is responsible for the inspection and load rating of bridge length structures on the county/city public road systems in compliance with the NBIS. ARDOT will annually bill the owner for 20% of the associated administration costs for the bridges under their jurisdiction.
- 3) ARDOT will inspect a bridge every two years, but a bridge requiring weight limit posting or having low element conditions will be inspected at least every twelve months. ARDOT will promptly notify the bridge owner if, during an inspection or analysis of any of their bridges, it is found that a problem with regard to safety or structural integrity is discovered.
- 4) Updated inspection data is reviewed by ARDOT and load rating data is revised as necessary.
- 5) ARDOT will notify the local owner if an analysis or review indicates a change in posting requirements or bridge closure is warranted.
- 6) Federal regulations require that a bridge incapable of supporting a 3-ton load limit requires closure. If conditions warrant bridge closure, the local owner will be contacted with direct contact by ARDOT's District Office and by certified letter from the Bridge Engineer. Upon notification by ARDOT's District Office, the owner should take **immediate** action to physically close the structure with acceptable barriers until it has been rehabilitated or replaced.

Note: If the bridge has not been satisfactorily closed or rehabilitated within two business days of being notified of the need for closure, an ARDOT Administrative Official will directly contact the local owner regarding immediate compliance with the bridge closure.

- 7) ARDOT will distribute a "Bridge Inventory / Posting Certification" report to each owner (city/county) by October 1 of each year. This report will list the owner's complete active bridge inventory and indicate bridges requiring posting or closing. See Flow Chart II (page 10) and pages 11 through 15 for an explanation of the use of this report. Unless otherwise noted, the report is to be initialed on each sheet, signed and dated on the last sheet by the local government administrator and returned to the ARDOT District Headquarters by December 31 of each year for transmittal to the ARDOT Bridge Division. ***This signed report will serve as certification that all bridges have been posted or closed as required.***
- 8) Upon written notification (Form VIII or other) by local governments of a structural change, or the addition or the removal of a bridge from their inventory, ARDOT will inspect the bridge if deemed necessary for an inventory update.
- 9) At the local government's option, weight limit sign distribution, requisition, monitoring and billing will be coordinated by the ARDOT. ARDOT Fiscal Services Division will be responsible for sending the billing statement to the local government and for receipt of the reimbursement check from the local government for the local agency's portion.
- 10) Upon change in local government administration, ARDOT will advise the new administrator regarding the objectives and responsibilities involved in the NBIS program.
- 11) ARDOT will include on the IDriveArkansas.com website, links to maps showing data regarding bridges with load posting recommendations on the state and local highway systems including the bridge location and posting values. These allow the data to be viewed by the general public for use in safe travel on public roads.

FLOW CHART II - Using Annual BRIDGE INVENTORY / POSTING CERTIFICATION Report



EXPLANATION AND USE OF THE
BRIDGE INVENTORY / POSTING CERTIFICATION REPORT

As part of the federally mandated National Bridge Inspection Program, the ARDOT intends to furnish the subject report annually to each local government entity responsible for bridge length structures on their public highway system(s).

This annual report will advise the owner of current status of the bridge inventory and weight limit requirements for bridges under their jurisdiction. Each standardized report will provide the complete inventory of all bridge length structures that are on the local governments' public road system (i.e., city, county). . Since each updated listing will provide the owner with complete inventory and weight limit information, the owner need only retain the most current report.

The following four pages provide examples of standardized report sheets marked to assist the owner in becoming familiar with the format, content, and interpretation of the report data.

BRIDGE INVENTORY / POSTING CERTIFICATION
 COUNTY OWNED STRUCTURES
 DISTRICT - 11; COUNTY - GILA

ARDOT ROUTE NO.	OTHER ROUTE DESIGNATION	MAP ZONE	FEATURE UNDER BRIDGE	LOCATION	MAIN SPAN MATERIAL & TYPE	BRIDGE DIMENSIONS		REQ'D POSTING (TONS)				BRIDGE NUMBER	REMARKS
						LENGTH	WIDTH	Code 4	Code 9	Code 5	Code 5		
1	Lodge Corner Rd	G	Slough	2.3 Mi W US 165-LodgeCmnr	Steel Ortho	51	20.3	8	8	9	21890		
1	Hagler Rd	G	Kaney Bayou	0.51 Mi W SH 276-Hagler	Timber Girder	28	22.6	9	11	16	10000		
1	Lodge Corner Rd	G	Creek	0.6 Mi W US 165-LodgeCmnr	Steel Ortho	89	18.0	Legal	Legal	28	21891		
2	Woodson Rd	H	Hurricane Bayou	3.2 Mi E SH 11-LodgeCmnr	Timber Girder	69	16.7	13	15	23	21320		
2	Woodson Rd	G	Creek	0.6 Mi W SH 276-Hagler	Timber Girder	27	20.0	18	23	32	10005		
4	W.10th St Exten	F	King Bayou Ditch	0.8 Mi E US 79-Stuttgart	Steel Ortho	51	19.4	7	8	9	21800		
6	Alcorn Gill Rd	G	Creek	2.8 Mi S US 79-Humphrey	Steel Ortho	88	18.0	18	19	20	21505		
6	Alcorn Gill Rd	G	Hurricane Slough	4.63 Mi S US 79-Humphrey	Steel Ortho	85	19.6	Legal	Legal	Legal	20771		
9	Luther D Rd	G	King Bayou Ditch	1.1 Mi E SH 343-Stuttgart	Steel Ortho	51	19.4	3	4	4	20505		
10	Roth Prairie Rd	G	King Bayou Ditch (East)	0.3 Mi W US 165-Stuttgart	Timber Girder	30	24.0	17	20	29	21504		
12	Tarkington Rd	E	Little LaGrue Bayou	1.32 Mi W SH 153-LaGrue	Steel Ortho	89	19.7	19	20	21	22163		
12	Gregory Fischer	G	King Bayou Ditch	0.4 Mi E SH 343-Stuttgart	Steel Culvert	35.6	0.0	Legal	Legal	Legal	23866		
14	Tindall Drier Rd	I	Elm Prong Mill Bayou	1.96 Mi E US165-Stuttgart	Steel Ortho	66.8	20.2	4	5	5	23760		
17	Old Almyra Hwy	E	Cherry Creek	0.67 Mi S Almyra Limits	Conc Chan Beam	58	24.0	Legal	Legal	Legal	03375		
19	Lone Tree Rd	F	King Bayou Ditch	0.5 Mi S US 79-Stuttgart	Steel Ortho	40	19.4	Legal	Legal	36	22426		
21	Old England Rd	F	Clearpoint Creek (Canal)	1.2 Mi W US 165-Stuttgart	Steel Ortho	50	19.7	Legal	23	27	21949		
33	South Yoder Rd	I	Creek	5.4 Mi N US165-LodgeCmnr	Steel Ortho	51	19.7	6	7	9	21799		
33	North Yoder Rd	E	Little LaGrue Bayou	0.31 Mi S SH146-Stuttgart	Conc T-Beam	118	27.6	Legal	Legal	Legal	04765		
33	South Yoder Rd	I	Elm Prong Mill Bayou	5.0 Mi S SH 130-Yoder	Conc Culv	37	0.0	Legal	Legal	Legal	X0491		
35	North Tower Rd	E	Little LaGrue Bayou	1.7 Mi N SH 130-Stuttgart	Steel Ortho	89	18.0	Legal	Legal	Legal	21751		
41	Vice Rd	E	Wildcat Ditch	0.75 Mi E SH 130-Almyra	Steel Ortho	45	19.0	Legal	Legal	Legal	21346		
47	KY Club Ln	G	Kings Bayou	0.5 Mi W US165-LodgeCmnr	Timber Girder	29	22.6	14	18	25	21531		
56	Phelps Rd	I	Creek	0.25 Mi E US165-LodgeCmnr	Steel Girder	30	16.4	9	12	13	10045		
59	Doughboy Rd	B	Point Deluce Bayou	0.5 Mi E US 165-LaFargue	Steel Ortho	89.3	20.0	Legal	22	23	23865		
60	Gander Street Rd	H	Flag Lake Sough	0.3 Mi N Main St-Gillett	Conc Slab	75	27.9	Legal	Legal	Legal	04335		
61	Old Post Rd	A	Little Post Bayou	2.1 Mi E US 165-Gillett	Conc Culv	21	0.0	Legal	Legal	Legal	10048		
62	Shirley Owen Rd	A	Little Post Bayou	2.11 Mi E US 165-Gillett	Steel Culvert	31.8	0.0	Legal	Legal	Legal	23286		
67	Woodrow T Rd	A	Caney Bayou	2.95 Mi S SH 44-Tichnor	Steel Ortho	89	18.7	Legal	26	28	21011		
70	Fletcher Levee Rd	B	Williams Branch	200' W SH 17-Deluce	Timber Girder	24	16.3	12	17	22	10053		
71	Hesser Levee Rd	B	Point Deluce Bayou	1.3 MI N SH 44-LaFargue	Steel Ortho	88.0	18.4	14	15	16	22025		

BRIDGE INVENTORY / POSTING CERTIFICATION
 COUNTY OWNED STRUCTURES
 DISTRICT - 11; COUNTY - GILA

ARDOT ROUTE NO.	OTHER ROUTE DESIGNATION	MAP ZONE	FEATURE UNDER BRIDGE	LOCATION	MAIN SPAN MATERIAL & TYPE	BRIDGE DIMENSIONS		REQ'D POSTING (TONS)			BRIDGE NUMBER	REMARKS
						LENGTH	WIDTH	Code 4	Code 9	Code 5		
287	Wilson Lane	F	Crooked Creek	0.30 Mi W SH 13-Humphrey	Steel Ortho	90	17.4	7	8	8	21502	
301	Lumsden Rd	G	Creek	1.91 Mi S US165-LodgeCorn	Timber Girder	24	16.1	17	24	30	10108	
312	Cox Cypress Rd	G	Bayou Meto	4.0 Mi W US 165-LodgeCorn	Steel Girder	200	27.9	Legal	Legal	Legal	04484	
375	Whitmore Rd	C	Tarleton Creek	0.57 Mi E CR 77-StCharles	Steel Ortho	89	19.0	18	19	20	22925	
381	Old Highway 17	C	Bass Creek	0.7 Mi S SH 1-St. Charles	Timber Girder	30	15.4	8	9	14	10114	
385	Padgett Levee Rd	C	Tarleton Creek	0.4 Mi W SH 17	Steel Ortho	54	18.7	Legal	Legal	Legal	21048	
408	Frownfelter Rd	I	Hurricane Bayou (Canal)	3.0 Mi N US165-LodgeCornr	Steel Ortho	45	18.7	Legal	Legal	Legal	21194	
411	Wolf Rd	I	Hurricane Bayou	0.9 Mi W CR408-LodgeCmrr	Steel Ortho	30.8	22.6	19	23	31	23998	
415	Long Bell Rd	G	Cross Bayou	0.16 Mi E JeffCo-BayouMet	Timber Girder	31	16.1	11	15	19	10110	
416	Seemore Rd	I	Elm Prong Mill Bayou	0.01 Mi E CR 33(Yoder Rd)	Timber Girder	30	17.4	13	15	22	22711	
431	Sebree Rd	I	Fish Lake Ditch	1.0 Mi W SH 130-Almyra	Steel Girder	25	18.0	5	5	7	21501	

This signature certifies that:

- * I have reviewed my agency's Bridge Inventory / Posting Certification report for bridge posting or closure requirements and initialed each page to indicate this review.
- * I have taken any necessary action to properly post or close the bridges according to the values indicated on the report or as notified otherwise.
- * I understand that the Federal Highway Administration (FHWA) may make random on-site inspections to verify proper load posting or bridge closure.
- * I agree that failure to sign, date and return this report to the ARDOT District Headquarters by December 31st will result in withholding of Federal Highway funds for future construction projects in my agency's jurisdiction, until the time that the report is submitted as required.

 County Judge

 Date Signed

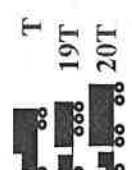
EXAMPLE (Last page)

BRIDGE INVENTORY / POSTING CERTIFICATION
 COUNTY OWNED STRUCTURES
 DISTRICT - 11; COUNTY - GILA

ARDOT ROUTE NO.	OTHER ROUTE DESIGNATION	MAP ZONE	FEATURE UNDER BRIDGE	LOCATION	MAIN SPAN MATERIAL & TYPE	BRIDGE DIMENSIONS		REQ'D POSTING (TONS)					BRIDGE NUMBER	REMARKS
						LENGTH	WIDTH	Code 4	Code 9	Code 5	Code 8	Code 3		
287	Wilson Lane	F	Crooked Creek	0.30 Mi W SH 13-Humphrey	Steel Ortho	90	17.4	7	8	8	8	21502		
301	Lumsden Rd	G	Creek	1.91 Mi S US165-LodgeCorn	Timber Girder	24	16.1	17	24	30	30	10108		
312	Cox Cypress Rd	G	Bayou Meto	4.0 Mi W US 165-LodgeCorn	Steel Girder	200	27.9	Legal	Legal	Legal	Legal	04484		
375	Whitmore Rd	C	Tarleton Creek	0.57 Mi E CR 77-StCharles	Steel Ortho	89	19.0	Legal	19	20	20	22925		
381	Old Highway 17	C	Bass Creek	0.7 Mi S SH 1-St. Charles	Timber Girder	30	15.4	8	9	14	14	10114		
385	Padgett Levee Rd	C	Tarleton Creek	0.4 Mi W SH 17	Steel Ortho	54	18.7	Legal	Legal	Legal	Legal	21048		
408	Frownfelter Rd	I	Hurricane Bayou (Canal)	3.0 Mi N US165-LodgeCorn	Steel Ortho	45	18.7	Legal	Legal	Legal	Legal	21194		
411	Wolf Rd	I	Hurricane Bayou	0.9 Mi W CR408-LodgeCorn	Steel Ortho	30.8	22.6	19	23	31	31	23998		
415	Long Bell Rd	G	Cross Bayou	0.16 Mi E JeffCo-BayouMet	Timber Girder	31	16.1	11	15	19	19	10110		
416	Secmore Rd	I	Elm Prong Mill Bayou	0.01 Mi E CR 33(Yoder Rd)	Timber Girder	30	17.3	13	15	22	22	22711		
431	Sobree Rd	I	Fish Lake Ditch	1.0 Mi W SH 136-Almyra	Steel Girder	25	18.0	0	0	0	0	21501		

Bridge must be closed to traffic

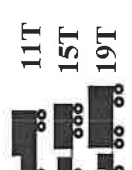
WEIGHT LIMIT



1T
19T
20T

No Posting required for Code 4 Vehicle
 Post for Code 9 Vehicle at 19 Tons
 Post for Code 5 Vehicle at 20 Tons

WEIGHT LIMIT



11T
15T
19T

Post for Code 4 Vehicle at 11 Tons
 Post for Code 9 Vehicle at 15 Tons
 Post for Code 5 Vehicle at 19 Tons

GUIDE TO INFORMATION AND NOTATIONS ON
“BRIDGE INVENTORY / POSTING CERTIFICATION” REPORT

Route and Zone Columns

- County reports
 - Route - ARDOT inventory route number on which the bridge is located.
 - Other Route Designation - Other known designation for the route, such as emergency/911 numbers or names.
 - Map Zone - Used on the maps included in the certification packet contents.
- City reports
 - Route Name - the city street name on which the bridge is located.
 - Zone – Not applicable, so not shown for city reports.

Main Span Information Columns

- The material composition and type of the bridge’s main span(s) is listed for identifying purposes.
- There may be approach spans of differing composition and type than the listed main span.

Bridge Dimensions (ft.) Columns (Length and Width)

- Length - Total bridge length.
- Width – Deck width, measured from curb to curb (c-c) if curbs are present or out to out if none are present.
- Most bridge length culverts with fill material on top of them will have the width listed as “0.0” if no lateral restrictions are present, such as guard railing, other railing, etc...

Recommended Posting (Tons) Columns

- Vehicle Maximum Legal Limits (Tons):
 - Code 4 = 22 T
 - Code 9 = 31 T
 - Code 5 = 40 T
- If “Legal” is listed under a column, there is no posting recommended for that vehicle.
- A *number* listed under a column indicates the recommended posting tonnage for that vehicle.
- If the recommended posting is “0” tons, the bridge must be closed to vehicular traffic.

Remarks Column

- Use for any notations or comments, as desired.
- If there is a valid reason for not posting or closing a bridge as indicated, please write a legible explanation in this column or attach a supplemental document of explanation.

Initials at Bottom of Page

- After reviewing each page for posting or closure recommendations, unless noted otherwise, the agency’s administrator must enter his initials on each page indicating all bridges on the page have been reviewed.

APPENDICES

<u>PAGES</u>	<u>CONTENTS</u>
A-1 thru A-3	National Bridge Inspection Standards Information
B-1 thru B-2	Definition of what constitutes a bridge length structure and examples
C-1 thru C-2	Weight limit sign and support details
D-1 thru D-2	Location of ARDOT District Headquarters
E-1	List of ARDOT Contact Persons (District Construction Engineers)
F-1 thru F-3	Form for designating a City/County contact person (Form VII) and examples
G-1	Resolution (City) that must be executed to participate in the Bridge Posting Materials Program
H-1 thru H-2	Court Order (County) that must be executed to participate in the Bridge Posting Materials Program
I-1 thru I-3	Form for reporting changes in bridge status to the ARDOT (Form VIII) and examples
J-1	Guidelines to determine what conditions should be reported using Form VIII (Appendix I)
K-1 thru K-3	Examples for the requisition of sign materials needed for bridge posting (Form IX usage)
L-1 thru L-2	Example of signed "Bridge Inventory / Posting Certification" report

§ 650.301 Purpose.

This subpart sets the national standards for the proper safety inspection and evaluation of all highway bridges in accordance with 23 U.S.C. 151.

§ 650.303 Applicability.

The National Bridge Inspection Standards (NBIS) in this subpart apply to all structures defined as highway bridges located on all public roads.

§ 650.305 Definitions.

Terms used in this subpart are defined as follows:

American Association of State Highway and Transportation Officials (AASHTO) Manual. “Manual for Condition Evaluation of Bridges,” second edition, published by the American Association of State Highway and Transportation Officials (incorporated by reference, *see* § 650.317).

Bridge. A structure including supports erected over a depression or an obstruction, such as water, highway, or railway, and having a track or passageway for carrying traffic or other moving loads, and having an opening measured along the center of the roadway of more than 20 feet between under copings of abutments or springlines of arches, or extreme ends of openings for multiple boxes; it may also include multiple pipes, where the clear distance between openings is less than half of the smaller contiguous opening.

Bridge inspection experience. Active participation in bridge inspections in accordance with the NBIS, in either a field inspection, supervisory, or management role. A combination of bridge design, bridge maintenance, bridge construction and bridge inspection experience, with the predominant amount in bridge inspection, is acceptable.

Bridge inspection refresher training. The National Highway Institute “Bridge Inspection Refresher Training Course”¹ or other State, local, or federally developed instruction aimed to improve quality of inspections, introduce new techniques, and maintain the consistency of the inspection program.

Bridge Inspector’s Reference Manual (BIRM). A comprehensive FHWA manual on programs, procedures and techniques for inspecting and evaluating a variety of in-service highway bridges. This manual may be purchased from the U.S. Government Printing Office, Washington, DC 20402 and from National Technical Information Service, Springfield, Virginia 22161, and is available at the following URL: <http://www.fhwa.dot.gov/bridge/bripub.htm>.

Complex bridge. Movable, suspension, cable stayed, and other bridges with unusual characteristics.

Comprehensive bridge inspection training. Training that covers all aspects of bridge inspection and enables inspectors to relate conditions observed on a bridge to established criteria (see the Bridge Inspector’s Reference Manual for the recommended material to be covered in a comprehensive training course).

Critical finding. A structural or safety related deficiency that requires immediate follow-up inspection or action.

Damage inspection. This is an unscheduled inspection to assess structural damage resulting from environmental factors or human actions.

Fracture critical member (FCM). A steel member in tension, or with a tension element, whose failure would probably cause a portion of or the entire bridge to collapse.

Fracture critical member inspection. A hands-on inspection of a fracture critical member or member components that may include visual and other nondestructive evaluation.

Hands-on. Inspection within arms length of the component. Inspection uses visual techniques that may be supplemented by nondestructive testing.

Highway. The term “highway” is defined in 23 U.S.C. 101(a)(11).

In-depth inspection. A close-up, inspection of one or more members above or below the water level to identify any deficiencies not readily detectable using routine inspection procedures; hands-on inspection may be necessary at some locations.

Initial inspection. The first inspection of a bridge as it becomes a part of the bridge file to provide all Structure Inventory and Appraisal (SI&A) data and other relevant data and to determine baseline structural conditions.

Legal load. The maximum legal load for each vehicle configuration permitted by law for the State in which the bridge is located.

Load rating. The determination of the live load carrying capacity of a bridge using bridge plans and supplemented by information gathered from a field inspection.

National Institute for Certification in Engineering Technologies (NICET). The NICET provides nationally applicable voluntary certification programs covering several broad engineering technology fields and a number of specialized subfields. For information on the NICET program certification contact: National Institute for Certification in Engineering Technologies, 1420 King Street, Alexandria, VA 22314-2794.

Operating rating. The maximum permissible live load to which the structure may be subjected for the load configuration used in the rating.

Professional engineer (PE). An individual, who has fulfilled education and experience requirements and passed rigorous exams that, under State licensure laws, permits them to offer engineering services directly to the public. Engineering licensure laws vary from State to State, but, in general, to become a PE an individual must be a graduate of an engineering program accredited by the Accreditation Board for Engineering and Technology, pass the Fundamentals of Engineering exam, gain four years of experience working under a PE, and pass the Principles of Practice of Engineering exam.

Program Manager. The individual in charge of the program, that has been assigned or delegated the duties and responsibilities for bridge inspection, reporting, and inventory. The program manager provides overall leadership and is

available to inspection teamleaders to provide guidance.

Public road. The term “public road” is defined in 23 U.S.C. 101(a)(27).

Quality assurance (QA). The use of sampling and other measures to assure the adequacy of quality control procedures in order to verify or measure the quality level of the entire bridge inspection and load rating program.

Quality control (QC). Procedures that are intended to maintain the quality of a bridge inspection and load rating at or above a specified level.

Routine inspection. Regularly scheduled inspection consisting of observations and/or measurements needed to determine the physical and functional condition of the bridge, to identify any changes from initial or previously recorded conditions, and to ensure that the structure continues to satisfy present service requirements.

Routine permit load. A live load, which has a gross weight, axle weight or distance between axles not conforming with State statutes for legally configured vehicles, authorized for unlimited trips over an extended period of time to move alongside other heavy vehicles on a regular basis.

Scour. Erosion of streambed or bank material due to flowing water; often considered as being localized around piers and abutments of bridges.

Scour critical bridge. A bridge with a foundation element that has been determined to be unstable for the observed or evaluated scour condition.

Special inspection. An inspection scheduled at the discretion of the bridge owner, used to monitor a particular known or suspected deficiency.

State transportation department. The term “State transportation department” is defined in 23 U.S.C. 101(a)(34).

Team leader. Individual in charge of an inspection team responsible for planning, preparing, and performing field inspection of the bridge.

Underwater diver bridge inspection training. Training that covers all aspects of underwater bridge inspection and enables inspectors to relate the conditions of underwater bridge elements to established criteria (see the Bridge Inspector’s Reference Manual section on underwater inspection for the recommended material to be covered in an underwater diver bridge inspection training course).

Underwater inspection. Inspection of the underwater portion of a bridge substructure and the surrounding channel, which cannot be inspected visually at low water by wading or probing, generally requiring diving or other appropriate techniques.

§ 650.307 Bridge inspection organization.

(a) Each State transportation department must inspect, or cause to be inspected, all highway bridges located on public roads that are fully or partially located within the State’s boundaries, except for bridges that are owned by Federal agencies.

(b) Federal agencies must inspect, or cause to be inspected, all highway bridges located on public roads that are fully or partially located within the respective agency responsibility or jurisdiction.

(c) Each State transportation department or Federal agency must include a bridge inspection organization that is responsible for the following:

(1) Statewide or Federal agency wide bridge inspection policies and procedures, quality assurance and quality control, and preparation and maintenance of a bridge inventory.

(2) Bridge inspections, reports, load ratings and other requirements of these standards.

(d) Functions identified in paragraphs (c)(1) and (2) of this section may be delegated, but such delegation does not relieve the State transportation department or Federal agency of any of its responsibilities under this subpart.

(e) The State transportation department or Federal agency bridge inspection organization must have a program manager with the qualifications defined in § 650.309(a), who has been delegated responsibility for paragraphs (c)(1) and (2) of this section.

§ 650.309 Qualifications of personnel.

(a) A program manager must, at a minimum:

(1) Be a registered professional engineer, or have ten years bridge inspection experience; and

(2) Successfully complete a Federal Highway Administration (FHWA) approved comprehensive bridge inspection training course.

(b) There are five ways to qualify as a team leader. A team leader must, at a minimum:

(1) Have the qualifications specified in paragraph (a) of this section; or

(2) Have five years bridge inspection experience and have successfully completed an FHWA approved comprehensive bridge inspection training course; or

(3) Be certified as a Level III or IV Bridge Safety Inspector under the National Society of Professional Engineer’s program for National Certification in Engineering Technologies (NICET) and have successfully completed an FHWA approved comprehensive bridge inspection training course, or

(4) Have all of the following:

(i) A bachelor’s degree in engineering from a college or university accredited by or determined as substantially equivalent by the Accreditation Board for Engineering and Technology;

(ii) Successfully passed the National Council of Examiners for Engineering and Surveying Fundamentals of Engineering examination;

(iii) Two years of bridge inspection experience; and

- or
- (iv) Successfully completed an FHWA approved comprehensive bridge inspection training course,
- (5) Have all of the following:
- (i) An associate's degree in engineering or engineering technology from a college or university accredited by or determined as substantially equivalent by the Accreditation Board for Engineering and Technology; Four years of bridge inspection experience; and
 - (ii) Successfully completed an FHWA approved comprehensive bridge inspection training course.
 - (c) The individual charged with the overall responsibility for load rating bridges must be a registered professional engineer.
 - (d) An underwater bridge inspection diver must complete an FHWA approved comprehensive bridge inspection training course or other FHWA approved underwater diver bridge inspection training course.

§ 650.311 Inspection frequency.

- (a) *Routine inspections.* (1) Inspect each bridge at regular intervals not to exceed twenty-four months.
- (2) Certain bridges require inspection at less than twenty-four-month intervals. Establish criteria to determine the level and frequency to which these bridges are inspected considering such factors as age, traffic characteristics, and known deficiencies.
- (3) Certain bridges may be inspected at greater than twenty-four month intervals, not to exceed forty-eight- months, with written FHWA approval. This may be appropriate when past inspection findings and analysis justifies the increased inspection interval.
- (b) *Underwater inspections.* (1) Inspect underwater structural elements at regular intervals not to exceed sixty months.
- (2) Certain underwater structural elements require inspection at less than sixty-month intervals. Establish criteria to determine the level and frequency to which these members are inspected considering such factors as construction material, environment, age, scour characteristics, condition rating from past inspections and known deficiencies.
- (3) Certain underwater structural elements may be inspected at greater than sixty-month intervals, not to exceed seventy-two months, with written FHWA approval. This may be appropriate when past inspection findings and analysis justifies the increased inspection interval.
- (c) *Fracture critical member (FCM) inspections.* (1) Inspect FCMs at intervals not to exceed twenty-four months.
- (2) Certain FCMs require inspection at less than twenty-four-month intervals. Establish criteria to determine the level and frequency to which these members are inspected considering such factors as age, traffic characteristics, and known deficiencies.
- (d) Damage, in-depth, and special inspections. Establish criteria to determine the level and frequency of these inspections.

§ 650.313 Inspection procedures.

- (a) Inspect each bridge in accordance with the inspection procedures in the AASHTO Manual (incorporated by reference, *see* § 650.317).
- (b) Provide at least one team leader, who meets the minimum qualifications stated in § 650.309, at the bridge at all times during each initial, routine, in- depth, fracture critical member and underwater inspection.
- (c) Rate each bridge as to its safe load- carrying capacity in accordance with the AASHTO Manual (incorporated by reference, *see* § 650.317). Post or restrict the bridge in accordance with the AASHTO Manual or in accordance with State law, when the maximum unrestricted legal loads or State routine permit loads exceed that allowed under the operating rating or equivalent rating factor.
- (d) Prepare bridge files as described in the AASHTO Manual (incorporated by reference, *see* § 650.317). Maintain reports on the results of bridge inspections together with notations of any action taken to address the findings of such inspections. Maintain relevant maintenance and inspection data to allow assessment of current bridge condition. Record the findings and results of bridge inspections on standard State or Federal agency forms.
- (e) Identify bridges with FCMs, bridges requiring underwater inspection, and bridges that are scour critical.
 - (1) Bridges with fracture critical members. In the inspection records, identify the location of FCMs and describe the FCM inspection frequency and procedures. Inspect FCMs according to these procedures.
 - (2) Bridges requiring underwater inspections. Identify the location of underwater elements and include a description of the underwater elements, the inspection frequency and the procedures in the inspection records for each bridge requiring underwater inspection. Inspect those elements requiring underwater inspections according to these procedures.
 - (3) Bridges that are scour critical. Prepare a plan of action to monitor known and potential deficiencies and to address critical findings. Monitor bridges that are scour critical in accordance with the plan.
- (f) *Complex bridges.* Identify specialized inspection procedures, and additional inspector training and experience required to inspect complex bridges. Inspect complex bridges according to those procedures.
- (g) *Quality control and quality assurance.* Assure systematic quality control (QC) and quality assurance (QA) procedures are used to maintain a high degree of accuracy and consistency in the inspection program. Include periodic

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field review of inspection teams, periodic bridge inspection refresher training for program managers and team leaders, and independent review of inspection reports and computations.

(h) *Follow-up on critical findings.* Establish a statewide or Federal agency wide procedure to assure that critical findings are addressed in a timely manner. Periodically notify the FHWA of the actions taken to resolve or monitor critical findings.

§ 650.315 Inventory.

(a) Each State or Federal agency must prepare and maintain an inventory of all bridges subject to the NBIS. Certain Structure Inventory and Appraisal (SI&A) data must be collected and retained by the State or Federal agency for collection by the FHWA as requested. A tabulation of this data is contained in the SI&A sheet distributed by the FHWA as part of the “Recording and Coding Guide for the Structure Inventory and Appraisal of the Nation’s Bridges,” (December 1995) together with subsequent interim changes or the most recent version. Report the data using FHWA established procedures as

outlined in the “Recording and Coding Guide for the Structure Inventory and Appraisal of the Nation’s Bridges.”

(b) For routine, in-depth, fracture critical member, underwater, damage and special inspections enter the SI&A data into the State or Federal agency inventory within 90 days of the date of inspection for State or Federal agency bridges and within 180 days of the date of inspection for all other bridges.

(c) For existing bridge modifications that alter previously recorded data and for new bridges, enter the SI&A data into the State or Federal agency inventory within 90 days after the completion of the work for State or Federal agency bridges and within 180 days after the completion of the work for all other bridges.

(d) For changes in load restriction or closure status, enter the SI&A data into the State or Federal agency inventory within 90 days after the change in status of the structure for State or Federal agency bridges and within 180 days after the change in status of the structure for all other bridges.

§ 650.317 Reference manuals.

(a) The materials listed in this subpart are incorporated by reference in the corresponding sections noted. These incorporations by reference were approved by the Director of the Federal Register in accordance with 5 U.S.C. 552(a) and 1 CFR part 51. These materials are incorporated as they exist on the date of the approval, and notice of any change in these documents will be published in the **Federal Register**. The materials are available for purchase at the address listed below, and are available for inspection at the National Archives and Records Administration (NARA). These materials may also be reviewed at the Department of Transportation Library, 400 Seventh Street, SW., Washington, DC, in Room 2200. For information on the availability of these materials at NARA call (202) 741-6030, or go to the following URL: http://www.archives.gov/federal_register/code_of_federal_regulations/ibr_locations.html. In the event there is a conflict between the standards in this subpart and any of these materials, the standards in this subpart will apply.

(b) The following materials are available for purchase from the American Association of State Highway and Transportation Officials, Suite 249, 444 N. Capitol Street, NW., Washington, DC 20001. The materials may also be ordered via the AASHTO bookstore located at the following URL: <http://www.aashto.org/aashto/home.nsf/FrontPage>.

(1) The Manual for Condition Evaluation of Bridges, 1994, second edition, as amended by the 1995, 1996, 1998, and 2000 interim revisions, AASHTO, incorporation by reference approved for §§ 650.305 and 650.313.

(2) 2001 Interim Revision to the Manual for Condition Evaluation of Bridges, AASHTO, incorporation by reference approved for §§ 650.305 and 650.313.

(3) 2003 Interim Revision to the Manual for Condition Evaluation of Bridges, AASHTO, incorporation by reference approved for §§ 650.305 and 650.313.

[FR Doc. 04-27355 Filed 12-13-04; 8:45 am]

BILLING CODE 4910-22-P

Definition and Examples of Bridge Length Structures

The National Bridge Inspection Standards (NBIS) require that all structures defined as bridges located on all public roads be inventoried, inspected and load rated (23CFR650.3).

WHAT IS A BRIDGE LENGTH STRUCTURE?

A bridge shall be defined as a structure that carries traffic over an obstruction, and whose gross length from inside face of abutment to inside face of abutment is more than 20 feet, measured along the center of the roadway.

A multiple pipe culvert shall be classified as a bridge length structure if the clear distance between openings is less than half of the smaller adjacent opening, and the overall length of structure (out to out of pipe openings) is more than 20 feet, measured along the center of the roadway.

The following page provides typical examples of bridge length structures.

EXAMPLES OF BRIDGE LENGTH STRUCTURES

3.1.3

NBIS Bridge Length

The *FHWA Recording and Coding Guide for the Structure Inventory and Appraisal of the Nation's Bridges* also states, in accordance with Item 112 – NBIS Bridge Length, that the minimum length for a structure to be considered a bridge for National Bridge Inspection Standards purposes, is to be 20 feet (see Figure 3.1.2).

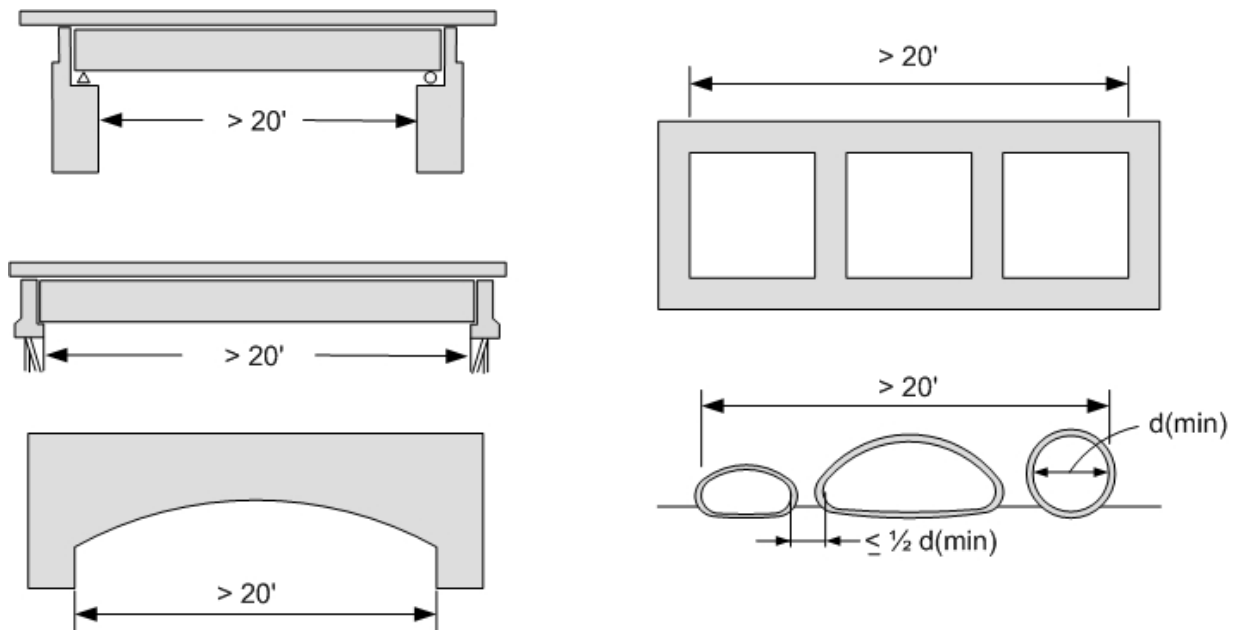
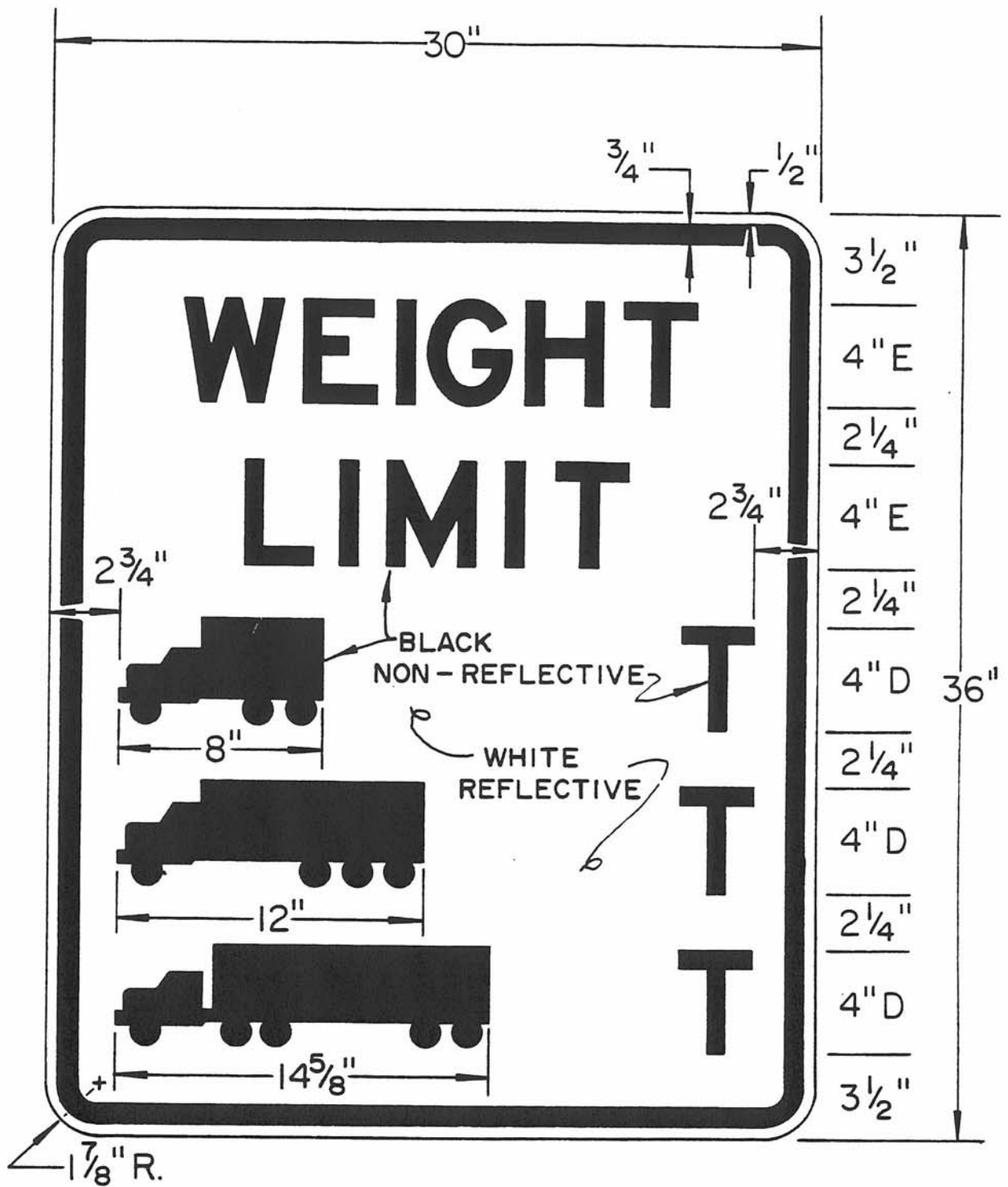


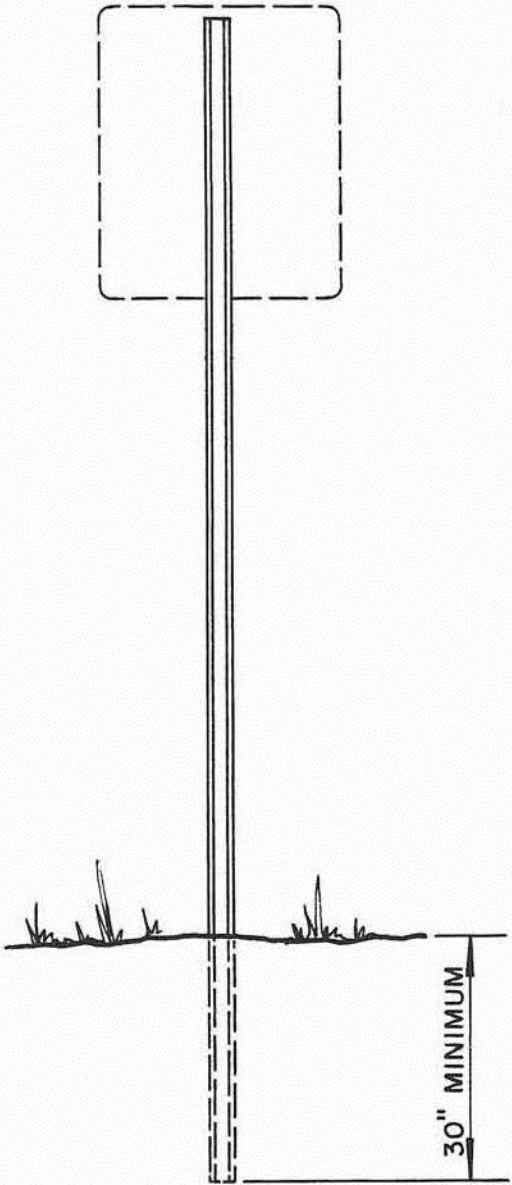
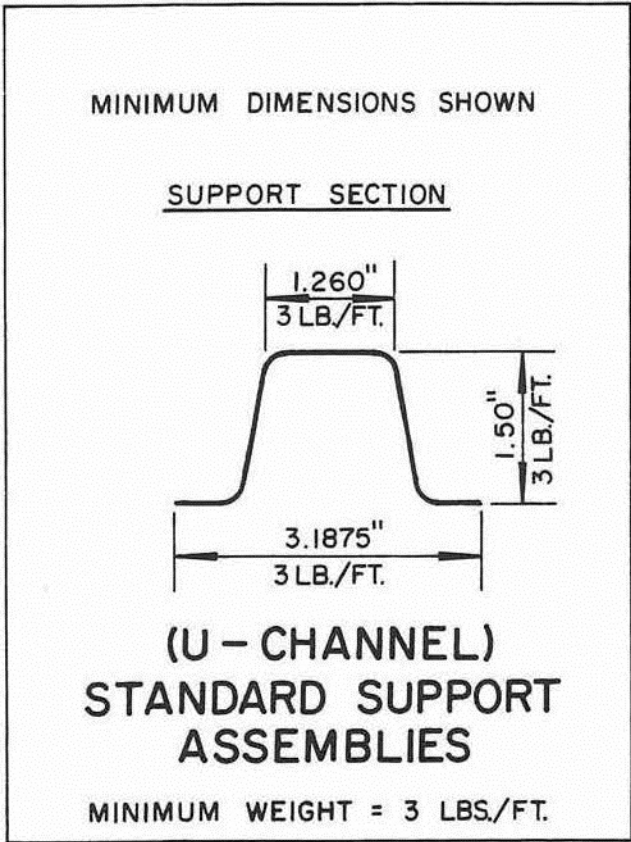
Figure 3.1.2 NBIS Bridge Length (Coding Guide Item 112)

23 CFR Part 650.305 Definitions gives the definition of a bridge as it applies to the NBIS regulations: A bridge is a structure including supports erected over a depression or an obstruction, such as water, highway, or railway, and having a track or passageway for carrying traffic or other moving loads, and having an opening measured along the center of the roadway of more than 20 feet between undercopings of abutments or spring lines of arches, or extreme ends of openings for multiple boxes; it may also include multiple pipes, where the clear distance between openings is less than half of the smaller contiguous opening.



**DETAILS FOR BRIDGE
WEIGHT LIMIT SIGNS**

DRAWING NO. BM 714



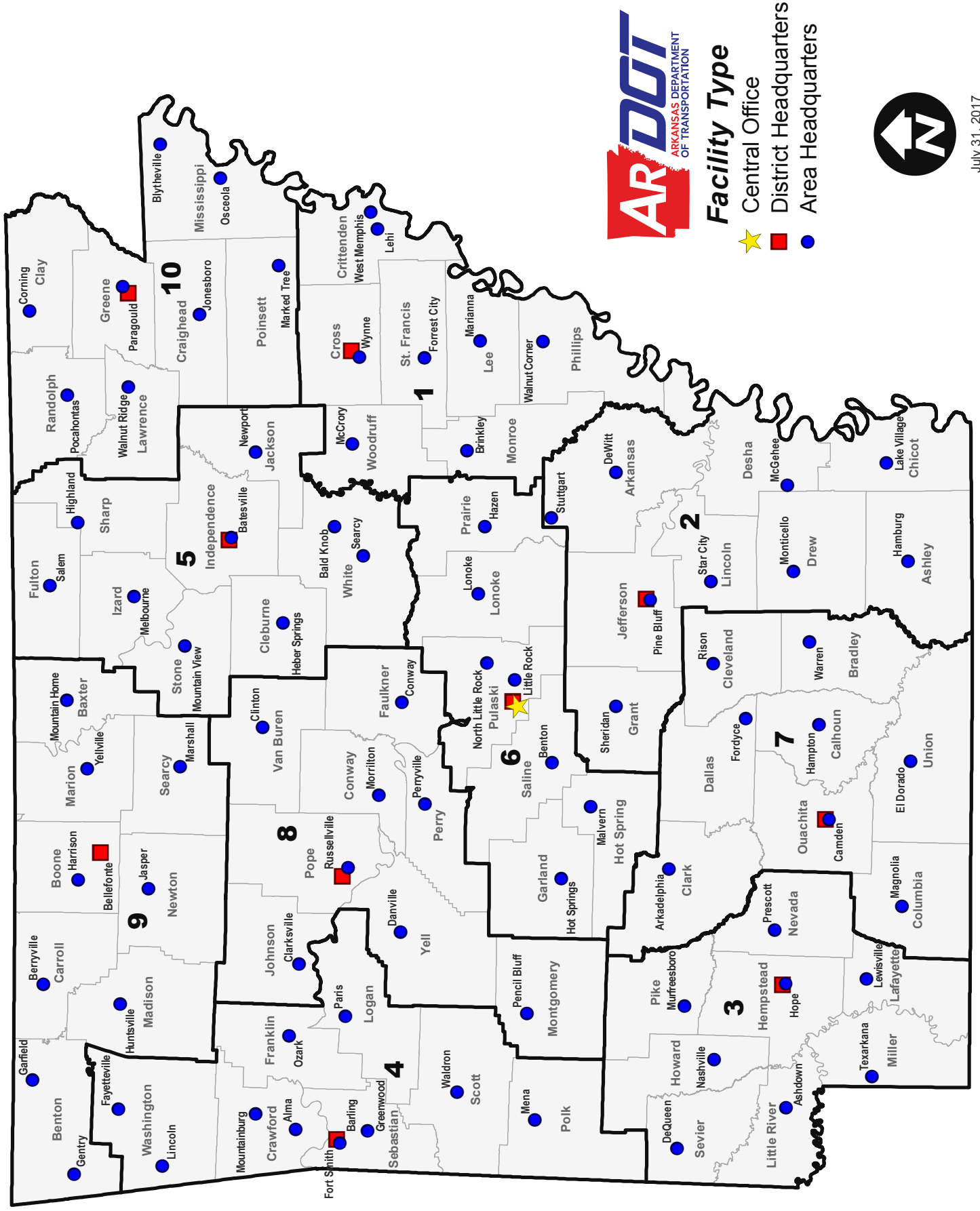
TYPE A

NOTE: LENGTH OF SIGN POSTS SHALL BE DETERMINED SO AS TO PROVIDE FOR MINIMUM VERTICAL CLEARANCES AS CALLED FOR IN THE SPECIFICATIONS PLUS A MINIMUM VERTICAL PENETRATION OF 30" IN THE SOIL.

**STANDARD HIGHWAY SIGNS
AND SUPPORT ASSEMBLIES**

Operations Headquarters

Arkansas Department of Transportation



Facility Type

- ★ Central Office
- District Headquarters
- Area Headquarters



LOCATION OF ARDOT DISTRICT HEADQUARTERS

<u>District One</u>	Wynne – 2701 US Hwy. 64; west of Jct. SH 1 North P.O. Box 278 Wynne, AR 72396-0278
<u>District Two</u>	Pine Bluff - 4900 US 65 South; east of Jct. US 425 and 65 P.O. Box 6836 Pine Bluff, AR 71611-6836
<u>District Three</u>	Hope – 2911 SH 29 North; I-30, Exit No. 31 P.O. Box 490 Hope, AR 71802-0490
<u>District Four</u>	Fort Smith/Barling – 808 Frontier Rd., Barling, AR P.O. Box 11170 Fort Smith, AR 72917-1170
<u>District Five</u>	Batesville – 1673 Batesville Blvd. (US 167); 3 miles south of Batesville P.O. Box 2376 Batesville, AR 72503-2376
<u>District Six</u>	Little Rock - 8900 Mabelvale Pike, Southwest Little Rock, I-30, Exit No. 130 (Baseline Rd./SH 338) P.O. Box 190296 Little Rock, AR 72219-0296
<u>District Seven</u>	Camden - 2245 California Ave. (US 79) P.O. Box 897 Camden, AR 71711-0897
<u>District Eight</u>	Russellville - 372 Aspen Ln., I-40 Exit No. 81, north of I-40 & east of SH 7 P.O. Box 70 Russellville, AR 72811-0070
<u>District Nine</u>	Harrison – 4590 US 65; 0.5 miles south of Jct. US 65 and 62 P.O. Box 610 Harrison, AR 72602-0610
<u>District Ten</u>	Paragould - 2510 US 412 West; 1 mile west of Jct. US 49 & 412 P.O. Box 98 Paragould, AR 72451-0098

LIST OF DISTRICT CONSTRUCTION ENGINEERS

District	Name	Mailing Address	Tel. Numbers
One	P. Jeff Adams	P. O. Box 278, Wynne, AR 72396-0278	(870)238-8144 (870)238-2994 (fax)
Two	Bryan Swinney	P. O. Box 6836, Pine Bluff, AR 71611-6836	(870)534-1612 (870)534-2038 (fax)
Three	William Cheatham	P. O. Box 490, Hope, AR 71802-0490	(870)777-3457 (870)777-3489 (fax)
Four	Marcus Rainwater	P. O. Box 11170, Fort Smith, AR 72917-1170	(479)484-5306 (479)484-5300 (fax)
Five	Stan Glover	P. O. Box 2376, Batesville, AR 72503-2376	(870)251-2374 (870)251-2393 (fax)
Six	Tony Evans	P. O. Box 190296, Little Rock, AR 72219-0296	(501)569-2169 (501)569-2366 (fax)
Seven	Todd Russell	P. O. Box 897, Camden, AR 71711-0897	(870)836-6401 (870)836-4864 (fax)
Eight	Thale Keisling	P. O. Box 70, Russellville, AR 72811-0070	(479)968-2286 (479)968-4006 (fax)
Nine	Mitchell Archer	P. O. Box 610, Harrison, AR 72602-0610	(870)743-2100 (870)743-4630 (fax)
Ten	Alan Walter	P. O. Box 98, Paragould, AR 72451-0098	(870)239-9511 (870)236-1156 (fax)

Date: _____

Arkansas' Bridge Inspection Program
For Local Governments

Designated Contact Person

The individual named below is hereby designated the principal contact person between this agency and the Arkansas Department of Transportation in matters relating to inventory, inspection and load rating of bridges on our public highway system.

Designated Contact:

Name: _____

Title: _____

Mailing Address: _____

Phone Number: _____

Email address:
(optional) _____

Official's Signature: _____

City Mayor/Administrator or County Judge

Name of City or County: _____

FORWARD THIS COMPLETED FORM TO YOUR ARDOT DISTRICT CONSTRUCTION
ENGINEER.

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Date: October 10, 2019

Arkansas' Bridge Inspection Program
For Local Governments

Designated Contact Person

The individual named below is hereby designated the principal contact person between this agency and the Arkansas Department of Transportation in matters relating to inventory, inspection and load rating of bridges on our public highway system.

Designated Contact:

Name: John E. Cash

Title: Mayor

Mailing Address: 700 Bear State Road

Kingsland , AR 71652

Phone Number: 078-555-1234

Email address:
(optional) walk.the.line@yahoo.net

Official's Signature: John E. Cash
City Mayor/Administrator or County Judge

Name of City or County: Bodenhammer County

FORWARD THIS COMPLETED FORM TO YOUR ARDOT DISTRICT CONSTRUCTION ENGINEER.

Date: October 12, 2019

Arkansas' Bridge Inspection Program
For Local Governments

Designated Contact Person

The individual named below is hereby designated the principal contact person between this agency and the Arkansas Department of Transportation in matters relating to inventory, inspection and load rating of bridges on our public highway system.

Designated Contact:

Name: Glenn Travis Campbell

Title: Bridge & Road Super.

Mailing Address: 725 Campbell Field Road

Billstown, AR 71940

Phone Number: 555-867-5309

Email address:
(optional) lineman@aristotle.com

Official's Signature: *Glenn J Campbell*

City Mayor/Administrator or County Judge

Name of City or County: *Wichita County*

FORWARD THIS COMPLETED FORM TO YOUR ARDOT DISTRICT CONSTRUCTION ENGINEER.

RESOLUTION NO. _____

A RESOLUTION EXPRESSING THE WILLINGNESS

OF THE CITY OF _____
TO COOPERATE IN A PROGRAM REGARDING
WARNING SIGNS AT BRIDGES

WHEREAS, the 1978 Surface Transportation Assistance Act allows participating Federal-Aid funds for the erection of warning signs at public highway bridges that are limited by weight, and

WHEREAS, there may be bridges under the City's jurisdiction that should have warning signs placed regarding weight limit restrictions, and

WHEREAS, it is the intent of the City to comply with the National Bridge Inspection Standards as administered by the Arkansas Department of Transportation (ARDOT) and described in the manual entitled "Local Government Procedures for Compliance With The National Bridge Inspection Standards", hereafter referred to as the "Local Government Compliance Manual", and

WHEREAS, this program has been approved by the ARDOT and the Federal Highway Administration (FHWA) for the installation of the necessary load limit signs, funded in part with City funds and part with Federal funds, and

WHEREAS, this Resolution supersedes all previous agreements regarding reimbursement for the installation of warning signs at bridges with restrictive weight limits,

NOW THEREFORE BE IT RESOLVED THAT:

1. The City intends to participate in this program as explained in the "Local Government Compliance Manual".
2. As the City installs the load limit sign(s) at the bridge locations designated by the ARDOT and in accordance with the specifications stated in the "Manual On Uniform Traffic Control Devices", the ARDOT will bill the City for twenty percent (20%) of the material costs. The City agrees to pay one hundred percent (100%) of the material costs if the installation and written notification is not made within one month from the date of material requisition.
3. The City may obtain bridge weight limit signing materials from the ARDOT not only for initial posting, but also for replacement signing or altered load limit signing.
4. Upon completion of the sign installation, the City will maintain the sign(s).
5. The City agrees to return to the ARDOT an annual certification agreement regarding bridge posting and closing status. As a supplement to this certification agreement, the ARDOT will provide to the City a current list of bridges requiring posting or closing for verification and return to the ARDOT.

THIS RESOLUTION adopted this day of _____, .

MAYOR

ATTEST: _____

(SEAL)

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IN THE _____ COUNTY COURT

ORDER

Now on this day comes for consideration the matter of load posting deficient county bridges as required by the National Bridge Inspection Standards (NBIS) in order to prevent the loss of Federal Aid Highway funds to the County government.

And it appears to the Court that this project has been approved by the Arkansas Department of Transportation (ARDOT) and the Federal Highway Administration (FHWA) for the installation of the necessary load limit signs as required by the NBIS funded in part with County funds and part with Federal funds.

It is therefore considered, ordered and adjudged by the Court that this County will agree to install the load limit signs at the required locations designated by the ARDOT and to the specifications stated in the "Manual On Uniform Traffic Control Devices" for the agreed price of twenty percent (20%) of the materials cost. The FHWA will provide eighty percent (80%) of the materials cost. Should the County fail to reimburse the ARDOT for their twenty percent (20%) share when billed, the ARDOT may cause this deficiency to be withheld from the allotment of gasoline tax returnable to the County, commonly called the County Turnback Fund. The County agrees to pay one hundred percent (100%) of material costs if installation and notification is not made within one month from the date of material requisition.

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Upon completion of the sign material installations, the County agrees to maintain the signs in accordance with NBIS regulations. The County agrees to return to the ARDOT an annual certification agreement regarding bridge posting and closing status. It is further agreed, this Court Order supersedes all previous agreements regarding reimbursement for the installation of warning signs at bridges with restrictive weight limits.

County Judge

Date

I, _____ County Clerk, do hereby certify that the foregoing order was duly signed on the _____ day of _____, _____, and the same is recorded in Record Book _____ on page _____ of the records of _____ County.

County Clerk

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**ARKANSAS' BRIDGE INSPECTION PROGRAM
FOR LOCAL GOVERNMENTS**

Bridge Revisions Affecting Inventory Data

*** NOT FOR USE IN BRIDGE WEIGHT LIMIT SIGN REQUISITION OR CERTIFICATION ***

County or City Name: _____ Official's Signature: _____

* Bridge Number	* Route Number or Name and Feature Under Bridge	Description of Work Performed	Date Work Performed	** Recommended Action (ARDOT Use Only)

* Refer to current "Bridge Inventory/Posting Certification" Report or other ARDOT notification (Maintenance Needs report, letter, etc...)

** Recommended Actions by ARDOT District Office:

1 = Schedule Bridge Inspection and update database with revised inspection data.

2 = Work performed does not necessitate additional inspection (No Further action Taken)

PLEASE SUBMIT THE COMPLETED FORM PROMPTLY TO YOUR ARDOT DISTRICT CONSTRUCTION ENGINEER

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**ARKANSAS' BRIDGE INSPECTION PROGRAM
FOR LOCAL GOVERNMENTS**

Bridge Revisions Affecting Inventory Data

*** NOT FOR USE IN BRIDGE WEIGHT LIMIT SIGN REQUISITION OR CERTIFICATION ***

County or City Name: Gila County

Official's Signature: Linda Senora

* Bridge Number	* Route Number or Name and Feature Under Bridge	Description of Work Performed	Date Work Performed	** Recommended Action (ARDOT Use Only)
27550	Co Rt 24 (Lizard Rd.) Lizard Creek	Bridge removed & replaced at existing location	8/22/17	
27450	Co Rt 26 (Abuelo Trl.) Grand Ditch	Repaired mushroomed pile#1 on Bt. 3	9/6/17	
27455	Co Rt 15 (Banana Creek Rd.) Banana Creek	Added 3 in. asphalt overlay on existing deck	9/20/17	

* Refer to current "Bridge Inventory/Posting Certification" Report or other ARDOT notification (Maintenance Needs report, letter, etc...)

** Recommended Actions by ARDOT District Office:

- 1 = Schedule Bridge Inspection and update database with revised inspection data
- 2 = Work performed does not necessitate reinspection (No Further Action Taken)

PLEASE SUBMIT THE COMPLETED FORM PROMPTLY TO YOUR ARDOT DISTRICT CONSTRUCTIONENGINEER

**ARKANSAS' BRIDGE INSPECTION PROGRAM
FOR LOCAL GOVERNMENTS**

Bridge Revisions Affecting Inventory Data

*** NOT FOR USE IN BRIDGE WEIGHT LIMIT SIGN REQUISITION OR CERTIFICATION ***

County or City Name: Damenville Official's Signature: Hans Gruber

* Bridge Number	* Route Number or Name and Feature Under Bridge	Description of Work Performed	Date Work Performed	** Recommended Action (ARDOT Use Only)
14444	Dresden Dr./Snake Creek	Replaced broken stringers in Spans 1 & 2	5/21/17	
M1428	Rhineland Cir./Ditch	Widened bridge from 12 ft. to 22 ft.	6/25/17	
27777	Stuttgart Pl. / Toad Creek	Physically closed bridge to traffic with barricade	6/5/17	

* Refer to current "Bridge Inventory/Posting Certification" Report or other ARDOT notification (Maintenance Needs report, letter, etc...)

** Recommended Actions by ARDOT District Office:

- 1 = Schedule Bridge Inspection and update database with revised inspection data.
- 2 = Work performed does not necessitate additional inspection (No Further Action Taken)

PLEASE SUBMIT THE COMPLETED FORM PROMPTLY TO YOUR ARDOT DISTRICT CONSTRUCTION ENGINEER

Examples of NBIS - Form VIII Utilization

The following list is intended to provide typical examples when revisions to an existing highway bridge should be reported on Form VIII and promptly forwarded to the District Construction Engineer by mail, fax, or scanned and attached to an email. The list is not intended as all inclusive, but does serve as a guide for general classes of revisions to be reported.

Complete and submit Form VIII when:

1. Bridge physically closed to traffic.
2. Bridge removed and not replaced (eliminated).
3. Bridge removed and replaced at existing location.
4. Existing bridge widened and/or length changed.
5. Bridge constructed at new location.
6. Existing deck and/or stringer(s) replaced or strengthened.
7. Existing pier(s)/bent(s)/abutment(s) replaced or strengthened.
8. Existing pile(s)/column(s) replaced or strengthened.
9. Additional pier(s)/bent(s) added to existing structure.
10. Additional surfacing (asphalt, gravel, concrete, timber) added over existing deck.
11. Existing bridge damaged by manmade or natural causes (i. e., burned, streambed eroded, accident damage).
12. Accumulated debris/drift removed from bridge channel.

Note: Updates to bridge information can be made by email, fax, or a letter to the District Construction as one prefers.

THESE MATERIALS MUST BE INSTALLED AND THIS FORM SIGNED AND RETURNED WITHIN ONE MONTH OR THE AGENCY WILL BE BILLED FOR 100% OF THE MATERIAL COST.

BRIDGE WEIGHT LIMIT SIGNS
MATERIAL REQUISITION AND INSTALLATION LOG

Bridge Number	Route Number or Name and Feature Under Bridge	Truck Weight Limits (Tons)					REQUISITION INFORMATION												INSTALLATION INFORMATION									
		Code 4		Code 9		Code 5	Material Quantity**												Install Date (M/D/Y)	Installation Certified By Agency Official (Signature)								
		Code 4	Code 9	Code 9	Code 5	Code 5	Number of Decals																					
0's	1's	2's	3's	4's	5's	6's	7's	8's	9's	Bl.	Signs #	Posts #	0's	1's	2's	3's	4's	5's	6's	7's	8's	9's	Bl.	Requisition Date (M/D/Y)	ARDOT Issuer (Initials)	Received by Agency (Initials)		
12249	CR 6 / DITCH NO. 8	3	3	3	3	3	1																		3/10/19	JOE	RED	
12256	CR 10 / DITCH	4	5	7																								
20235	CR 13 / 15 mi. CAK	7	9	12																								
21925	CR 356 / DITCH #2	12	12	17																								
												Totals: 5 2 1 5 4 3 2 2 5 2 2 3																

Instructions For Completing Form:

- Complete columns of the "Bridge Information" area, using information from ARDOT notification*.
- Determine material quantities needed and complete the columns of "Requisition Information" area as necessary for each bridge.
- Agency Administrator signs "Requesting Official's Signature" where materials are requisitioned using the columns of the "Requisition Information" area.
- Completed and signed form is delivered to ARDOT District Headquarters where materials are requisitioned using the columns of the "Requisition Information" area.
- The requesting agency retains the original and one copy of the form and the District Headquarters forwards a single copy to Bridge Division.
- Upon properly installing requisitioned materials, the requesting agency completes "Installation Information" and sends original to District Headquarters.
- After review of completed form, District Headquarters sends original to Bridge Division for processing and billing.

* Use the most current information from:

- The latest "Bridge Inventory / Posting Certification" report, OR
- Other ARDOT notification (Maintenance Needs report, letter, etc.)

** A maximum of 12 single digit decals, 2 posts, and/or 2 signs per bridge may be requisitioned at one time.

Notes:

- By completing columns in the "Installation Information" area, the agency which requisitioned the signing material certifies that these materials were properly installed at the designated bridge site on the date shown.
- The agency requisitioning the material will be billed for only 20% of the material cost if "Installation Information" is completed and the original form is returned to the District Headquarters within one month of the date shown in the "Requisition Information" area. If completed and returned beyond the one month limit, the agency will be billed for 100% of the material cost.

Requesting Official's Signature

County of SONOMA

County Judge

Requesting Official's Signature

City of _____

Mayor / City Administrator

FOR ARDOT USE ONLY

Date Forwarded to Bridge Division:

EXAMPLE (BEFORE MATERIAL INSTALLATION)

THESE MATERIALS MUST BE INSTALLED AND THIS FORM SIGNED AND RETURNED WITHIN ONE MONTH OR THE AGENCY WILL BE BILLED FOR 100% OF THE MATERIAL COST.

BRIDGE WEIGHT LIMIT SIGNS
MATERIAL REQUISITION AND INSTALLATION LOG

Bridge Number	BRIDGE INFORMATION *		REQUISITION INFORMATION										INSTALLATION INFORMATION						
	Route Number or Name and Feature Under Bridge	Truck Weight Limits (Tons) Code 4 Code 5 Code 9	Material Quantity**										Requisition Date (M/D/Y)	ARDOT Issuer (Initials)	Received by Agency (Initials)	Install Date (M/D/Y)	Installation Certified By Agency Official (Signature)		
			0's	1's	2's	3's	4's	5's	6's	7's	8's	9's						Bl.	
12249	CR 6 / DITCH NO. 8	3 3 3	1	3										3/10/19	JOE	RED	10/19/19	Ed King	
12256	CR 10 / DITCH	4 5 7						2	2								10/19/19	Sf King	
20235	CR 13 / 15 mi. CAK	7 9 12	2	2				2	2								10/20/19	Ed King	
21925	CR 356 / DITCH #2	12 12 17	1	3				1									10/21/19	Ed King	
			Totals:	5	12	3	2	2	5	2									

Instructions For Completing Form:

- Complete columns of the "Bridge Information" area, using information from ARDOT notification.
- Determine material quantities needed and complete the columns of "Requisition Information" area as necessary for each bridge.
- Agency Administrator signs "Requesting Official's Signature" where materials are requisitioned using the columns of the "Requisition Information" area.
- Completed and signed form is delivered to ARDOT District Headquarters where materials are requisitioned using the columns of the "Requisition Information" area.
- The requesting agency retains the original and one copy of the form and the District Headquarters forwards a single copy to Bridge Division.
- Upon properly installing requisitioned materials, the requesting agency completes "Installation Information" and sends original to District Headquarters.
- After review of completed form, District Headquarters sends original to Bridge Division for processing and billing.

FOR ARDOT USE ONLY
Date Forwarded to Bridge Division:

- * Use the most current information from:
- The latest "Bridge Inventory / Posting Certification" report, OR
 - Other ARDOT notification (Maintenance Needs report, letter, etc.)
- ** A maximum of 12 single digit decals, 2 posts, and/or 2 signs per bridge may be requisitioned at one time.

- Notes:
- By completing columns in the "Installation Information" area, the agency which requisitioned the signing material certifies that these materials were properly installed at the designated bridge site on the date shown.
 - The agency requisitioning the material will be billed for only 20% of the material cost if "Installation Information" is completed and the original form is returned to the District Headquarters within one month of the date shown in the "Requisition Information" area. If completed and returned beyond the one month limit, the agency will be billed for 100% of the material cost.

Requesting Official's Signature _____ County Judge
County of SONOMA
Requesting Official's Signature _____ Mayor / City Administrator
City of _____

EXAMPLE (AFTER MATERIAL INSTALLATION)

BRIDGE INVENTORY / POSTING CERTIFICATION
 COUNTY OWNED STRUCTURES
 DISTRICT - 11; COUNTY - GILA

ARDOT ROUTE NO.	OTHER ROUTE DESIGNATION	MAP ZONE	FEATURE UNDER BRIDGE	LOCATION	MAIN SPAN MATERIAL & TYPE	BRIDGE DIMENSIONS		REQ'D POSTING (TONS)				BRIDGE NUMBER	REMARKS
						LENGTH	WIDTH	Code 4	Code 9	Code 5	Code 5		
1	Lodge Corner Rd	G	Slough	2.3 Mi W US 165-LodgeCmr	Steel Ortho	51	20.3	8	8	9	21890	POSTED ✓	
1	Hagler Rd	G	Kaney Bayou	0.51 Mi W SH 276-Hagler	Timber Girder	28	22.6	9	11	16	10000	SIGN DOWN ✓	
1	Lodge Corner Rd	G	Creek	0.6 Mi W US 165-LodgeCmr	Steel Ortho	89	18.0	Legal	Legal	28	21891	TRAILER DECKS ✓	
2	Woodson Rd	H	Hurricane Bayou	3.2 Mi E SH 11-LodgeCmr	Timber Girder	69	16.7	13	15	23	21320	✓	
2	Woodson Rd	G	Creek	0.6 Mi W SH 276-Hagler	Timber Girder	27	20.0	18	23	32	10005	✓	
4	W.10th St Exten	F	King Bayou Ditch	0.8 Mi E US 79-Stuttgart	Steel Ortho	51	19.4	7	8	9	21800	✓	
6	Alcorn Gill Rd	G	Creek	2.8 Mi S US 79-Humphrey	Steel Ortho	88	18.0	18	19	20	21505	✓	
6	Alcorn Gill Rd	G	Hurricane Slough	4.63 Mi S US 79-Humphrey	Steel Ortho	85	19.6	Legal	Legal	Legal	20771	✓	
9	Luther D Rd	G	King Bayou Ditch	1.1 Mi E SH 343-Stuttgart	Steel Ortho	51	19.4	3	4	4	20505	Lowered DECKS ✓	
10	Roth Prairie Rd	G	King Bayou Ditch (East)	0.3 Mi W US 165-Stuttgart	Timber Girder	30	24.0	17	20	29	21504	✓	
12	Tarkington Rd	E	Little LaGrue Bayou	1.32 Mi W SH 153-Lagru	Steel Ortho	89	19.7	19	20	21	22163	✓	
12	Gregory Fischer	G	King Bayou Ditch	0.4 Mi E SH 343-Stuttgart	Steel Culvert	35.6	0.0	Legal	Legal	Legal	23866	✓	
14	Tindall Drier Rd	I	Elm Prong Mill Bayou	1.96 Mi E US165-Stuttgart	Steel Ortho	66.8	20.2	4	5	5	23760	✓	
17	Old Almyra Hwy	E	Cherry Creek	0.67 Mi S Almyra Limits	Conc Chan Beam	58	24.0	Legal	Legal	Legal	03375	✓	
19	Lone Tree Rd	F	King Bayou Ditch	0.5 Mi S US 79-Stuttgart	Steel Ortho	40	19.4	Legal	Legal	36	22426	✓	
21	Old England Rd	F	Clearpoint Creek (Canal)	1.2 Mi W US 165-Stuttgart	Steel Ortho	50	19.7	Legal	23	27	21949	✓	
33	South Yoder Rd	I	Creek	5.4 Mi N US165-LodgeCmr	Steel Ortho	51	19.7	6	7	9	21799	CLOSED 10/21 ✓	
33	North Yoder Rd	E	Little LaGrue Bayou	0.31 Mi S SH146-Stuttgart	Conc T-Beam	118	27.6	Legal	Legal	Legal	04765	✓	
33	South Yoder Rd	I	Elm Prong Mill Bayou	5.0 Mi S SH 130-Yoder	Conc Culv	37	0.0	Legal	Legal	Legal	X0491	✓	
35	North Tower Rd	E	Little Lagru Bayou	1.7 Mi N SH 130-Stuttgart	Steel Ortho	89	18.0	Legal	Legal	Legal	21751	✓	
41	Vice Rd	E	Wildcat Ditch	0.75 Mi E SH 130-Almyra	Steel Ortho	45	19.0	Legal	Legal	Legal	21346	✓	
47	KY Club Ln	G	Kings Bayou	0.5 Mi W US165-LodgeCmr	Timber Girder	29	22.6	14	18	25	21531	✓	
56	Phelps Rd	I	Creek	0.25 Mi E US165-LodgeCmr	Steel Girder	30	16.4	9	12	13	10045	NEW SIGNS ✓	
59	Doughboy Rd	B	Point Deluce Bayou	0.5 Mi E US 165-LaFargue	Steel Ortho	89.3	20.0	Legal	22	23	23865	✓	
60	Gander Street Rd	H	Flag Lake Sough	0.3 Mi N Main St-Gillett	Conc Slab	75	27.9	Legal	Legal	Legal	04335	✓	
61	Old Post Rd	A	Little Post Bayou	2.1 Mi E US 165-Gillett	Conc Culv	21	0.0	Legal	Legal	Legal	10048	✓	
62	Shirley Owen Rd	A	Little Post Bayou	2.11 Mi E US 165-Gillett	Steel Culvert	31.8	0.0	Legal	Legal	Legal	23286	✓	
67	Woodrow T Rd	A	Caney Bayou	2.95 Mi S SH 44-Tschnor	Steel Ortho	89	18.7	Legal	26	28	21011	✓	
70	Fletcher Levee Rd	B	Williams Branch	200' W SH 17-Deluce	Timber Girder	24	16.3	12	17	22	10053	Lowered DECKS ✓	
71	Hesser Levee Rd	B	Point Deluce Bayou	1.3 Mi N SH 44-LaFargue	Steel Ortho	88.0	18.4	14	15	16	22025	✓	

This page reviewed EJK
 Initials

EXAMPLE (First or intermediate page)

BRIDGE INVENTORY / POSTING CERTIFICATION
 COUNTY OWNED STRUCTURES
 DISTRICT - 11; COUNTY - GILA

ARDOT ROUTE NO.	OTHER ROUTE DESIGNATION	MAP ZONE	FEATURE UNDER BRIDGE	LOCATION	MAIN SPAN MATERIAL & TYPE	BRIDGE DIMENSIONS		REQ'D POSTING (TONS)					BRIDGE NUMBER	REMARKS
						LENGTH	WIDTH	Code 4	Code 9	Code 5	Code 8	Code 3		
287	Wilson Lane	F	Crooked Creek	0.30 Mi W SH 13-Humphrey	Steel Ortho	90	17.4	7	8	8	8	21502	✓	
301	Lumsden Rd	G	Creek	1.91 Mi S US165-LodgeCorn	Timber Girder	24	16.1	17	24	30	30	10108	✓	
312	Cox Cypress Rd	G	Bayou Meto	4.0 Mi W US 165-LodgeCorn	Steel Girder	200	27.9	Legal	Legal	Legal	Legal	04484	✓	
375	Whitmore Rd	C	Tarleton Creek	0.57 Mi E CR 77-StCharles	Steel Ortho	89	19.0	18	19	20	20	22925	✓	
381	Old Highway 17	C	Bass Creek	0.7 Mi S SH 1-St. Charles	Timber Girder	30	15.4	8	9	14	14	10114	ADD SIGN	
385	Padgett Levee Rd	C	Tarleton Creek	0.4 Mi W SH 17	Steel Ortho	54	18.7	Legal	Legal	Legal	Legal	21048		
408	Frownfelter Rd	I	Hurricane Bayou (Canal)	3.0 Mi N US165-LodgeCorn	Steel Ortho	45	18.7	Legal	Legal	Legal	Legal	21194		
411	Wolf Rd	I	Hurricane Bayou	0.9 Mi W CR408-LodgeCorn	Steel Ortho	30.8	22.6	19	23	31	31	23998	✓	
415	Long Bell Rd	G	Cross Bayou	0.16 Mi E JeffCo-BayouMet	Timber Girder	31	16.1	11	15	19	19	10110	✓	
416	Secmore Rd	I	Elm Prong Mill Bayou	0.01 Mi E CR 33(Yoder Rd)	Timber Girder	30	17.4	13	15	22	22	22711	✓	
431	Sebre Rd	I	Fish Lake Ditch	1.0 Mi W SH 130-Almyra	Steel Girder	25	18.0	5	5	7	7	21501	Closed 10-10-19	

This signature certifies that:

- * I have reviewed my agency's Bridge Inventory / Posting Certification report for bridge posting or closure requirements and initialed each page to indicate this review.
- * I have taken any necessary action to properly post or close the bridges according to the values indicated on the report or as notified otherwise.
- * I understand that the Federal Highway Administration (FHWA) may make random on-site inspections to verify proper load posting or bridge closure.
- * I agree that failure to sign, date and return this report to the ARDOT District Headquarters by December 31st will result in withholding of Federal Highway funds for future construction projects in my agency's jurisdiction, until the time that the report is submitted as required.

E. K...
 County Judge

11-2-2019
 Date Signed

EXAMPLE (Last page)

This page reviewed *EJK*
 initials